

Erosion Control and Grading Notes

- Expose as small an area of soil as possible on the site for no more than 15 days. Keep dust within tolerable limits by sprinkling or other acceptable means.
- All cut/fill areas to have a minimum of 6" of topsoil cover. Areas dressed with topsoil shall receive 12 lbs. per 1000 sq. ft. of 10-10-10 fertilizer (unless otherwise specified in written specifications), 5 lbs. or more of Kentucky 31 fescue seed per 1000 sq. ft., and a straw mulch cover of 70%-80% coverage (approximately 125 lbs. per 1000 sq. ft.), unless otherwise noted within written specifications.
- Erosion control barrier is called out on plans and is to comply with the Metropolitan stormwater management manual, volume four, section TCP-14.
- Disturbed areas are to be graded to drain as indicated in the plan to sediment barriers during and upon the completion of construction.
- The contractor shall be responsible for the verification and the location of any existing utilities. It shall be the responsibility of the contractor to avoid damage to all existing utilities during construction. If damage does occur to any such installation, full repair will be accomplished as per the current specification governing such work.
- Any access routes to the site shall be based with crushed stone, ASTM #1 stone, 100' long and at least 6" thick.
- The placing and spreading of any fill material is to be started at the lowest point and brought up in horizontal layers of 8" thickness (or as directed by the soils investigative report). Said fill material is to be free of sod, roots, frozen soils, or any other decomposable material. Said fill is to be compacted to a minimum of 95% standard proctor, or as otherwise specified by the soils report or written specifications.
- The contractor shall notify the Metro Davidson County department of Public Works construction compliance division, three days prior to beginning the work.
- The contractor shall locate and stake the layout of the site in the field for inspection by the engineer. The contractor shall check the grades and final dimensions on the ground, and report any discrepancies to the engineer immediately for a decision.
- Surplus excavation of topsoil shall be placed on the site as approved by the owner for the purpose of future landscape use.
- The contractor shall furnish and install all necessary temporary works for the protection of the public and employees, including warning signs and lights.
- The contractor shall be responsible for any damage done to the premises or adjacent premises or injuries to the public during the construction caused by himself, his sub-contractors, or the carelessness of any of his employees.
- All work is to be completed with compliance to the rules and regulations set forth by Metro Water Services. The contractor shall give all necessary notice, obtain all permits, and pay fees required for the completion of his portion of the work. He shall also comply with all city, county and state laws and ordinance or regulations relating to portions of work which he is to perform.
- All erosion control measures shall remain in place until site is stabilized & construction is complete.
- Contractor to provide an area for concrete wash down and equipment fueling in accordance with metro CP-10 and CP-13, respectively. Contractor to coordinate exact location with NPDES department during the pre-construction meeting. Grading permittee to include bmp's designed to control site wastes such as discarded building materials, chemicals, litter and sanitary wastes that may cause adverse impacts to water quality. The location of and/or notes referring to said bmp's shall be shown on the EPSC plan.
- The buffer along waterways will be an area where the surface is left in a natural state, and is not disturbed by construction activity. This is in accordance with the Stormwater Management Manual Volume 1 - Regulations.
- Drawing is for illustration purposes to indicate the basic premise of the development, as it pertains to storm water approval / comments only. The final lot count and details of the plan shall be governed by the appropriate Storm Water regulations at the time of final application.

Standard SP Notes

- The purpose of this SP is to receive preliminary approval 5 multi-family residential units
- Any excavation, fill or disturbance of the existing ground elevation must be done in accordance with Storm Water Management Ordinance No. 78-840 & Approved by the Metropolitan Department of Water Services.
- This Property Does Not Lie Within a Flood Hazard Area as Depicted on the Current Flood Insurance Rate Maps (FIRM) Numbers 47037CO105H dated 4/5/2017.
- All public sidewalks are to be constructed in conformance with metro public works sidewalk design standards.
- Wheel chair accessible curb ramps, complying with applicable metro public works standards, shall be constructed at street crossings.
- The required fire flow shall be determined by the metropolitan fire marshal's office, prior to the issuance of a building permit.
- Size driveway culverts per the design criteria set forth by the Metro Stormwater Manual (minimum driveway culvert in Metro right of way is 15" RCP).
- Metro Water Services shall be provided sufficient & unencumbered ingress & egress at all times in order to maintain, repair, replace & inspect any stormwater facilities within the property.
- The development of this project shall comply with the requirements of the Adopted Tree Ordinance 2008-328 (Metro Code Chapter 17.24, Article II, Tree Protection and Replacement; and Chapter 17.40, Article X, Tree Protection and Replacement Procedures).
- Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
- For any development standards, regulations and requirements not specifically shown on the SP plan and/or included as a condition of commission or council approval, the property shall be subject to the standards, regulations and requirements of the RM4 zoning district as of the date of the applicable request or application.
- The final unit count and details of the plan shall be governed by the appropriate regulations at the time of final application.
- The final site plan / building permit site plan shall depict required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to issuance of use and occupancy permits, existing vertical obstructions shall be located outside of the required sidewalks. Vertical obstructions are only permitted within the required grass strip or frontage zone.
- All development within the boundaries of this plan meet the requirements of the Americans with Disabilities Act and the Fair Housing Act.

Valley View Improvement Requirements

This development is required to provide roadway improvements per the MPW Designation: ST-252
Standard Right-of-Way: 50 feet
Half of Standard Right-of-Way: 25 feet
Planting Strip Width: 4.00
Sidewalk Width: 5.00
Roadway improvements will be designed and constructed per the requirements of the Metro Nashville Department of Transportation

Landscape Buffering Requirements

Existing vegetation will be maintained along the perimeter of the property. Where the property abuts the existing residential and SP Zoning, the existing perimeter vegetation will be supplemented in order to establish a Class "B" Buffer

Traffic Engineer

Prior to the submittal of a Final SP, A Traffic Access Study Shall Be Scoped By The Metro Traffic Engineer and a Resulting Traffic Access Study Shall Be Completed and Submitted to the Traffic Engineer For Review and Approval. All Traffic Improvements Warranted by Said Study Shall Be Designed and Submitted with the Application for The Final SP.

Stormwater Notes

- This site is responsible for water quality and water quantity.
- Design of stormwater features will be provided during the final SP process.
- Storm sewer system on this plan is shown schematically. Final design will be provided during the final SP process and will meet the requirements of the stormwater management manual.
- This project will disturb more than 1 acre, therefore, a NOI will be submitted to TDEC during final SP process.
- This drawing is for illustration purposes to indicate the basic premise of the development. The final lot count and details of the plan shall be governed by the appropriate regulations at the time of final application.
- (Any excavation, fill, or disturbance of the existing ground elevation must be done in accordance with storm water management ordinance No. 78/840 and approved by The Metropolitan Department of Water Services.)
- (The buffer along waterways will be an area where the surface is left in a natural state, and is not disturbed by construction activity. This is in accordance with the Stormwater Management Manual Volume 1 - Regulations.)
- (Metro Water Services shall be provided sufficient and unencumbered access in order to maintain and repair utilities in this site.)
- (Size driveway culverts per the design criteria set forth by the Metro Stormwater Management Manual (Minimum driveway culvert in Metro ROW is 15" RCP).

NES Notes

- NES can meet with developer upon request to determine service options.
- NES shall be placed in 20"PU" along the front of each property.
- This phase will need to have underground power that will be connected to stubouts from phase 1 and phase 1B.
- NES follows the National Fire Protection Association rules; Refer to NFPA 70 article 450-27; and NESC Section 15 - 152.A.2 for complete rules (see NES Construction Guidelines under "Builders and Contractors" tab @ www.nespower.com).
- Final quantity and location of NES equipment to be determined by the NES Engineer after receiving final plans and electrical load information. A preliminary Exhibit "B" design will be sent to the developer or representatives of the developer for review. Suggestions or requests to the design should be made during this review process. Any changes requiring re-design, after this document has been signed, will be at the developer's expense.
- Developer's vegetation design shall meet both Metro requirements and NES Vegetation Management requirements/clearances.
- NES facilities will not be allowed to sit in or to pass through retention areas including rain gardens, bio-retention, bio swales and the like. This includes primary duct between pad-mounted equipment, as well as service duct to a meter or meter center.
- NES needs electrical load information including any house, irrigation, pump, or compactor services.
- Postal plan is required before NES's final construction drawings can be approved.

TO APPLY FOR SERVICE:

DIGITAL COPY: Provide copy of civil site plan on a CD (no pdfs; no x-ref's in .dwg file) registered to the TN State Plane Coordinate System, North American Datum 1983 (NAD83) with NO Datum Adjustment applied. Put data in separate layers and label them for easy identification.

Developer/Engineer will provide one complete set of approved plans by Metro Planning w/ all changes from other departments (fire, storm water, public works, etc...) to NES.

Developer/Engineer to provide a proposed easement drawing for the electric, phone and CATV. .

Go to www.nespower.com click on the "BUILDERS & CONTRACTORS" tab. Next click on the "Apply for Residential Subdivision" fill out the form. Then follow the direction for sending the digital drawing and the forms

Metro Water and Sewerage Services

Public and/or private Water and Sanitary Sewer construction plans must be submitted and approved prior to Final Site Plan/SP approval. The approved construction plans must match the Final Site Plan/SP plans. Submittal of an availability study is required before the Final SP can be reviewed. Once this study has been submitted, the applicant will need to address any outstanding issues brought forth by the results of this study. A minimum of 30% W&S Capacity must be paid before issuance of building permits.

Metro Fire Marshal

Limited building detail, and/ or building construction information provided. Construction must meet all applicable building and fire codes. Any additional fire code or access issues will be addressed during the construction permitting process. Site plans or construction may require changes to meet adopted fire and building codes.

Architectural Requirements

- Building façades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 15% glazing.
- Windows shall be vertically oriented at a ratio of 1.5:1 or greater, except for dormers.
- Building facades shall be constructed of brick, brick veneer, stone, cast stone, cementitious siding, glass, or materials substantially similar in form and function, unless otherwise approved on detailed building elevations included with the preliminary SP.
- Porches shall provide a minimum of six feet of depth.
- A raised foundation of 18"- 36" is required for all residential structures.

Landscape Requirements

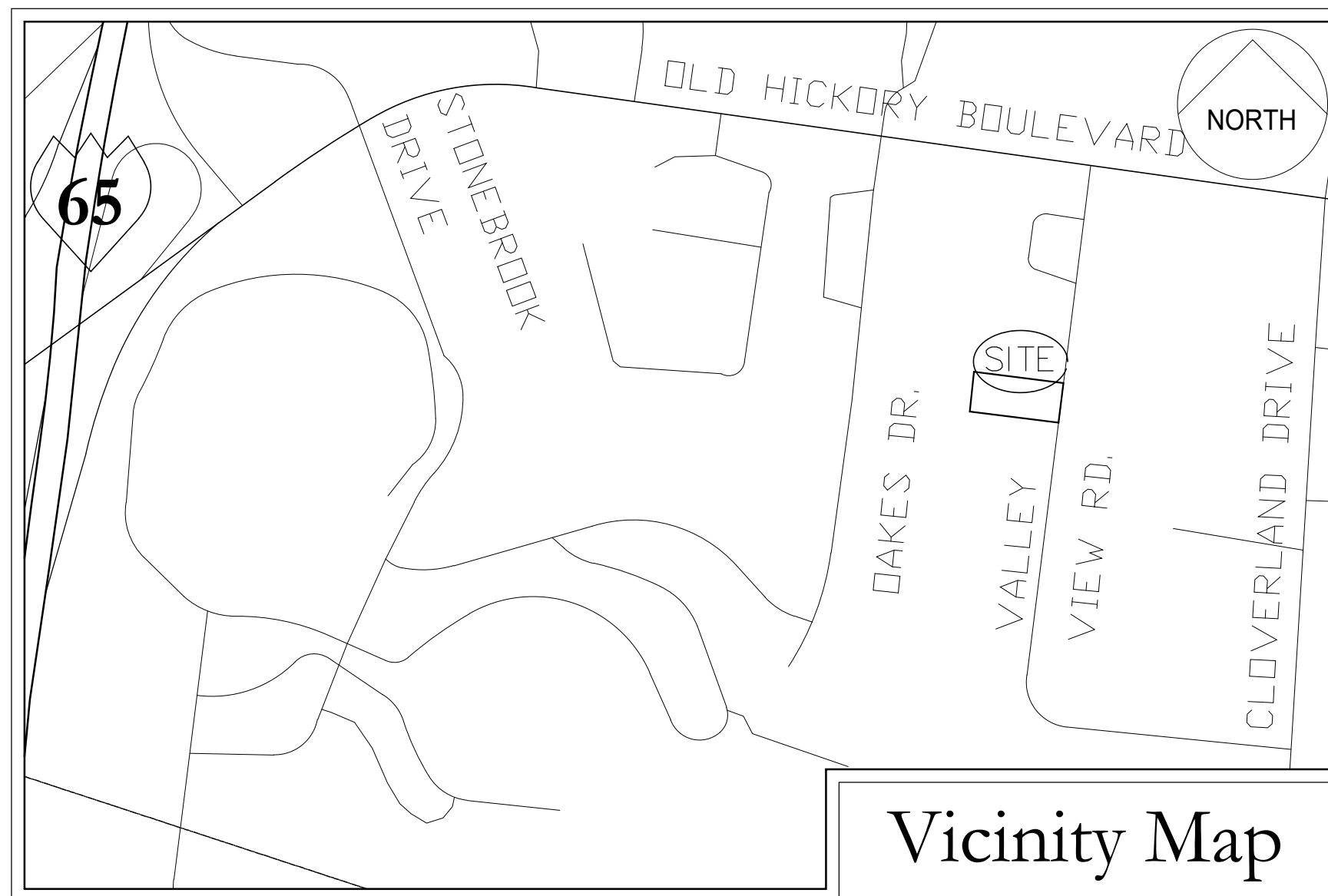
Landscaping and tree density requirements per Metro Zoning Code. A complete landscape plan will be required with the Final SP submittal.
Existing vegetation will be maintained along the entire perimeter of the property. Where the property abuts the existing residential and SP Zoning, the existing perimeter vegetation will be supplemented in order to establish a Class "B" Buffer with 10 foot of width, 4.5 canopy trees, 1.8 understorey trees, and 18 shrubs per each 100 linear feet.

Nashville Department of Transportation

With the final: Callout the following per NDOT ST- detail sections: access and ADA ramps, sidewalks, curb & gutter. All ROW frontages shall adhere to the Major Collector Street Plan(MCSP). Dedicate ROW along all frontages to accommodate MCSP requirements. Provide call outs on final site plan for MCSP requirements. Show 'Now Entering Private Drive' signage where applicable off public roads into site. Provide internal stop control at private or public intersections. Where there may be potential vehicular or pedestrian sight distance issues, provide stopping sight distance exhibits at any relevant intersections and accesses. Provide adequate sight distance spacing at all access ramps and public streets with on-street parking. Dimension ROW pavement widths for clarity. Provide a waste plan for the development. Note: A private hauler will be required for site waste/recycle disposal.

A Preliminary SP Brentwood Chase 2

Case No. 2023SP-052-001
Being Parcel 117 on Tax Map 160
Nashville, Davidson County, Tennessee



GENERAL PLAN CONSISTENCY NOTE

T3 Suburban Neighborhood Maintenance (T3-NM) areas will experience some change over time, primarily when buildings are expanded or replaced. Efforts should be made to retain the existing character of the neighborhood, in terms of its development pattern, building form, land use, and the public realm. Where transportation infrastructure is insufficient or not present, enhancements may be necessary to improve pedestrian, bicycle, and vehicular connectivity. T3-NM areas tend to be dominated by single-family detached and two-family plex housing; but may also contain other building types, such as tri- and quad-plexhouses, townhouses, low-rise flats, and courtyard flats.

This SP meets the goals of the T3-NM Policy by providing a transitional mixture of housing needed in this area. Wider Homes along Valley View Drive with deep setbacks maintain the character of the current street while developing a sidewalk for connectivity to the nearby Commercial and Retail areas. The single family cottages along the westerly boundary creates smaller predominately one level homes for this aging demographic. This development the second phase to the adjacent SP approved in 2022 and as presented to the community during the prior adjacent SP zoning.

Development Summary		
Property Information Map 60 Parcel 117 John and Kathleen Hyland 5629 Valley View Drive Nashville, TN 37027 Zoned: R40	Floodnote This Property Does Not Lie Within a Flood Hazard Area as Depicted on the Current Flood Insurance Rate Maps (FIRM) Number 47037CO105H dated 4/5/2017.	Telephone Service Beilsouth 866.620.6000 Utility Location Tennessee One-Call 800.351.1111
Total Site Area: 1.55 Ac Council District 4 - Robert Swope	Electric Service Nashville Electric Service (NES) 1214 Church Street Nashville, Tennessee 37246 615.747.6807	Gas Service Nashville Gas (Piedmont) 615.734.0734
Developer John and Kathleen Hyland 5629 Valley View Drive Nashville, TN 37027 Email:kathymtyland@yahoo.com	Water and Sewer Service Metro Water Service 1600 2nd Avenue North Nashville, TN 37208 615.862.4598	
Civil Engineer and Surveyor Dale & Associates (Roy Dale, PE) 516 Heather Place Nashville, Tennessee 37204 615.297.5166		

Specific Plan Development Summary	
Uses	5 Multifamily Residential Units
CCM T3NM	
Project Area	1.55 Acres
Property zoning: R40	Surrounding Zoning: RM6, R10 and R40
Fall Back Zoning	RM4
Minimum lot size	N/A
FAR	0.60
ISR	0.90
Density	5 Units/ 3.2 Units Per Acre
Street Setbacks	80' measured from exist R.O.W.
Side yard	10'
Rear yard	20'
Height standards	Two Stories in 35 feet
Parking and Access	
Proposed Ramp Location and Number	1 Shared Drive
Distance to intersection	850 ft Old Hickory Blvd
Parking	Per Metro Zoning Code

Sheet Schedule

- C1.0 Notes & Project Standards
- C2.0 Existing Conditions
- C3.0 Proposed Overall SP Layout

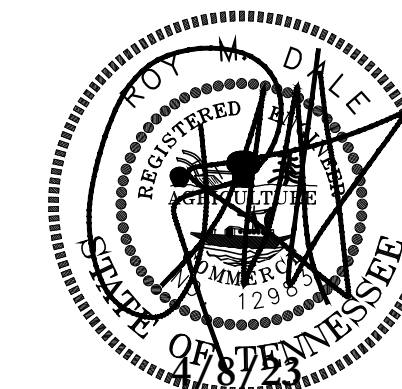
ARCHITECTUAL NOTES

BUILDING ELEVATIONS FOR ALL FACADES SHALL BE PROVIDED WITH THE FINAL SITE PLAN. THE FOLLOWING STANDARDS SHALL BE MET:

A. BUILDING FACADES FRONTING A STREET SHALL PROVIDE A MINIMUM OF ONE PRINCIPAL ENTRANCE (DOORWAY) AND A MINIMUM OF 25% GLAZING.

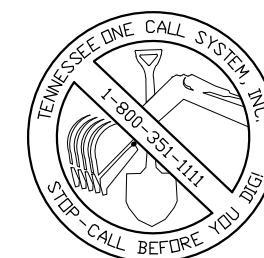
B. WINDOWS SHALL BE VERTICALLY ORIENTED AT A RATIO OF 2:1 OR GREATER, EXCEPT FOR DORMERS. .

C. EIFS, VINYL SIDING, AND UNTREATED WOOD,SHALL BE PROHIBITED.



Rev 5/14/23

Rev 5/24/23



Dale & Associates
Civil Engineering
Land Planning & Zoning
Surveying
516 Heather Place
Nashville, TN 37204
(615) 297-5166

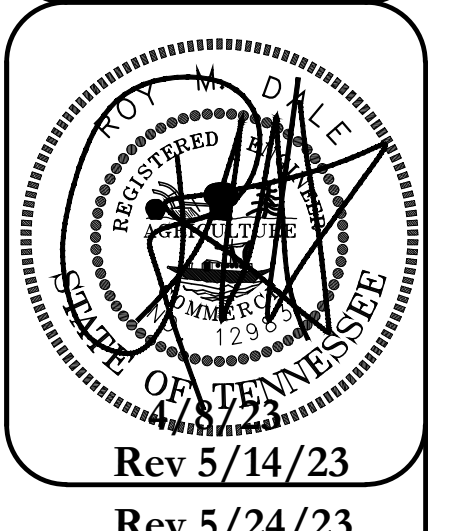
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C1.0

Notes and Project Standards

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Brentwood Chose 2
Parcel 117 Tax Map 160
Valley View Drive
Davidson County, Tennessee



Rev 5/14/23
Rev 5/24/23

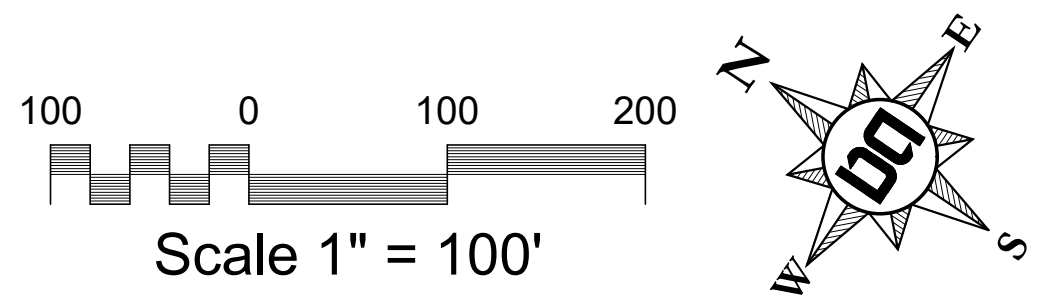
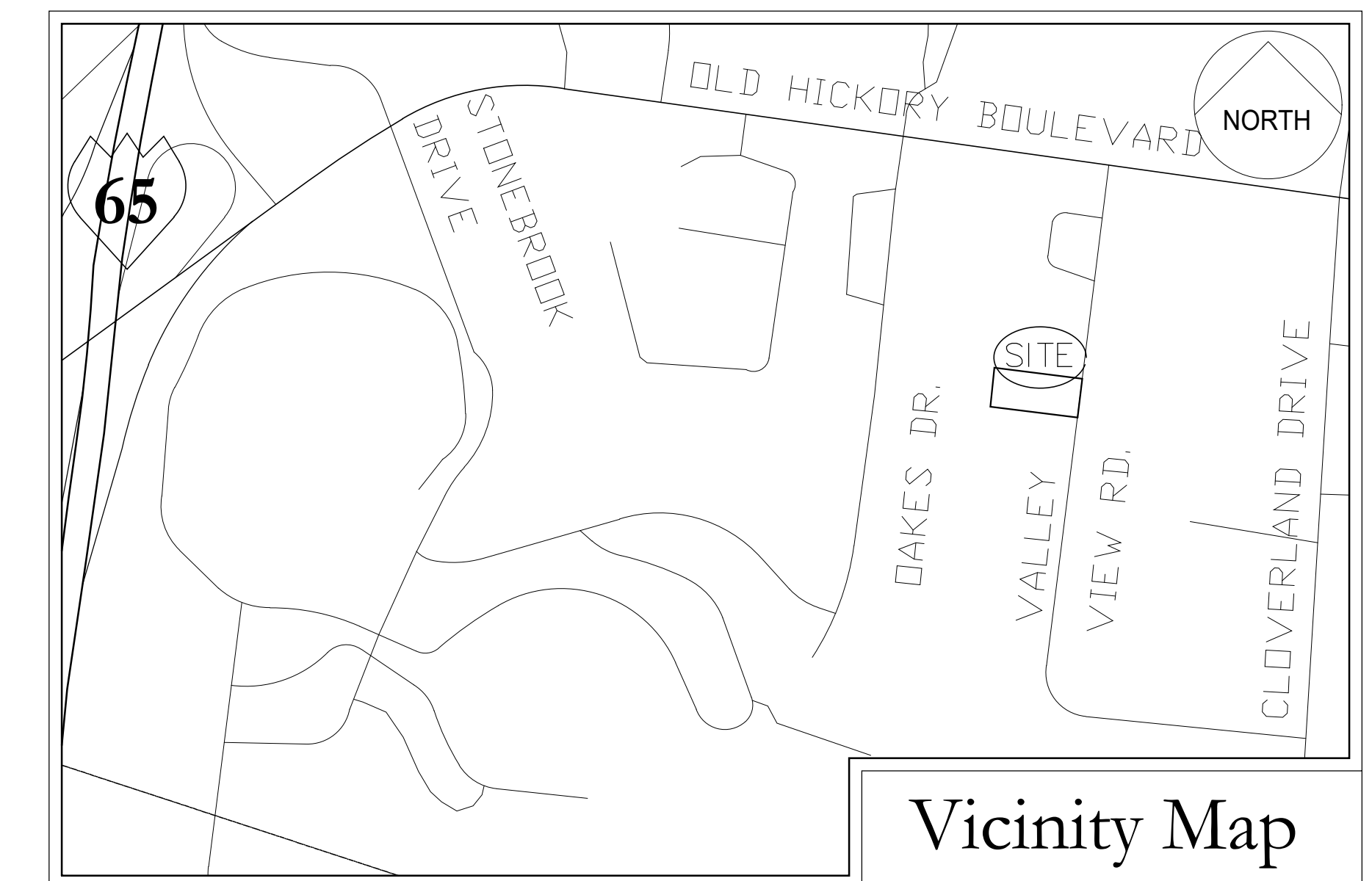
Dale & Associates
Civil Engineering,
Landscape Architecture & Planning,
Surveying

516 Heather Place
Nashville, TN 37204
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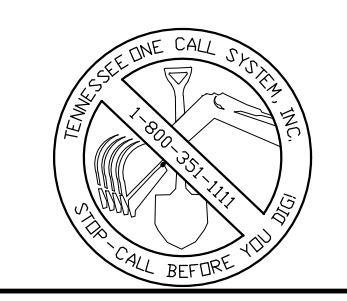
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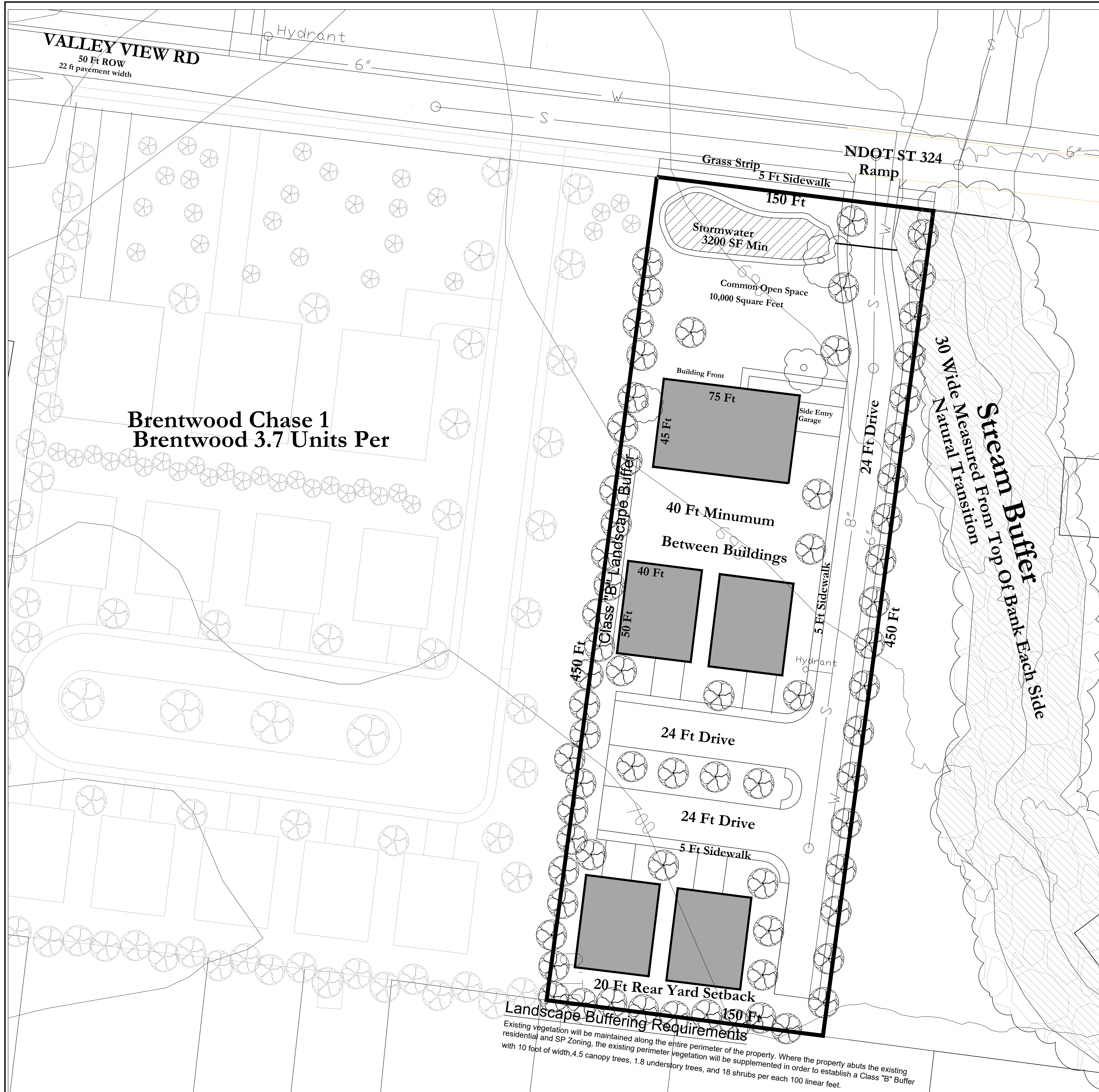
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CCM T3NM	
Project Area	1.55 Acres
Property zoning: R40	Surrounding Zoning: RM6, R10 and R40
Fall Back Zoning	RM4
Minimum lot size	N/A
FAR	0.60
ISR	0.90
Density	5 Units/ 3.2 Units Per Acre
Street Setbacks	80' measured from exist R.O.W.
Side yard	10'
Rear yard	20'
Height standards	Two Stories in 35 feet
Parking and Access	1 Shared Drive
Proposed Ramp Location and Number	850 ft Old Hickory Blvd
Distance to intersection	Per Metro Zoning Code
Parking	



Number	Parcel ID	Owner	Property Address	Property City	State	Zipcode	Zoning	Recorded
1	16000011700	HASSEY, JAMES D ESTATE & HASSEY, CHANDLER FORD	5625 VALLEY VIEW RD	BRENTWOOD	TN	37027	SP	Bk 20211008 Pg 0135311
2	16014001700	HOLLAND, L KELL & MARIA	5640 OAKES DR	BRENTWOOD	TN	37027	R10	Bk 20210817 Pg 0111031
3	16014001800	CUMMINGS, DENNIS K.	5642 OAKES DR	BRENTWOOD	TN	37027	R10	Bk 20040618 Pg 0072249
4	16014001900	TENG, RENJING & LI NIE	5644 OAKES DR	BRENTWOOD	TN	37027	R10	Bk 11647 Pg 245
5	17100004800	WALLACE, CHARLES M. ET UX	5633 VALLEY VIEW RD	BRENTWOOD	TN	37027	R40	Bk 8826 Pg 848

Existing Conditions





Brentwood Chase 1
Brentwood 3.7 Units Per

Landscape Buffering Requirements
 Existing vegetation will be maintained along the entire perimeter of the property. Where the property abuts the existing residential and SP Zoning, the existing perimeter vegetation will be supplemented in order to establish a Class "B" Buffer with 10 foot of width, 4.5 canopy trees, 1.8 understory trees, and 18 shrubs per each 100 linear feet.

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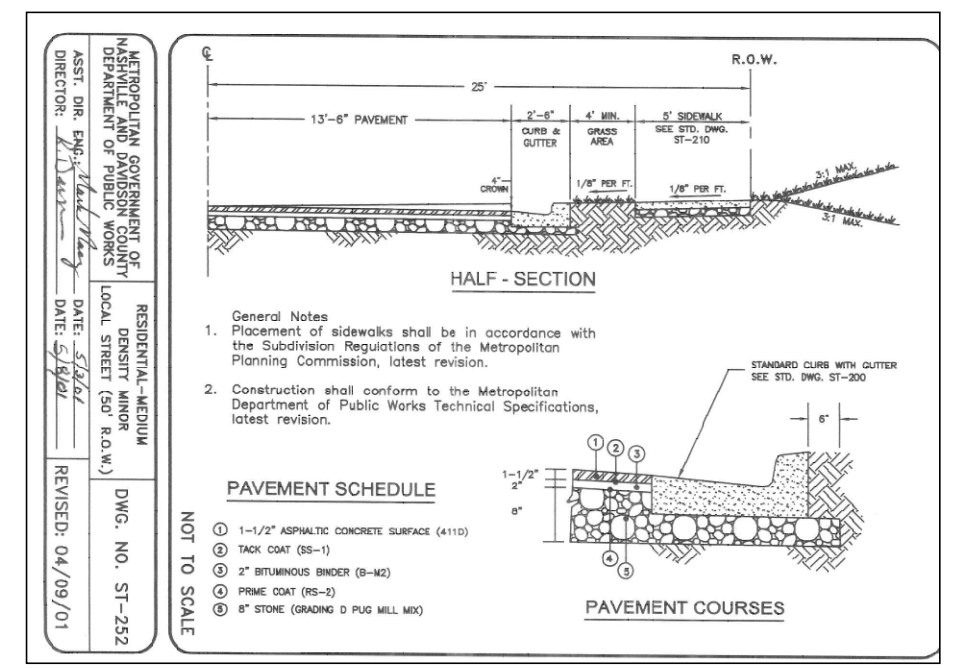
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Rear yard	20'
Height standards	Two Stories in 35 feet
Parking and Access	
Proposed Ramp Location and Number	1 Shared Drive
Distance to intersection	850 ft Old Hickory Blvd
Parking	Per Metro Zoning Code

Landscape Notes

- A Detailed Tree Survey will be provided with the Final Sp Plans. This survey will locate and identify all trees that are 6 inch in caliper or larger. Any tree with 24 inch or greater in breast height, or that qualifies as a heritage tree (sec. 17.40.450.B) shall be survey located and depicted on the Final SP Plan
- Proposed Trees Indicated on this Preliminary SP are general locations for proposed vegetation. The Final SP will include a detailed Landscape Plan to satisfy the requirements of Chapter 17.24 of the Zoning Code
- The landscape contractor shall coordinate all construction with the appropriate utility company and shall be responsible for and damage to utilities. the landscape contractor shall verify the exact location of all utilities and take precautions to prevent damage to the utilities.
- All planting and mulch beds shall be sprayed with round-up (contractor's option) prior to the installation of mulch.
- Plant materials and stumps indicated for removal shall be removed and disposed off-site by the contractor. Backfill holes with topsoil free of roots and rocks.
- The landscape contractor shall be responsible for the fine grading of all planting areas.
- All planting areas shall be fertilized with 12#/1000 s.f. of 10-10-10 fertilizer.
- All planting beds shall have a minimum of 3" depth of shredded hardwood bark mulch.
- The landscape contractor shall verify all material quantities. In the event of a discrepancy, the quantities shown on the plan will take precedence.
- The landscape contractor shall provide the owner with written instructions on the proper care of all specified plant materials prior to final payment.
- Existing trees to remain shall be protected from construction damage. Selectively prune dead wood.
- All disturbed areas shall be planted with turf as indicated on the materials schedule.
- All deciduous trees, existing and proposed shall be pruned to provide 4' minimum clear trunk unless otherwise noted.
- The landscape contractor shall provide a one year warranty on all plant materials and replace any dead or dying material within that time period.
- No plant materials should be substituted without authorization by Dale & Associates. Plant sizes shown are minimums required by the local municipality and materials shown have been selected specifically for this project.
- All wire baskets shall be completely removed and disposed of, burlap should be removed or punctured in at least 5 places. Remove all twine from burlapped materials.
- Guying is not allowed unless required by municipality or site conditions. The landscape contractor shall remove wires after a one year period.
- No canopy tree shall be located within 15' of an overhead utility. No tree shall be located within a public utility easement. Locating plant materials within a drainage easement is acceptable, but only if installed as not to disturb existing drainage flow. In such instances, the materials shall be located no closer than 5' from the centerline of drainage.
- Landscape and tree density requirements per Metro Zoning Code. A complete landscape plan will be required with the Final SP submittal.

Preliminary Stormwater Calculations

Site Area = 1.55 acres
 Soils Type - B
 Exist NCN 73.7
 Prop NCN 82.1
 Tv Required 3017 Cubic Feet
 Ro Volume 200% x 3017 CF - 6035 Cubic Feet
 2 ft Depth -3017 SF Required

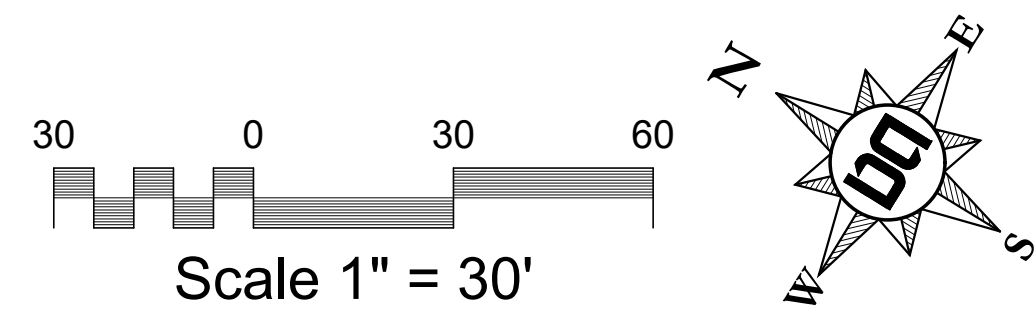


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Traffic Engineer

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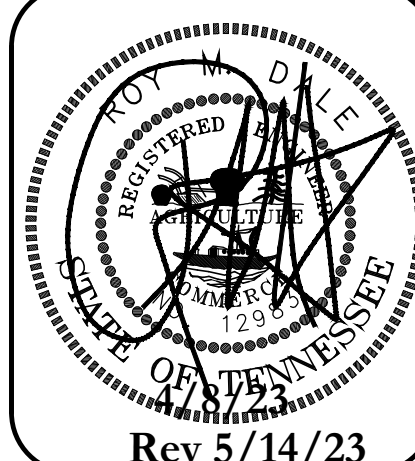


Proposed Valley View Street X Section
Proposed Overall SP Layout

Drawing Date:

Revisions

Brentwood Chase 2
 Parcel 117 Tax Map 160
 Valley View Drive
 Davidson County, Tennessee



Rev 5/14/23
 Rev 5/24/23

Dale & Associates
 Civil Engineering
 Land Planning & Zoning
 Surveying
 516 Heather Place
 Nashville, TN 37204
 (615) 297-5166

Case No. 2023SP-052-001

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