

Specific Plan documents for

LINCOLN TECH

NASHVILLE, TENNESSEE

SUBMISSION DATE: 28 SEPTEMBER 2022
REV1: 18 OCTOBER 2022
REV2: 07 DECEMBER 2022
REV3: 01 JANUARY 2023

AMENDMENT #1: 26 NOVEMBER 2025
AMENDMENT #1 REV1: 30 JANUARY 2026
AMENDMENT #1 REV2: 17 MARCH 2026
AMENDMENT #1 REV3: 26 MARCH 2026

CASE No. 2022SP-075-002

CONTACT INFORMATION:

OWNER: East Nashville Owner, LLC
3990 Hillsboro Pike, Suite 400
Nashville, TN 37215
615.778.2884
Contact: Michael McNally
michael.mcnally@southernland.com

DESIGNER: Ben Crenshaw
Landscape Architect/ Planner
Southern Land Company
3990 Hillsboro Pike, Suite 400
Nashville, TN 37215
615.778.2884
ben.crenshaw@southernland.com

CIVIL ENGINEER: John Gore
Barge Civil Associates
Civil Engineer
6606 Charlotte Pike, Suite 210
Nashville, TN 37209
615.356.9911
jgore@bcacivil.com

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COMPANY**

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PURPOSE NOTE

The purpose of this SP is to rezone approximately (16.35) 19.09 acres to provide for a mixed-use development with a maximum (300,000sf) 350,000sf commercial, 800 multi-family residential units, 150 single-family or attached multi-family (townhomes) units, and green space. Uses are as defined within this document. Additionally, this SP previously abandoned the existing Institutional Overlay as established by the Nashville Auto-Diesel College Master Development Plan established on September 21, 2005.

VISION

To Create an Artful Heart of East Nashville

The overarching goal of the Lincoln Tech Specific Plan (SP) is to create a vibrant, mixed-use community that will artfully serve the residents of East Nashville, thoughtfully crafting a place to live, work, gather, shop, and dine in a highly connected, pedestrian friendly urban district.

East Nashville has long developed around various commercial nodes at the intersections of key corridors. Five Points, Walden, Riverside Village have evolved to concentrated points of commercial activity, but most do not include housing and high quality open space in their immediate area.

The Lincoln Tech SP started with the idea of creating a central public space for both the future residents of the development, as well as the surrounding neighborhood and patrons of the retail and restaurant establishments. This space, activated by adjacent retail and regularly programmed events, fills a need in an area that has limited access to high quality open space. In addition to the central public space, pedestrian connectivity has been elevated through the incorporation of sidewalks, mews spaces and enhanced streetscape improvements in an area severely lacking in pedestrian infrastructure.

Multi-modal improvements will help to encourage alternate modes of transportation to and from the site, such as bike sharing and the proximity to the Gallatin Pike BRT transit line immediately adjacent to the site.

Buildings have been thoughtfully configured to transition the scale of the proposed development to the adjacent single family neighborhood. Townhomes buffer the larger buildings and provides a unique single-family oriented product at a scale not provided in the surrounding neighborhoods.



SPECIFIC PLAN ZONING OVERVIEW



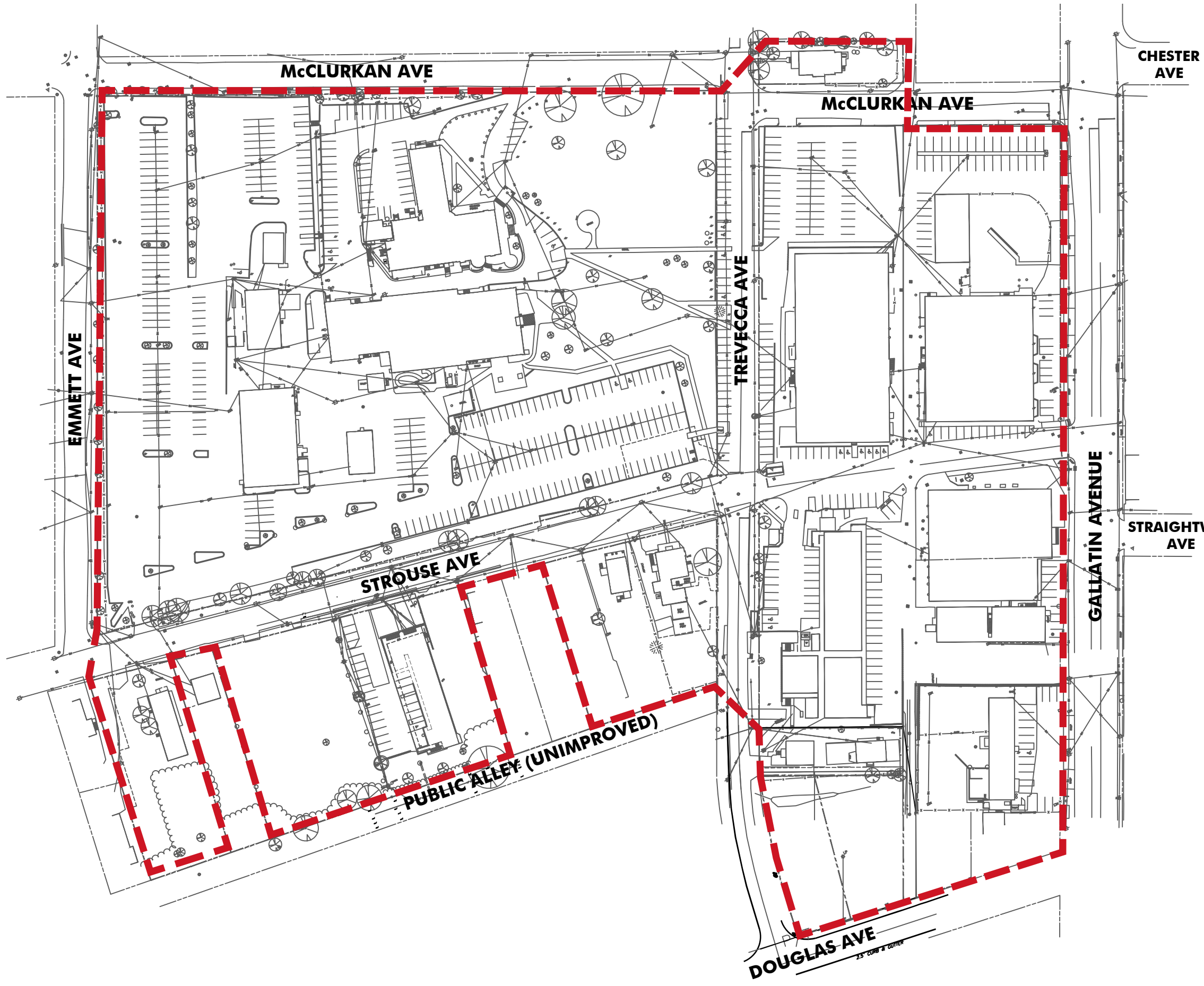
Located near the intersection of Gallatin Avenue and Douglas Avenue, the project site is located in the Renraw neighborhood named after the former home of Percy Warner which exists on site and has served administration functions for the college. The Lincoln Tech Mixed-use Project will become an active commercial node in the overall East Nashville community, complementing Five Points to the south, Walden to the East, and Riverside Village to the northeast, as well as support the growth of the Gallatin Avenue multi-modal corridor through increasing adjacent density.

The ±(16.35) 19.09 acre site is currently zoned a mix of ORI, OR20, CS, RM20, and RS5 base zoning over multiple parcels. The standards and regulations of MUG-A and R6-A, including specific modifications, implements the vision developed by NashvilleNext and the Community Character Manual.

In addition to the base zoning, the area is governed through an Institutional Overlay guided by the Nashville Auto-Diesel College Master Development Plan. As a condition of this submittal, the Institutional Overlay would be withdrawn and parcels outside of the SP Request would continue to be governed by base zoning. Per the Overlay language, the Institutional Overlay would only come into effect as parcels were acquired by the Nashville Auto Diesel College/ Lincoln Tech.

The proposed project will consist of ~~(300,000)~~ 350,000sf Commercial, 800 Multi-family Residential units, and 150 Single-family or attached Multi-family (townhomes) Residential units. Retail is envisioned to activate the streetscape frontages and central open space at Strouse and Trevecca Avenues, serving as a neighborhood amenity both for the proposed development and larger East Nashville community and adjacent neighborhoods.

The project is currently planned to be built in phases.



EXISTING CONDITIONS

COUNCIL DISTRICT

05 SEAN PARKER

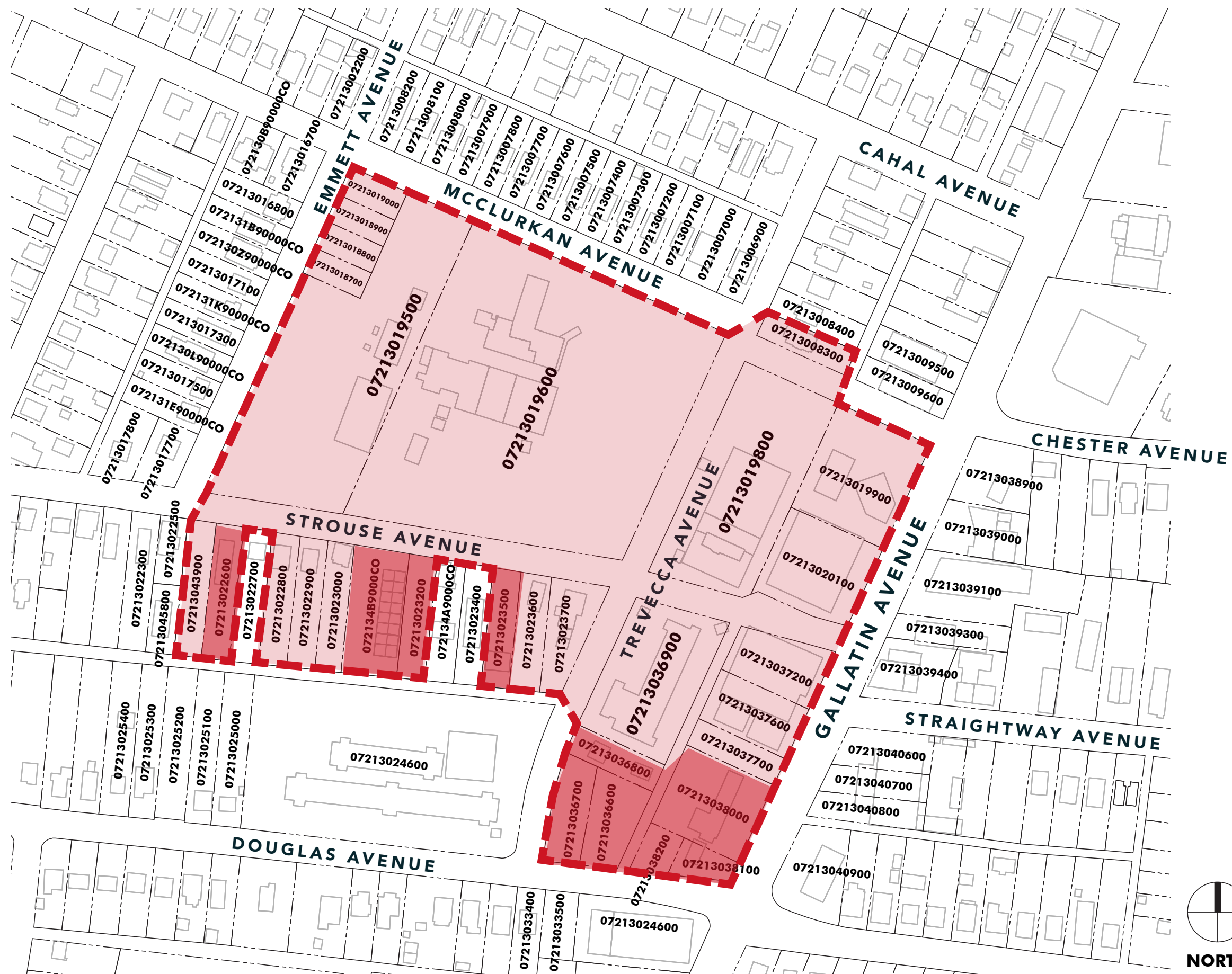
SITE PARCELS/ ACREAGE

| | |
|--------------------------|----------------------------|
| 1302 GALLATIN AVE | 0.59ac (25,700sf) |
| 1308 GALLATIN AVE | 0.19ac (8,276sf) |
| 1310 GALLATIN AVE | 0.37ac (16,117sf) |
| 1324 GALLATIN AVE | 0.38ac (16,553sf) |
| 1524 GALLATIN AVE | 0.69ac (30,056sf) |
| 1528 GALLATIN AVE | 0.84ac (36,590sf) |
| 2308 TREVECCA AVE | 0.21ac (9,147sf) |
| 2310 TREVECCA AVE | 1.06ac (46,174sf) |
| 2410 TREVECCA AVE | 1.66ac (72,310sf) |
| 1000 MCCLURKAN AVE | 0.14ac (6,098sf) |
| 1008 MCCLURKAN AVE | 2.70ac (117,612sf) |
| 1016 MCCLURKAN AVE | 5.84ac (254,390sf) |
| 2416 EMMETT AVE | 0.12ac (5,227sf) |
| 2418 EMMETT AVE | 0.12ac (5,227sf) |
| 2420 EMMETT AVE | 0.12ac (5,227sf) |
| 2500 TREVECCA AVE | 0.18ac (7,841sf) |
| 954 STROUSE AVE | 0.28ac (12,197sf) |
| 956 STROUSE AVE | 0.28ac (12,197sf) |
| 960 STROUSE AVE | 0.27ac (11,761sf) |
| 962 STROUSE AVE | 0.28ac (12,197sf) |
| 964 STROUSE AVE | 0.27ac (11,761sf) |
| 968 STROUSE AVE | 0.50ac (21,780sf) |
| 970 STROUSE AVE | 0.23ac (10,019sf) |
| 976 STROUSE AVE | 0.23ac (10,019sf) |
| 978 STROUSE AVE | 0.22ac (9,583sf) |
| 980 STROUSE AVE | 0.39ac (16,988sf) |
| 1019 DOUGLAS AVE | 0.30ac (13,068sf) |
| 1021 DOUGLAS AVE | 0.34ac (14,810sf) |
| 1023 DOUGLAS AVE | 0.13ac (5,663sf) |
| 1300 GALLATIN AVE | 0.16ac (6,970sf) |
| | 19.09ac (831,560sf) |

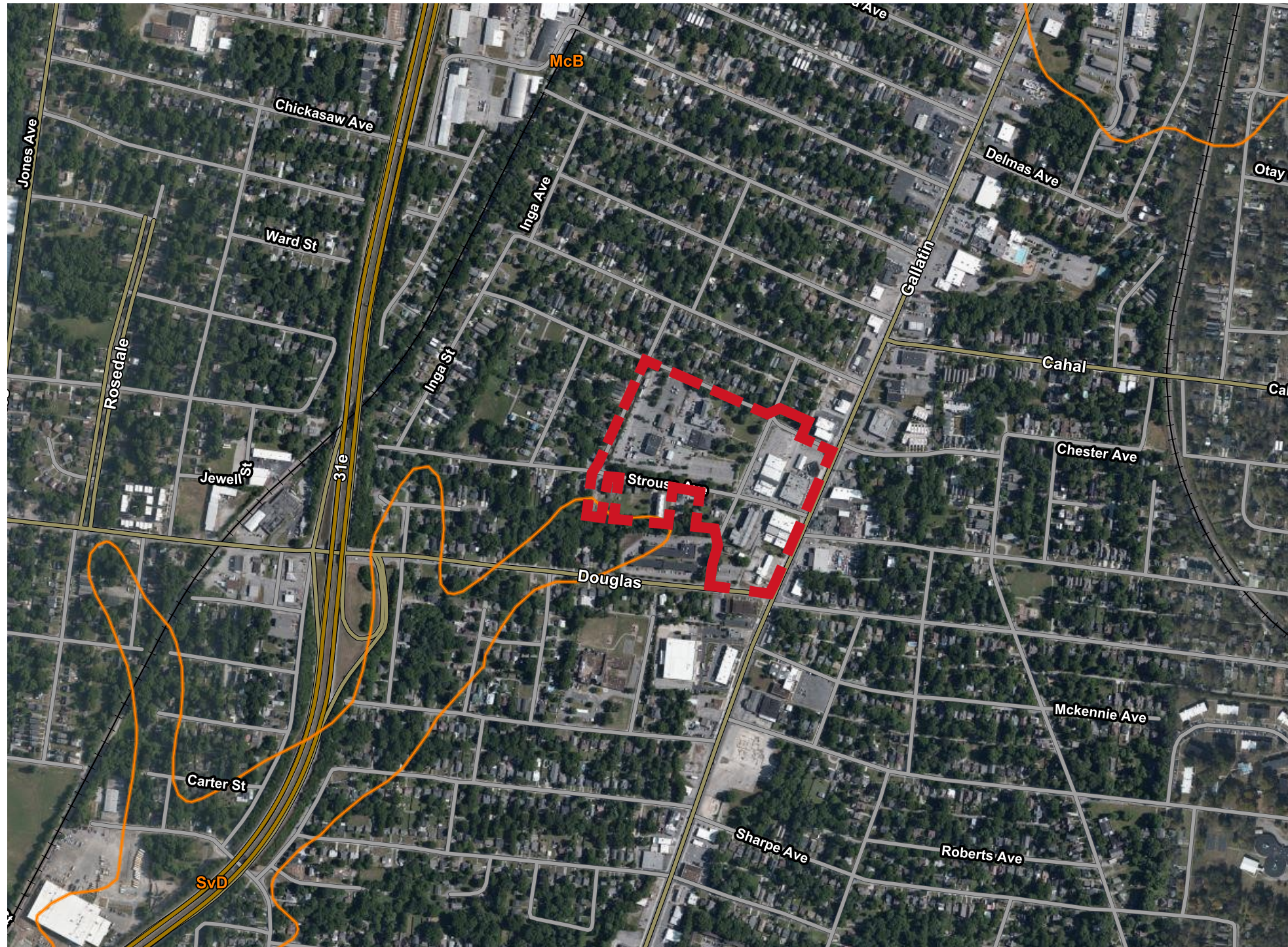
NOTE: PARCELS IN BOLD ARE ADDITIONS TO THE PREVIOUSLY APPROVED PRELIM. SP.

HORIZONTAL BEARING REFERENCE
TN STATE PLANE NAD83

PARCEL INFORMATION



SOILS



Soils within the confines of the Lincoln Tech Project are primarily classified as McB - Maury-Urban Land Complex (2-7% Slopes).

There are generally no naturally occurring slopes in excess of 15% on the site.



EXISTING ZONING

Existing zoning for the Lincoln Tech Campus is a mix of SP, OR20, CS, and RS5 base zoning over multiple parcels.

The Institutional Overlay was canceled per the previous Preliminary SP and is shown for reference only:

In addition to the base zoning, the area is governed by an Institutional Overlay guided by the Nashville Auto-Diesel College Master Development Plan.

As a condition of this submittal, the Institutional Overlay would be withdrawn and parcels outside of the SP Request would continue to be governed by base zoning. Per the Overlay language, the Institutional Overlay would only come into effect as parcels were acquired by the Nashville Auto Diesel College/ Lincoln Tech.





SPECIFIC PLAN

- LIVE-WORK UNITS
attached multi-family)
- RELOCATED RENRAW
HOUSE
- BLDG 1 (MIXED USE)
- TOWNHOMES/ SINGLE
FAMILY (attached multi-
family)
- BLDG 2 (MIXED USE)

- BLDG 3A (MIXED USE)
- BLDG 3B (MIXED USE)

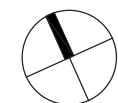
- BLDG 3D (MIXED USE)
- BLDG 4 (MIXED USE)

- BLDG 3C (MIXED USE)
- BLDG 3E (MIXED USE)

- TOWNHOMES/ SINGLE
FAMILY (attached multi-
family)



SCALE: 1" = 120' - 0"



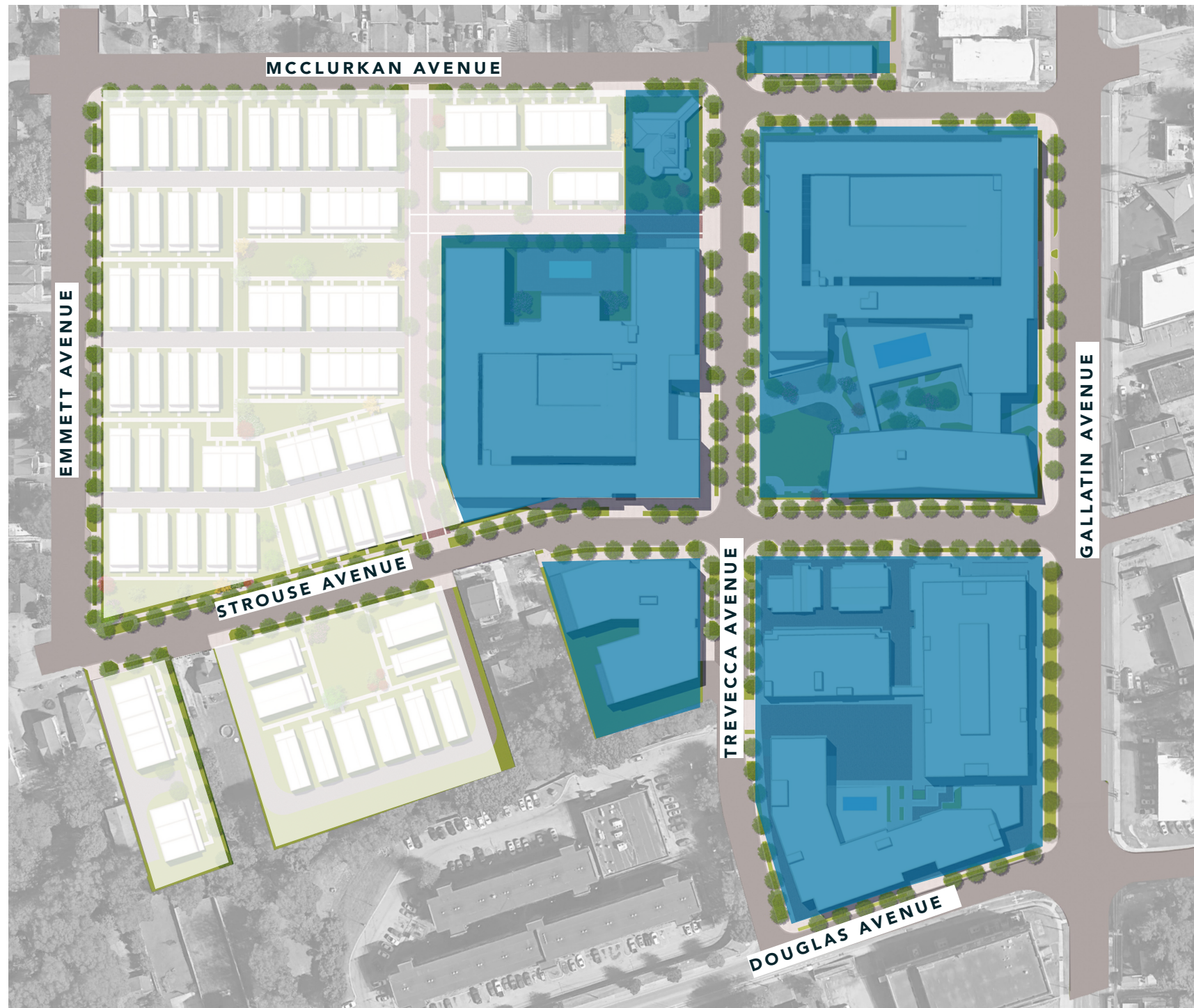
NORTH

OVERALL SITE DATA

| | |
|----------------------------------|---|
| ACRES | ±(16.35) 19.09ac |
| COMMERCIAL | (300,000sf) 350,000sf Retail/ Restaurant/ Office/ All other allowed non-Residential Uses |
| NO. RESIDENTIAL UNITS | 950 |
| OVERALL DENSITY | (58.10) 49.76 du/ ac |
| TOTAL OPEN SPACE | (3.09 (18.9%)) 3.48ac (18.2%) |
| PUBLIC | (2.22) 2.56ac |
| PRIVATE | (0.87) 0.92ac |

The preceding totals for Commercial area and Residential units are maximums allowed within the proposed SP.

ZONING ANALYSIS - MIXED USE DISTRICT



PROPOSED ZONING USES SP TO PERMIT A MIXED-USE DEVELOPMENT WITH A MAXIMUM OF 800 RESIDENTIAL UNITS AND (300,000sf) 350,000sf COMMERCIAL (including 99 rm max HOTEL). USES AS PERMITTED BY THE MUG-A ZONING DISTRICT. SHORT TERM OWNER AND NOT-OWNER OCCUPIED RENTAL SHALL BE PROHIBITED.

MAX FAR 3.0 AS PER MUG-A (blended total including Residential District)

THE FLOOR AREA USED FOR THE PROVISION OF OFF-STREET PARKING SPACES AND LOADING BERTHS (AND ASSOCIATED DRIVEWAYS AND AISLES) SHALL NOT BE COUNTED AS FLOOR AREA FOR THE PURPOSE OF CALCULATING FLOOR AREA RATIO.

MAX ISR 1.0

NO. RESIDENTIAL UNITS 800

BUILD-TO ZONE 0-15ft (except at Public Park/ Outdoor Dining Locations)

MAX OVERALL HEIGHT SEE BUILDING HEIGHT/ MASSING DIAGRAM
Mezzanine and basement levels shall not count towards overall building height.

STEP-BACK See Architectural Standards/ Note 2 - Facade Articulation, Page 28

MIN REAR SETBACK NONE REQUIRED
20ft @ Live-Work Units north of McClurkan Avenue

MIN SIDE SETBACK NONE REQUIRED
5ft @ Live-Work Units north of McClurkan Avenue

PARKING PER UZO ZONING REQUIREMENTS or APPROVED SHARED PARKING AGREEMENT.

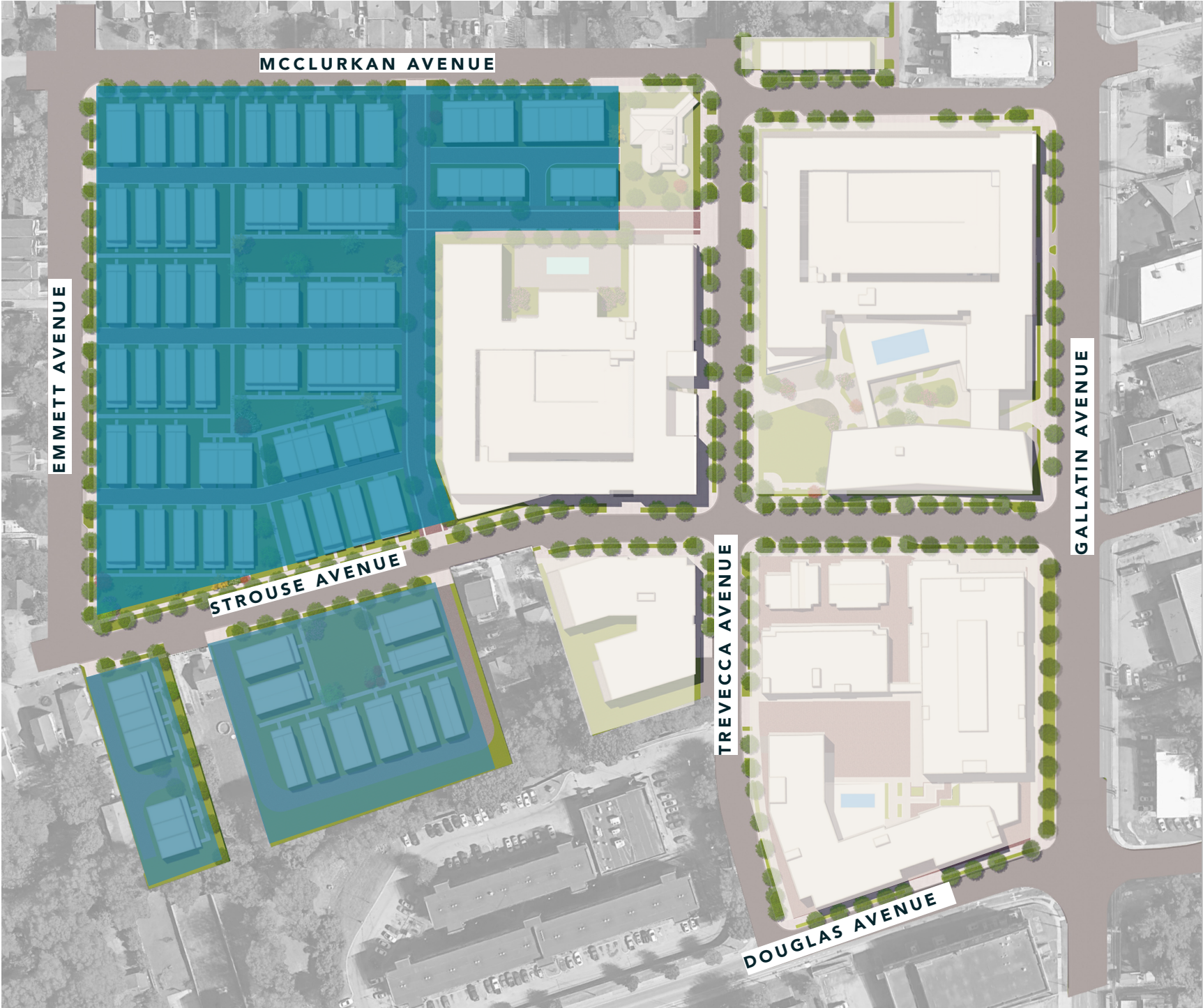
ACTIVE GROUND FLOOR 50% ACTIVE GROUND FLOOR USES ON A DISTRICT-WIDE BASIS
(Excludes pedestrian and vehicular entrances)

ALLOWED USES AS PER MUG-A with the following exceptions:

- | | |
|---------------------------------------|-----------------------------|
| Permitted: | |
| • Historic Bed and Breakfast Homestay | • Animal Boarding |
| • Community Gardening (noncommercial) | • Mobile Vendor |
| • Artisan Distillery | • Microbrewery |
| | • Small Outdoor Music Event |

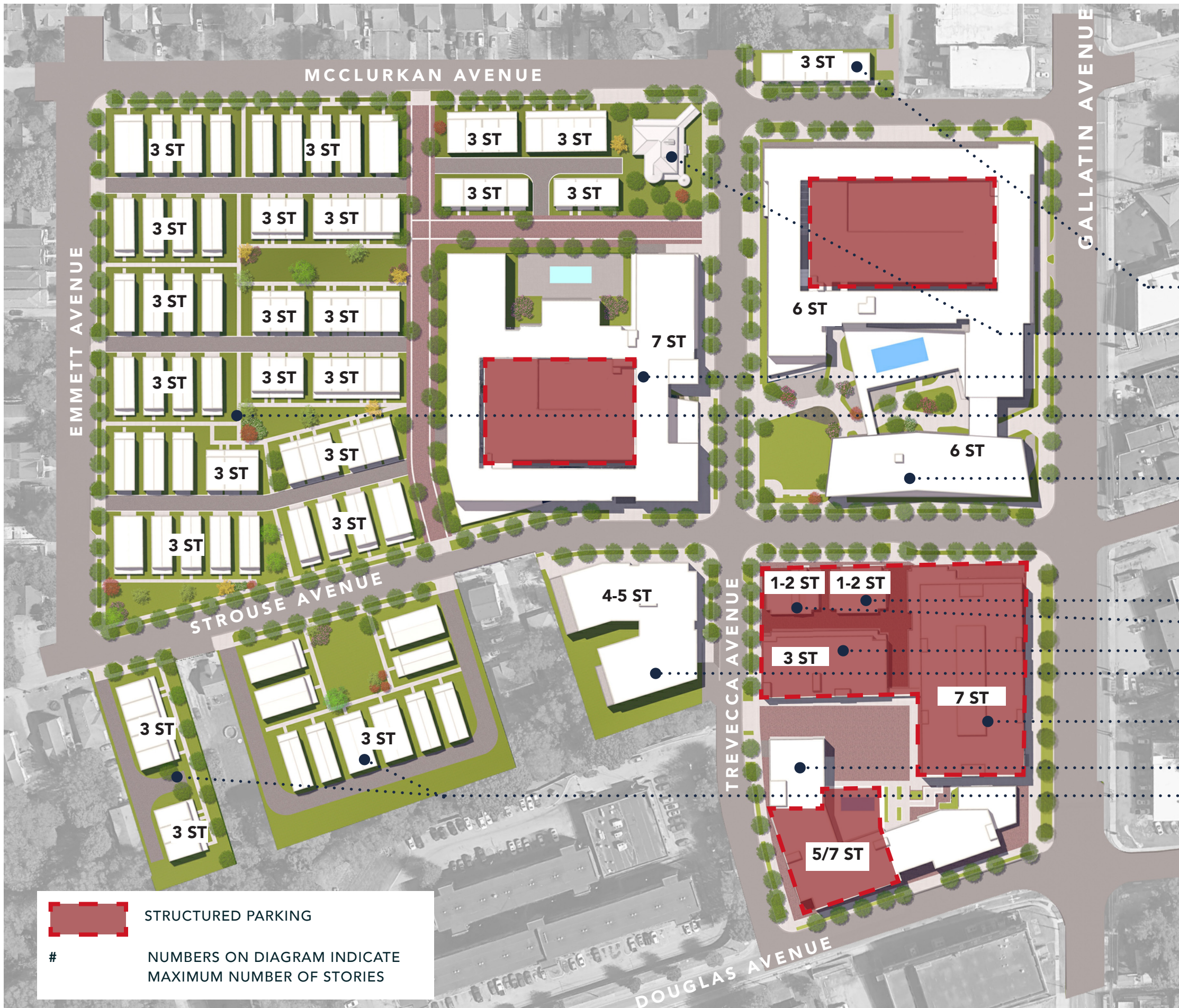


ZONING ANALYSIS - RESIDENTIAL DISTRICT




| | |
|------------------------------|---|
| PROPOSED ZONING USES | AS PART OF THE SP TO PERMIT A MIXED-USE DEVELOPMENT, THE PLAN INCLUDES A MAXIMUM OF 150 SINGLE-FAMILY (DETACHED AND ATTACHED) OR MULTI-FAMILY (TOWNHOMES) RESIDENTIAL UNITS. USES AS PERMITTED BY THE R6-A ZONING DISTRICT PLUS MULTI-FAMILY (TOWNHOMES). SHORT TERM NOT-OWNER OCCUPIED RENTAL SHALL BE PROHIBITED. |
| MAX FAR | 3.0 AS PER MUG-A (blended total including Mixed Use District) |
| MAX ISR | 0.90 (district total) |
| NO. RESIDENTIAL UNITS | 150 |
| BUILD-TO ZONE | 5-15ft (Single family/ Multi-family Townhomes) |
| MAX OVERALL HEIGHT | 3 Stories in 45ft. See Building Height Massing Diagram <i>Mezzanine and basement levels shall not count towards overall building height.</i> |
| MIN LOT SIZE | 1,000sf |
| MIN REAR SETBACK | 5ft |
| MIN SIDE SETBACK | 0/5ft (6ft min between buildings) |
| PARKING | 1.5 parking spaces/ unit. Parking will be provided through a combination of private garages, surface parking behind units, and on-street parking. |
| ALLOWED USES | MULTI-FAMILY (TOWNHOMES) AND USES AS PER R6-A |





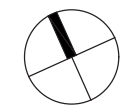
BLDG HEIGHT/ MASSING

- LIVE-WORK UNITS
- RELOCATED HOUSE
- BLDG 1 (MIXED USE)
- SINGLE FAMILY/
TOWNHOMES/ LIVE-WORK
(attached multi-family)
- BLDG 2 (MIXED USE)
- BLDG 3A (MIXED USE)
- BLDG 3B (MIXED USE)
- BLDG 3D (MIXED USE)
- BLDG 4 (MIXED USE)
- BLDG 3C (MIXED USE)
- BLDG 3E (MIXED USE)
- TOWNHOMES/ SINGLE
FAMILY (attached multi-
family)

 STRUCTURED PARKING

NUMBERS ON DIAGRAM INDICATE
MAXIMUM NUMBER OF STORIES

NOTE:
- SINGLE FAMILY/TOWNHOMES/ LIVE-WORK TO BE 3
STORIES
- BUILDING HEIGHT SHALL BE MEASURED PER METRO
CODE



SCALE: 1" = 120' - 0"

NORTH

BUILDING MASSING



- **BLDG 3E/** 5/7 STORIES
- **BLDG 3C/** 7 STORIES
- **BLDG 3D/** 3 STORIES
- **BLDG 3A-B/** 1-2 STORIES
- **BLDG 4/** 4-5 STORIES

- **BLDG 1/** 7 STORIES
- **BLDG 2/** 6 STORIES

- **SINGLE FAMILY/**
TOWNHOMES/ LIVE-WORK
(attached multi-family)/
3 STORIES

- **RELOCATED RENRAW**
HOUSE

- **LIVE-WORK/** 3 STORIES

RENDERING TO ILLUSTRATE MASSING.
OVERALL HEIGHT TO BE GOVERNED BY
BUILDING HEIGHT/ MASSING DIAGRAM.

BUILDING MASSING



..... **BLDG 1/ 7 STORIES**

..... **SINGLE FAMILY/
TOWNHOMES/ LIVE-WORK
(attached multi-family)/
3 STORIES**

..... **BLDG 2/ 6 STORIES**

..... **RELOCATED RENRAW
HOUSE**

RENDERING TO ILLUSTRATE MASSING.
OVERALL HEIGHT TO BE GOVERNED BY
BUILDING HEIGHT/ MASSING DIAGRAM.

BUILDING MASSING



..... **BLDG 2/** 6 STORIES

..... **BLDG 1/** 7 STORIES

..... **BLDG 3C/** 7 STORIES (max)

..... **BLDG 3B/** 1-2 STORIES

..... **BLDG 3A/** 1-2 STORIES

..... **BLDG 4/** 4-5 STORIES

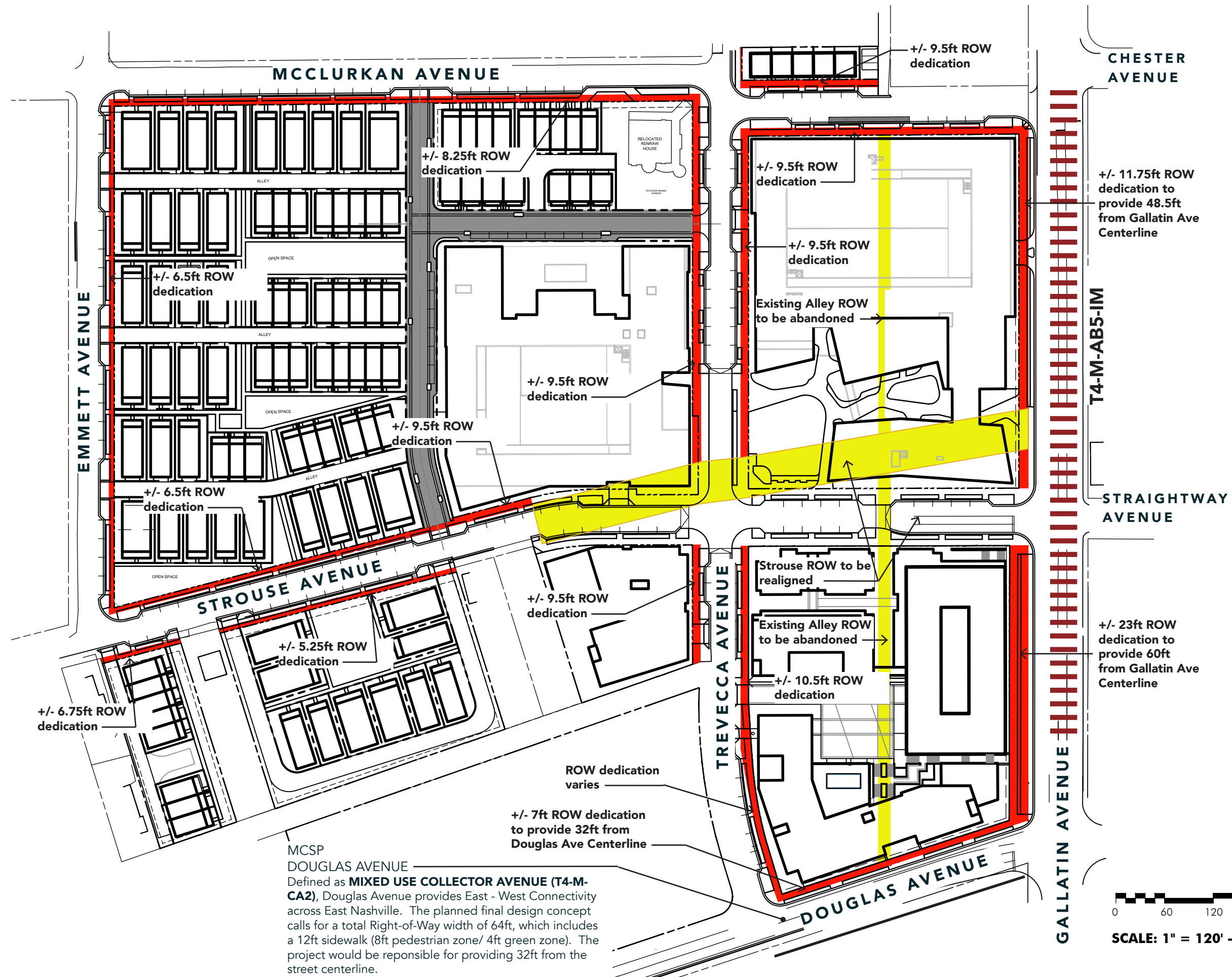
..... **BLDG 3D/** 1-2 STORIES

RENDERING TO ILLUSTRATE MASSING.
OVERALL HEIGHT TO BE GOVERNED BY
BUILDING HEIGHT/ MASSING DIAGRAM.

PARKING GARAGE ENHANCEMENTS



FACADES FOR PARKING GARAGES WITH DIRECT FRONTAGE ALONG A PUBLIC STREET WILL BE INTEGRATED INTO THE OVERALL ARCHITECTURAL DESIGN OF EACH ASSOCIATED BUILDING. FACADE TREATMENTS SHALL PROVIDE AN OPPORTUNITY FOR ARTISTIC EXPRESSION TO CELEBRATE THE SITE AND NEIGHBORHOOD HISTORY. OPENINGS FOR NATURAL VENTILATION ARE PERMISSIBLE WHEN INTEGRATED INTO THE OVERALL FACADE/ BUILDING DESIGN.



MCSP/ RIGHT-OF-WAY

GALLATIN AVENUE
Defined as an **Urban Mixed Use Arterial Boulevard (T4-M-AB5-IM)** and **Immediate Need Multi-modal Corridor**, Gallatin Avenue connects downtown Nashville with outlying suburbs/ satellite cities.

All other streets are considered Local Streets.

GALLATIN AVENUE - SEGMENT 13**
The planned final design concept calls for a total Right-of-Way width of 97ft, which includes a 14ft sidewalk (10ft pedestrian zone/ 4ft green zone). The project would be responsible for providing 48.5ft from the street centerline.

GALLATIN AVENUE - SEGMENT 12**
The planned final design concept calls for a total Right-of-Way width of 120ft, which includes a 14ft sidewalk (10ft pedestrian zone/ 4ft green zone). The project would be responsible for providing 60ft from the street centerline.

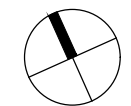
Immediate Need Multimodal Corridors are anticipated to provide more frequent transit. Currently, Gallatin Avenue has a BRT Lite/ Express route in operation (56).

**** NOTE:** Final cross section and alignment details are subject to change and will be determined with the final site plan review.

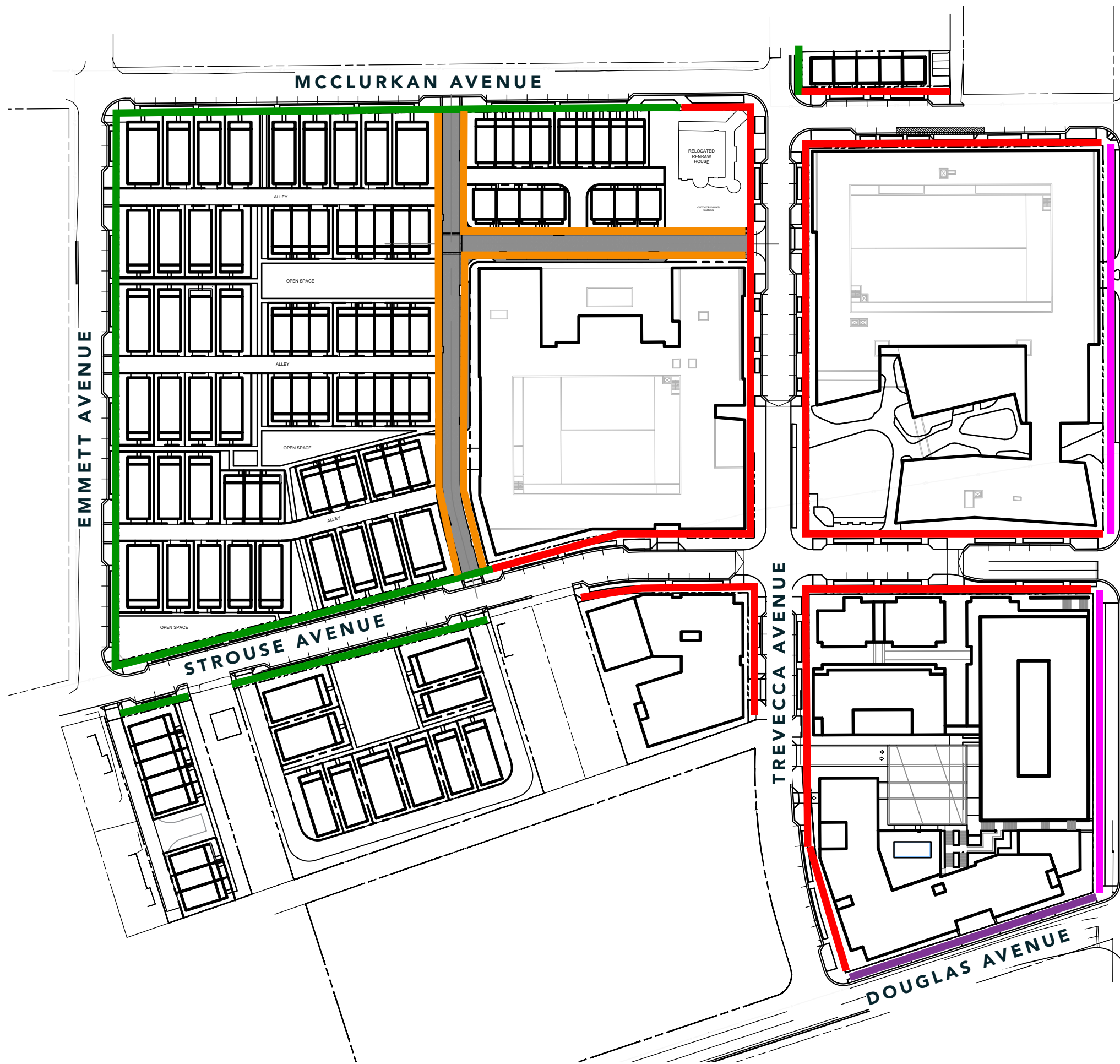
NOTE: DEVELOPMENT TO COORDINATE WITH WeGO TO REVIEW POTENTIAL BUS STOP IMPROVEMENTS/ UPGRADES ON GALLATIN AVENUE PRIOR TO FINAL SP.



SCALE: 1" = 120' - 0"



NORTH



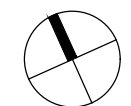
PEDESTRIAN INFRASTRUCTURE

NOTE:
 The final site plan/ building permit site plan shall depict the required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.

- GALLATIN AVENUE/ MCSP
- DOUGLAS AVENUE/ MCSP
- ACTIVE STREET
- NEIGHBORHOOD STREET
- SHARED STREET (PRIVATE)



SCALE: 1" = 120' - 0"

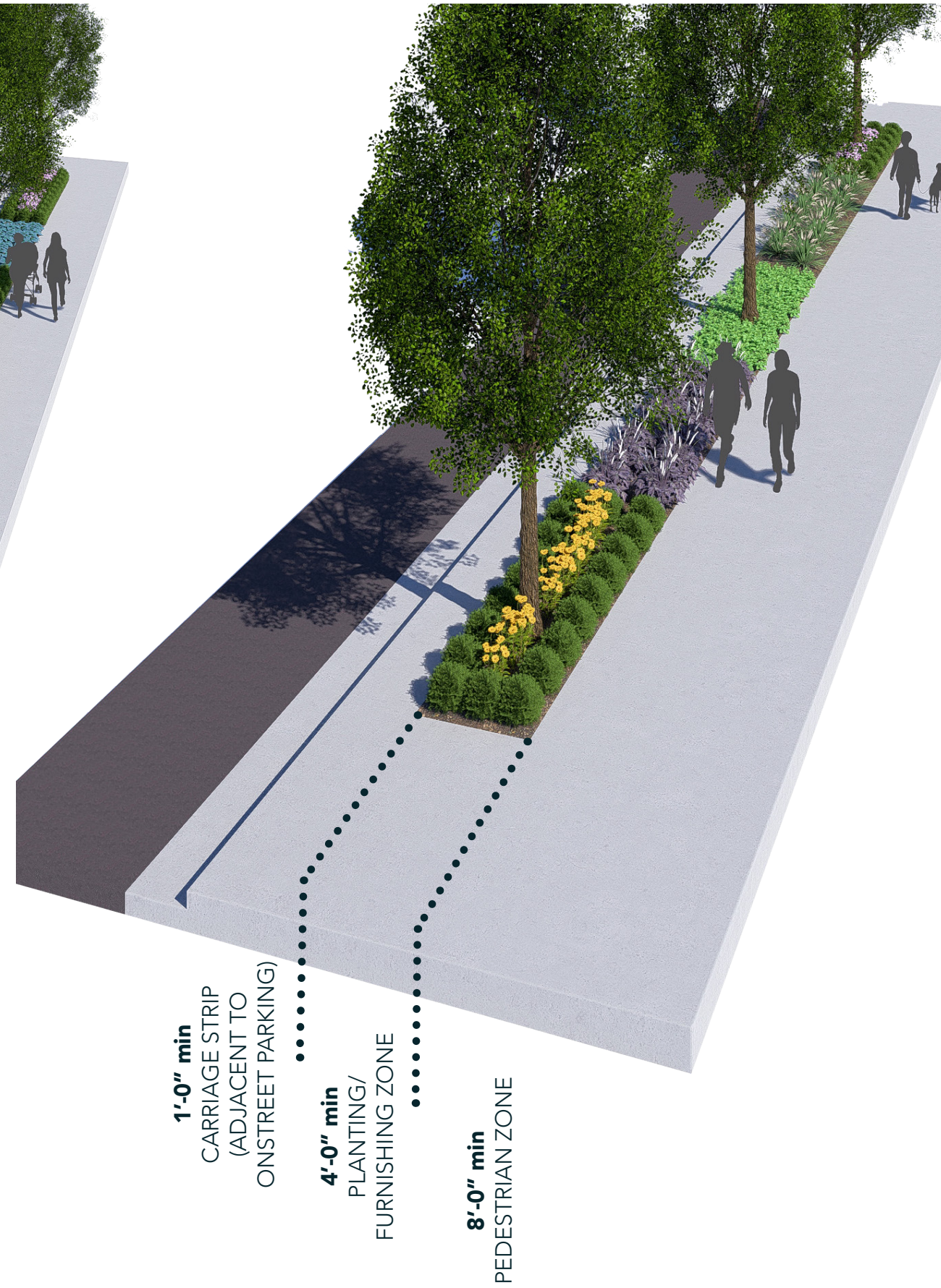
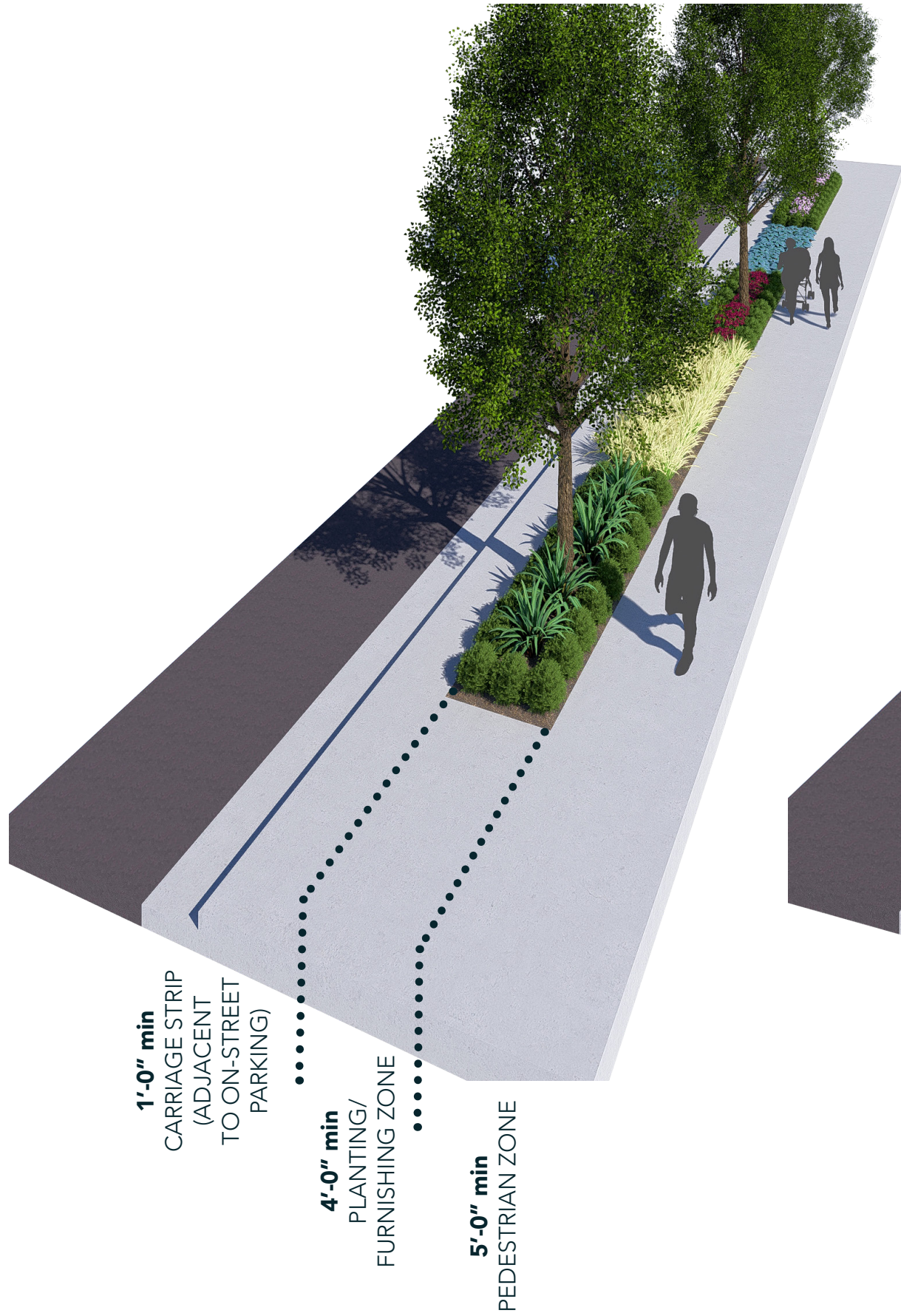


NORTH

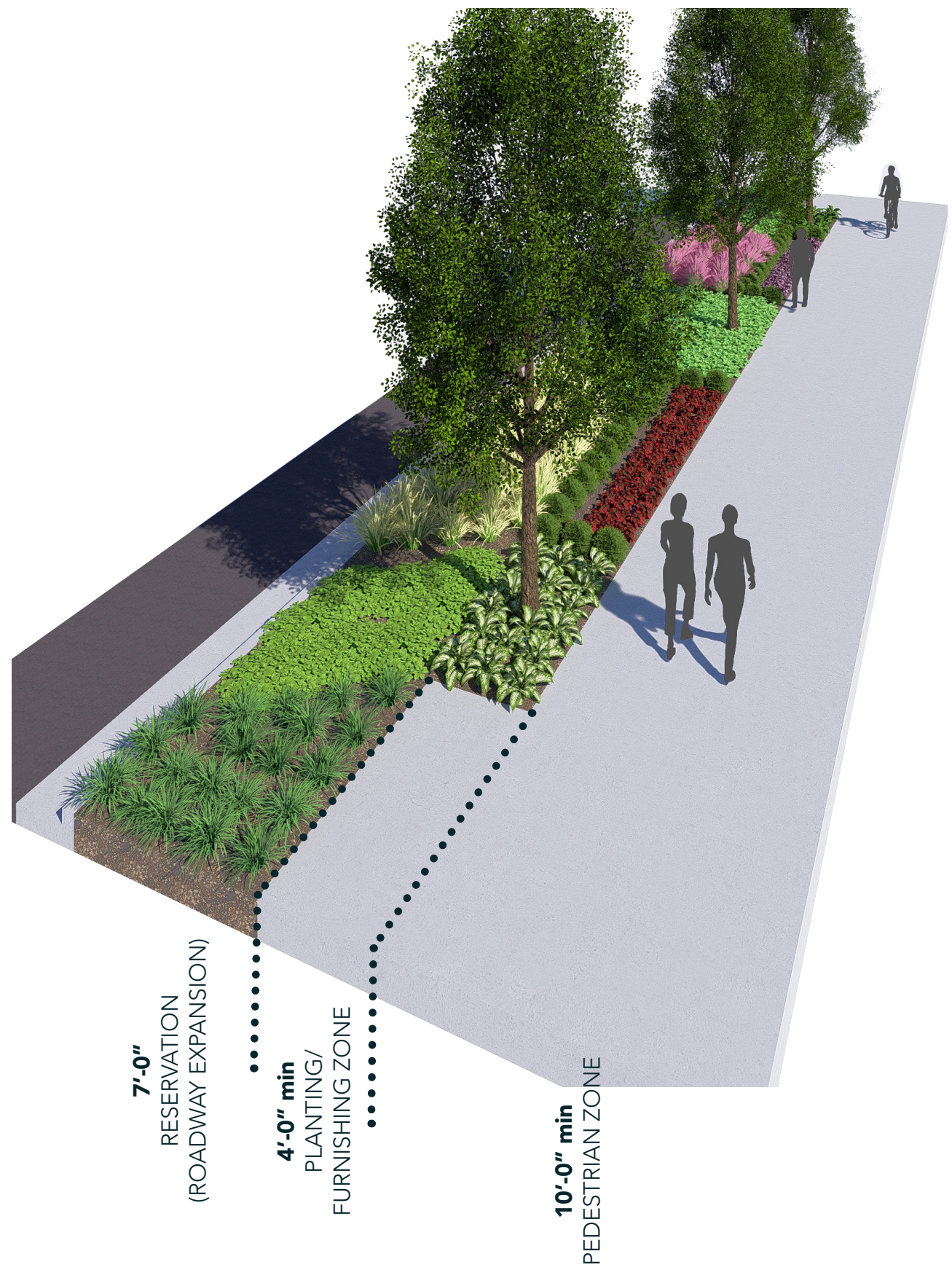
NEIGHBORHOOD STREET

ACTIVE STREET

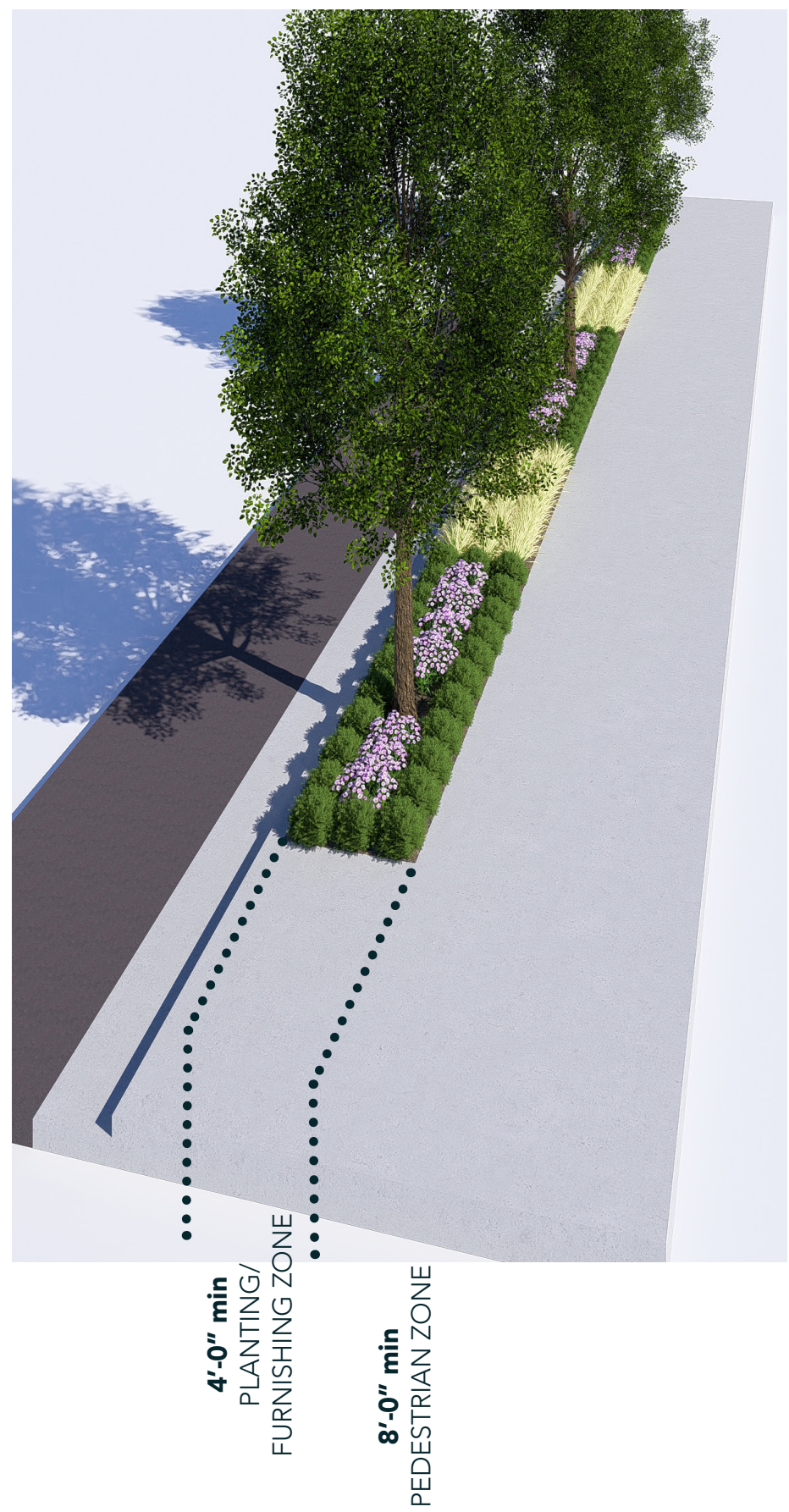
**PEDESTRIAN
INFRASTRUCTURE**



GALLATIN AVENUE/ MCSP

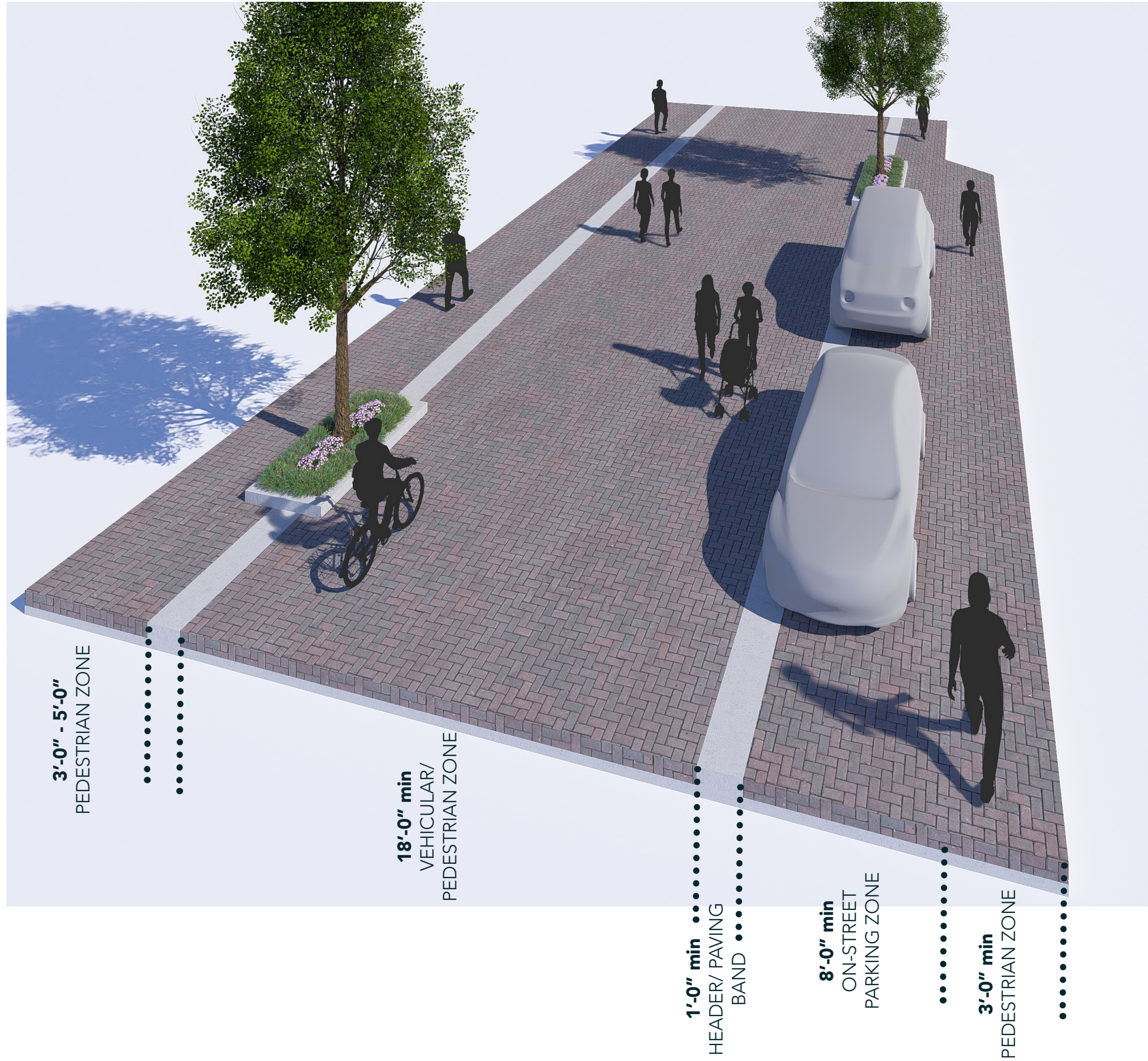


DOUGLAS AVENUE/ MCSP



**PEDESTRIAN
INFRASTRUCTURE**

SHARED STREET



PEDESTRIAN INFRASTRUCTURE

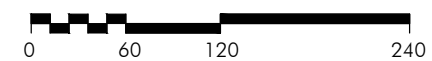


OPEN SPACE PLANS

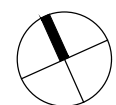
The layout and connectivity of the Open Space is a central theme in the overall development concept. A large, active central park/ plaza will provide a much needed open space for the greater neighborhood, while smaller, more passive open space options are provided as nodes within the lower density portions of the development, connected via the shared street and mews spaces.

Final layout and areas subject to change.

- PUBLIC OPEN SPACE
- PRIVATE OPEN SPACE

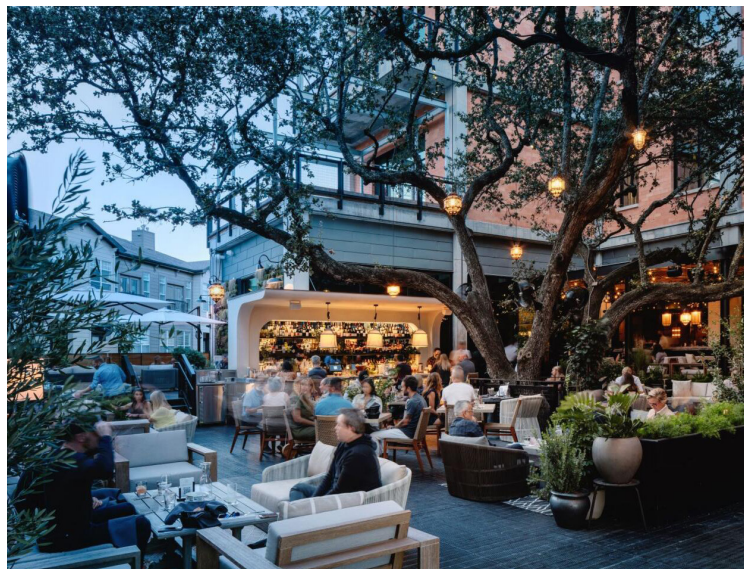


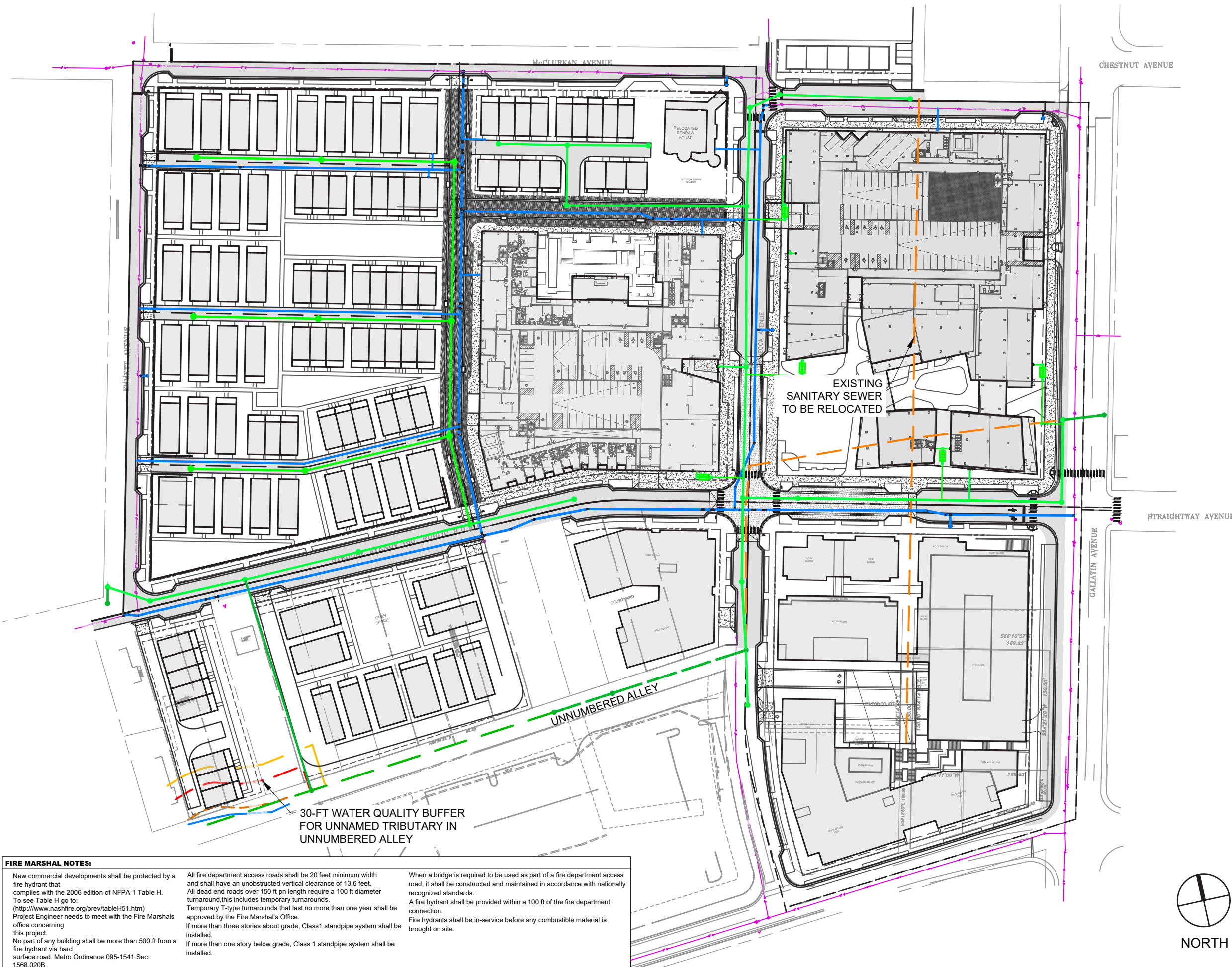
SCALE: 1" = 120' - 0"



NORTH

OPEN SPACE CONCEPT IMAGERY





| LEGEND | |
|---|--|
| EXISTING WATER TO REMAIN | |
| EXISTING SANITARY SEWER TO REMAIN | |
| EXISTING SANITARY SEWER TO BE RELOCATED | |
| PROPOSED SANITARY SEWER | |
| PROPOSED WATER | |

- PUBLIC WORKS NOTE:**
- THE FINAL SITE PLAN / BUILDING PERMIT SITE PLAN SHALL DEPICT THE REQUIRED PUBLIC SIDEWALKS, ANY REQUIRED GRASS STRIP FRONTAGE OR FRONTAGE ZONE, AND THE LOCATION OF ALL EXISTING AND PROPOSED VERTICAL OBSTRUCTIONS WITHIN THE REQUIRED SIDEWALK AND GRASS STRIP OR FRONTAGE ZONE. PRIOR TO THE ISSUANCE OF USE AND OCCUPANCY PERMITS, EXISTING VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUTSIDE OF THE REQUIRED SIDEWALK, WHERE FEASIBLE, VERTICAL OBSTRUCTIONS ARE ONLY PERMITTED WITHIN THE REQUIRED GRASS STRIP OR FRONTAGE ZONE.
 - PARKING RATIOS SHALL BE PROVIDED AT OR ABOVE THE METRO ZONING CODE UZO PARKING STANDARDS.
 - ANY REQUIRED RIGHT-OF-WAY WITHIN THE PROJECT SITE THAT IS IDENTIFIED AS NECESSARY TO MEET THE ADOPTED ROADWAY PLANS SHALL BE DEDICATED.
 - THE DEVELOPER'S FINAL CONSTRUCTION DRAWINGS SHALL COMPLY WITH THE DESIGN REGULATIONS ESTABLISHED BY THE DEPARTMENT OF PUBLIC WORKS, IN EFFECT AT THE TIME OF THE APPROVAL OF THE PRELIMINARY DEVELOPMENT PLAN, OR FINAL DEVELOPMENT PLAN OR BUILDING PERMIT, AS APPLICABLE.
 - THE DESIGN OF THE PUBLIC INFRASTRUCTURE IS TO BE COORDINATED WITH THE FINAL SP. THE ROADS, PEDESTRIAN INFRASTRUCTURE, BICYCLE ROUTES, ETC. ARE TO BE DESIGNED AND CONSTRUCTED PER MPW STANDARDS AND SPECIFICATIONS.
 - ALL CONSTRUCTION WITHIN THE RIGHT OF WAY SHALL COMPLY WITH ADA AND METRO PUBLIC WORKS STANDARDS AND SPECIFICATIONS. ALL STREETS AND ALLEYS TO BE PER METRO PUBLIC WORKS STANDARDS.
 - ALL SIDEWALKS ON THE PROPERTY FRONTAGE ARE TO BE ADA COMPLIANT PRIOR TO U/O PERMIT.

- METRO WATER SERVICES NOTE:**
- ANY EXCAVATION, FILL OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO.78-840, AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.
 - METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED INGRESS AND EGRESS AT ALL TIMES IN ORDER TO MAINTAIN, REPAIR, REPLACE, AND INSPECT ANY STORMWATER FACILITIES WITHIN THE PROPERTY.

30-FT WATER QUALITY BUFFER FOR UNNAMED TRIBUTARY IN UNNUMBERED ALLEY

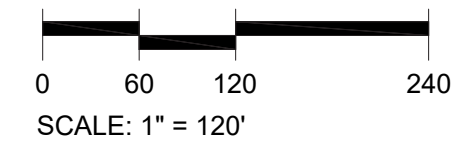
FIRE MARSHAL NOTES:

New commercial developments shall be protected by a fire hydrant that complies with the 2006 edition of NFPA 1 Table H. To see Table H go to: (<http://www.nashfire.org/prev/tableH51.htm>) Project Engineer needs to meet with the Fire Marshals office concerning this project. No part of any building shall be more than 500 ft from a fire hydrant via hard surface road. Metro Ordinance 095-1541 Sec: 1568.020B.

All fire department access roads shall be 20 feet minimum width and shall have an unobstructed vertical clearance of 13.6 feet. All dead end roads over 150 ft in length require a 100 ft diameter turnaround, this includes temporary turnarounds. Temporary T-type turnarounds that last no more than one year shall be approved by the Fire Marshal's Office. If more than three stories above grade, Class 1 standpipe system shall be installed. If more than one story below grade, Class 1 standpipe system shall be installed.

When a bridge is required to be used as part of a fire department access road, it shall be constructed and maintained in accordance with nationally recognized standards. A fire hydrant shall be provided within a 100 ft of the fire department connection. Fire hydrants shall be in-service before any combustible material is brought on site.

SITE UTILITY SHEET







PAVING

EXISTING STORM DRAINAGE TO BE RELOCATED

CHESTNUT AVENUE

LEGEND

| | |
|--|---|
| EXISTING STORM DRAINAGE TO REMAIN |  |
| EXISTING STORM DRAINAGE TO BE ABANDONED/REPLACED |  |
| PROPOSED STORM INFRASTRUCTURE |  |
| POTENTIAL WATER QUALITY TREATMENT STRUCTURE |  |

PERVIOUS PAVING
PROPERTY DOES NOT LIE WITHIN FLOOD HAZARD AREAS AND IS DETERMINED TO BE IN ZONE "X" AS PER FEDERAL EMERGENCY MANAGEMENT AGENCY FIRM PANEL MAP NUMBERS: 47037C0253H, DATED: APRIL 05, 2017

LANDSCAPE NOTE:
THIS DEVELOPMENT SHALL MEET THE REQUIREMENTS OF SECTION 17.24 OF THE METRO ZONING ORDINANCE, LANDSCAPE, BUFFERING AND TREE REPLACEMENT.

WATER QUALITY UNIT, TYP.

WATER QUALITY UNIT, TYP.

STORMWATER DETENTION VAULT

STRAIGHTWAY AVENUE

30-FT WATER QUALITY BUFFER FOR UNNAMED TRIBUTARY IN UNNUMBERED ALLEY

EXISTING DITCH

UNNUMBERED ALLEY

STORMWATER NOTES:

78-840 NOTE: ANY EXCAVATION, FILL, OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH THE STORM WATER MANAGEMENT ORDINANCE NO. 78-840 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.

PRELIMINARY PLAN NOTE: THIS DRAWING IS FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT. THE FINAL LOT COUNT AND DETAILS OF THE PLAN SHALL BE GOVERNED BY THE APPROPRIATE REGULATIONS AT THE TIME OF THE FINAL APPLICATION.

METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED ACCESS IN ORDER TO MAINTAIN AND REPAIR UTILITIES IN THE SITE.

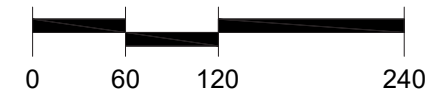
SIZE DRIVEWAY CULVERTS PER THE DESIGN CRITERIA SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL (MINIMUM DRIVEWAY CULVERT IN METRO R.O.W. IS 15" CMP.)

STORMWATER QUALITY CONCEPT WILL BE A COMBINATION OF LID (BIORETENTION & PAVERS) AND/OR WATER QUALITY VAULT IF APPROVED BY VARIANCE.

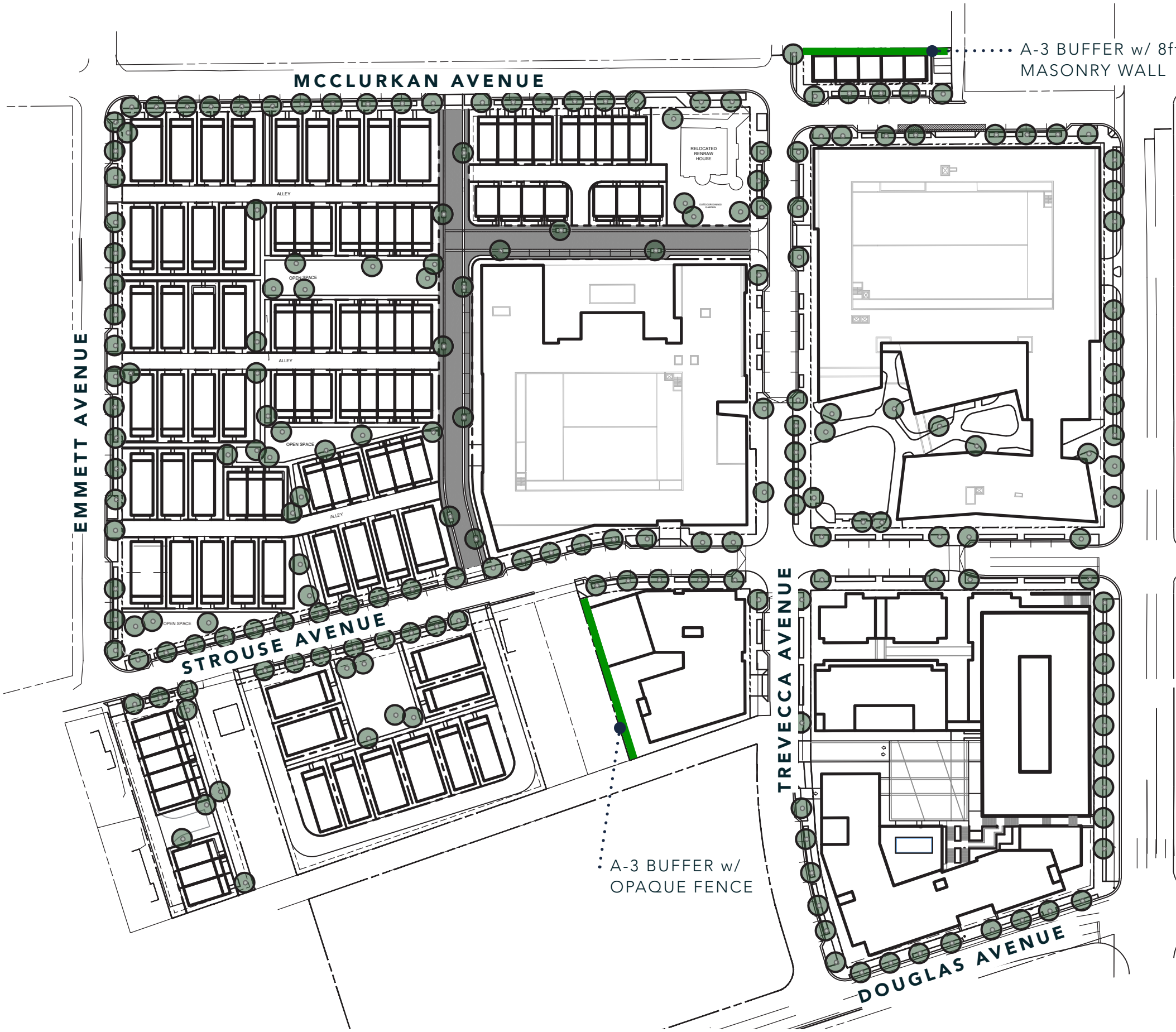
SITE DRAINAGE SHEET



NORTH



SCALE: 1" = 120'



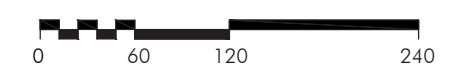
LANDSCAPE PLANS

LANDSCAPE
 The development of this project shall comply with the requirements for Metro Zoning Code chapter 17.24. A detailed landscape ordinance plan shall be submitted with the final sp submittal.

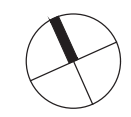
Street trees shall be provided, irrigated and maintained by Owner along all street frontages spaced at 30 - 40 feet on center to allow flexibility for utility accommodation and maximum mature canopy growth spread, with the exception of at retail frontages. All street trees placed within the Right-of-Way shall count toward tree unit density. Street trees shall be a minimum of 3" caliper of healthy stock trees from the Urban Forestry Recommended and Prohibited Tree and Shrub list.

LANDSCAPE BUFFERS
 No buffers are required, however an A-3 buffer w/ opaque fence shall be provided at the side property lines for all residential/ mixed use properties South of Strouse and West of Trevecca and an A-3 buffer w/ 8ft masonry wall shall be provided at the live/work bldg North of McClurkan.

STRAIGHTWAY AVENUE



SCALE: 1" = 120' - 0"



NORTH

REGULATIONS

STANDARD NOTES

1. The purpose of this SP is to receive preliminary approval to permit the development of a 46.35 19.09 acre mixed-used development as shown.
2. For any development standards, regulations and requirements not specifically shown on the SP plan and/or included as a condition of Council approval, the property shall be subject to the standards, regulations, and requirements of MUG-A for the Mixed Use District or R6-A base zoning for the Residential District as of the date of the application request or application. See Zoning Analysis.
3. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering, or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by the Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance.
4. The development is currently planned to be constructed in multiple phases and will begin the planning and design stages for Final SP after approval of the preliminary SP by Metro Planning Commission.
5. The final site plan/ building permit site plan shall depict any required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.
6. Any proposed Hotel use shall be limited to Block 3.

ARCHITECTURAL STANDARDS

1. New buildings shall avoid continuous uninterrupted blank facades. At a minimum, the facade plane shall be interrupted by one of the following for every thirty (30) linear feet of street frontage:
 - A change in building material or building openings.
 - A horizontal undulation in the building facade of two (2) feet or greater.
 - A porch, stoop, window or balcony; porches shall be a min. six (6) feet in depth and stoops shall be a min. four (4) feet in depth.
2. Façade articulation is required for building faces fronting a public street right of way for all buildings greater than 5 stories. The façade articulation requirement shall be satisfied by at least one of the options below:
 - A 5' stepback at any level above level one for a minimum of 75% of the length of the façade.
 - A prominent corner feature (including but not limited to change in façade material, increase in glazing, change in façade pattern) measuring no more than 20% of the length of the façade.
 - Building footprint is set back outside of the Build-To Zone (15ft min setback).
 - Building mass (at or above first level) recessed a minimum of 20 feet in depth for at least 25 linear feet along the frontage.
 - 75% Minimum Building Length as a Percentage of Frontage (Measured at Frontage Setback Line)
 - Building façade material or color change above level 5.
3. A primary entrance to the building shall be located along the building facade within the build-to zone along a public street or when fronting public open space.
4. A parcel located at the intersection of two public streets shall have a building occupy that corner unless it is occupied by a public open space.
5. For first floor residential uses, a minimum raised foundation of 18-36" is required with the exception of accessible units, visitable units, and topographically challenged units. Challenging site topography may result in raised/lowered foundations at strategic locations. Appropriate landscape screening is required when raised foundations exceed 36" along public streets, easements and open spaces.
6. Minimum glazing requirements shall be required on building facades facing public streets or public open space. Minimum glazing requirements for the non-residential

- uses in the Mixed-Use District shall be 50%. Minimum glazing requirements for all residential uses in both the Mixed-Use and Residential Districts shall be 20%. The first floor transparent glazing area calculation shall be measured from finished grade at the setback to finished floor elevation of the second floor. Glazing on upper floors facing a public street or public open space shall be a minimum of 15%.
7. Windows shall be vertically oriented at a ratio of 1.5:1 or greater; planning staff may allow modifications to this standard for dormers, decorative windows, clerestory windows, egress windows, storefront windows, curtain walls and other special conditions.
 8. Refuse collection, recycling and mechanical equipment shall be fully screened from public view by the combination of fences, walls or landscaping.
 9. Building facades shall be constructed of brick, brick veneer, stone, cast stone, cementitious siding, glass, metal panel, stucco, or materials substantially similar in form or function, unless otherwise approved on detailed building elevations included with the Preliminary SP.
 - Stucco shall be prohibited at the first floor level of any mixed use building. Stucco shall be limited to 20% maximum of the exterior facades above the first level visible from the public right-of-way, excluding balconies, interior courtyards, and single family residential units.
 10. HVAC units shall be located at the rear half of the side of unit, behind the unit, or on the roof of each building.
 11. Passages: Landscaped passages are encouraged between buildings/ townhome groupings to provide access from rear parking areas to the building fronts.
 12. Basements and Mezzanine levels shall not be calculated towards allowed building stories. Building Height shall be measured per Code, with exceptions granted as stipulated in Section 17.12.060. See Building Height/ Massing Diagram for reference.
 13. In the Residential District parking, driveways and all other impervious surfaces in the required setback shall not exceed twelve feet in width except where fire emergency access is required.
 14. Parking decks located at street level shall have no less than seventy-five percent of the lineal street frontage devoted to non-residential or residential uses at a minimum depth of twenty feet or shall complement the architectural character of the primary structure. Any portion of parking garage facade above street level that is visible and facing a public street shall also complement the architectural character of the primary structure. This may be accomplished through one or a combination of the following design strategies: vertical and/or horizontal articulation, opening design and rhythm, architectural cladding, screening, materiality, color or art/mural.

NDOT NOTES

1. The final site plan/building permit shall depict the required public sidewalks, any required grass strip or frontage zone, and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of Use and Occupancy Permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Where feasible, vertical obstructions are only permitted within the required grass strip or frontage zone.
2. The development totals represented in the Traffic Impact Study represent the current target development scenarios for maximum residential units and non-residential SF. The final allocation, density and intensity of the proposed uses may adjust based on market conditions prior to FINAL SP application. If the development program allocation changes, a revised traffic study may be required for Metro review and approval prior to the filing of the FINAL SP application. All Metro recommended improvements must be provided within the FINAL SP application per any revised and approved traffic study.
3. All parking regulations to meet UZO parking requirements and standards. A shared parking plan may be approved by the metropolitan traffic engineer based upon a parking and/or loading study that satisfactorily demonstrates that there is a reduction in the number of trips based on the determinations of the approved Transportation Demand Management Plan (TDM).
4. Roadway Improvements that are a direct result of this specific project as determined by the approved Traffic Impact Study and the Department of Public Works shall be constructed.
5. Any new improvements within existing public right-of-way within the project site that is identified as necessary to meet the adopted roadway plans shall be dedicated.
6. Developer will ensure bike lanes are continuous through intersections.
7. The developer's final construction drawings shall comply with the design regulations established by the Nashville Department of Transportation, in effect at the time of the approval of the preliminary development plan or final development plan or building permit, as applicable. Final design may vary based on field conditions.

8. The design of the public infrastructure is to be coordinated with the Final SP. The roads, pedestrian infrastructure, bicycle routes, etc. are to be designed and constructed per NDOT standards and specifications.
9. With the Final SP, indicate the location of the back of house for each structure, loading zones, vehicular and pedestrian access points, code required bicycle parking, etc.
10. All construction within the right of way shall comply with ADA and NDOT Standards and Specifications.
11. There shall be no vertical obstructions (signs, power poles, fire hydrants, etc.) within the proposed sidewalks. Where feasible, vertical obstructions shall be relocated out of the proposed sidewalks, where applicable.

FIRE MARSHAL NOTES

1. New commercial developments shall be protected by a fire hydrant that complies with the 2006 edition of NFPA 1 Table H. To see Table H go to: (<http://www.nashfire.org/prev/tableH51.htm>)
2. No part of any building shall be more than 500 ft. from a fire hydrant via a hard surface road. Metro Ordinance 095-1541 Sec. 1568.020 B
3. All fire department access roads shall be 20 feet minimum width and shall have an unobstructed vertical clearance of 13.5 feet.
4. All dead-end roads over 150 ft. in length require a 100-ft. diameter turnaround, this includes temporary turnarounds.
5. Temporary T-type turnarounds that last no more than one year shall be approved by the Fire Marshal's Office.
6. If more than three stories above grade, Class I standpipe system shall be installed.
7. If more than one story below grade, Class I standpipe system shall be installed.
8. When a bridge is required to be used as part of a fire department access road, it shall be constructed and maintained in accordance with nationally recognized standards.
9. A fire hydrant shall be provided within 100 ft. of the fire department connection.
10. Fire hydrants shall be in-service before any combustible material is brought on site.

NES NOTES

1. Where feasible, this development will be served with underground power and pad-mounted transformers.
2. New facilities will not be allowed to sit in or to pass through retention areas, including rain gardens, bioretention areas, bioswales, and the like. This includes primary duct between pad-mounted transformers equipment, as well as service duct to a meter.

STORMWATER NOTES

1. Any excavation, fill, or disturbance of the existing ground elevation must be done in accordance with Stormwater Management Ordinance No. 78840 and approved by the Metropolitan Department of Water Services.
2. Metro Water Services shall be provided sufficient and unencumbered ingress and egress at all times in order to maintain, repair, replace, and inspect any stormwater facilities within the property.
3. Size driveway culverts per the design criteria set forth by the Metro Stormwater Management Manual. (Minimum driveway culvert in Metro ROW is 15" CMP.)
4. Project intent is to be redeveloped per the requirements of the current Stormwater Management Manual. Detention will be provided or post developed runoff will be less than predeveloped runoff due to LID implementation.

BICYCLE PARKING

1. The development will include bike parking located within 50ft of the primary building entrance(s). The development will also include covered bicycle storage and maintenance room(s) for residential portions of the development. Bicycle parking shall be provided per the Metro Zoning Ordinance.

REGULATIONS

RESIDENTIAL ARCHITECTURAL STANDARDS

1. For side elevations facing a Street Right-of-Way and those that terminate a vista, it is important that the elevation be as thoroughly thought out as the front elevation. The architecture, landscape, and hardscape shall respond to the side street or vista and be articulated in such a way that creates just as interesting a streetscape as that of the front of the house.



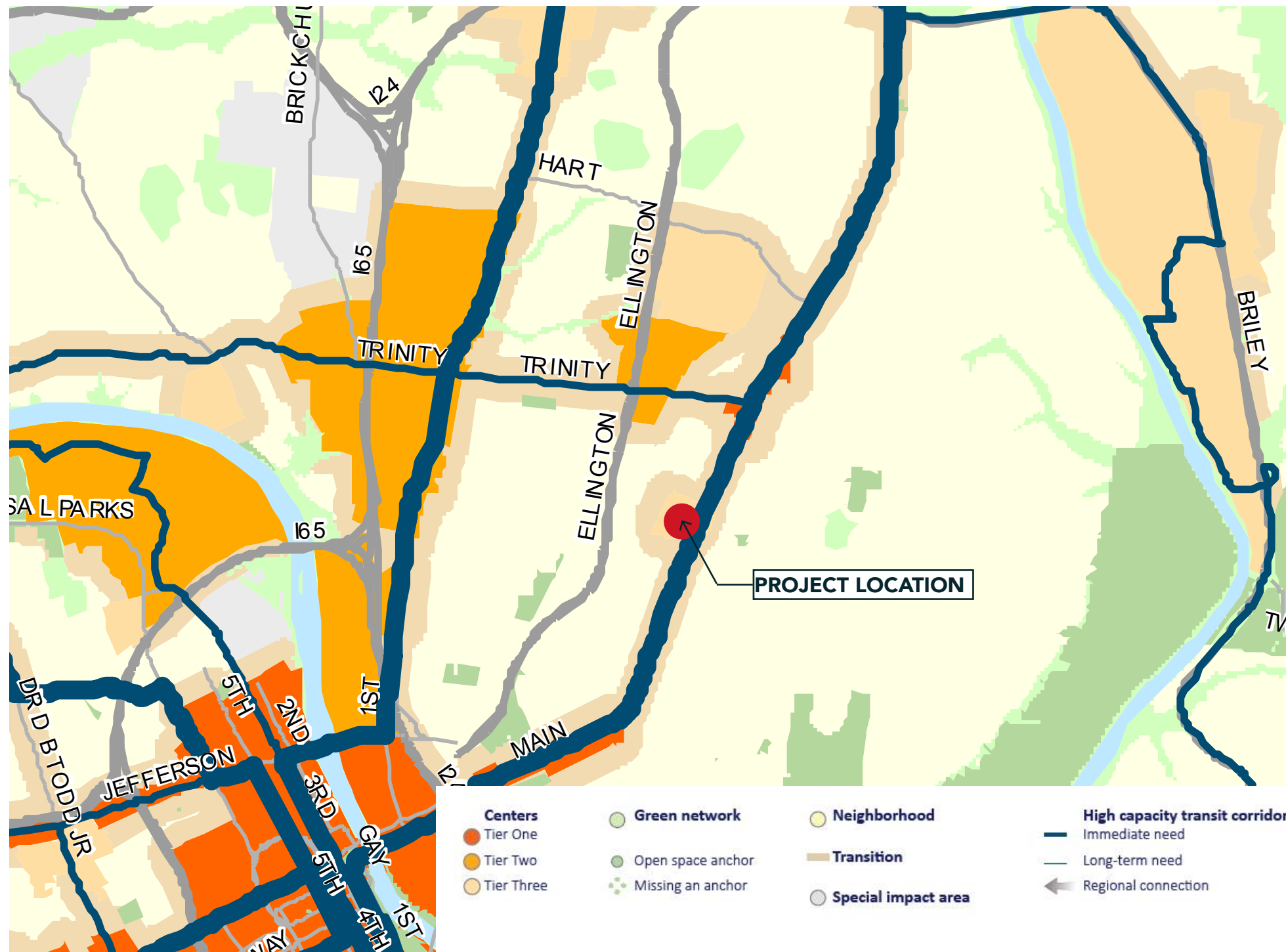
Representative images provided for illustrative purposes and not intended to convey architectural style or materiality.

NASHVILLE NEXT

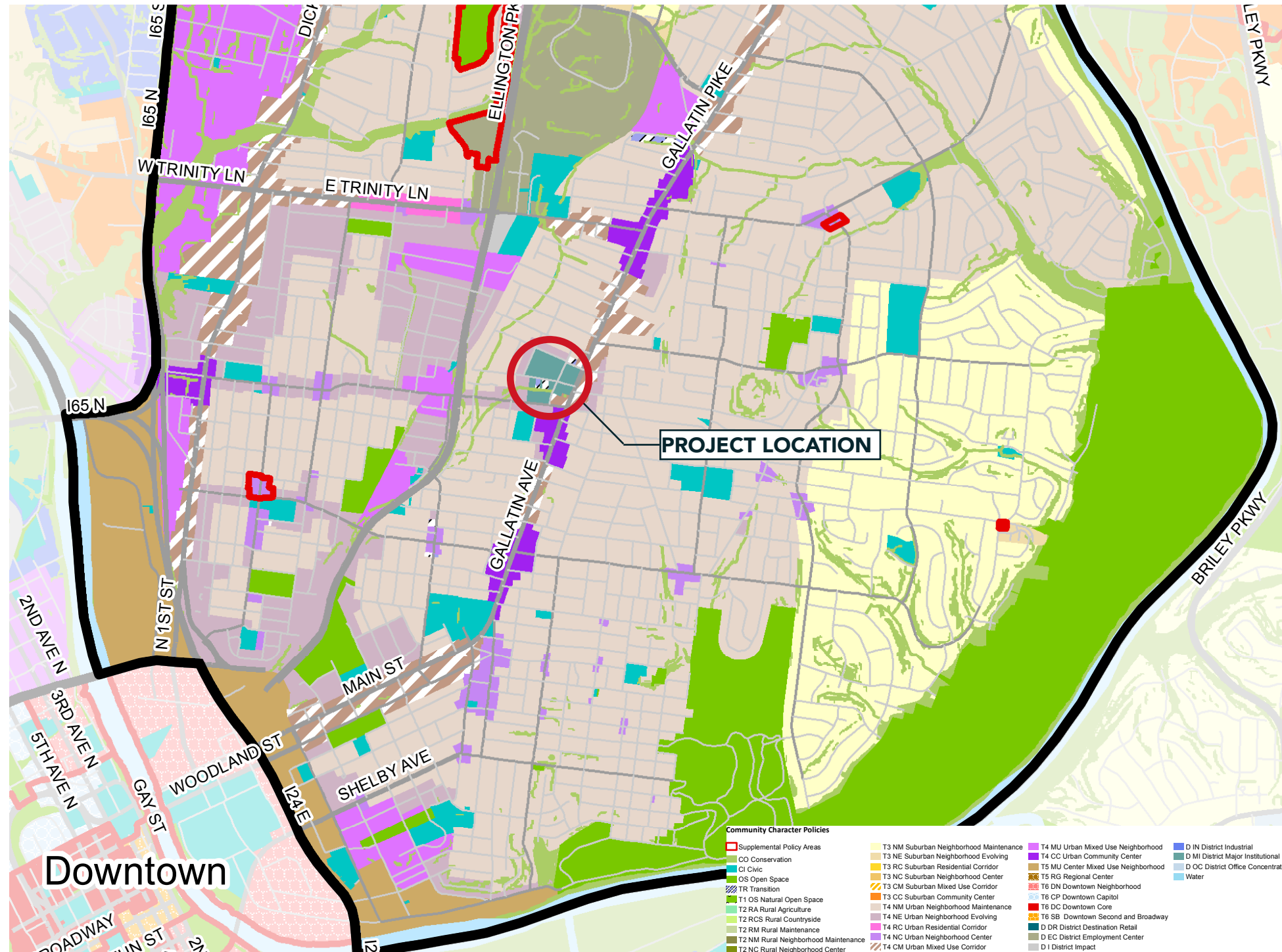
The proposed Lincoln Tech Specific Plan is located as a Center in a Tier 3 Zone as defined by the NashvilleNext Growth and Preservation Concept Plan Map and along Gallatin Road, which is identified as an immediate need High Capacity Transit Corridor.

Centers are defined as "Pedestrian-friendly areas with frequent transit service that contain a dense mix of homes, shops, jobs, and parks, as well as services, schools, and cultural amenities.

Tier 3 zones are defined as "...centers that could receive coordinated investments in response to opportunities identified by the private sector. This zone is surrounded by the Transitions and Infill zone which notes that higher density housing is appropriate along and around corridors and centers to provide a harmonious connection to surrounding neighborhoods.

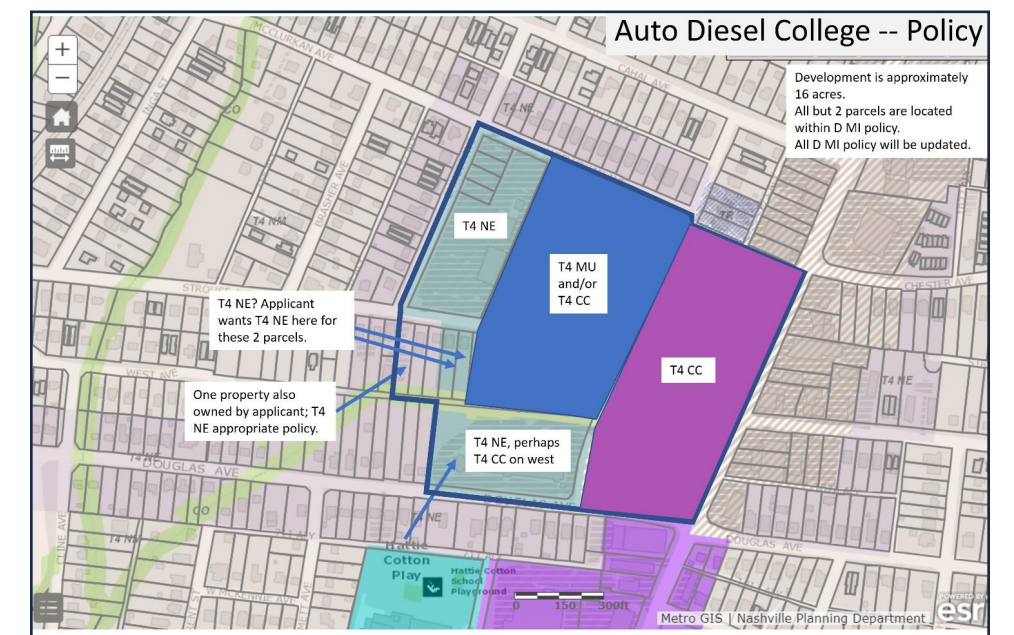


COMMUNITY CHARACTER POLICY



The Community Character Policy further defines the Lincoln Tech Specific Plan area as District Major Institutional policy. The request to amend the policy to T4 MU Urban Mixed Use Neighborhood and T4 CC Urban Community Center.

PROPOSED POLICY AMENDMENT



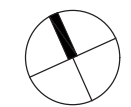


SPECIFIC PLAN

EXISTING RENRAW HOUSE FOOTPRINT



SCALE: 1" = 120' - 0"



NORTH



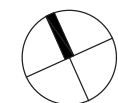
ACTIVE GROUND FLOOR USE

 ACTIVE GROUND FLOOR USE

DIAGRAM IS CONCEPTUAL IN NATURE AND PROVIDED TO ILLUSTRATE INTENT. FINAL LAYOUT IS SUBJECT TO CHANGE.

0 60 120 240

SCALE: 1" = 120' - 0"



NORTH



CIRCULATION DIAGRAM






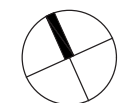
-  PEDESTRIAN CIRCULATION
-  SHARED STREET/ PLAZA
-  VEHICULAR ACCESS POINT
-  PEDESTRIAN ACCESS POINT
-  SPEED TABLE/ CURB-LESS ROAD SECTION

DIAGRAM IS CONCEPTUAL IN NATURE AND PROVIDED TO ILLUSTRATE INTENT. FINAL LAYOUT IS SUBJECT TO CHANGE.



SCALE: 1" = 120' - 0"



NORTH



TEMPORARY ACTIVATION



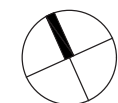
ALLOWED TEMPORARY ACTIVATION ZONES

TEMPORARY ACTIVATION MAY INCLUDE RETAIL, RESTAURANT, ENTERTAINMENT OR OTHER USES AS ALLOWED BY BASE ZONING (MUG-A)

DIAGRAM IS CONCEPTUAL IN NATURE AND PROVIDED TO ILLUSTRATE INTENT. FINAL LAYOUT IS SUBJECT TO CHANGE.



SCALE: 1" = 120' - 0"



NORTH