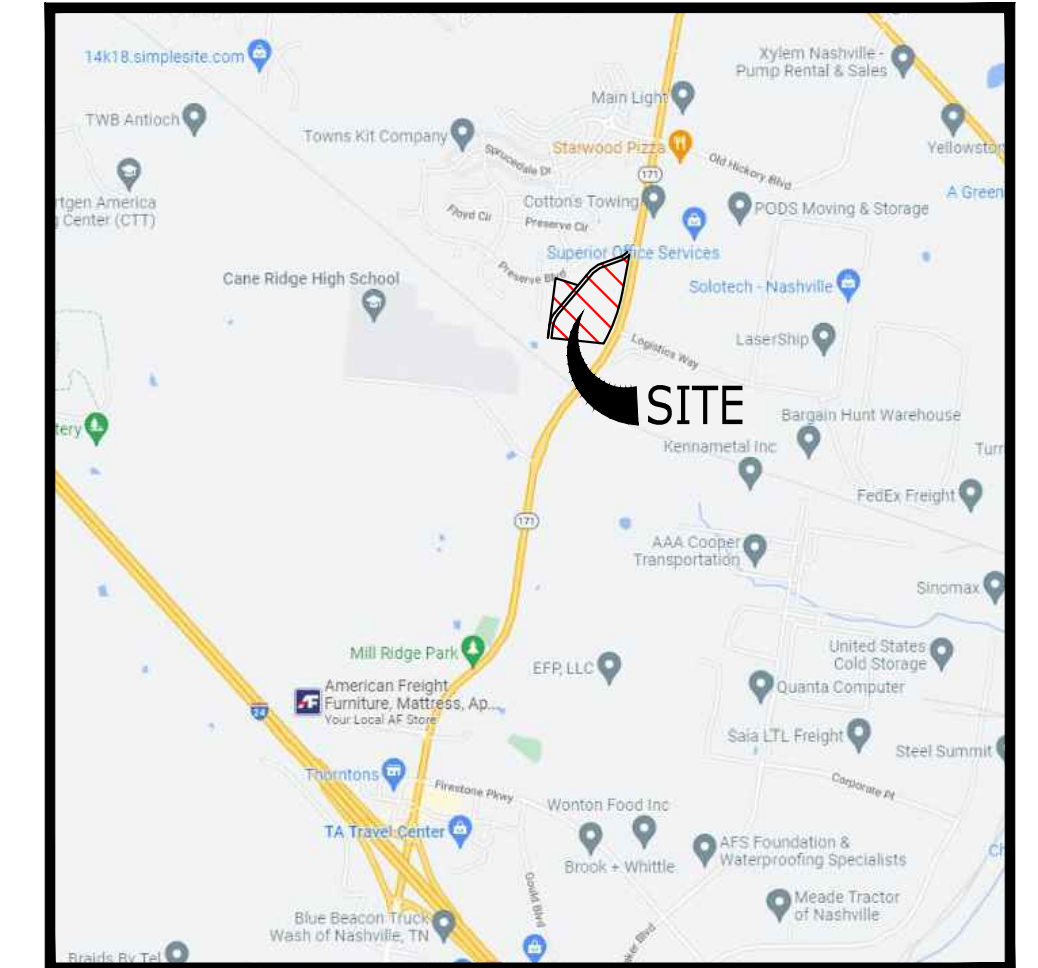


PRELIMINARY SP 12782 OLD HICKORY BLVD. RESIDENCES

ANTIOCH, DAVIDSON COUNTY, TENNESSEE

CASE NO. 2023SP-007-001
CATALYST PROJECT NO. 20220249
NOVEMBER 30, 2022; FEBRUARY 28, 2023; MARCH 13, 2023



VICINITY MAP
NOT TO SCALE

DEVELOPMENT SUMMARY

• SP NAME: 12782 OLD HICKORY BLVD. RESIDENCES
• SP NUMBER: 2023SP-007-001
• COUNCIL DISTRICT: 32
• COUNCIL MEMBER: JOY STYLES

OWNER: SHEKHAR C. THAKUR
ADDRESS: 231 NORTH AVENUE
BATTLE CREEK, MI 49017

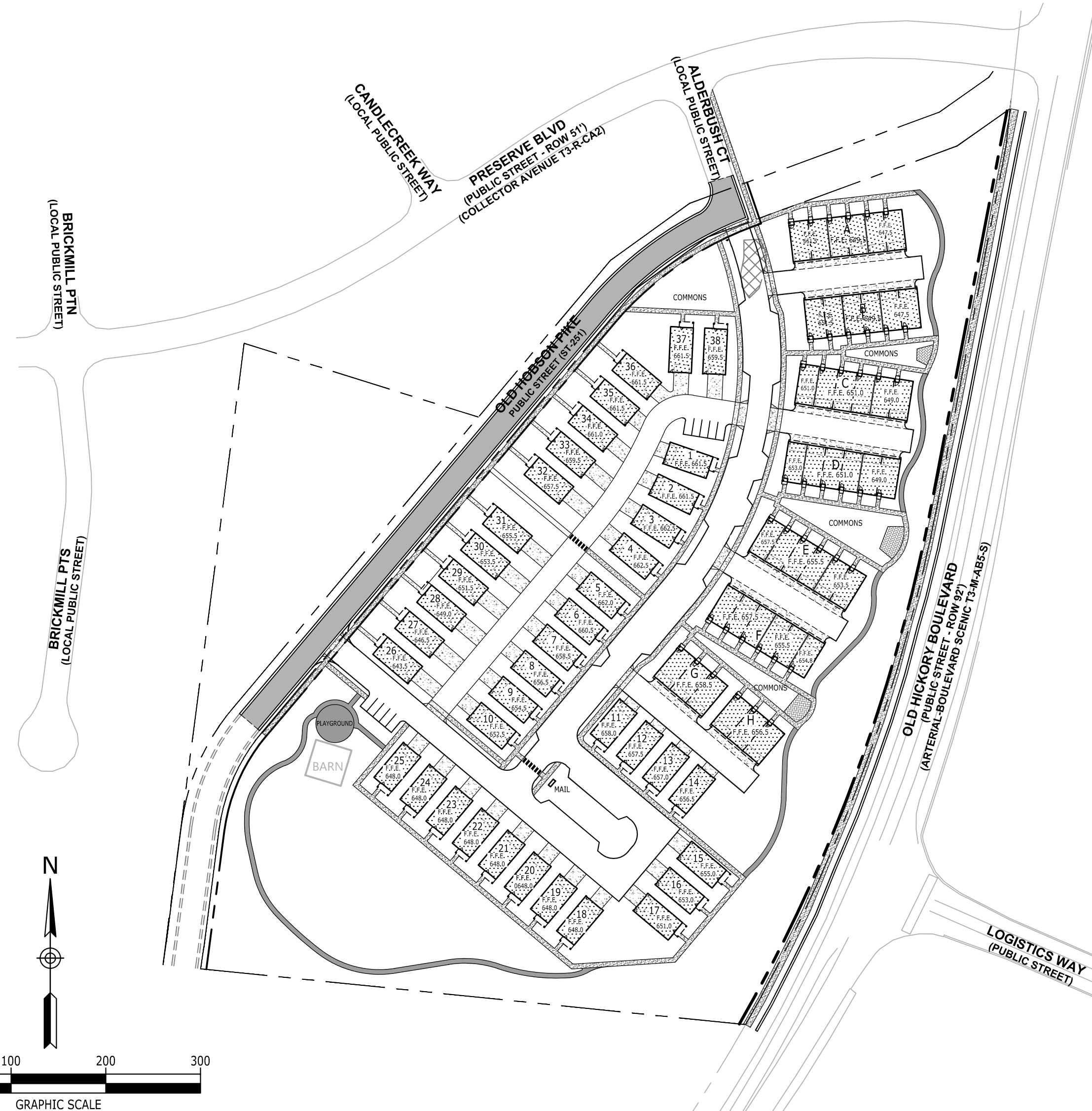
DEVELOPER: PIERCE DEVELOPMENT
1900 BELMONT BLVD.
NASHVILLE, TN 37212
CONTACT: JIMMY PIERCE
CONTACT EMAIL: piercedevdevelopmentgroup@gmail.com

PROJECT REPRESENTATIVE: CATALYST DESIGN GROUP
ADDRESS: 1524 WILLIAMS AVE., SUITE 201
MURFREESBORO, TN 37129
615.622.7200
CONTACT: JACK PARKER
CONTACT EMAIL: jparker@catalyst-dg.com

NOTE:
THE PROPERTY SHOWN HEREON IS NOT LOCATED WITHIN AREAS DESIGNATED AS 100 YEAR FLOOD ZONE PER F.E.M.A. MAP 47037C04111, DATED FEB. 25, 2022 AND F.E.M.A. MAP 47037C04133, DATED FEB. 25, 2022.

SITE DATA TABLE

SITE ACREAGE:	
PARCEL 20	1.31 AC. (56,917 S.F.)
PARCEL 33	10.42 AC. (453,998 S.F.)
TOTAL ACREAGE	11.73 AC. (510,915 S.F.)
EXISTING ZONING:	AR2A
PROPOSED ZONING:	SP
EXISTING LAND USE:	VACANT RURAL LAND (PARCEL 20) FARM BUILDINGS ONLY (PARCEL 33)
PROPOSED LAND USE:	MULTIFAMILY TOWNHOMES AND COTTAGE RESIDENCES
RESIDENTIAL SUMMARY:	
COTTAGE RESIDENCES 2/3 STORY RESIDENCES	38 UNITS
TOWNHOMES 2/3 STORY RESIDENCES	42 UNITS
TOTAL UNITS	80 UNITS
PROPOSED DENSITY:	6.8 UNITS/AC.
PROPOSED FAR:	0.5
IMPERVIOUS SURFACE:	
BUILDINGS	83,130 S.F.
ROADS/SIDEWALKS/DRIVES	41,703 S.F.
	124,833 S.F. (2.87 AC.)
TOTAL ISR:	
DISTRICT 1	0.70
DISTRICT 2	0.80
PARKING REQUIRED: PARKING TO BE PROVIDED PER METRO CODE REQUIREMENTS.	
PARKING PROVIDED:	
COTTAGES GARAGES (2 GARAGE SP. / UNIT)	76 SPACES
TOWNHOME GARAGES (2 GARAGE SP. / UNIT)	84 SPACES
SURFACE PARKING	40 SPACES
TOTAL PARKING PROVIDED	200 SPACES
PRESERVED OPEN SPACE: (UNDISTURBED AREA)	2.3 AC. (100,188 S.F.)



DEVELOPMENT NOTES

1. THE PURPOSE OF THIS SP IS TO PERMIT UP TO 80 MULTIFAMILY RESIDENTIAL UNITS.
2. ALL DEVELOPMENT WITHIN THE BOUNDARIES OF THIS PLAN MEETS THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT AND THE FAIR HOUSING ACT.
ADA: <http://www.ada.gov/>
U.S. Justice Dept.: http://www.justice.gov/crt/housing/fairhousing/about_fairhousingact.htm
3. TRASH AND RECYCLING SERVICE SHALL BE CONTRACTED THROUGH A PRIVATE COLLECTION SERVICE.
4. THE FINAL SITE PLAN/BUILDING PERMIT SITE PLAN SHALL DEPICT THE REQUIRED PUBLIC SIDEWALKS, ANY REQUIRED GRASS STRIP OR FRONTAGE ZONE AND THE LOCATION OF ALL EXISTING AND PROPOSED VERTICAL OBSTRUCTIONS WITHIN THE REQUIRED SIDEWALK AND GRASS STRIP OR FRONTAGE ZONE. PRIOR TO THE ISSUANCE OF USE AND OCCUPANCY PERMITS, EXISTING VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUTSIDE OF THE REQUIRED SIDEWALK. VERTICAL OBSTRUCTIONS ARE ONLY PERMITTED WITHIN THE REQUIRED GRASS STRIP OR FRONTAGE ZONE.
5. PERMITTED RESIDENTIAL USES SHALL INCLUDE:
- MULTIFAMILY
6. FOR ANY DEVELOPMENT STANDARDS, REGULATIONS AND REQUIREMENTS NOT SPECIFICALLY SHOWN ON THE SP PLAN AND/OR INCLUDED AS A CONDITION OF COUNCIL APPROVAL, THE PROPERTY SHALL BE SUBJECT TO THE STANDARDS, REGULATIONS AND REQUIREMENTS OF THE RM-9 BASE ZONING DESIGNATIONS AS OF THE DATE OF THE APPLICATION REQUEST OR APPLICATION.
7. THE REQUIREMENTS OF THE METRO FIRE MARSHAL'S OFFICE FOR EMERGENCY VEHICLE ACCESS AND ADEQUATE WATER SUPPLY FOR FIRE PROTECTION MUST BE MET PRIOR TO THE ISSUANCE OF ANY BUILDING PERMITS.
8. PARKING GARAGE WIDTHS SHALL HAVE A MINIMUM OF 17'.

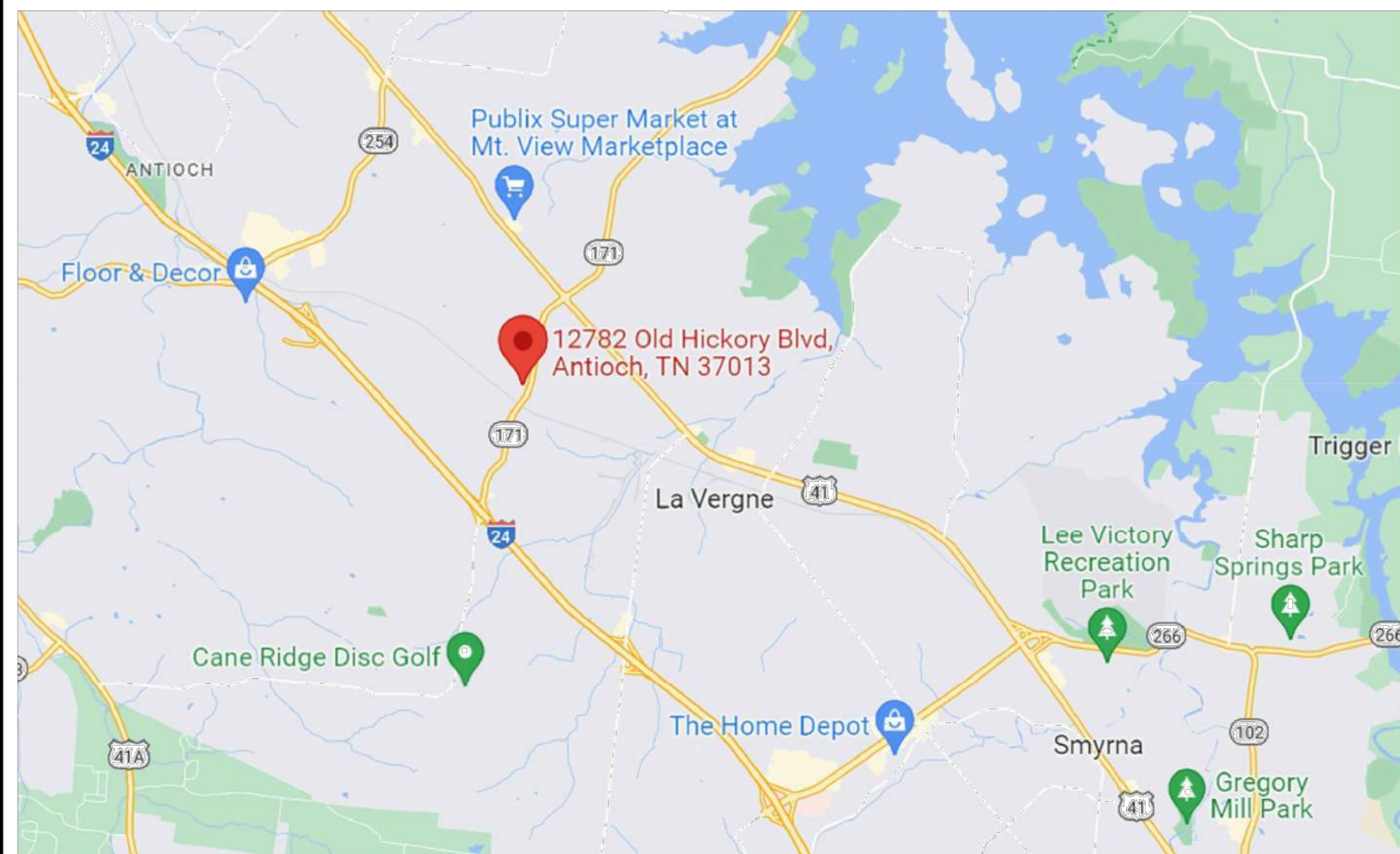
AMENITY PROGRAMMING NOTES

1. COMMONS AREA COURTYARDS WILL INCLUDE A VARYING MIXTURE OF THE FOLLOWING AMENITY FEATURES:
- FIRE PIT
- OUTDOOR GRILLING STATION
- BENCH AND FURNITURE SEATING
- DINING TABLE AND CHAIRS
- GAMING AMENITIES SUCH AS CORN HOLE AND BOCCIE, ETC.
- NIGHTTIME LIGHTING
- PLAYGROUND

DEVELOPER
PIERCE DEVELOPMENT
1900 BELMONT BLVD.
NASHVILLE, TN 37212
615.691.3916

CIVIL ENGINEER/LANDSCAPE ARCHITECT





VICINITY MAP
NOT TO SCALE

OLD HOBSON PIKE RIGHT-OF-WAY

THE RIGHT-OF-WAY FOR THE OLD HOBSON PIKE ROADWAY IS SHOWN HEREON AS FORTY (40) FEET IN TOTAL WIDTH AND WAS DETERMINED BY REVIEWING EXISTING CITY AND COUNTY MAPS WHICH WERE PUBLIC RECORDS AVAILABLE AT THE TIME OF THE SURVEY. THE SURVEYOR MAKES NO GUARANTEE FOR THE ROADWAY & R/W WIDTH OR ABANDONMENT STATUS.

SURVEYOR'S DESCRIPTION

DAVIDSON COUNTY
PROPERTY MAP 175 PARCEL 33.00
12782 OLD HICKORY BOULEVARD
ANTIOCH, TN 37013

BEING A TRACT OR PARCEL OF LAND LOCATED IN ANTIOCH, DAVIDSON COUNTY, TENNESSEE, SAID TRACT BEING A PORTION OF THE PROPERTY CONVEYED BY DEED IN INSTR. #20061127_0145612, R.O.D.C. AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS;

BEGINNING AT A POINT LOCATED ALONG THE WESTERLY RIGHT-OF-WAY LINE OF OLD HICKORY BOULEVARD, SAID POINT BEING THE NORTHEASTERLY CORNER OF PROPERTY CONVEYED TO TIM SMITH AND WIFE MELINDA SMITH BY QUITCLAIM DEED OF RECORD IN INSTR. #20090709_0064014, R.O.D.C. AND BEING THE SOUTHEASTERLY CORNER OF THE HEREIN DESCRIBED TRACT,

THENCE, LEAVING SAID WESTERLY RIGHT-OF-WAY LINE WITH THE NORTHERLY LINE OF THE AFOREMENTIONED TIM SMITH PROPERTY N 84DEG 07MIN 00SEC W, 578.38 FEET TO AN IRON ROD (NEW WRLS 1638 CAP),

THENCE, LEAVING SAID NORTHERLY LINE OF SMITH WITH THE EASTERLY RIGHT-OF-WAY LINE OF OLD HOBSON PIKE N 0DEG 17MIN 24SEC E, 22.96 FEET TO AN IRON ROD (NEW WRLS 1638 CAP),

THENCE, CONTINUING N 7DEG 24MIN 48SEC E, 124.87 FEET TO AN IRON ROD (NEW WRLS 1638 CAP),

THENCE, CONTINUING WITH A CURVE TO THE RIGHT 114.25 FEET, SAID CURVE HAVING A CENTRAL ANGLE OF 32DEG 43MIN 49SEC, A RADIUS OF 200.00 FEET AND A CHORD OF N 23DEG 46MIN 43SEC E, 112.70 FEET TO AN IRON ROD (NEW WRLS 1638 CAP),

THENCE, CONTINUING N 40DEG 08MIN 37SEC E, 613.58 FEET TO AN IRON ROD (NEW WRLS 1638 CAP),

THENCE, CONTINUING N 54DEG 17MIN 51SEC E, 92.52 FEET TO AN IRON ROD (NEW WRLS 1638 CAP),

THENCE, CONTINUING N 69DEG 57MIN 46SEC E, 279.67 FEET TO AN IRON ROD (NEW WRLS 1638 CAP),

THENCE, CONTINUING N 54DEG 54MIN 31SEC E, 68.98 FEET TO AN IRON ROD (NEW WRLS 1638 CAP),

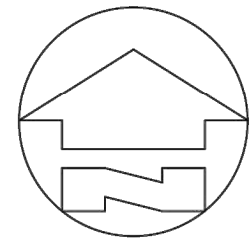
THENCE, LEAVING SAID RIGHT-OF-WAY LINE WITH THE WESTERLY RIGHT-OF-WAY LINE OF OLD HICKORY BOULEVARD S 6DEG 20MIN 45SEC W, 39.92 FEET TO AN EXISTING TDOT R/W MONUMENT,

THENCE, CONTINUING S 10DEG 50MIN 45SEC W, 385.19 FEET TO AN EXISTING TDOT R/W MONUMENT,

THENCE, CONTINUING WITH A CURVE TO THE RIGHT 588.26 FEET, SAID CURVE HAVING A CENTRAL ANGLE OF 18DEG 46MIN 26SEC, A RADIUS OF 1,795.30 FEET AND A CHORD OF S 20DEG 13MIN 59SEC W, 585.63 FEET TO AN EXISTING TDOT R/W MONUMENT AND THE POINT OF BEGINNING.

CONTAINING 453,998 SQUARE FEET OR 10.422 ACRES, MORE OR LESS.

SITE BENCHMARK
MAG SPIKE SET IN EXISTING
POWER/TELLIGHT/POLE #1380-6033
ELEVATION = 653.50 FEET (NAVD 88)



DATE: 09/30/2022

SURVEYOR'S DESCRIPTION

DAVIDSON COUNTY
PROPERTY MAP 175 PARCEL 20.00
0 OLD HICKORY BOULEVARD
ANTIOCH, TN 37013

BEING A TRACT OR PARCEL OF LAND LOCATED IN ANTIOCH, DAVIDSON COUNTY, TENNESSEE, SAID TRACT BEING A PORTION OF THE PROPERTY CONVEYED BY DEED IN INSTR. #20061127_0145612, R.O.D.C. AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS;

BEGINNING AT A POINT LOCATED ALONG THE SOUTHERLY RIGHT-OF-WAY LINE OF PRESERVE BOULEVARD, SAID POINT BEING AN INTERIOR CORNER OF THE DEDICATED OPEN SPACE AREA AS SHOWN ON THE FINAL PLAT OF THE PRESERVE AT OLD HICKORY, SECTION ONE, AS OF RECORD IN PLAT INSTR. #20060824 - 0104545, R.O.D.C. AND BEING THE NORTHWESTERLY CORNER OF THE HEREIN DESCRIBED TRACT,

THENCE, LEAVING SAID RIGHT-OF-WAY LINE WITH THE SOUTHERLY LINE OF THE AFOREMENTIONED DEDICATED OPEN SPACE AREA OF THE PRESERVE AT OLD HICKORY, SECTION ONE, S 72DEG 29MIN 27SEC E, 269.20 FEET TO AN IRON ROD (NEW WRLS 1638 CAP),

THENCE, LEAVING SAID SOUTHERLY OPEN SPACE LINE WITH THE WESTERLY RIGHT-OF-WAY LINE OF OLD HOBSON PIKE S 40DEG 08MIN 37SEC W, 401.49 FEET TO AN IRON ROD (NEW WRLS 1638 CAP),

THENCE, CONTINUING WITH A CURVE TO THE LEFT 137.10 FEET, SAID CURVE HAVING A CENTRAL ANGLE OF 32DEG 43MIN 49SEC, A RADIUS OF 240.00 FEET AND A CHORD OF S 23DEG 46MIN 43SEC W, 135.24 FEET TO AN IRON ROD (NEW WRLS 1638 CAP),

THENCE, LEAVING SAID WESTERLY RIGHT-OF-WAY LINE WITH THE EASTERLY LINE OF THE DEDICATED OPEN SPACE OF THE PRESERVE AT OLD HICKORY, SECTION ONE, N 70DEG 24MIN 48SEC E, 512.94 FEET TO AN IRON ROD (NEW WRLS 1638 CAP) AND THE POINT OF BEGINNING.

CONTAINING 56,917 SQUARE FEET OR 1.307 ACRES, MORE OR LESS.

GENERAL NOTES

- BEARING BASIS: GPS OBSERVATIONS / TDOT NETWORK / TN 4100 / NAD83.
- MAP REFERENCE (0 OLD HICKORY BLVD.): DAVIDSON CO. PROPERTY MAP 175, PARCEL 20.00.
- MAP REFERENCE (12782 OLD HICKORY BLVD.): DAVIDSON CO. PROPERTY MAP 175, PARCEL 33.00.
- PLAT REFERENCE: PLAN NOT OF RECORD.
- DEED REFERENCE (PARCEL 20.00): DEED INSTR. #20061127_0145612, R.O.D.C.
- PRIOR DEED REFERENCE (PARCEL 20.00): DEED BOOK 9610, PAGE 458, R.O.D.C.
- DEED REFERENCE (PARCEL 33.00): DEED INSTR. #20061127_0145612, R.O.D.C.
- PRIOR DEED REFERENCE (PARCEL 33.00): DEED BOOK 9610, PAGE 458, R.O.D.C.
- ADDRESS (PARCEL 20.00): 0 OLD HICKORY BOULEVARD
ANTIOCH, TN 37013
- ADDRESS (PARCEL 33.00): 12782 OLD HICKORY BOULEVARD
ANTIOCH, TN 37013
- AREA (PARCEL 20.00): PROPERTY CONTAINS 56,917 SQUARE FEET OR +/- 1.307 ACRES.
- AREA (PARCEL 33.00): PROPERTY CONTAINS 453,998 SQUARE FEET OR +/- 10.422 ACRES.
- AREA (PARCELS 20.00 & 33.00): TWO PARCELS CONTAIN 510,915 SQ. FEET OR +/- 11.729 ACRES.
- OLD HOBSON PIKE AREA: THE OLD ROADWAY CONTAINS 53,516 SQ. FEET OR +/- 1.229 ACRES.
- NO TITLE REPORT WAS FURNISHED TO THIS SURVEYOR.
- SURVEY AS SHOWN IS SUBJECT TO THE FINDINGS OF AN ACCURATE TITLE REPORT.
- PROPERTY CORNERS EXIST (O) / SET (N) AS SHOWN HEREON.
- DATUM REFERENCE: GPS OBSERVATIONS / TDOT NETWORK / NAVD 1988.
- UTILITIES: EXISTING VISIBLE UTILITY FEATURES WERE FIELD LOCATED AS A PART OF THIS SURVEY.
- OTHER SITE UTILITIES, ABOVE AND/OR BELOW GROUND, MAY EXIST OF WHICH THE SURVEYOR IS UNAWARE. CALL 811 BEFORE YOU DIG.
- FEMA: THE PROPERTY SHOWN HEREON IS NOT LOCATED WITHIN AREAS DESIGNATED AS 100 YEAR FLOOD ZONE PER F.E.M.A. MAP #470370011J DATED FEBRUARY 25, 2022 AND PER F.E.M.A. MAP #470370013J DATED FEBRUARY 25, 2022.

Hydrologic Soil Groups			
Symbol	Name and Description	Rating	Acres
BbD	Barfield-Rock outcrop complex, 5 to 20 percent slopes	D	7.0
TbC	Talbot silt loam, 2 to 10 percent slopes	C	0.9
TrC	Talbot-Rock outcrop complex, 5 to 15 percent slopes		5.0

NOTE: SOIL INFORMATION ON THIS SHEET WAS OBTAINED FROM NRC'S ONLINE SOIL SURVEY DATA. AREAS ARE APPROXIMATE AND ARE FOR REFERENCE ONLY.

Slopes Table				
Number	Minimum Slope	Maximum Slope	Area	Color
1	15.00%	20.00%	27216.21	[Color swatch]
2	25.00%	100.00%	3117.27	[Color swatch]



PIERCE DEVELOPMENT
1900 BELMONT BLVD.
NASHVILLE, TN 37212
615.691.3916

12782 OLD HICKORY BLVD.
RESIDENCES
0 & 12782 OLD HICKORY BLVD.
ANTIOCH, DAVIDSON COUNTY, TENNESSEE, 37013

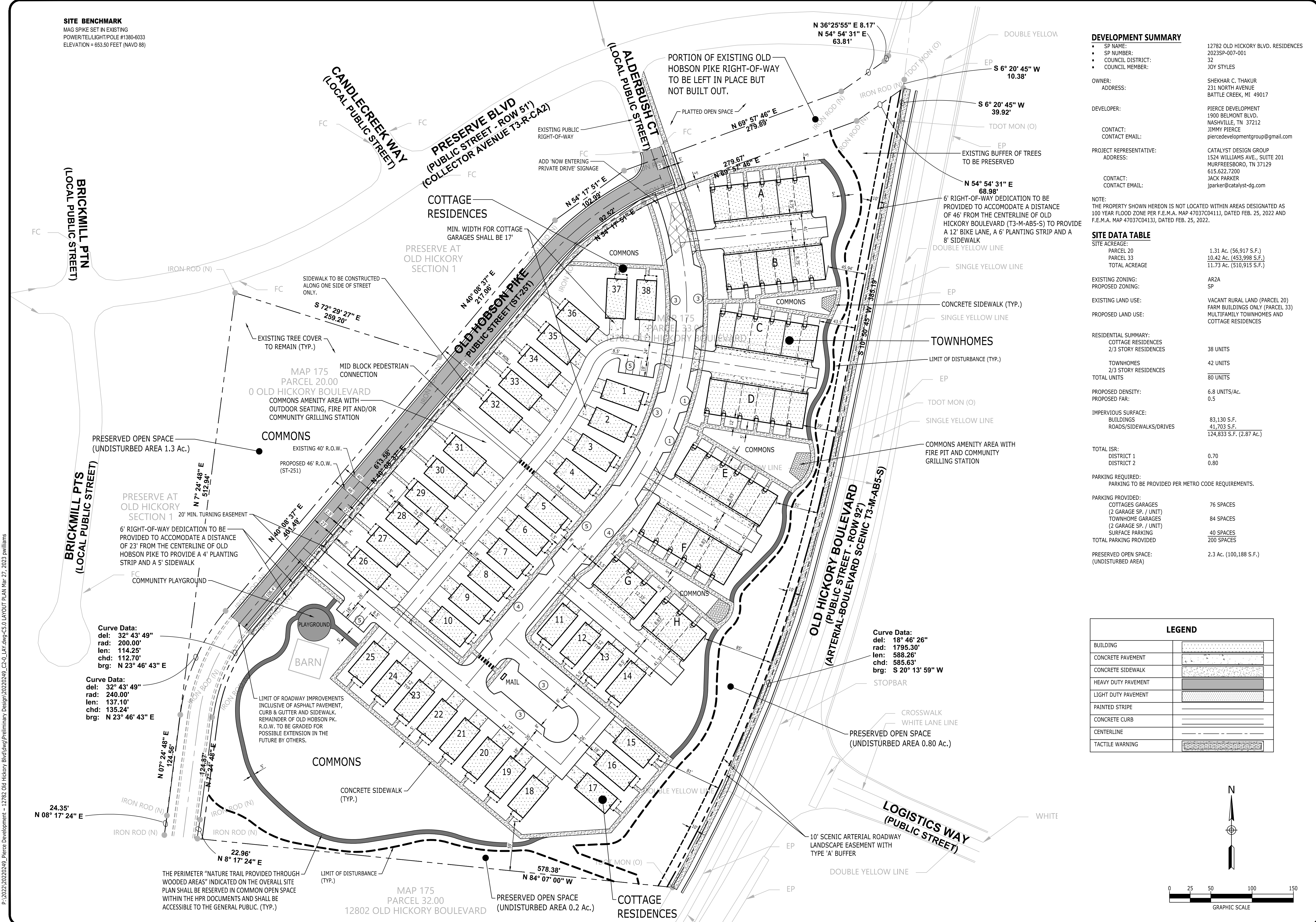
NO.	DATE	DESCRIPTION
2023-05-13		PRELIMINARY SP RESUBMITTAL
2023-02-28		PRELIMINARY SP RESUBMITTAL
2022-11-30		PRELIMINARY SP RESUBMITTAL

DRAWING TITLE
SITE SURVEY

PROJECT NUMBER
202202049

DRAWING NUMBER
C1.0

SITE BENCHMARK
 MAG SPIKE SET IN EXISTING
 POWER/TELEPHONE POLE #1380-6033
 ELEVATION = 683.50 FEET (NAVD 88)



DEVELOPMENT SUMMARY

SP NAME: 12782 OLD HICKORY BLVD. RESIDENCES
 SP NUMBER: 2023SP-007-001
 COUNCIL DISTRICT: 32
 COUNCIL MEMBER: JOY STYLES

OWNER: SHEKHAR C. THAKUR
 ADDRESS: 231 NORTH AVENUE, BATTLE CREEK, MI 49017

DEVELOPER: PIERCE DEVELOPMENT
 ADDRESS: 1900 BELMONT BLVD., NASHVILLE, TN 37212
 CONTACT: JIMMY PIERCE
 CONTACT EMAIL: piercedevdevelopmentgroup@gmail.com

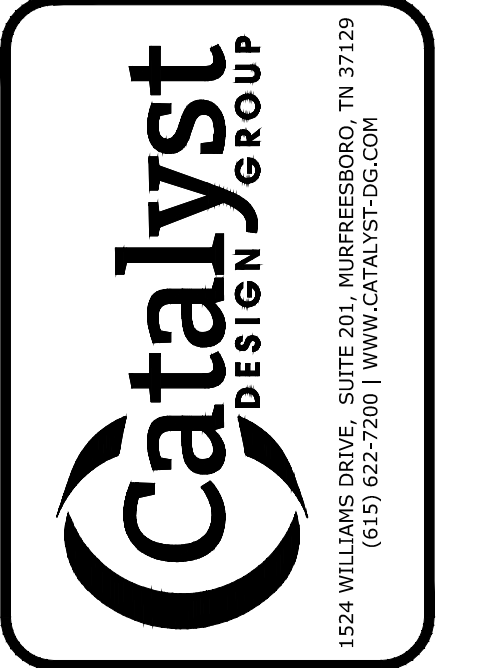
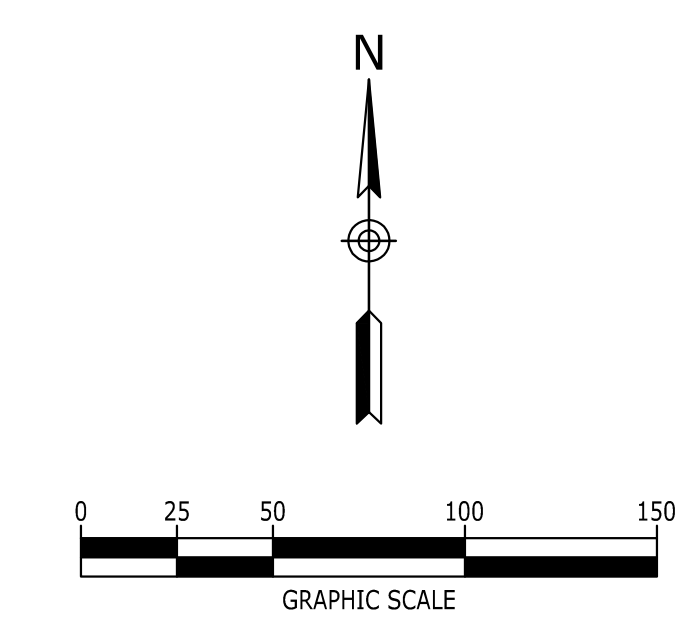
PROJECT REPRESENTATIVE: CATALYST DESIGN GROUP
 ADDRESS: 1524 WILLIAMS AVE., SUITE 201, MURFREESBORO, TN 37129
 CONTACT: JACK PARKER
 CONTACT EMAIL: jparker@catalyst-dg.com

SITE DATA TABLE

SITE ACREAGE:	
PARCEL 20	1.31 Ac. (56,917 S.F.)
PARCEL 33	10.42 Ac. (453,998 S.F.)
TOTAL ACREAGE	11.73 Ac. (510,915 S.F.)
EXISTING ZONING: AR2A	
PROPOSED ZONING: SP	
EXISTING LAND USE: VACANT RURAL LAND (PARCEL 20) FARM BUILDINGS ONLY (PARCEL 33)	
PROPOSED LAND USE: MULTIFAMILY TOWNHOMES AND COTTAGE RESIDENCES	
RESIDENTIAL SUMMARY:	
COTTAGE RESIDENCES	38 UNITS
TOWNHOMES	42 UNITS
TOTAL UNITS	80 UNITS
PROPOSED DENSITY: 6.8 UNITS/AC.	
PROPOSED FAR: 0.5	
IMPERVIOUS SURFACE:	
BUILDINGS	83,130 S.F.
ROADS/SIDEWALKS/DRIVES	41,703 S.F.
TOTAL	124,833 S.F. (2.87 AC.)
TOTAL ISR:	
DISTRICT 1	0.70
DISTRICT 2	0.80
PARKING REQUIRED: PARKING TO BE PROVIDED PER METRO CODE REQUIREMENTS.	
PARKING PROVIDED:	
COTTAGES GARAGES (2 GARAGE SP. / UNIT)	76 SPACES
TOWNHOME GARAGES (2 GARAGE SP. / UNIT)	84 SPACES
SURFACE PARKING	40 SPACES
TOTAL PARKING PROVIDED	200 SPACES
PRESERVED OPEN SPACE: (UNDISTURBED AREA)	
	2.3 AC. (100,188 S.F.)

LEGEND

BUILDING	[Symbol]
CONCRETE PAVEMENT	[Symbol]
CONCRETE SIDEWALK	[Symbol]
HEAVY DUTY PAVEMENT	[Symbol]
LIGHT DUTY PAVEMENT	[Symbol]
PAINTED STRIPE	[Symbol]
CONCRETE CURB	[Symbol]
CENTERLINE	[Symbol]
TACTILE WARNING	[Symbol]



PIERCE DEVELOPMENT
 1900 BELMONT BLVD.
 NASHVILLE, TN 37212
 615.691.3916

CATALYST DESIGN GROUP
 1524 WILLIAMS AVE., SUITE 201
 MURFREESBORO, TN 37129
 615.622.7200
 JACK PARKER
 jparker@catalyst-dg.com

CASE NO. 2023SP-007-001
 PRELIMINARY SP
12782 OLD HICKORY BLVD. RESIDENCES
 0 & 12782 OLD HICKORY BLVD.
 ANTIUCH, DAVIDSON COUNTY, TENNESSEE, 37013

NO.	DATE	DESCRIPTION
2023-03-13		PRELIMINARY SP RESUBMITTAL
2023-02-28		PRELIMINARY SP RESUBMITTAL
2022-11-30		PRELIMINARY SP RESUBMITTAL

DRAWING TITLE
OVERALL SITE PLAN

PROJECT NUMBER
 20220249

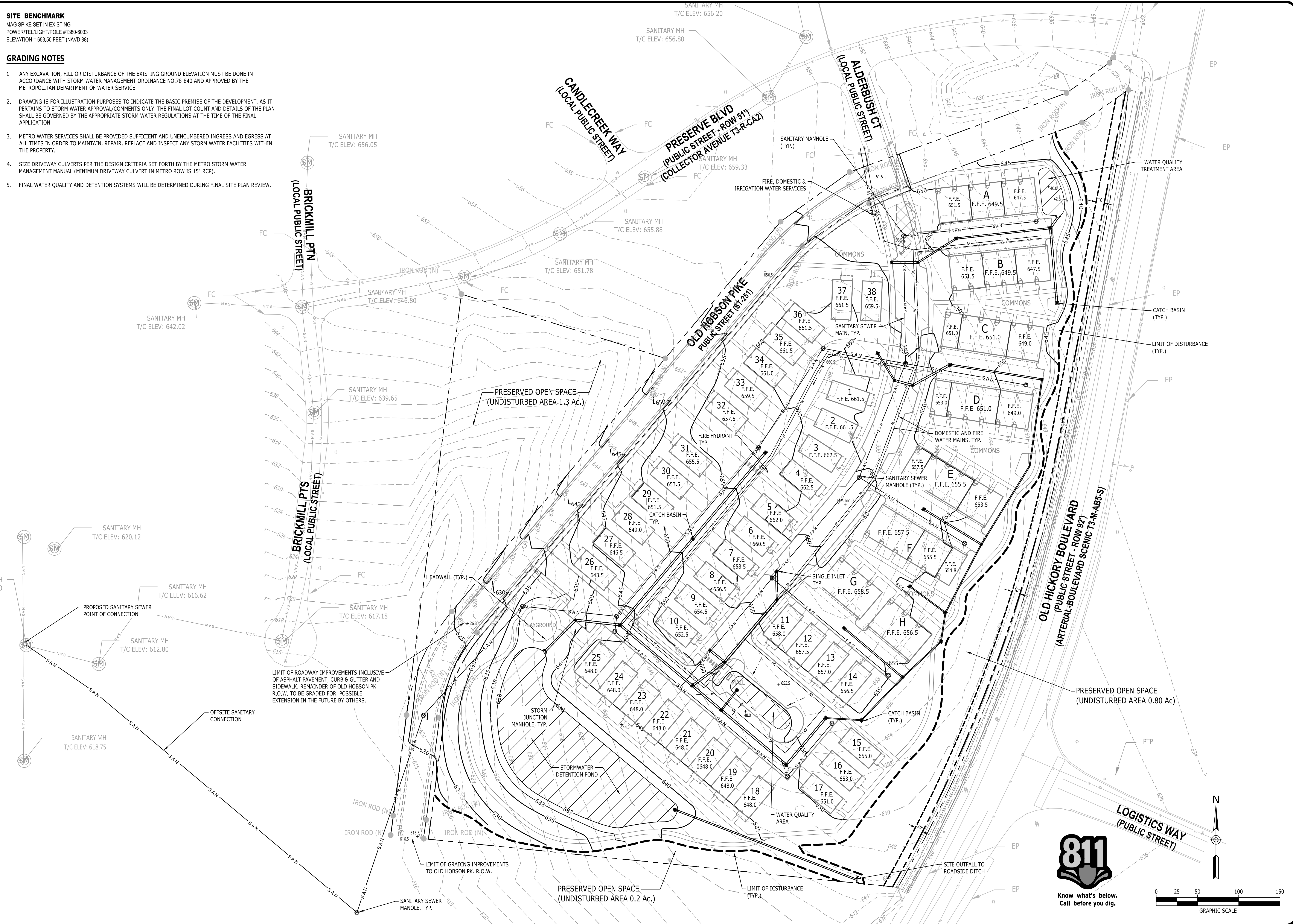
DRAWING NUMBER
C2.0

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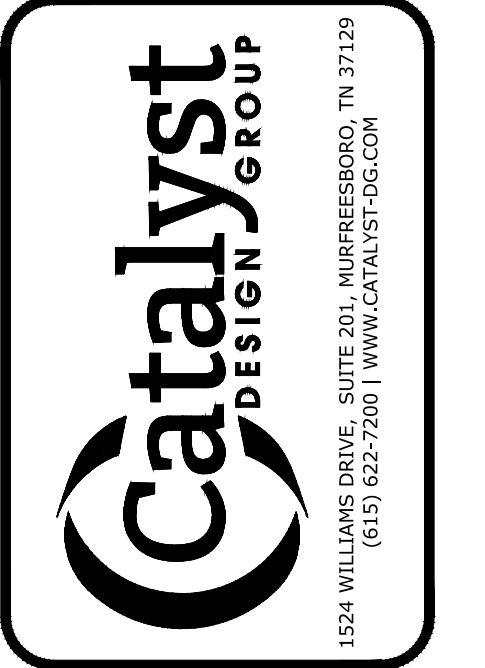
SITE BENCHMARK
 MAG SPIKE SET IN EXISTING
 POWER/TELEPHONE POLE #1380-6033
 ELEVATION = 653.50 FEET (NAVD 88)

GRADING NOTES

1. ANY EXCAVATION, FILL OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO.78-840 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICE.
2. DRAWING IS FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT, AS IT PERTAINS TO STORM WATER APPROVAL/COMMENTS ONLY. THE FINAL LOT COUNT AND DETAILS OF THE PLAN SHALL BE GOVERNED BY THE APPROPRIATE STORM WATER REGULATIONS AT THE TIME OF THE FINAL APPLICATION.
3. METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED INGRESS AND EGRESS AT ALL TIMES IN ORDER TO MAINTAIN, REPAIR, REPLACE AND INSPECT ANY STORM WATER FACILITIES WITHIN THE PROPERTY.
4. SIZE DRIVEWAY CULVERTS PER THE DESIGN CRITERIA SET FORTH BY THE METRO STORM WATER MANAGEMENT MANUAL (MINIMUM DRIVEWAY CULVERT IN METRO ROW IS 15" RCP).
5. FINAL WATER QUALITY AND DETENTION SYSTEMS WILL BE DETERMINED DURING FINAL SITE PLAN REVIEW.



P:\2022\202202049_Pierce Development - 12782 Old Hickory Blvd\dwg\Preliminary Design\202202049_C3-0_GRA.dwg; C6-0 GRADING & DRAINAGE PLAN Mar 13, 2023 pwilliams



PIERCE DEVELOPMENT
 1900 BELMONT BLVD.
 NASHVILLE, TN 37212
 615.691.3916

CASE NO. 2023SP-007-001
 PRELIMINARY SP

12782 OLD HICKORY BLVD.
RESIDENCES
 0 & 12782 OLD HICKORY BLVD.
 ANTIPOCH, DAVIDSON COUNTY, TENNESSEE, 37013

NO.	DATE	DESCRIPTION
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2022-11-30		PRELIMINARY SP RESUBMITTAL

DRAWING TITLE
OVERALL SITE GRADING & UTILITIES

PROJECT NUMBER
 202202049

DRAWING NUMBER
C3.0



Know what's below.
 Call before you dig.



SITE BENCHMARK
 MAG SPIKE SET IN EXISTING
 POWER/TELEPHONE POLE #1380-6033
 ELEVATION = 683.50 FEET (NAVD 88)

DEVELOPMENT SUMMARY

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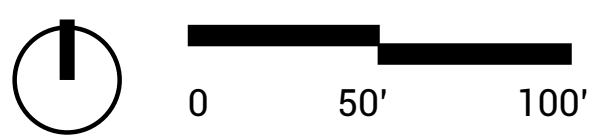
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 NASHVILLE, TN 37212
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 PROJECT REPRESENTATIVE: CATALYST DESIGN GROUP
 ADDRESS: 1524 WILLIAMS AVE., SUITE 201
 MURFREESBORO, TN 37129
 615.622.7200
 JACK PARKER
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 NOTE:
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 F.E.M.A. MAP 47037C04133, DATED FEB. 25, 2022.

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TOTAL PARKING PROVIDED	200 SPACES
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12782 OLD HICKORY BLVD | SP Site Plan

MARCH 2023





Site Specific Plan

12782 OLD HICKORY BLVD.

(Inclusive of parcels 17500003300 & 17500002000)

Case No. 2023SP-007-001

Prepared By:



Owner:

231 North Avenue
Battle Creek, MI 49017
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Developer:

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Revised: March 13, 2023
Revised: February 28, 2023
Submitted: November 30, 2022

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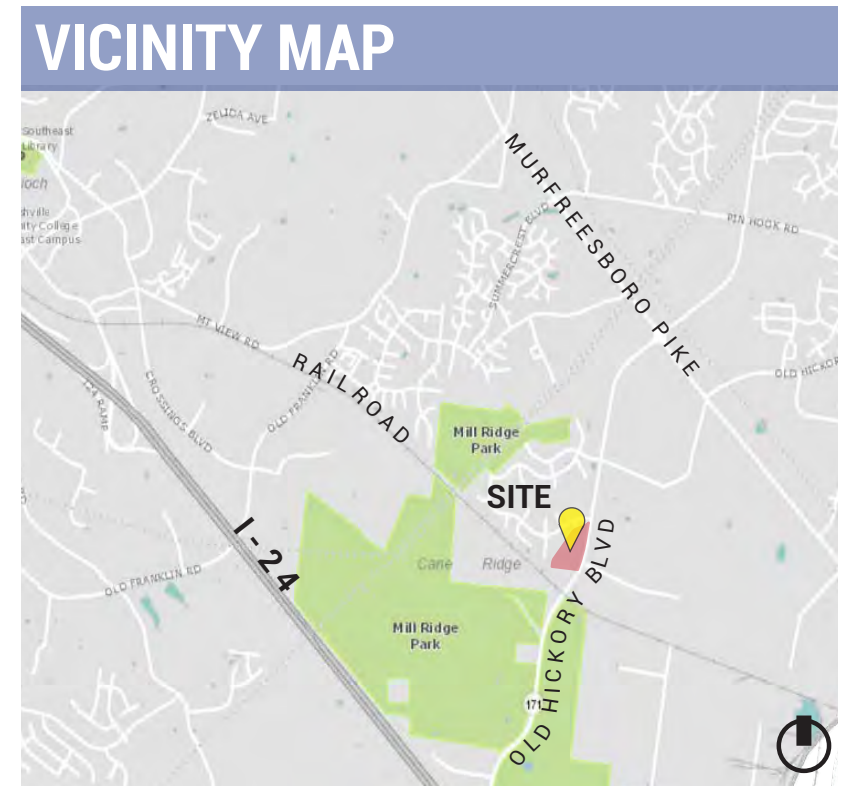
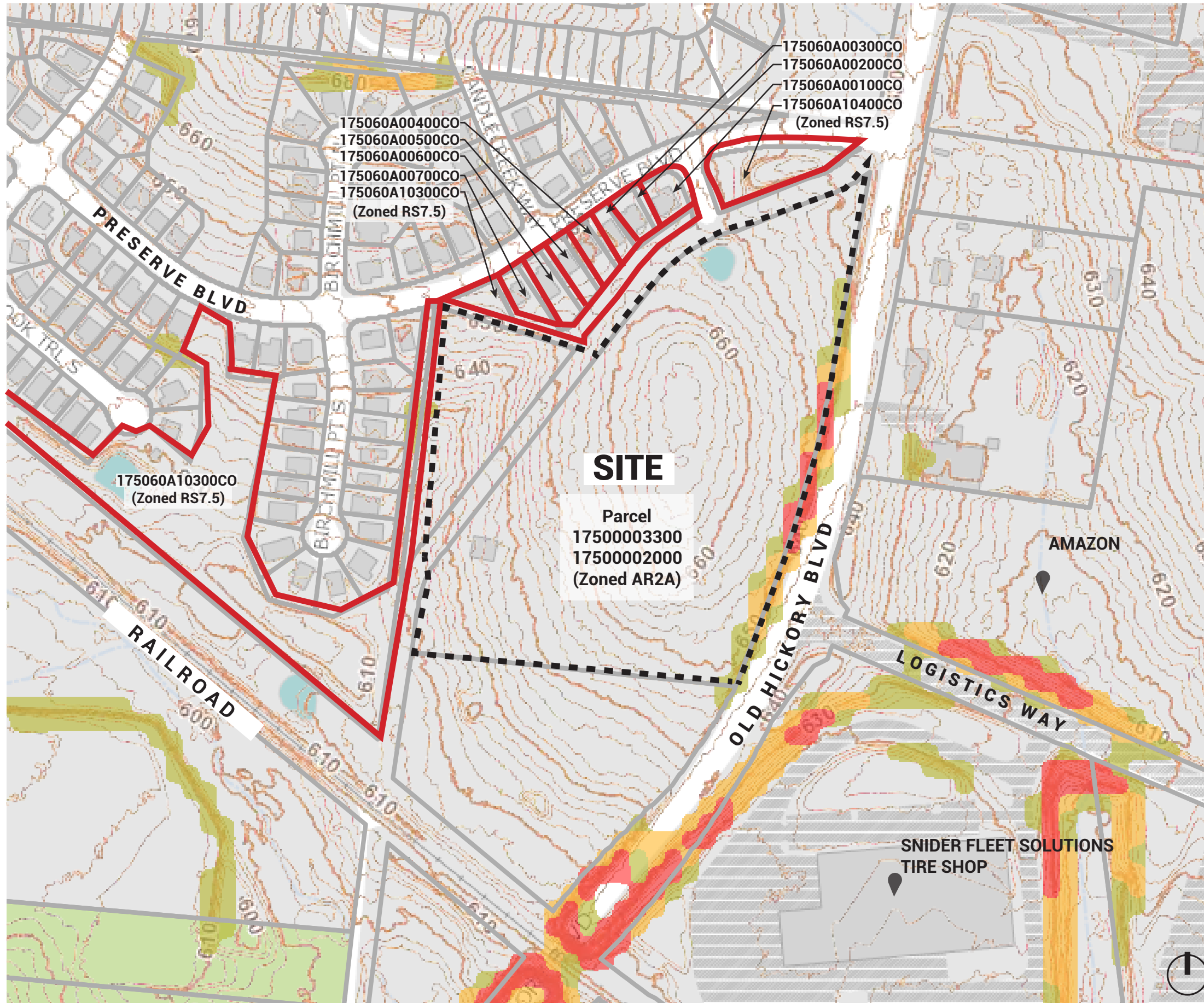
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SITE DATA

Acreage -	13.37 Ac.
Current Zoning -	AR2A

SLOPE LEGEND

- 15-20% Slopes
- 20-25% Slopes
- 25+% Slopes



LEGEND

Major and Collector Street Plan

- Arterial-Boulevard Scenic
- Arterial-Boulevard
- Collector-Avenue
- Planned Collector Avenue

Adopted CCM

- CO Conservation
- CI Civic
- OS Open Space
- T3 NM Suburban Neighborhood Maintenance
- T3 NE Suburban Neighborhood Evolving
- T3 CC Suburban Community Center
- T3 CM Suburban Mixed Use Corridor
- D IN District Industrial
- Water

T3 NM - SUBURBAN NEIGHBORHOOD MAINTENANCE

The 12782 Old Hickory Blvd. site lies within an AR2A base zone and has a CCM policy for Neighborhood Maint. We submitted this development plan for review by the PRC and the conclusion was that a base rezoning allowing these uses would not be supported under the current policy. However, it was noted that an SP rezoning application may qualify without a Community Plan Amendment. Therein, the SP application seeks to allow for varied housing types to be developed in accordance with the goal outlined in the T3 Neighborhood Maintenance policy.

The property has frontage along Old Hickory Blvd., a scenic arterial roadway; although, access to the site will be directed from an existing public street stub, Alderbush Ct., that was constructed with the initial phase of The Preserve at Old Hickory subdivision. A buffer of existing wooded vegetation will be preserved on parcel 20 along the rear of the existing residential lots along with preservation of a wooded buffer along Old Hickory Blvd. The proposed use serves as a transition between the industrial uses located to the east of Old Hickory and the existing residential community.

After consultation with NDOT, the intent is to maintain the Old Hobson Pike right-of-way bisecting the two parcels of land while building-out a portion of the roadway improvements in accordance with an ST-251 street standard. Any private development improvements have been kept outside of the limits of the remaining ROW not being improved in accordance with NDOT policy.

The Community Character policy for the parcel is T3 Neighborhood Maintenance. Within that policy designation, Design-based Zoning is an appropriate zoning and appropriate building types include low-rise and mid-rise townhomes along with single-family detached residences. It is our contention that this site appropriately serves as a transitional development.

LAND USE CONTEXT

T3 NM - SUBURBAN NEIGHBORHOOD MAINTENANCE

SITE BENCHMARK
 MAG SPIKE SET IN EXISTING
 POWER TELLIGHT POLE #1380-6033
 ELEVATION = 653.50 FEET (NAVD 88)



DEVELOPMENT SUMMARY

• SP NAME: 12782 OLD HICKORY BLVD. RESIDENCES
 • SP NUMBER: 2023SP-007-001
 • COUNCIL DISTRICT: 32
 • COUNCIL MEMBER: JOY STYLES
 OWNER: SHEKHAR C. THAKUR
 ADDRESS: 231 NORTH AVENUE, BATTLE CREEK, MI 49017
 DEVELOPER: PIERCE DEVELOPMENT
 ADDRESS: 1900 BELMONT BLVD., NASHVILLE, TN 37212
 CONTACT: JIMMY PIERCE
 CONTACT EMAIL: piercedevgroup@gmail.com
 PROJECT REPRESENTATIVE: CATALYST DESIGN GROUP
 ADDRESS: 1524 WILLIAMS AVE., SUITE 201, MURFREESBORO, TN 37129
 CONTACT: JACK PARKER
 CONTACT EMAIL: jparker@catalyst-dg.com

NOTE: THE PROPERTY SHOWN HEREON IS NOT LOCATED WITHIN AREAS DESIGNATED AS 100 YEAR FLOOD ZONE PER F.E.M.A. MAP 47037CD4111, DATED FEB. 25, 2022 AND F.E.M.A. MAP 47037CD4131, DATED FEB. 25, 2022.

SITE DATA TABLE

SITE ACREAGE:	1.31 Ac. (56,917 S.F.)
PARCEL 20	10.42 Ac. (453,998 S.F.)
PARCEL 33	11.73 Ac. (510,915 S.F.)
TOTAL ACREAGE	
EXISTING ZONING:	AR2A
PROPOSED ZONING:	SP
EXISTING LAND USE:	VACANT RURAL LAND (PARCEL 20) FARM BUILDINGS ONLY (PARCEL 33)
PROPOSED LAND USE:	MULTIFAMILY TOWNHOMES AND COTTAGE RESIDENCES
RESIDENTIAL SUMMARY:	
COTTAGE RESIDENCES	38 UNITS
2/3 STORY RESIDENCES	42 UNITS
TOWNHOMES	
2/3 STORY RESIDENCES	80 UNITS
TOTAL UNITS	
PROPOSED DENSITY:	6.8 UNITS/AC.
PROPOSED FAR:	0.5
IMPERVIOUS SURFACE:	
BUILDINGS	83,130 S.F.
ROADS/SIDEWALKS/DRIVES	41,703 S.F.
	124,833 S.F. (2.87 Ac.)
TOTAL ISR:	
DISTRICT 1	0.70
DISTRICT 2	0.80
PARKING REQUIRED:	PARKING TO BE PROVIDED PER METRO CODE REQUIREMENTS.
PARKING PROVIDED:	
COTTAGES GARAGES (2 GARAGE SP. / UNIT)	76 SPACES
TOWNHOME GARAGES (2 GARAGE SP. / UNIT)	84 SPACES
SURFACE PARKING	40 SPACES
TOTAL PARKING PROVIDED	200 SPACES
PRESERVED OPEN SPACE: (UNDISTURBED AREA)	2.3 Ac. (100,188 S.F.)
T3-M-AB5-S	
92' ROW	
8' SW	
6' PS	
12' BL	



SP SITE PLAN

DISTRICT 1



REFER TO PAGE 5



LAND USE: MULTIFAMILY

BULK REGULATIONS

"District 1" Uses	Cottage Residences (Multifamily)	
FAR/Density¹	38 Cottage Residences Proposed FAR 0.5	
ISR	0.70	
General Maximum Building Height	35 Feet To The Ridgeline Of The Pitched Roofs in 3 Stories Maximum	
Front Setback	Min. 12 Feet From the Curb Line of Any Public Street or Private Drive	
Side/Rear Setback	Min. of 10 feet Clearance Between the Sides Of Any Two Residences and 35 Feet Clearance Between The Rear Facades Of Residences	
Parking Requirements	Per Metro Code	
Glazing²	<i>Residential</i>	15% min. glazing on all stories of facades facing a public street
Raised Foundations³	<i>Residential</i>	18 in. Min to 36 in. Max
<p>1 The FAR shall be calculated utilizing the total site acreage, inclusive of any right of way dedication.</p> <p>2 Minimum glazing requirements shall be required on building facades facing public streets. Glazing calculations shall be measured from floor to floor.</p> <p>3 With the exception of accessible units, topographically challenged units may result in raised/lowered foundations at such locations exceeding the 36" max foundation height.</p> <p>4 Screening is required when raised foundations exceed 36" along public streets. Maintaining ADA access to units in order to meet fair housing requirements shall preempt this requirement.</p>		

ARCHITECTURAL STANDARDS

- The following design standards shall apply:
 - Building facades fronting a public street shall provide a minimum of one principal entrance (doorway) and a minimum of 15% glazing on all stories of facades facing a public street.
 - Windows shall be vertically oriented at a ratio of 1.5:1 or greater. Planning staff may allow modifications to this standard for dormers, decorative windows, clerestory windows, egress windows, storefronts, curtain walls and other special conditions.
 - Building facades shall be constructed of brick, brick veneer, stone, cast stone, cementitious siding, glass or materials substantially similar in form and function, unless otherwise approved on detailed building elevations included with the preliminary SP.
 - Porches shall provide a minimum of 5 feet of depth.
 - A raised foundation of 18"-36" is required for all residential structures fronting toward a public or private street fronting toward a public or private street.
- Refuse collection and recycling facilities shall be screened from views from the public way through the combined use of fences, walls and landscaping in accordance with Metro Code standards.

ACCESS & PARKING

- Surface parking shall be located towards the interior of the site or be screened by landscaping.
- Bicycle parking shall be provided per the Metro Zoning Code.
- All parking regulations to meet Metro Code.

LANDSCAPE STANDARDS

- Landscaping and tree density requirements per Metro Zoning Ordinance. A complete Landscape Plan will be submitted with the Final SP application.
- The existing tree cover on parcel 17500003200 (1.47 A.C.) shall be preserved to the fullest extent possible with the exception of site work necessary to build-out Old Hobson Pk. as a public street and any requirements to install infrastructure utilities or stormwater mitigation measures. A minimum undisturbed area of 2.3 acres shall be provided for the entirety of the SP.

DISTRICT 2



REFER TO PAGE 5



LAND USE: MULTIFAMILY

BULK REGULATIONS

"District 1" Uses	Townhomes (Multifamily)	
FAR/Density¹	42 Townhomes Proposed FAR 0.5	
ISR	0.80	
General Maximum Building Height	42 Feet To The Ridgeline Of The Pitched Roofs In 3 Stories Maximum	
Front Setback	Min. 35 Feet Clearance Between The Front Facades of Two Residences (N.I. porches)	
Side/Rear Setback	Min. of 10 feet Clearance Between the Sides Of Any Two Residences and 35 Feet Clearance Between The Rear Facades Of Residences	
Parking Requirements	Per Metro Code	
Glazing²	<i>Residential</i>	20% Min. on the first floor level and 15% min. glazing on all upper stories of facades facing a public street
Raised Foundations³	<i>Residential</i>	18 in. Min to 36 in. Max
<p>1 The FAR shall be calculated utilizing the total site acreage, inclusive of any right of way dedication.</p> <p>2 Minimum glazing requirements shall be required on building facades facing public streets. Glazing calculations shall be measured from floor to floor.</p> <p>3 With the exception of accessible units, topographically challenged units may result in raised/lowered foundations at such locations exceeding the 36" max foundation height.</p> <p>4 Screening is required when raised foundations exceed 36" along public streets. Maintaining ADA access to units in order to meet fair housing requirements shall preempt this requirement.</p>		

ARCHITECTURAL STANDARDS

- Residential structures shall be oriented to a private street where possible. When not possible, residential structures may be oriented towards internal open space common areas.
- Buildings shall provide a functional entry onto the street/ sidewalk network or other common space to promote activity at the street level/ amenity area. Residential units fronting a public street or green space shall provide a connection/ entrance to the public or private sidewalk from the porch or stoop. Corner units must include primary pedestrian entrances and facade requirements along both public street frontages.
- The following design standards shall apply:
 - Building facades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 15% glazing.
 - Windows shall be vertically oriented at a ratio of 1.5:1 or greater. Planning staff may allow modifications to this standard for dormers, decorative windows, clerestory windows, egress windows, storefronts, curtain walls and other special conditions.
 - Building facades shall be constructed of brick, brick veneer, stone, cast stone, cementitious siding, glass or materials substantially similar in form and function, unless otherwise approved on detailed building elevations included with the preliminary SP.
 - Porches shall provide a minimum of 5 feet of depth.
 - A raised foundation of 18"-36" is required for all residential structures.
- Refuse collection and recycling facilities shall be screened from views from the public way through the combined use of fences, walls and landscaping in accordance with Metro Code standards.
- Townhome units shall accommodate a minimum of a two-car internal garage.
- Maximum height allowance is based upon pitched roofs.

ACCESS & PARKING

- Surface parking shall be located towards the interior of the site or be screened by landscaping.
- All parking regulations to meet Metro Code.
- Bicycle parking shall be provided per the Metro Zoning Code.

LANDSCAPE STANDARDS

- Landscaping and tree density requirements per Metro Zoning Ordinance. A complete Landscape Plan will be submitted with the Final SP application.
- The existing tree cover on parcel 17500003200 (1.47 A.C.) shall be preserved to the fullest extent possible with the exception of site work necessary to build-out Old Hobson Pk. as a public street and any requirements to install infrastructure utilities or stormwater mitigation measures. A minimum undisturbed area of 2.3 acres shall be provided for the entirety of the SP.

ADDITIONAL REGULATIONS + NOTES

Specific Plan Notes:

1. The purpose of this SP is to permit up to 80 multifamily residential units.
2. For any development standards, regulations and requirements not specifically shown on the SP plan and/or included as a condition of Council approval, the property shall be subject to the standards, regulations, and requirements of the RM-9 base zoning designations as of the date of the application request or application.
3. STRP uses, both owner occupied and non-owner occupied, shall be prohibited.
4. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering, or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by the Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance.

FEMA Note:

5. This property does not lie in an area designated as an area of minimal flood hazard according to Federal Emergency Management Agency Flood Insurance Rate Map Panel Number 47037C0411J and 47037C0413JH, effective Feb. 25, 2022.

NDOT Notes:

6. The final site plan/building permit shall depict the required public sidewalks, any required grass strip or frontage zone, and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of Use and Occupancy Permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Where feasible, vertical obstructions are only permitted within the required

grass strip or frontage zone.

7. Roadway Improvements that are a direct result of this specific project or as determined by an approved Traffic Impact Study and the Department of Public Works shall be constructed.
8. Any required right-of-way within the project site that is identified as necessary to meet the adopted Major and Collector Street Plan (MCSP).
9. The developer's final construction drawings shall comply with the design regulations established by the Nashville Department of Transportation (NDOT), in effect at the time of the approval of the preliminary development plan or final development plan or building permit, as applicable. Final design may vary based on field conditions.
10. All construction within the right of way shall comply with ADA and NDOT Standards and Specifications.
11. There shall be no vertical obstructions (signs, power poles, fire hydrants, etc.) within the proposed sidewalks. Where feasible, vertical obstructions shall be relocated out of the proposed sidewalks, where applicable.
12. If sidewalks are required then they should be shown on the plans per MCSP and NDOT standards and specs.
13. Submit copy of ROW dedications prior to bldg. permit sign off.
14. Development is to have a dumpster and recycling container(s), and/or curbside pick-up serviced by a private hauler.
15. ADA Curb Ramps shall be provided at the intersection of the new roadway with Massman Drive and at the Intersection of the Frontage Road with Massman Drive.

Fire Marshal Notes:

16. No part of any building shall be more than 500ft. from a fire hydrant via a hard surface road. Metro Ordinance 095-1541 Sec. 1568.020 B
17. All fire department access roads shall be 20 feet minimum width and shall have an unobstructed vertical clearance of 13.5 feet.
18. All dead-end roads over 150 ft. in length require a turnaround, this includes temporary turnarounds.
19. A fire hydrant shall be provided within 100 ft. of any

fire department connection, if applicable.

20. Fire hydrants shall be in-service before any combustible material is brought on site.
21. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.

NES Notes:

22. Where feasible, this development will be served with underground power and pad-mounted transformers.
23. New facilities will not be allowed to sit in or to pass through retention areas, including rain gardens, bioretention areas, bioswales, and the like. This includes primary duct between pad-mounted transformers equipment, as well as service duct to a meter.

Stormwater Notes:

24. Any excavation, fill, or disturbance of the existing ground elevation must be done in accordance with Stormwater Management Ordinance No. 78-840 and approved by the Metropolitan Department of Water Services.
25. Metro Water Services shall be provided sufficient and unencumbered ingress and egress at all times in order to maintain, repair, replace, and inspect any stormwater facilities within the property.
26. Drawing is for illustration purposes to indicate the basic premise of the development, as it pertains to Stormwater approval/comments only. The final lot count and details of the plan shall be governed by the appropriate stormwater regulations at the time of final application.
27. Size driveway culverts per the design criteria set forth by the Metro Stormwater Management Manual. (Minimum driveway culvert in Metro ROW is 15" RCP.)
28. Project intent is to be redeveloped per the requirements of Volume 5 (LID) of the Stormwater Manual. Detention will be provided or post developed runoff will be less than predeveloped runoff due to LID implementation

Federal Compliance:

29. All development within the boundaries of this plan will meet the requirements of the Americans with Disabilities Act and the Fair Housing Act.

Development Notes:

30. Overall building height shall be measured per Metro Code. Per Metro Code, a maximum of 35' height to the ridge line of the roof of the Cottage Residences and a maximum of 42' height to the ridge line of the roof of the Townhome Residences shall be used.
31. The final site plan/ building permit site plan shall depict the required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.

VISIONING IMAGERY - COTTAGE RESIDENCES



VISIONING IMAGERY - TOWNHOMES

