

# ***Cothern Property***

## **A QUALITY RESIDENTIAL DEVELOPMENT SPECIFIC PLAN (SP) DISTRICT**

**SP No. 2021SP-080-001**

Date: October 5, 2021  
Revised: October 25, 2021  
Revised: November 23, 2021  
Revised: December 2, 2021  
Revised: March 7, 2022  
Revised: September 26, 2022

Owner:  
**Don Cameron III**

Developer:  
**Cameron Properties**

Prepared By:



**Anderson, Delk, Epps & Associates Inc.**  
ENGINEERING / PLANNING / SURVEYING  
618 Grassmere Park Drive - Suite 4  
Nashville, Tennessee 37211

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## COTHERN PROPERTY SPECIFIC PLAN ZONING DISTRICT

**Application #:** 2021SP-080-001

**Council District:** 31



**Council Member:**



Mr. John Rutherford

**Parcel ID #:** Map 173, Parcel 36.00

**Submittal Date:** October 5, 2021

**Owner:** Don Cameron, III  
1503 Columbia Avenue  
Franklin, Tennessee 37064  
Instrument Number 20051121-0140598, R.O.D.C.

**Developer:** Cameron Properties  
1503 Columbia Avenue  
Franklin, Tennessee 37064  
Contact: Don Cameron  
(615) 791-0093

**Submitted by:** Anderson, Delk, Epps & Associates, Inc.  
618 Grassmere Park Drive, Suite 4  
Nashville, Tennessee 37211

## **PURPOSE AND INTENT OF THE PROPOSED SP DISTRICT:**

The purpose of the proposed Specific Plan (SP) District is to allow for the development of a 283 lot residential neighborhood. At the Pre-Application Conference with Metro Planning, the staff suggested that the Developer use the SP District due to the site constraints, current land use policies, and the local neighborhood input. The site constraints consist of the size, shape, and location of the property, sensitive environmental features such as streams and steep slopes, minimum building setbacks, roadway right-of-ways, etc. Due to all of the existing restrictions on the property, the SP District provides the necessary flexibility to develop the tract of land. The Developer believes the SP District will allow the property to be developed in a way that will meet the neighborhood's concerns, market demands, and be suitable to the site and its surroundings.

## **PROPOSED PLANS CONSISTENCY WITH THE GOALS/OBJECTIVES OF THE GENERAL PLAN:**

The Current Land Use Policy is a combination of "T3 Neighborhood Evolving" (T3-NE) and "Conservation" (CO).

The predominant Land Use Policy across the site is T3-NE with the CO Land Use Policy being scattered throughout the property around the sensitive environmental features. In *The Community Character Manual, Volume III: Community Plans 2015, as amended in 2017*, T3-NE areas are described as "undeveloped, underdeveloped, or suitable for substantial infill and redevelopment and are anticipated to be developed in suburban residential patterns, but at higher densities and with greater housing variety than classic suburban neighborhoods". They are characterized by moderate-density development patterns, lots accessed generally by streets but may have alley access, are developed with creative thinking in environmentally sensitive areas to balance the increased growth and density with its impact on the area, and incorporate nature into the design while still allowing the buildings to serve a more prominent street-framing role. Compact site design and building massing, used in areas of the plan, are design features guided by Conservation policy to protect environmentally sensitive areas.

The plan meets the goals of the T3 Suburban (Suburban Neighborhood Transect Category) as set forth in *The Community Character Manual, Volume III: Community Plans 2015, as amended in 2017*.



**PROPOSED SP DEVELOPMENT FOR T3 NEIGHBORHOOD EVOLVING  
TRANSECT CATEGORY WOULD HAVE THE FOLLOWING  
CHARACTERISTICS:**

1. Moderate-density development. All homes in the development will be for sale.
2. Diverse mixture of housing consisting of attached and detached single family housing types.
3. Moderate setbacks and spacing.
4. Homes will be accessed by public roads, private drives, or public alleys.
5. Designed spaces with formal and informal landscaping.
6. Open Space is incorporated throughout the development for easy access and use by the residents.
7. Intentionally planned with regards to environmentally sensitive building and site development techniques to balance the increased growth and density with its impact on area streams and rivers.
8. Development strategy focuses on preserving mature trees, vegetation, grasses, native top soil, treed slopes, and pockets of native cane.
9. Lot configuration in development areas is grouped to minimize the disturbance of the sensitive environmental features and areas of land with steep slopes.
10. The Open Space provided conserves existing environmentally sensitive areas and minimize the impacts of the development, allowing for a greater number of existing trees to remain undisturbed.
11. Buffer/conservation areas shall be placed along the drains, streams, pond, and wetlands on the site to minimize impacts on area streams and rivers. These areas will remain undisturbed and their natural conditions will be preserved.
12. During the development process, any invasive species encountered will be removed.
13. Non-invasive plants as identified by the Tennessee Invasive Plants Council will be used in the development. Native Tennessee plants will be planted wherever possible.

## **LIST OF PROPOSED ALLOWABLE USES:**

The uses within the proposed SP District will be attached and detached Single Family Residences. All homes in the development will be for sale.

### **EXAMPLES OF ATTACHED SINGLE-FAMILY HOMES**



### **EXAMPLES OF DETACHED SINGLE-FAMILY HOMES**



## EXISTING CONDITIONS ON THE SITE:

The site currently has a single residence which uses the property for farming. It is bounded on the north by a parcel zoned RM15, three parcels zoned AR2A, and two parcels zoned SP, one of which consists of the Forest View multi-family development; the south by Old Hickory Boulevard and a parcel zoned AR2A; the west by an unnamed blue line drain to the Whittemore Branch of Mill Creek and several parcels zoned AR2A and one parcel zoned RM15; and to the east by open space of the Forest View SP and the Oak Highlands Residential PUD subdivision. The site consists of primarily grass pastures with wooded areas comprised of small to large trees around the blue line drains on the property. Existing blue line drains run from south to north along the western portion of the property and from south to north in the middle portion of the property, both draining to the Whittemore Branch of Mill Creek. There is a wetland to the east of this blue line drain in the middle portion of the property and another portion of wetlands in the northeast corner of the property. Another blue line drain runs west to east on the eastern portion of the property to an unnamed drain to Mill Creek. There is a farm pond on the western portion of the property. Steep slopes exist on the property around these drains and pond, adding to the sensitive environmental features on the property.

The site is currently zoned AR2A Agricultural.

The existing Land Use Policy is a combination of T3-NE and CO. The predominant Land Use Policy of the site is T3-NE with areas of CO being located around the sensitive environmental features on the property. The T3 Suburban Transect Category is the bridge between rural and urban transect areas with the purpose of thoughtfully transitioning from the least dense natural and rural environment to the denser urban environment. The proposed development would be classified in the T3 Suburban Neighborhood Evolving Transect Category.

The Proposed SP District would permit attached and detached Single-Family Residences. All homes in the development will be for sale.

## BULK PROVISIONS:

**Site Area:** 80.07± Acres

**Number of Lots Proposed (All homes will be for sale):**

Single-Family House – Lot Type “A”	93
Single-Family House – Lot Type “B”	33
Single-Family House – Lot Type “C”	18
Single-Family Detached Townhouse – Lot Type “D”	21
Single-Family Attached Front-Loaded Townhouse – Lot Type “T <sub>F</sub> ”	83
Single Family Attached Rear-Loaded Townhouse – Lot Type “T <sub>R</sub> ”	35
Total Number of Lots Proposed	283

**Proposed Density:** 3.53 Units/Acre

**Maximum Building Height:** 3 Stories

**Open Space** (Approximate Areas):

Usable	30.6 ± Ac.
Stream Buffers	7.5 ± Ac.
Landscape Easements	0.5 ± Ac.
Estimated Detention / Water Quality Areas	4.7 ± Ac.
Total	43.3 ± Ac. (54.1% of Total Site)



## BUILDING AND LOT STANDARDS – FRONT LOADED DETACHED HOME (TYPE “A”):

Type “A” is a single-family residential building that occupies the center of its lot with setbacks on all sides. Vehicular access is via a front driveway with a primary pedestrian entrance located along the public road frontage.

### **Bulk Standards:**

**Minimum Lot Area:** 5,200 square feet

**Typical Lot Size:** 50 ± ft. x 110 ± ft.

**Maximum Building Height:** 3 Stories

**Parking Requirements:** 2 On-site Spaces per Home

**Parking Proposed:** 4 Spaces per Home  
(2 Garage and 2 Driveway Spaces per Home)

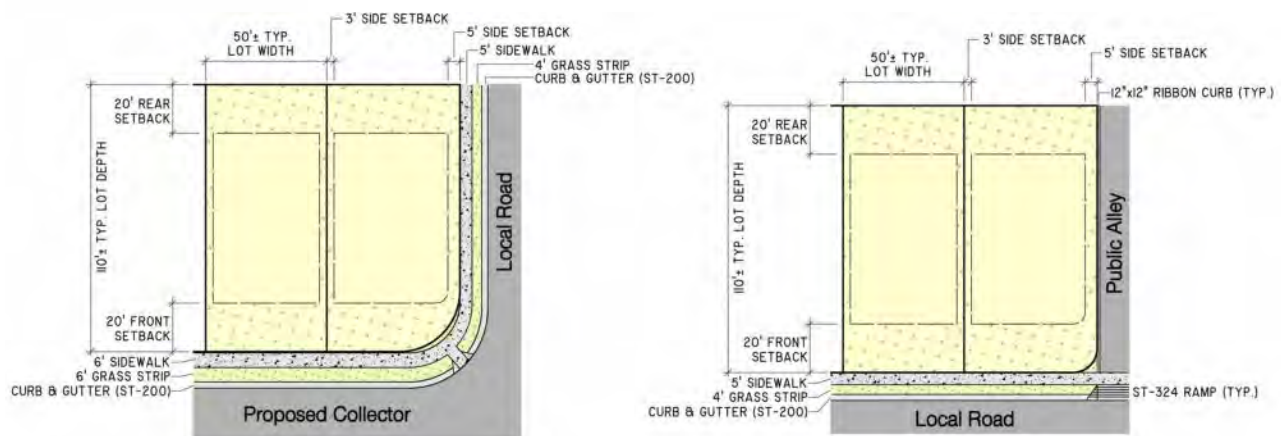
### **Building Setbacks :**

Front Setback: 20 ft.

Rear Setback: 20 ft.

Side Setback: 3 ft. Lot / Open Space  
5 ft. Adjoining Right-of-Way

**All homes will be for sale**



***TYPICAL BUILDING SETBACK DETAILS***  
(NOT TO SCALE)



## BUILDING AND LOT STANDARDS – FRONT AND REAR LOADED DETACHED HOMES (TYPE “B”):

Type “B” is a single-family residential building that occupies its lot with setbacks on all sides. The front loaded lots have vehicular access via a front drive with a primary pedestrian entrance located along the public road frontage. The rear loaded lots have vehicular access via a rear public alley with a primary pedestrian entrance located along the sidewalk in the open space the lot fronts.

### **Bulk Standards:**

**Minimum Lot Area:** 3,400 square feet

**Typical Lot Size:** 32 ± ft. x 110 ± ft.

**Maximum Building Height:** 3 Stories

**Parking Requirements:** 2 On-site Spaces per Home

**Parking Proposed:** 3 Spaces per Home  
(1 Garage and 2 Driveway Spaces per Home)

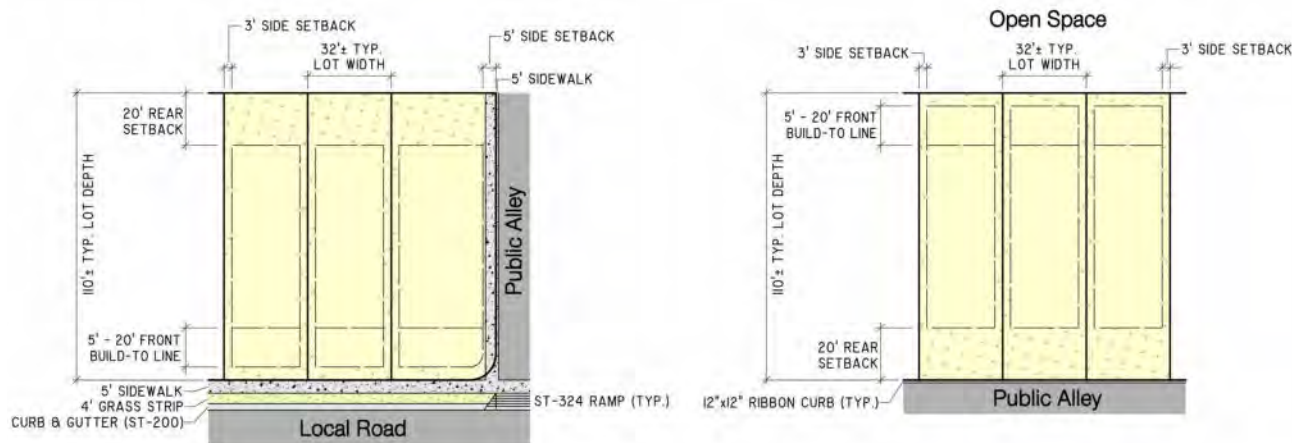
### **Building Setbacks:**

Front Setback: 5 ft. - 20 ft. Build-to Line (From ROW or Open Space)

Rear Setback: 20 ft.

Side Setback: 3 ft. Lot / Open Space  
5 ft. Adjoining Right-of-Way

**All homes will be for sale**



***TYPICAL BUILDING SETBACK DETAILS  
(NOT TO SCALE)***

## BUILDING AND LOT STANDARDS – DETACHED REAR ALLEY LOADED HOMES (TYPE “C”):

Type “C” is a single-family residential building that occupies its lot with setbacks on all sides. Vehicular access is via a public alley along the rear of the lot. The primary pedestrian entrance is either along the public road frontage if it fronts a public road or along a sidewalk in open space if the lot fronts open space.

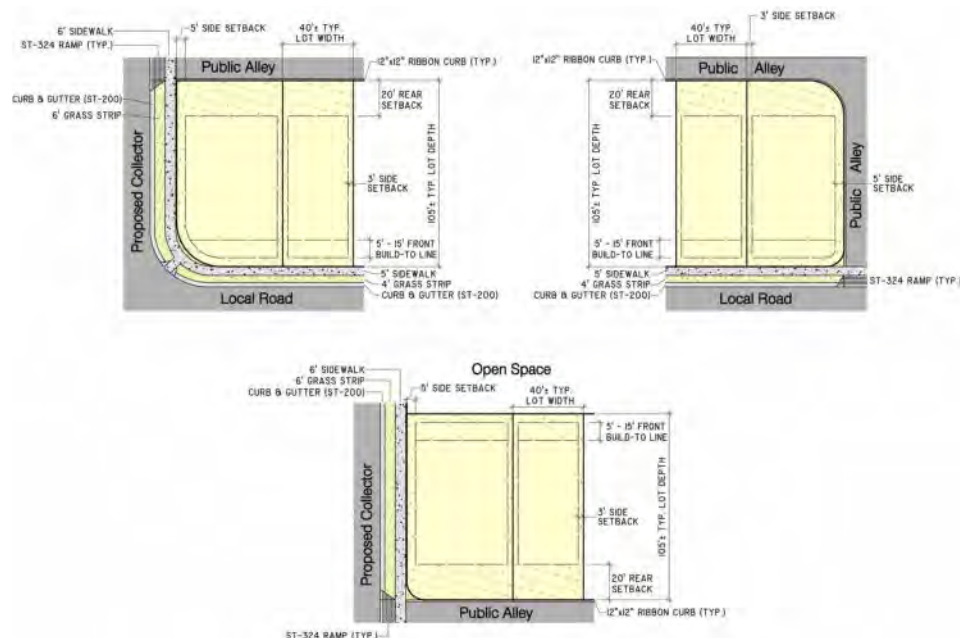
### **Bulk Standards:**

<b>Minimum Lot Area:</b>	4,200 square feet
<b>Typical Lot Size:</b>	40 ± ft. x 105 ± ft.
<b>Maximum Building Height:</b>	3 Stories
<b>Parking Requirements:</b>	2 On-site Spaces per Home
<b>Parking Proposed:</b>	3 Spaces per Home (1 Garage and 2 Driveway Spaces per Home)

### **Building Setbacks :**

Front Setback:	5 ft. - 15 ft. Build-to Line
Rear Setback:	20 ft.
Side Setback:	3 ft. Lot / Open Space 5 ft. Adjoining Right-of-Way

**All homes will be for sale**



***TYPICAL BUILDING SETBACK DETAILS***  
***(NOT TO SCALE)***

## BUILDING AND LOT STANDARDS – REAR LOADED DETACHED TOWNHOMES (TYPE “D”):

Type “D” is a single-family residential detached townhome that occupies its lot with setbacks on all sides. Vehicular access is via a public alley along the rear of the lot. The primary pedestrian entrance is either along the public road frontage if it fronts a public road or along a sidewalk in open space if the lot fronts open space.

### **Bulk Standards:**

**Minimum Lot Area:** 3,100 square feet

**Typical Lot Size:** 30 ± ft. x 105 ± ft.

**Parking Requirements:** 2 On-site Spaces per Townhome

**Parking Proposed:** 4 Spaces per Townhome  
(2 Garage and 2 Driveway Spaces per Townhome)

**Maximum Building Height:** 3 Stories

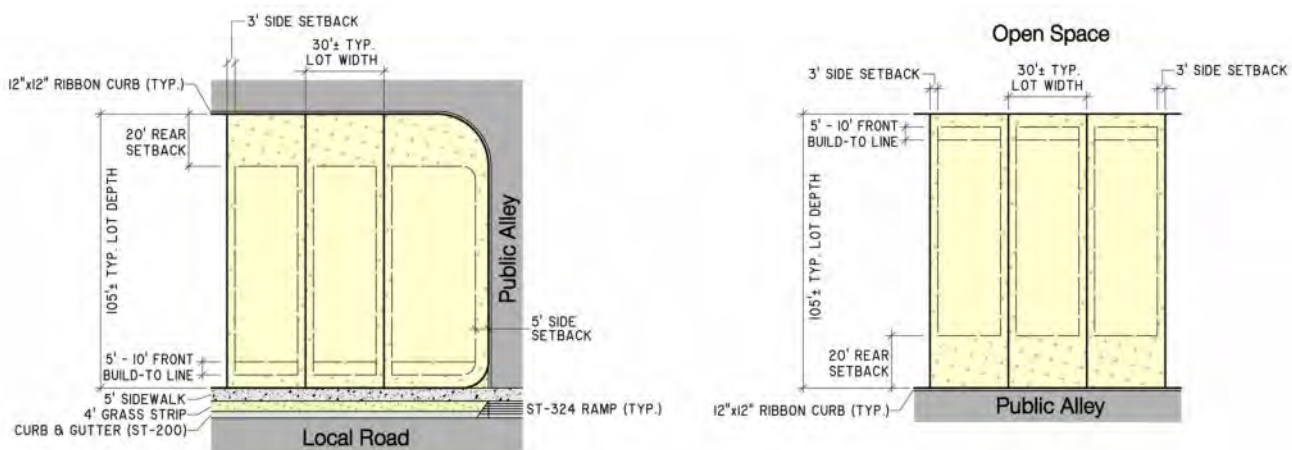
### **Building Setbacks :**

Front Setback: 5 ft. - 10 ft. Build-to Line

Rear Setback: 20 ft.

Side Setback: 3 ft. Lot / Open Space  
5 ft. Adjoining Right-of-Way

**All homes will be for sale**



***TYPICAL BUILDING SETBACK DETAILS***  
(NOT TO SCALE)



## BUILDING AND LOT STANDARDS – FRONT LOADED ATTACHED TOWNHOMES (TYPE “T<sub>F</sub>”):

Type “T<sub>F</sub>” is a single-family residential townhomes attached with adjoining walls along lot lines. The building as a whole has setbacks in the front and sides. Vehicular access via a front drive with primary pedestrian entrances from the sidewalks along the open spaces on the other side of the private drives.

### **Bulk Standards:**

**Minimum Lot Area:** 2,000 square feet

**Typical Lot Size:** Width Varies x 80 ± ft.

**Parking Requirements:** 2 On-site Spaces per Townhome (2-Bedroom)  
(Attached Townhomes parking should be per code for multifamily)

**Parking Proposed:** 3 or 4 Spaces per Townhome  
(1 or 2 Garage and 2 Driveway Spaces per Townhome)

**Maximum Building Height:** 3 Stories

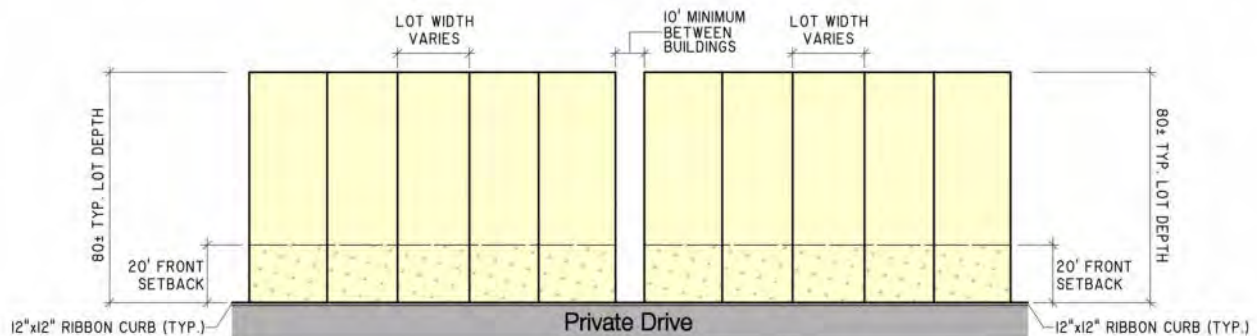
### **Building Setbacks :**

Front Setback: 20 ft. from Right-of-Way

Rear Setback: 0 ft.

Side Setback: 10 ft. Minimum between Buildings

**All homes will be for sale**



***TYPICAL BUILDING SETBACK DETAILS***  
(NOT TO SCALE)

## BUILDING AND LOT STANDARDS – REAR LOADED ATTACHED TOWNHOMES (TYPE “T<sub>R</sub>”):

Type “T<sub>R</sub>” is a single-family residential townhomes attached with adjoining walls along lot lines. The building as a whole has setbacks in the rear and the sides with a build-to line in the front. Vehicular access is via a rear private alley with the primary pedestrian entrances from the sidewalks along the open space fronting the lots.

### **Bulk Standards:**

**Minimum Lot Area:** 2,000 square feet

**Typical Lot Size:** Width Varies x 80 ± ft.

**Parking Requirements:** 2 On-site Spaces per Townhome

**Parking Proposed:** 3 or 4 Spaces per Townhome  
(1 or 2 Garage and 2 Driveway Spaces per Townhome)

**Maximum Building Height:** 3 Stories

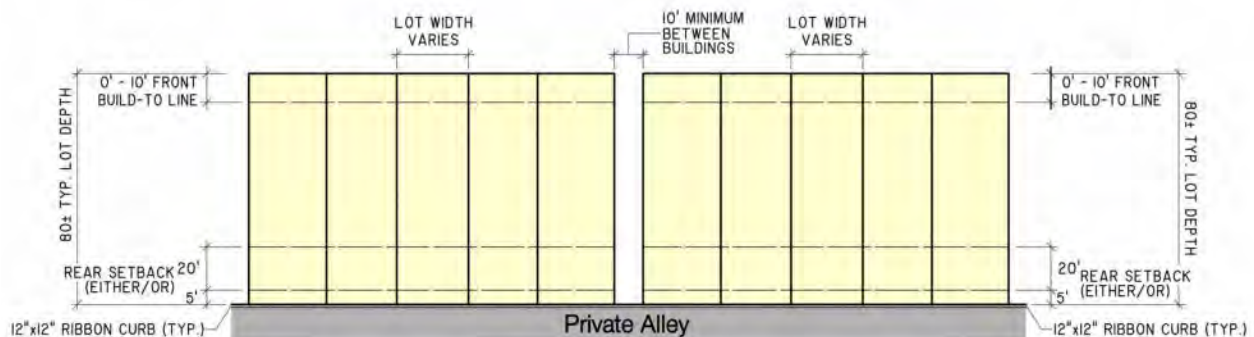
### **Building Setbacks :**

Front Setback: 0 ft. - 10 ft. Build-to Line

Rear Setback: 5 ft. or 20 ft.

Side Setback: 10 ft. Minimum between Buildings

**All homes will be for sale**



***TYPICAL BUILDING SETBACK DETAILS***  
(NOT TO SCALE)

## ARCHITECTURAL STANDARDS:

### General:

- Rhythm of ground floor architectural features shall harmonize with rhythm of upper stories.
- Long, uninterrupted wall planes on drives or paths shall be avoided.
- Most of the buildings shall have relatively flat fronts.
- Outdoor equipment such as HVAC equipment, meters, and panels shall be placed to the side or rear of the building or otherwise screened from streets.
- Buildings on corner lots shall address both drives with architectural features and massing elements, including porches, windows, bays, or other facade enhancements.
- Wall openings in adjacent buildings shall not face each other to give privacy to occupants. On adjacent lots, the building built first shall set precedence with respect to location and positioning of wall openings
- Building facades fronting a street shall provide a minimum of one principal entrance (doorway), provide a porch or covered stoop and a minimum of 15% glazing.
- Building facades shall be constructed of brick, brick veneer, stone, cast stone, cementitious siding, glass, or materials substantially similar in form and function, unless otherwise approved on detailed building elevations included with the preliminary SP.
- Porches, if provided, shall provide a minimum of six feet of depth.



### Windows and Doors:

- Windows shall be vertically oriented at a ratio of 1.5:1 or greater, except for dormers.
- Pre-finished aluminum and vinyl clad wood windows.
- Clear or slightly tinted glazing.
- True divided lights.
- Simulated divided lights.
- Shutters sized to fit window opening when closed.

## **ARCHITECTURAL STANDARDS (CONTINUED):**

### **Walls:**

- Building walls shall be finished in brick, brick veneer, painted brick veneer, stone, cast stone, cultured stone (ashlar pattern, drystack pattern), masonry siding products, cementitious and fiber cement composite siding (e.g. Hardie, Certaineed), or applied cement stucco.
- Building foundations shall be parged or textured block, brick or stone masonry, or masonry veneer.
- Retaining walls shall be concrete, brick, stone, masonry, or masonry veneer.

### **Roofs:**

- Roofs shall be clad in asphalt or fiberglass composite shingles, standing seam metal, natural or synthetic slate, galvanized or painted steel, or shingle-styled concrete tile.

### **Architectural Detailing:**

- Vinyl or Aluminum Trim.
- Painted or stained wood.
- High density rigid polyurethane (HRP).
- Smooth surface composite or cellular PVC or comparable in standard wood trim sizes.
- Vinyl soffit and fascia.
- Cast stone.
- Copper.

## **NOT PERMITTED RESIDENTIAL MATERIALS**

### **Inappropriate Building Cladding (facade wall):**

- Oversized brick – non-modular.
- Exterior Insulation Finish System (EIFS).
- Metal siding.
- Vinyl siding.
- Concrete masonry units (CMU) or split-faced.
- Concrete panels.

### **Inappropriate Windows and Doors:**

- Glass block (on frontages).
- Dark tinted, color tinted or mirror finished glazing.
- Shutters that are not sized to fit the window opening when closed.

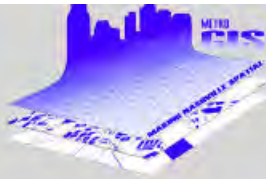
### **Inappropriate Roofs:**

- Spanish tile.
- Wood shingle.

### **Inappropriate Architectural Detailing:**

- Extruded polystyrene.
- PVC.

## METRO GIS PROPERTY INFORMATION - MAP 173, PARCEL 36.00:



Nashville Planning Department

800 2nd Ave S

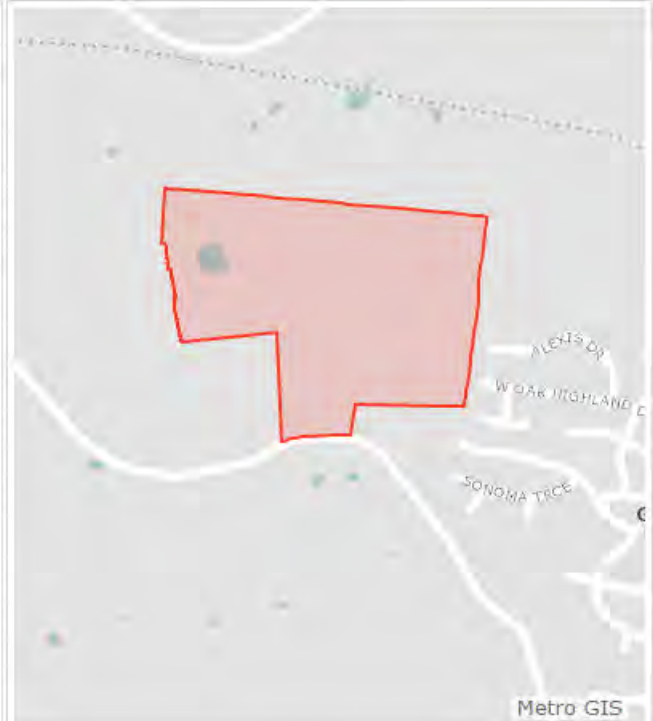
P.O. Box 196300

Nashville, TN 37219-6300

[maps.nashville.gov](http://maps.nashville.gov)

### Parcel Details

<b>Parcel ID:</b>	17300003600
<b>Parcel Address:</b>	14656 OLD HICKORY BLVD ANTIOCH, TN 37013
<b>Owner:</b>	CAMERON, DON, III
<b>Acquired Date:</b>	11/13/2005
<b>Sale Price:</b>	\$ 2,006,600.00
<b>Sale Instrument:</b>	DB-20051121 0140598
<b>Mailing Address:</b>	1503 COLUMBIA AV FRANKLIN, TN 37064
<b>Legal Description:</b>	E SIDE OLD HICKORY BLVD N OF PRESTON RD
<b>Acreage:</b>	81.38
<b>Frontage Dimension:</b>	0
<b>Side Dimension:</b>	0
<b>Parcel Instrument:</b>	DB-00010525 0000201
<b>Parcel Instrument Date:</b>	7/9/1997
<b>Census Tract:</b>	37019112
<b>Tax District:</b>	GSD
<b>Council District:</b>	31
<b>Land Use Description:</b>	SINGLE FAMILY



### Zoning

[Hide](#)

<b>Zone Code</b>	AR2A
<b>Zone Description</b>	AGRICULTURAL/RESIDENTIAL REQUIRES A MINIMUM LOT SIZE OF 2 ACRES AND INTENDED FOR USES THAT GENERALLY OCCUR IN RURAL AREAS, INCLUDING SINGLE-FAMILY, TWO-FAMILY, AND MOBILE HOMES AT A DENSITY OF ONE DWELLING UNIT PER 2 ACRES. THE AR2A DISTRICT IS INTENDED
<b>Effective Date</b>	12/23/1974
<b>Ordinance</b>	073-650
<b>Case Number</b>	
<b>Zone Code</b>	OV-AIR
<b>Zone Description</b>	
<b>Effective Date</b>	12/23/1974
<b>Ordinance</b>	073-650
<b>Case Number</b>	

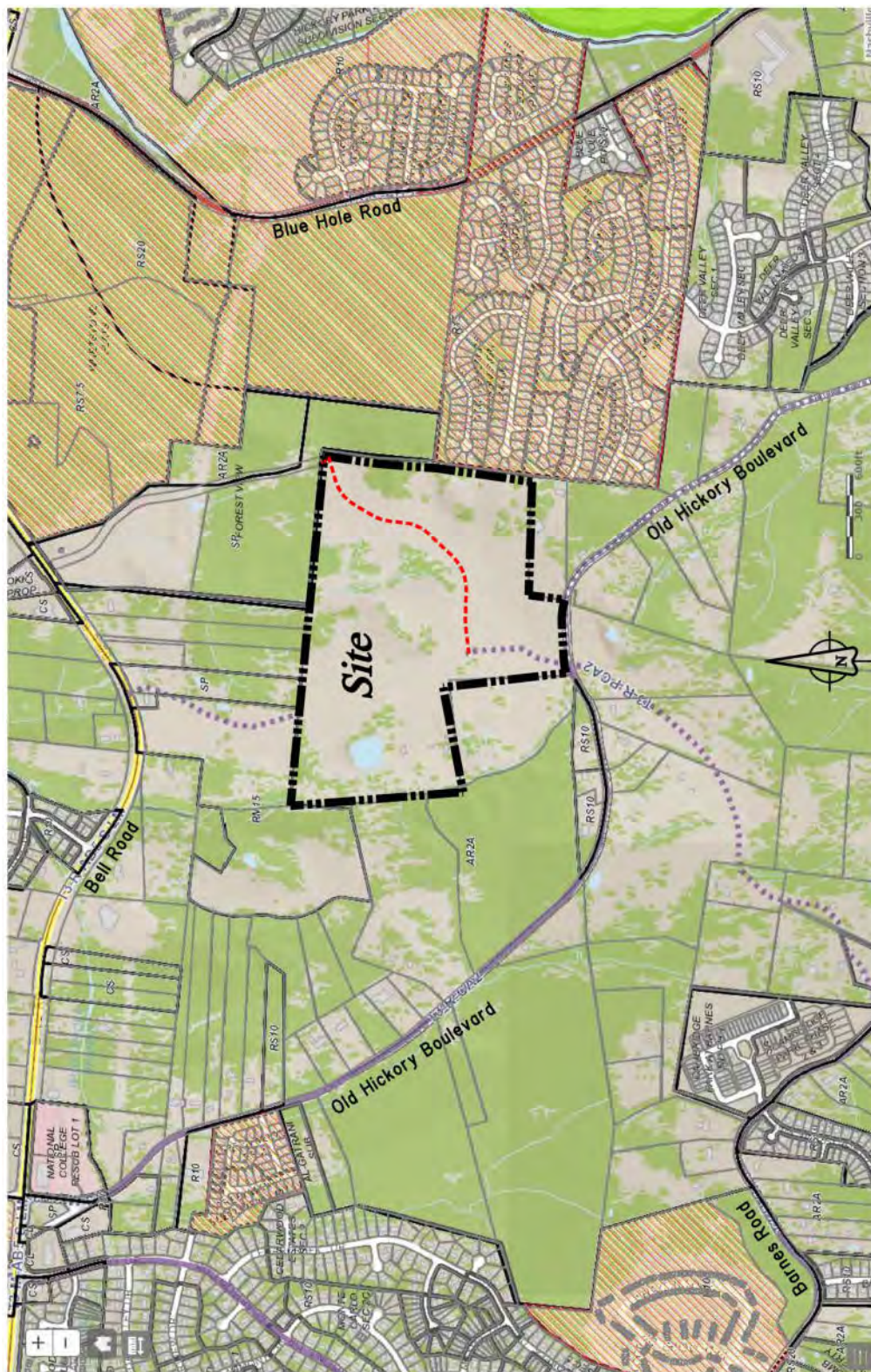
### Owner History

[Hide](#)

<b>Owner Name</b>	CAMERON, DON, III
<b>Acquired Date</b>	11/14/2005
<b>Sale Instrument</b>	DB-20051121 014059
<b>Mailing Address</b>	1503 COLUMBIA AV, FRANKLIN TN 37064
<b>Mailing Country</b>	US



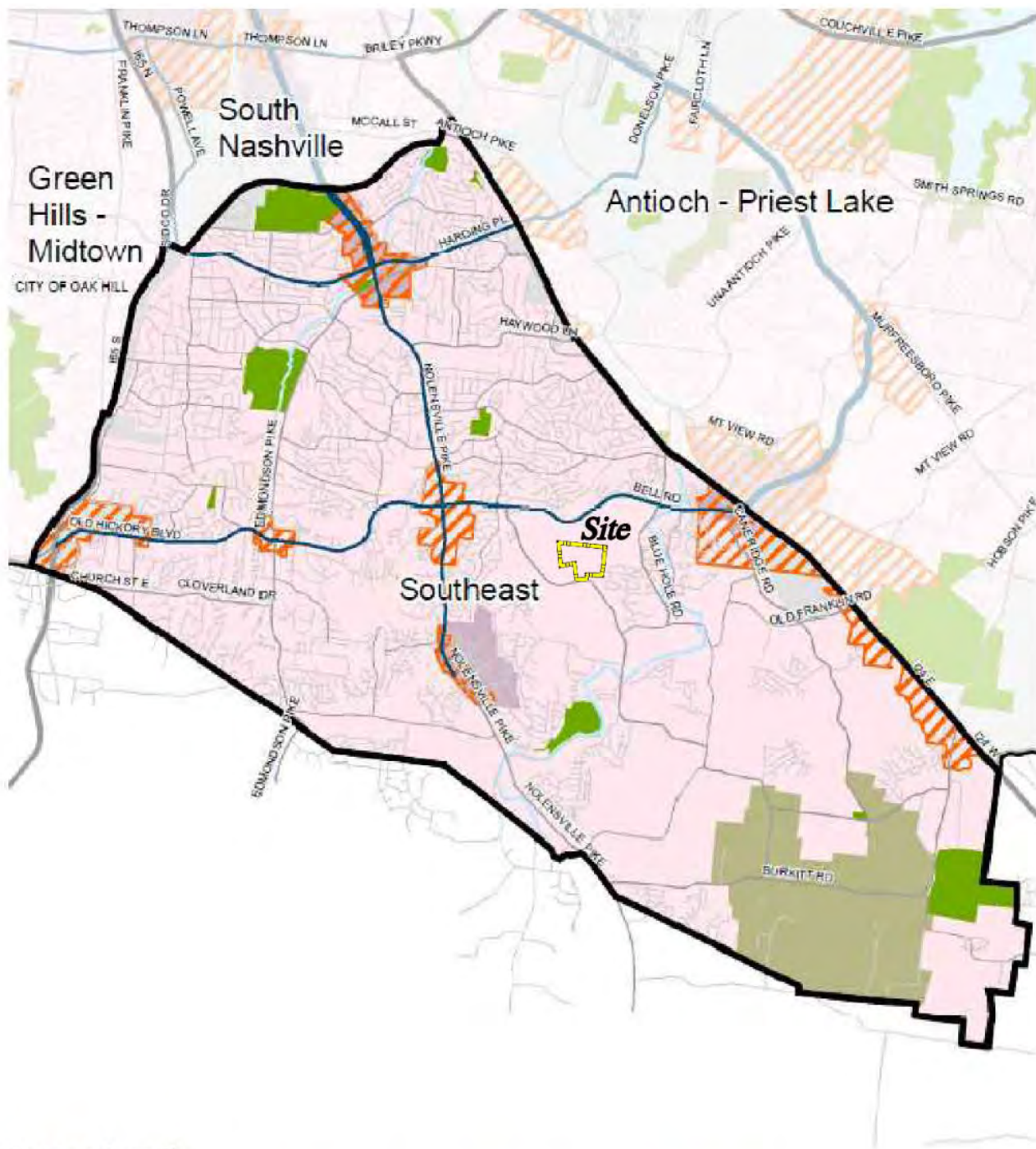
## METRO GIS LOCATION MAP:
















--- REVISED COLLECTOR ROAD LOCATION  
PER NDOT AND METRO PLANNING



## TRANSECT MAP – SOUTHEAST COMMUNITY PLAN:



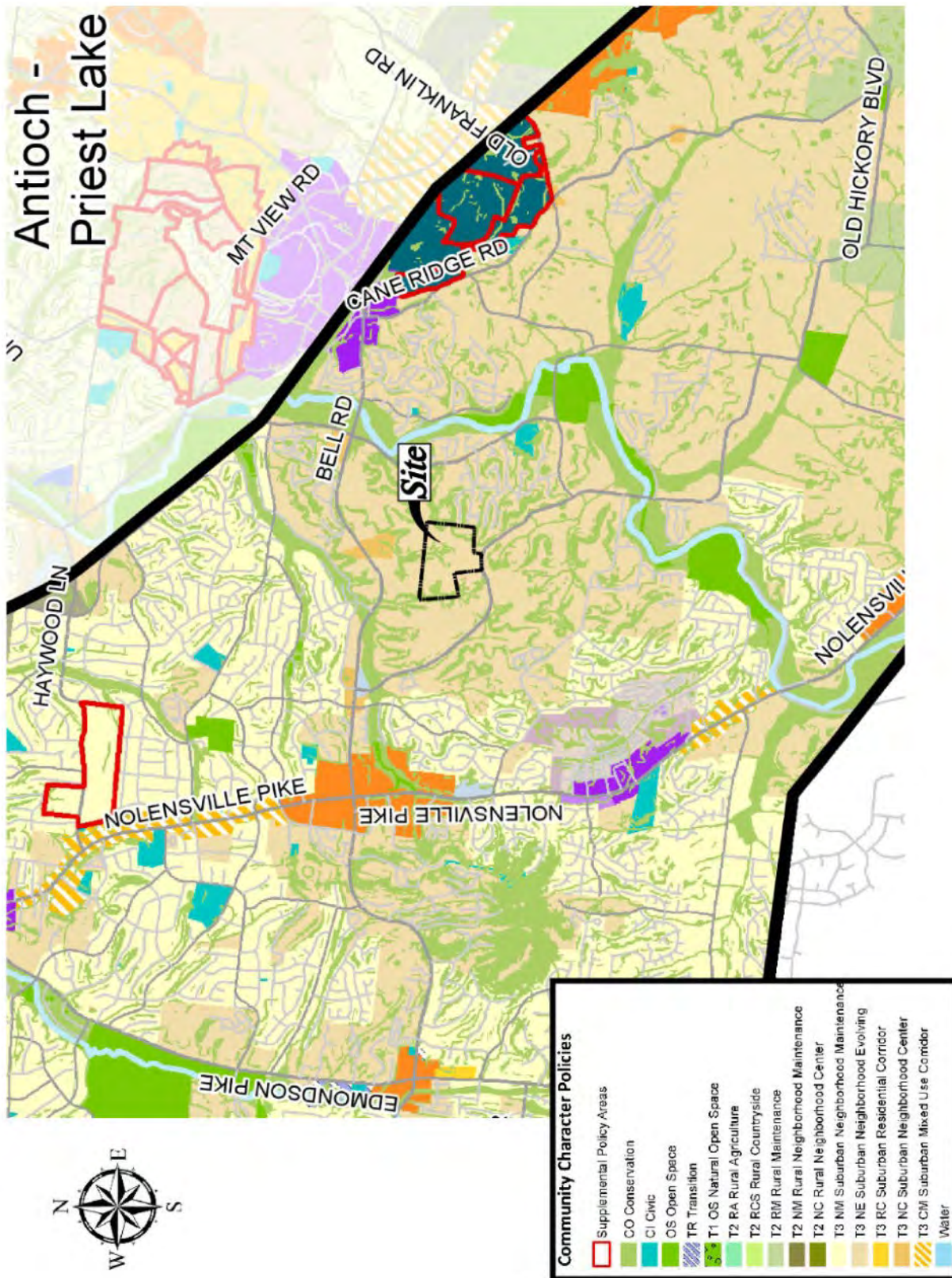
### Transects Legend

 Centers	 Immediate Need	 T1 Natural	 T5 Center
 Subarea Boundaries	 Long-Term Need	 T2 Rural	 T6 Downtown
 Anchor Parks		 T3 Suburban	 D District
		 T4 Urban	 Water



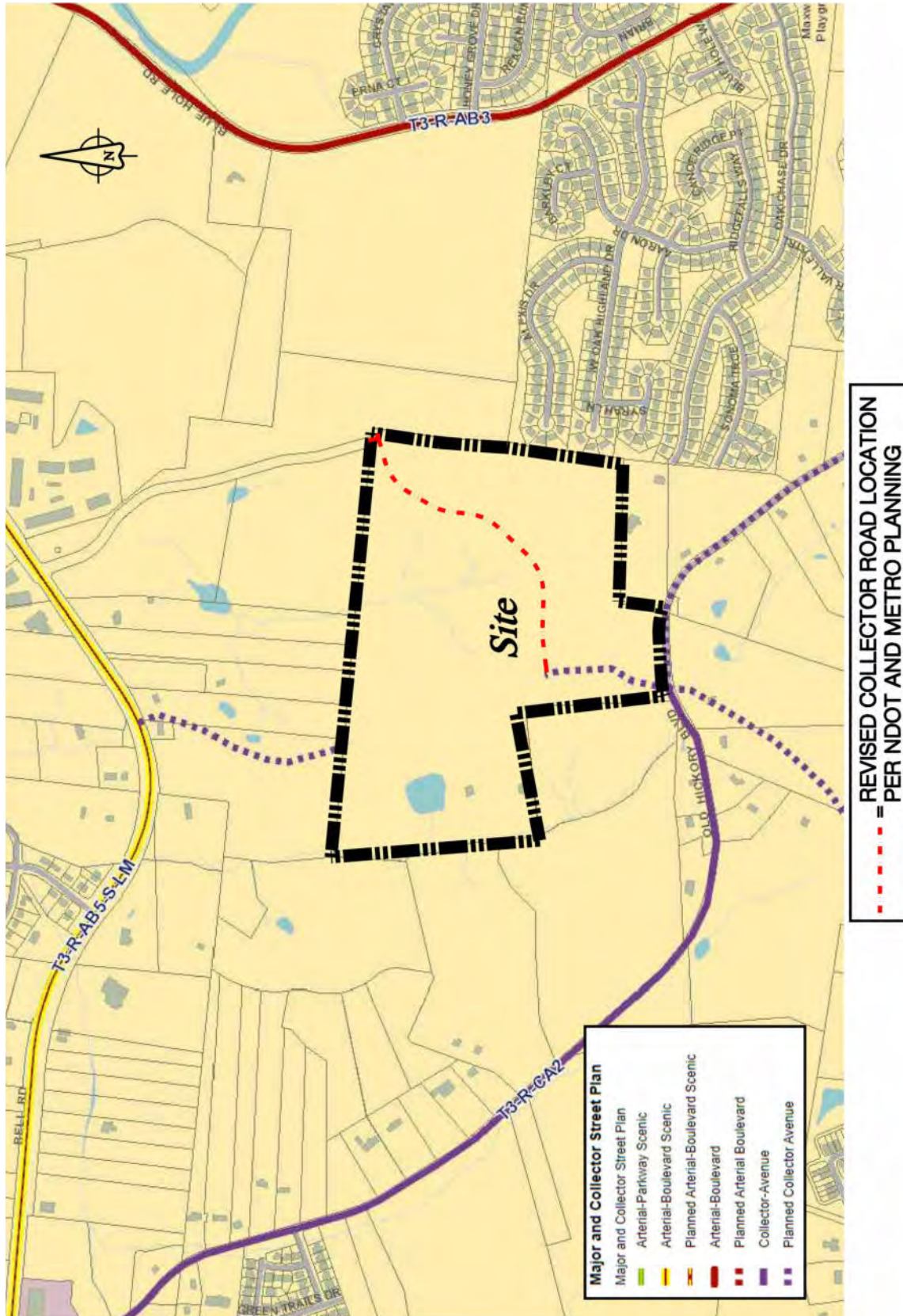


# COMMUNITY CHARACTER POLICY MAP – SOUTHEAST COMMUNITY PLAN:



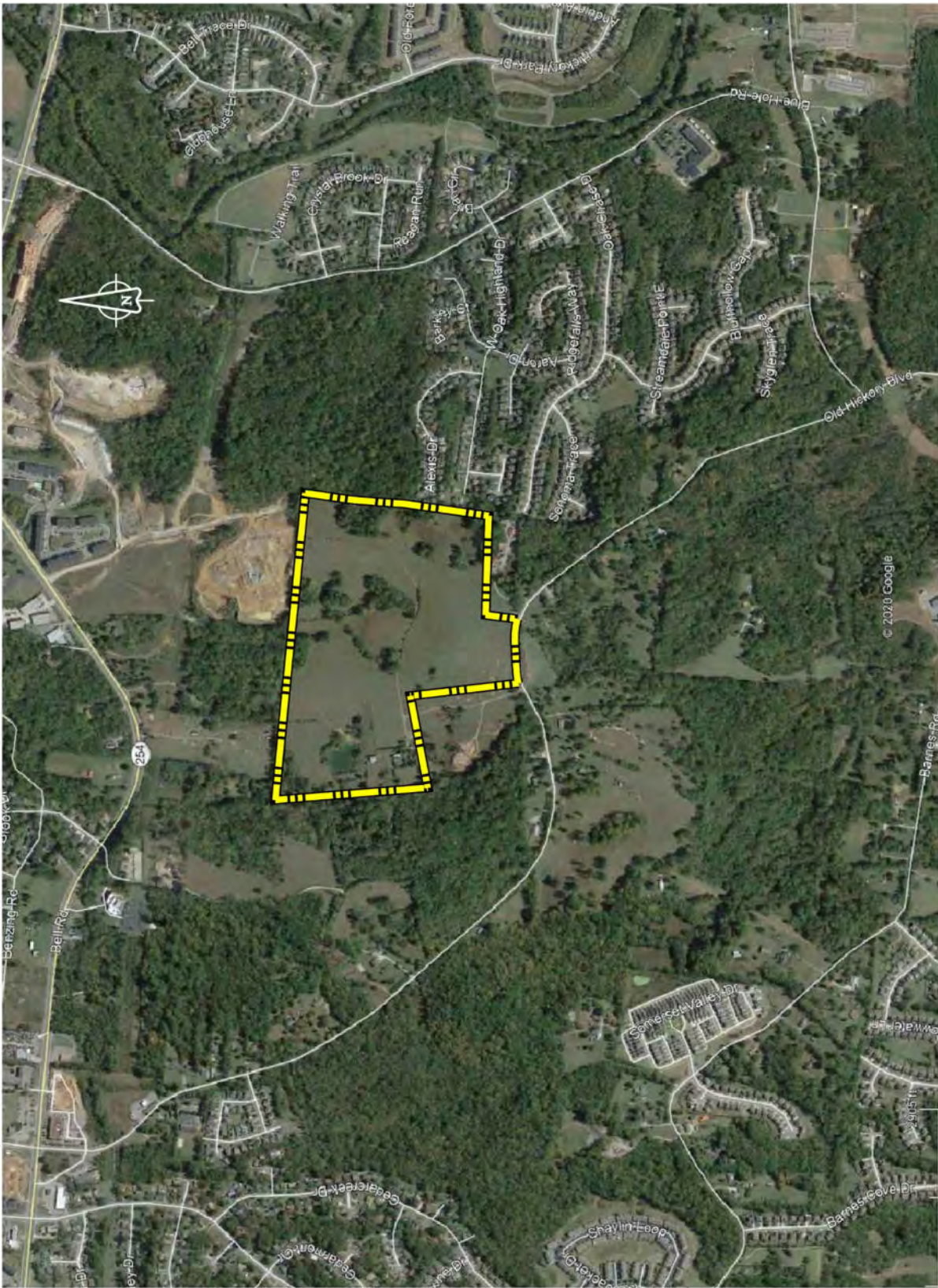


## METRO MAJOR AND COLLECTOR STREET PLAN:



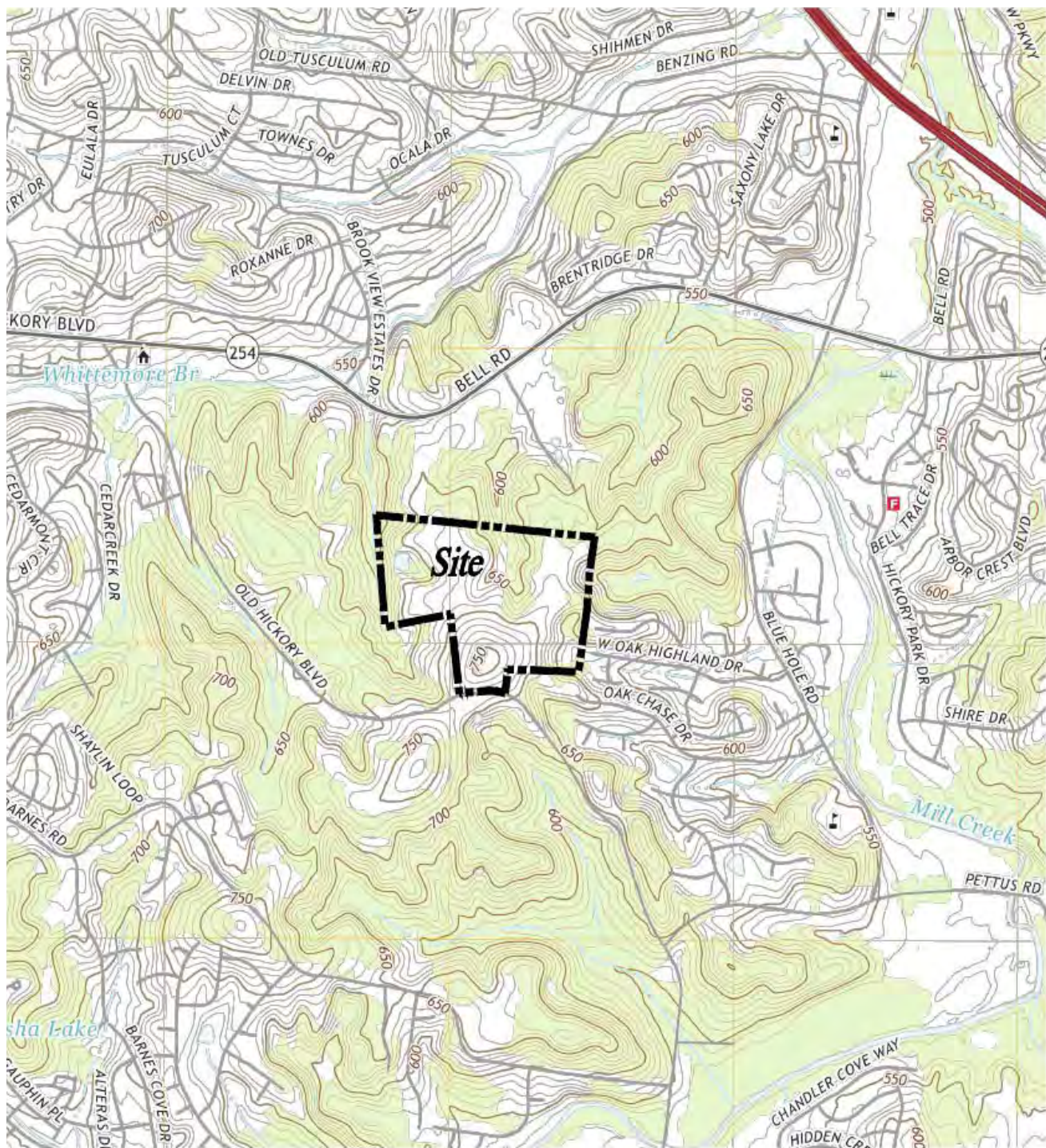


**AERIAL PHOTO OF SITE – FROM GOOGLE EARTH (IMAGERY DATE 10/22/19):**





## USGS TOPOGRAPHIC MAP:

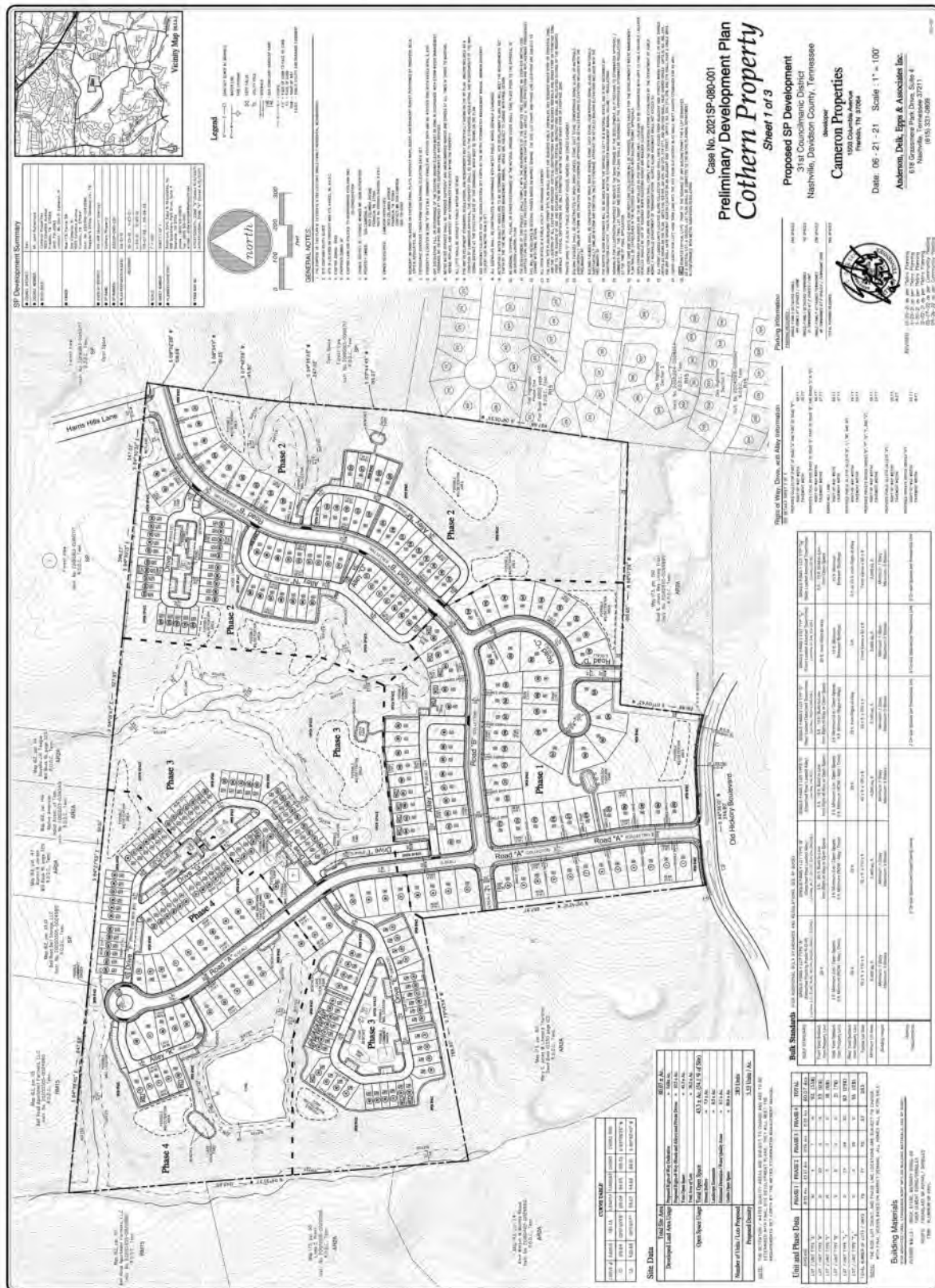


U.S. DEPARTMENT OF THE INTERIOR  
U.S. GEOLOGICAL SURVEY

ANTIOCH QUADRANGLE  
TENNESSEE  
7.5-MINUTE SERIES  
2019



**Cothern Property – SP District**





# PRELIMINARY DEVELOPMENT PLAN, SHEET 2 OF 3 (REDUCED):



# PRELIMINARY DEVELOPMENT PLAN, SHEET 3 OF 3 (REDUCED):

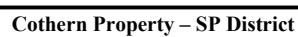






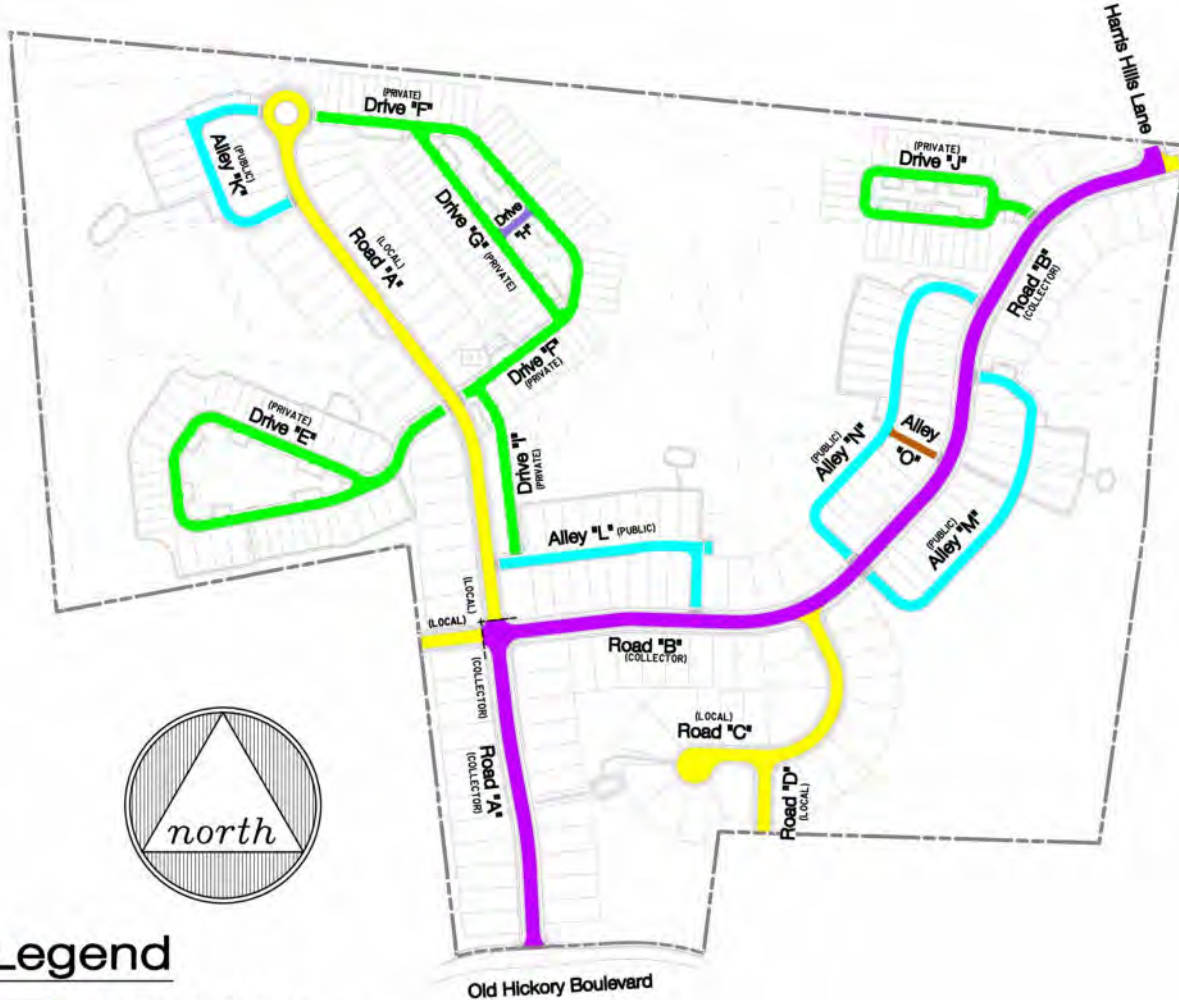


**Cothern Property – SP District**





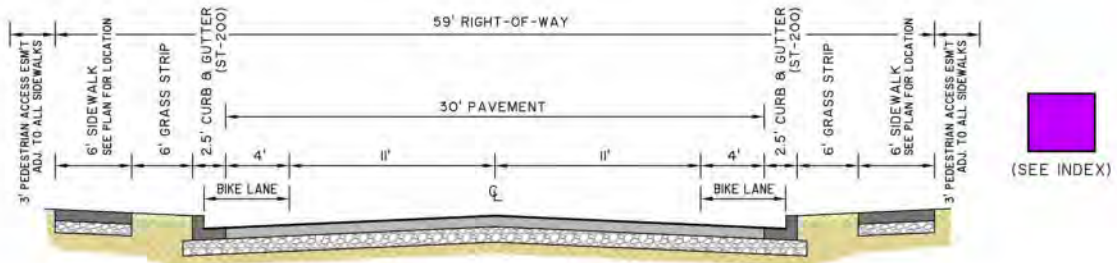
## ROADWAY LAYOUT (NOT TO SCALE):



### Legend

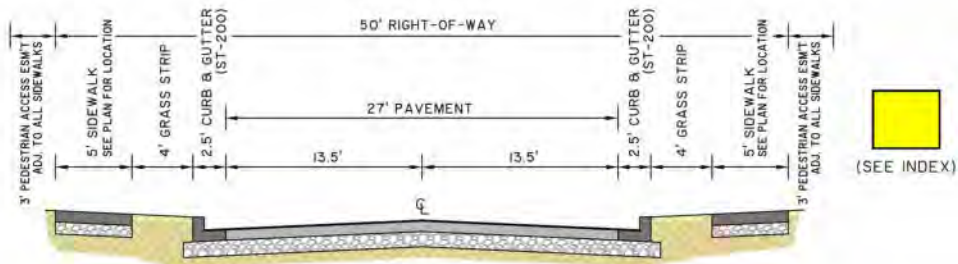
- COLLECTOR ROAD**  
(59' RIGHT-OF-WAY AND 30' PAVEMENT WIDTH)
- LOCAL ROAD**  
(50' RIGHT-OF-WAY AND 27' PAVEMENT WIDTH)
- PRIVATE DRIVE**  
(26' PRIVATE RIGHT-OF-WAY AND 24' PAVEMENT WIDTH)  
\*PRIVATE DRIVE "F" IS ALSO A PUBLIC EMERGENCY ACCESS, INGRESS, AND EGRESS EASEMENT. (SEE NOTE #25 ON PRELIMINARY SP)\*
- PRIVATE DRIVE**  
(20' PRIVATE RIGHT-OF-WAY AND 18' PAVEMENT WIDTH)
- LOCAL ALLEY**  
(26' RIGHT-OF-WAY AND 24' PAVEMENT WIDTH)
- LOCAL ALLEY**  
(20' RIGHT-OF-WAY AND 18' PAVEMENT WIDTH)

# ROADWAY SECTIONS – SHEET 1 OF 2 (NOT TO SCALE):



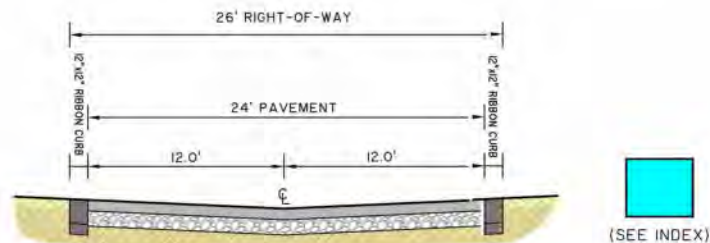
TYPICAL COLLECTOR ROAD SECTION (N.T.S.)

HARRIS HILL LANE  
PART OF ROAD "A"  
PART OF ROAD "B"



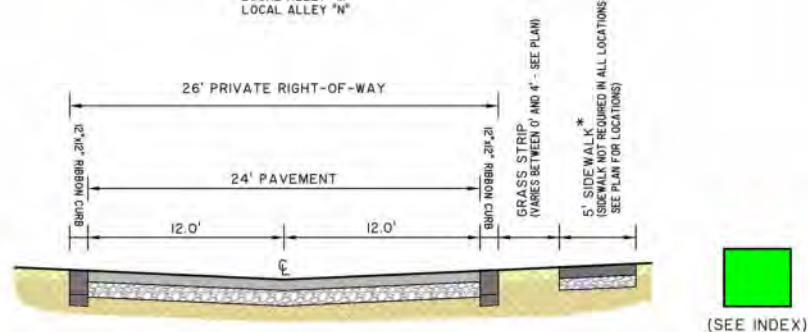
TYPICAL LOCAL ROAD SECTION (N.T.S.)

PART OF ROAD "A"  
PART OF ROAD "B"  
ROAD "C"  
ROAD "D"



TYPICAL LOCAL ALLEY SECTION (N.T.S.)

LOCAL ALLEY "K"  
LOCAL ALLEY "L"  
LOCAL ALLEY "M"  
LOCAL ALLEY "N"



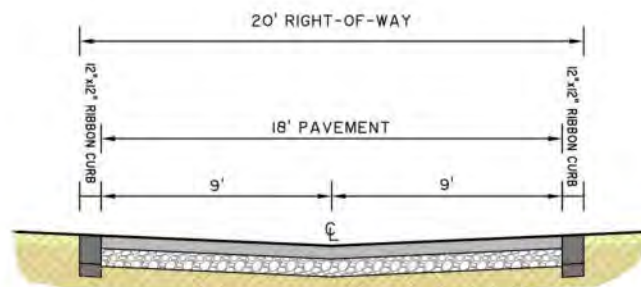
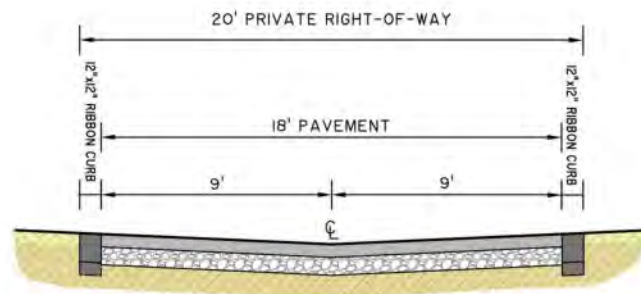
TYPICAL PRIVATE DRIVE SECTION (N.T.S.)

PRIVATE DRIVE "E"  
PRIVATE DRIVE "F" \*\*  
PRIVATE DRIVE "G"  
PRIVATE DRIVE "H"  
PRIVATE DRIVE "J"

\* SIDEWALK IS NOT REQUIRED IN ALL LOCATIONS.  
(SEE PLAN VIEW FOR SIDEWALK LOCATIONS) \*

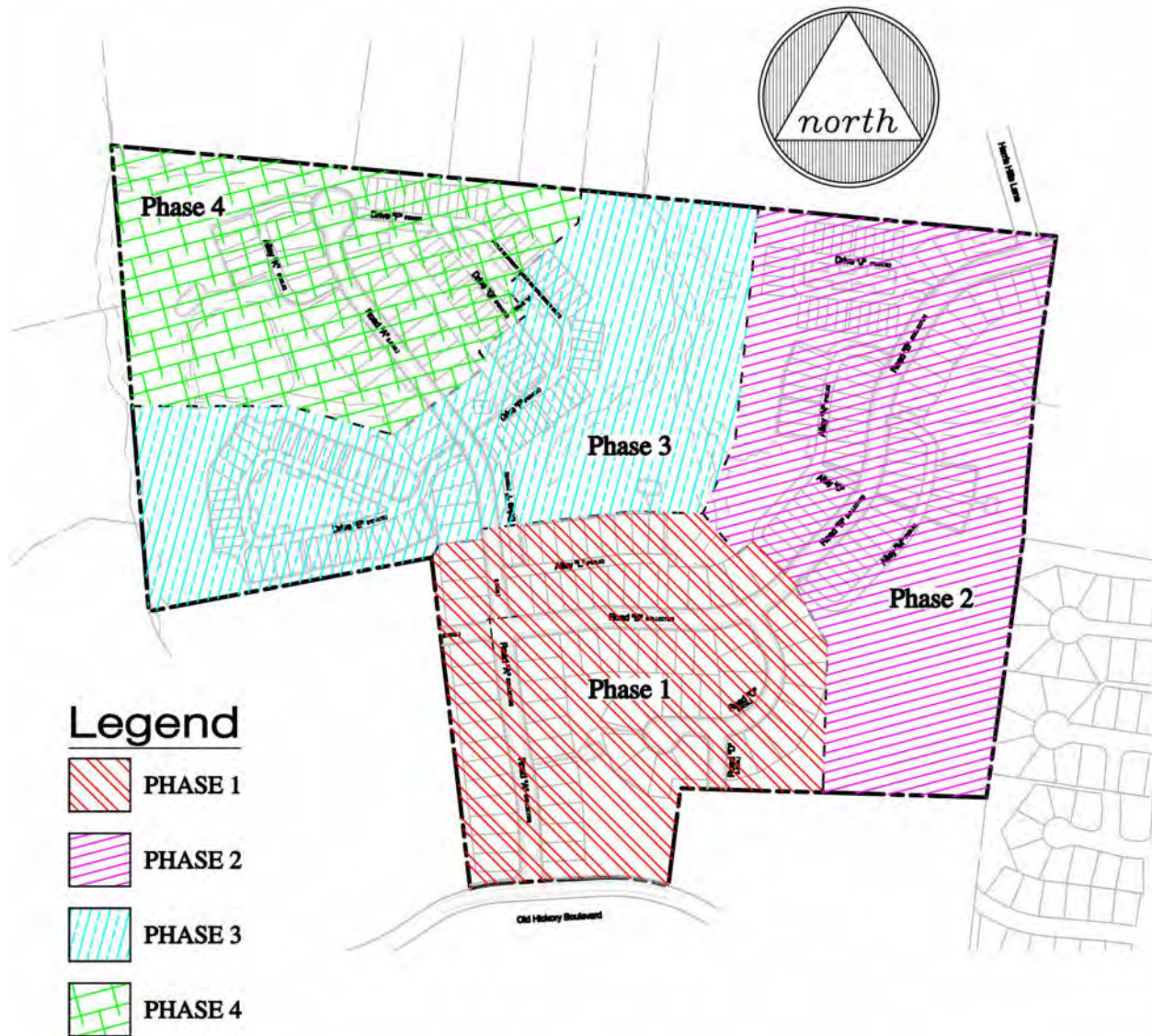
\*\* PRIVATE DRIVE "F" IS ALSO A PUBLIC EMERGENCY  
ACCESS, INGRESS, AND EGRESS EASEMENT.  
(SEE NOTE #25 ON PRELIMINARY SP PLAN) \*\*

## ROADWAY SECTIONS - SHEET 2 OF 2 (NOT TO SCALE):

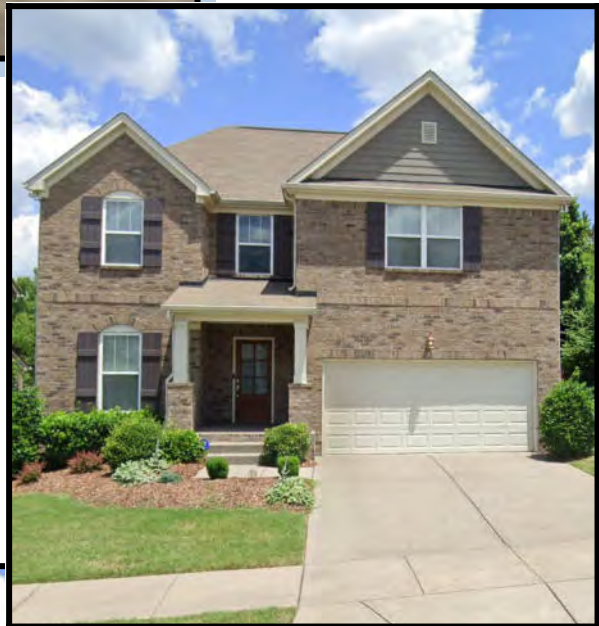




**PRELIMINARY PHASING PLAN (NOT TO SCALE):**



**BUILDING EXAMPLES – FRONT LOADED DETACHED HOME (A):**





**BUILDING EXAMPLES – DETACHED HOME FRONTING RIGHT-OF-WAY (B):**



**BUILDING EXAMPLES – DETACHED HOME FRONTING OPEN SPACE (B):**





## BUILDING EXAMPLES – REAR ALLEY LOADED DETACHED HOME (C):



*Photo or Rendering May Reflect  
Changes Made in Field.*

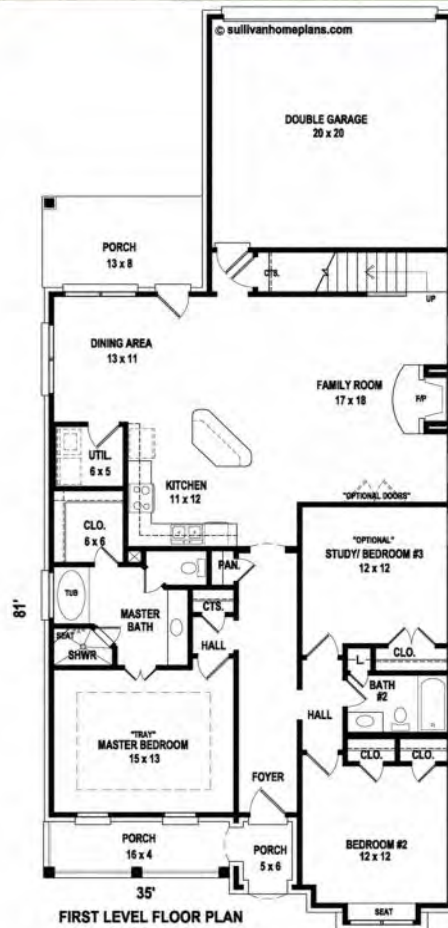


**SECOND LEVEL FLOOR PLAN**

9'-0" CEILINGS DOWN  
9'-0" CEILINGS UP  
EXCEPT WHERE NOTED  
OTHERWISE ON PLAN

**SQUARE FOOTAGE**  
1829 HEATED DOWN  
376 HEATED UP  
449 GARAGE & STORAGE  
215 FRONT & REAR PORCHES  
2205 TOTAL HEATED AREA  
2581 TOTAL ENCLOSED AREA  
2796 TOTAL UNDER ROOF

*Artist's conceptions, all plans  
subject to change.*  
BEW 5-18-2015



**FIRST LEVEL FLOOR PLAN**

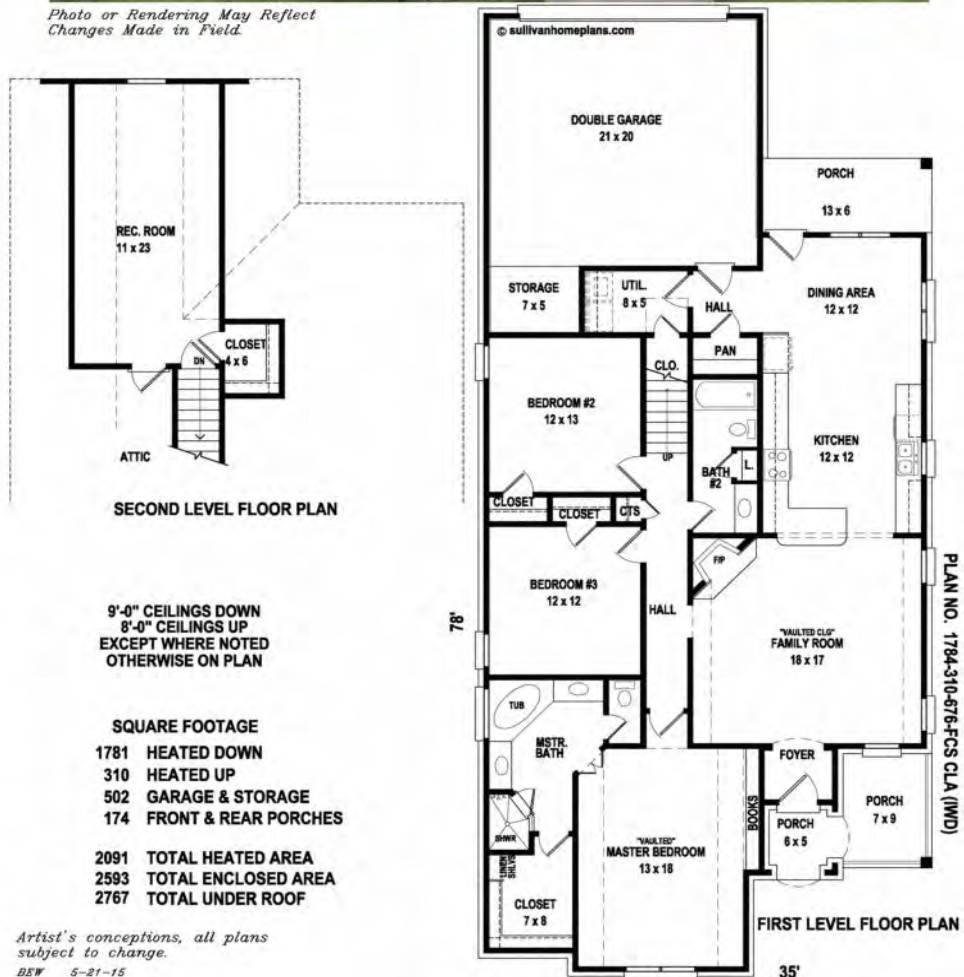
PLAN NO. 1796-376-664-FT CIA (MID)



## BUILDING EXAMPLES – REAR ALLEY LOADED DETACHED HOME (C):



*Photo or Rendering May Reflect Changes Made in Field.*



**BUILDING EXAMPLES – REAR LOADED DETACHED TOWNHOME (D):**

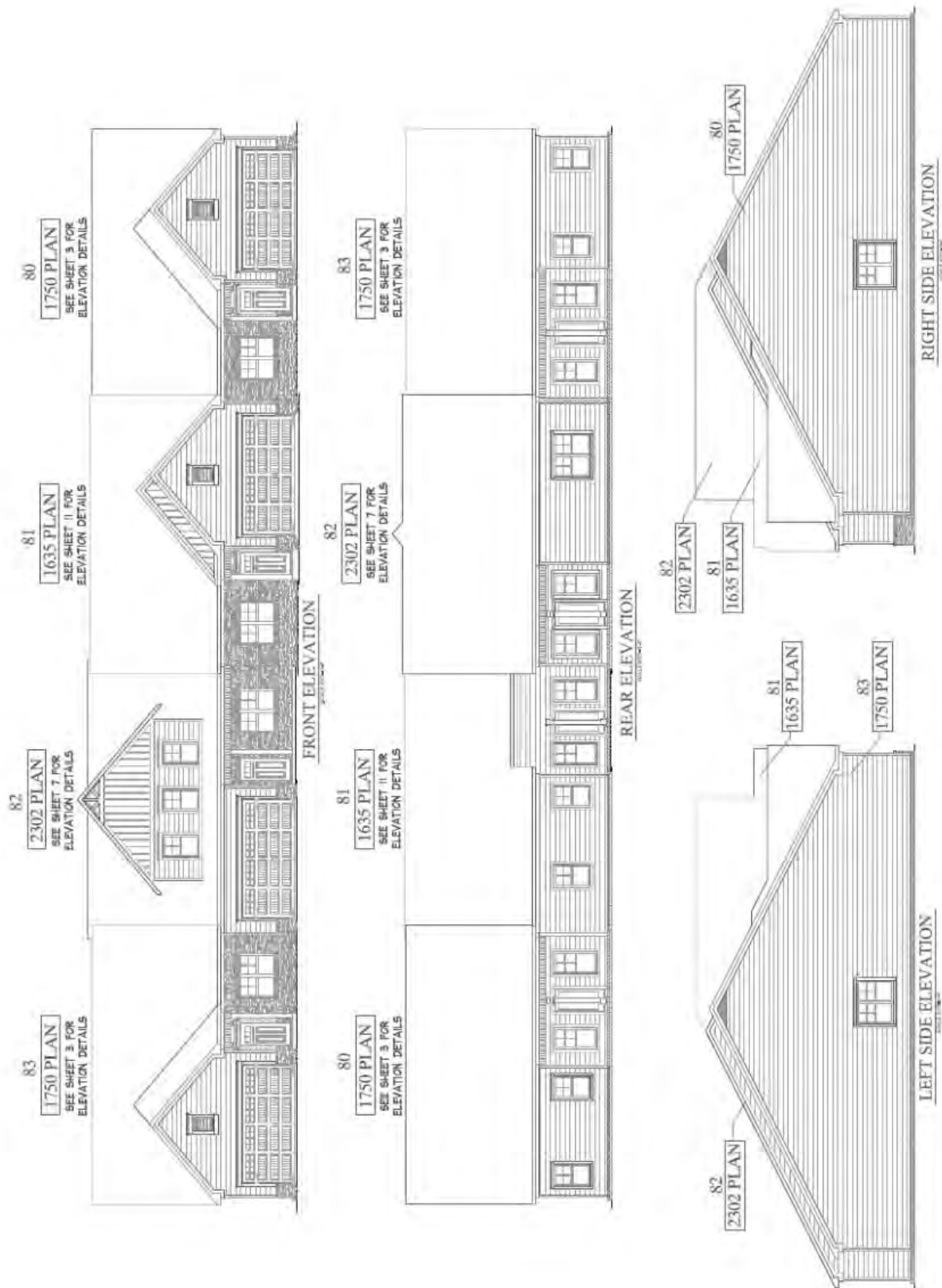




**BUILDING EXAMPLES – FRONT LOADED ATTACHED TOWNHOME (T<sub>F</sub>):**



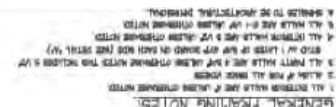
## EXAMPLE BUILDING PLAN – FRONT LOADED ATTACHED TOWNHOME (T<sub>F</sub>):





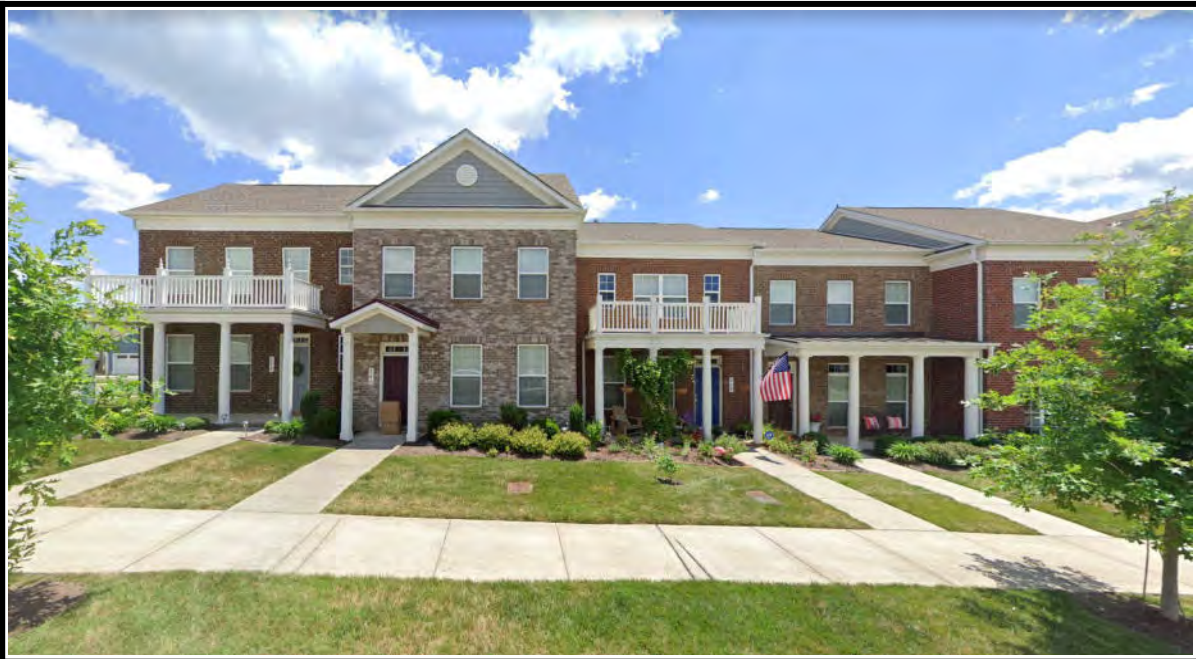
**BUILDING EXAMPLES – FRONT LOADED ATTACHED TOWNHOME (T<sub>F</sub>):**



**Cothern Property – SP District**



**BUILDING EXAMPLES – REAR LOADED ATTACHED TOWNHOME (T<sub>R</sub>):**







CURVE TABLE					
CURVE #	RADIUS	DELTA	LENGTH	TANGENT	CHORD
C1	519.64	020°30'48"	186.04	94.03	185.05
C2	430.68	010°50'57"	89.07	44.69	88.91

Site Data	
Total Site Area	80.07 ± Ac.
Developed Land Area Usage	Proposed Right-of-Way Dedication = 0.08± Ac. Proposed Right-of-Way (Roads and Alleys) and Private Drives = 10.5 ± Ac. Total Open Space = 43.3 ± Ac. Total Area of Lots = 26.2 ± Ac.
Open Space Usage	Total Open Space = 43.3 ± Ac. (54.1 % of Site) Stream Buffers = 7.5 ± Ac. Landscape Easements = 0.5 ± Ac. Estimated Detention / Water Quality Areas = 4.7 ± Ac. Usable Open Space = 30.6 ± Ac.
Number of Units / Lots Proposed	283 Units
Proposed Density	3.53 Units / Ac.

NOTE: THE DETENTION / WATER QUALITY AREAS ARE SUBJECT TO CHANGE AND ARE TO BE DETERMINED WITH FINAL SITE DEVELOPMENT PLANS. THEY WILL MEET THE REQUIREMENTS SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL.

Unit and Phase Data	PHASE 1	PHASE 2	PHASE 3	PHASE 4	TOTAL
ACREAGE	18.99 Ac.	23.57 Ac.	21.56 Ac.	15.95 Ac.	80.07 Ac.
LOT / UNIT TYPE "A"	61	11	7	14	93 (33%)
LOT / UNIT TYPE "B"	0	20	0	13	33 (12%)
LOT / UNIT TYPE "C"	18	0	0	0	18 (6%)
LOT / UNIT TYPE "D"	0	21	0	0	21 (7%)
LOT / UNIT TYPE "E"	0	25	28	30	83 (29%)
LOT / UNIT TYPE "F"	0	0	35	0	35 (12%)
TOTAL NUMBER OF LOTS / UNITS	79	77	70	57	283

NOTE: THE SIZE, LOT COUNT, AND PHASE LINE LOCATIONS ARE SUBJECT TO CHANGE WITH FINAL DESIGN BASED ON MARKET DEMAND. ALL HOMES WILL BE FOR SALE.

#### Building Materials

(FOR ARCHITECTURAL STANDARDS MORE INFO ON BUILDING MATERIALS, SEE SP BOOK)

OUTSIDE WALLS : BRICK, STONE, MASONRY SIDING, OR FIBER CEMENT SIDING/SHINGLES  
ROOFS: FIBERGLASS OR ASPHALT SHINGLES  
TRIM: ALUMINUM OR VINYL

#### Bulk Standards

(FOR ADDITIONAL BULK STANDARDS AND REGULATIONS, SEE SP BOOK)

BULK STANDARD	SINGLE-FAMILY LOT TYPE "A" (Detached Fronting Public R-O-W) (Lot Nos. 1-21, 27-64, 70-86, 103-190, 240-295, 299-277, 279-293)	SINGLE-FAMILY LOT TYPE "B" (Detached Rear Loaded Alley) (Lot Nos. 65-77, 191-197, 201-250, 260-268)	SINGLE-FAMILY LOT TYPE "C" (Detached Rear Loaded Alley) (Lot Nos. 145-162)	SINGLE-FAMILY LOT TYPE "D" (Rear Loaded Attached Townhome) (Lot Nos. 199-214, 269-288)	SINGLE-FAMILY LOT TYPE "E" (Front Loaded Attached Townhome) (Lot Nos. 87-144, 215-229)	SINGLE-FAMILY LOT TYPE "F" (Rear Loaded Attached Townhome) (Lot Nos. 296-308)
Front Yard Setback from Property Line	20 ft.	5 ft. - 20 ft. Build-to-Line from Right-of-Way or Open Space	5 ft. - 15 ft. Build-to-Line from Right-of-Way or Open Space	5 ft. - 10 ft. Build-to-Line from Right-of-Way or Open Space	20 ft. from Right-of-Way	0 ft. - 10 ft. Build-to-Line from Open Space
Side Yard Setback from Property Line	3 ft. Minimum (Lot / Open Space) 5 ft. Minimum (ROW / Alley / Drive)	3 ft. Minimum (Lot / Open Space) 5 ft. Minimum (ROW / Alley / Drive)	3 ft. Minimum (Lot / Open Space) 5 ft. Minimum (ROW / Alley / Drive)	3 ft. Minimum (Lot / Open Space) 5 ft. Minimum (ROW / Alley / Drive)	10 ft. Minimum Between Buildings	10 ft. Minimum Between Buildings
Rear Yard Setback from Property Line	20 ft.	20 ft.	20 ft.	20 ft. from Right-of-Way	0 ft.	5 ft. or 20 ft. from Right-of-Way
Typical Lot Size	50 ± ft. x 110 ± ft.	32 ± ft. x 110 ± ft.	40 ± ft. x 105 ± ft.	30 ± ft. x 105 ± ft.	Front Varies x 80 ± ft.	Front Varies x 80 ± ft.
Minimum Lot Area	5,200 sq. ft.	3,400 sq. ft.	4,200 sq. ft.	3,100 sq. ft.	2,000 sq. ft.	2,000 sq. ft.
Building Height	Minimum: 1 Story Maximum: 3 Stories	Minimum: 1 Story Maximum: 3 Stories	Minimum: 1 Story Maximum: 3 Stories	Minimum: 1 Story Maximum: 3 Stories	Minimum: 1 Story Maximum: 3 Stories	Minimum: 1 Story Maximum: 3 Stories
Parking Requirements	2 On-site Spaces per Single-Family Home			2 On-site Spaces per Residential Unit	2 On-site Spaces per Residential Unit	2 On-site Spaces per Residential Unit

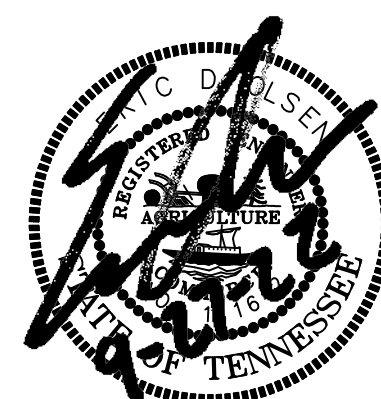
#### Right of Way, Drive, and Alley Information

SEE DETAILS SHEET 2 OF 2

PROPOSED COLLECTOR (PART OF ROAD "A" AND PART OF ROAD "B")	59 FT. 30 FT.
RIGHT-OF-WAY WIDTH	59 FT.
PAVEMENT WIDTH	30 FT.
PROPOSED LOCAL ROADS (PART OF ROAD "A", PART OF ROAD "B", AND ROADS "C" & "D")	50 FT. 27 FT.
RIGHT-OF-WAY WIDTHS	50 FT.
PAVEMENT WIDTHS	27 FT.
HARRIS HILL LANE	60 FT. 37 FT.
RIGHT-OF-WAY WIDTH	60 FT.
PAVEMENT WIDTHS	37 FT.
PROPOSED PUBLIC ALLEYS (ALLEYS "K", "L", "M", AND "N")	26 FT. 24 FT.
RIGHT-OF-WAY WIDTHS	26 FT.
PAVEMENT WIDTHS	24 FT.
PROPOSED PRIVATE DRIVES (DRIVES "E", "F", "G", "H", AND "J")	26 FT. 24 FT.
RIGHT-OF-WAY WIDTHS	26 FT.
PAVEMENT WIDTHS	24 FT.
PROPOSED PUBLIC ALLEYS (ALLEYS "O")	20 FT. 18 FT.
RIGHT-OF-WAY WIDTHS	20 FT.
PAVEMENT WIDTHS	18 FT.
PROPOSED PRIVATE DRIVES (DRIVES "I")	20 FT. 18 FT.
RIGHT-OF-WAY WIDTHS	20 FT.
PAVEMENT WIDTHS	18 FT.

#### Parking Information

PARKING REQUIRED:	288 SPACES
SINGLE-FAMILY DETACHED HOMES	144 HOMES AT 2 SPACES / HOME
SINGLE-FAMILY DETACHED TOWNHOMES	27 TOWNHOMES AT 2 SPACES / UNIT
SINGLE-FAMILY ATTACHED TOWNHOMES	236 SPACES
W TOWNHOMES AT 2 SPACES / TOWNHOME (288)	
TOTAL PARKING REQUIRED	566 SPACES



REVISED : 10-25-21 as per Metro Planning  
11-23-21 as per Metro Planning  
12-02-21 as per Metro Planning  
03-07-22 as per Community Meeting  
09-26-22 as per Community Meeting

## Case No. 2021SP-080-001 Preliminary Development Plan Cothorn Property Sheet 1 of 3

### Proposed SP Development 31st Councilmanic District Nashville, Davidson County, Tennessee

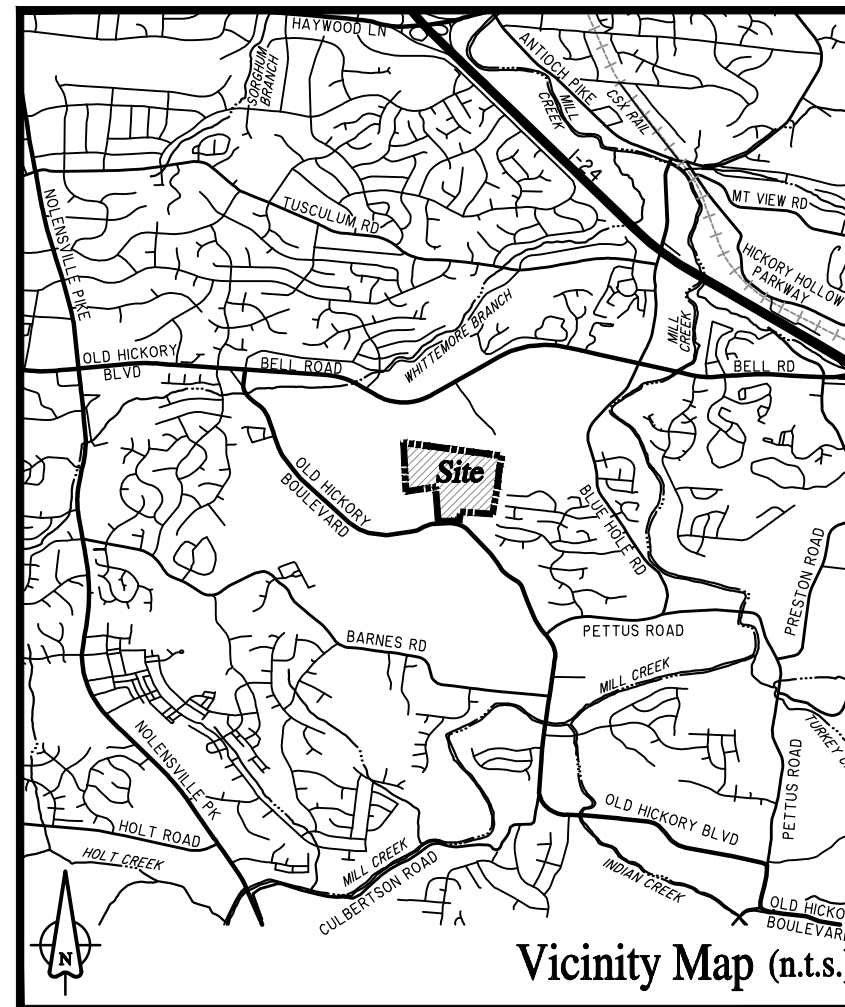
developer  
**Cameron Properties**  
1503 Columbia Avenue  
Franklin, TN 37064

Date: 06 - 21 - 21 Scale : 1" = 100'

**Anderson, Delk, Epps & Associates, Inc.**  
618 Grassmere Park Drive, Suite 4  
Nashville, Tennessee 37211  
(615) 331-0809

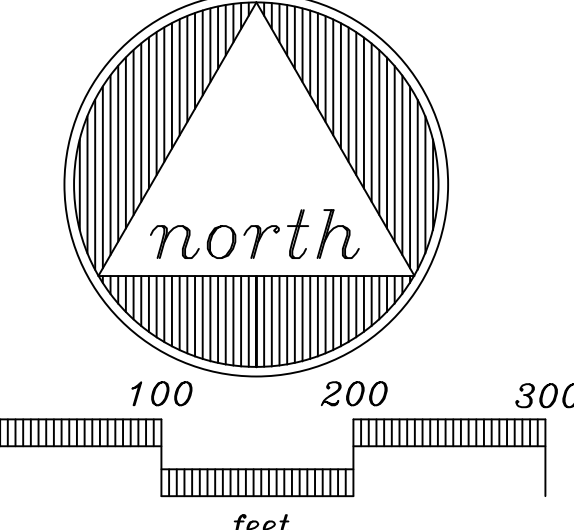
#### SP Development Summary

● COUNCIL DISTRICT:	31st
● COUNCIL MEMBER:	Mr. John Rutherford
● DEVELOPER:	Cameron Properties 1503 Columbia Avenue Franklin, TN 37064 (615) 791-0053 contact : Mr. Don R. Cameron, III
● OWNER:	Map 173, Parcel 36 Don Cameron, III 1503 Columbia Avenue Franklin, TN 37064 Instr. No. 2005012-040598 Register's Office, Davidson Co., TN
● OVERLAY DISTRICT:	OV-AIR (Airport Overlay)
● SP NAME:	Cothorn Property
● SP NUMBER:	2021SP-080-001
● PLAN PREPARATION DATE:	06-21-21
REVISIONS:	10-25-21 : 11-23-21 11-30-21 : 12-02-21 03-07-22 : 09-26-22
● SCALE:	1" = 100'
● SHEET NUMBER:	SHEET 1 OF 1
● PLANNER/SURVEYOR:	Anderson, Delk, Epps & Associates, Inc. 618 Grassmere Park Drive, Suite 4 Nashville, TN 37211 phone : (615) 331-0809 fax : (615) 331-0810 e-mail : andersondelkepps@bellsouth.net
● FEMA MAP NO.	47037C0367H, ZONE "X" (Dated 4/5/2017) 47037C0399H, ZONE "X" (Dated 4/5/2017)



#### Legend

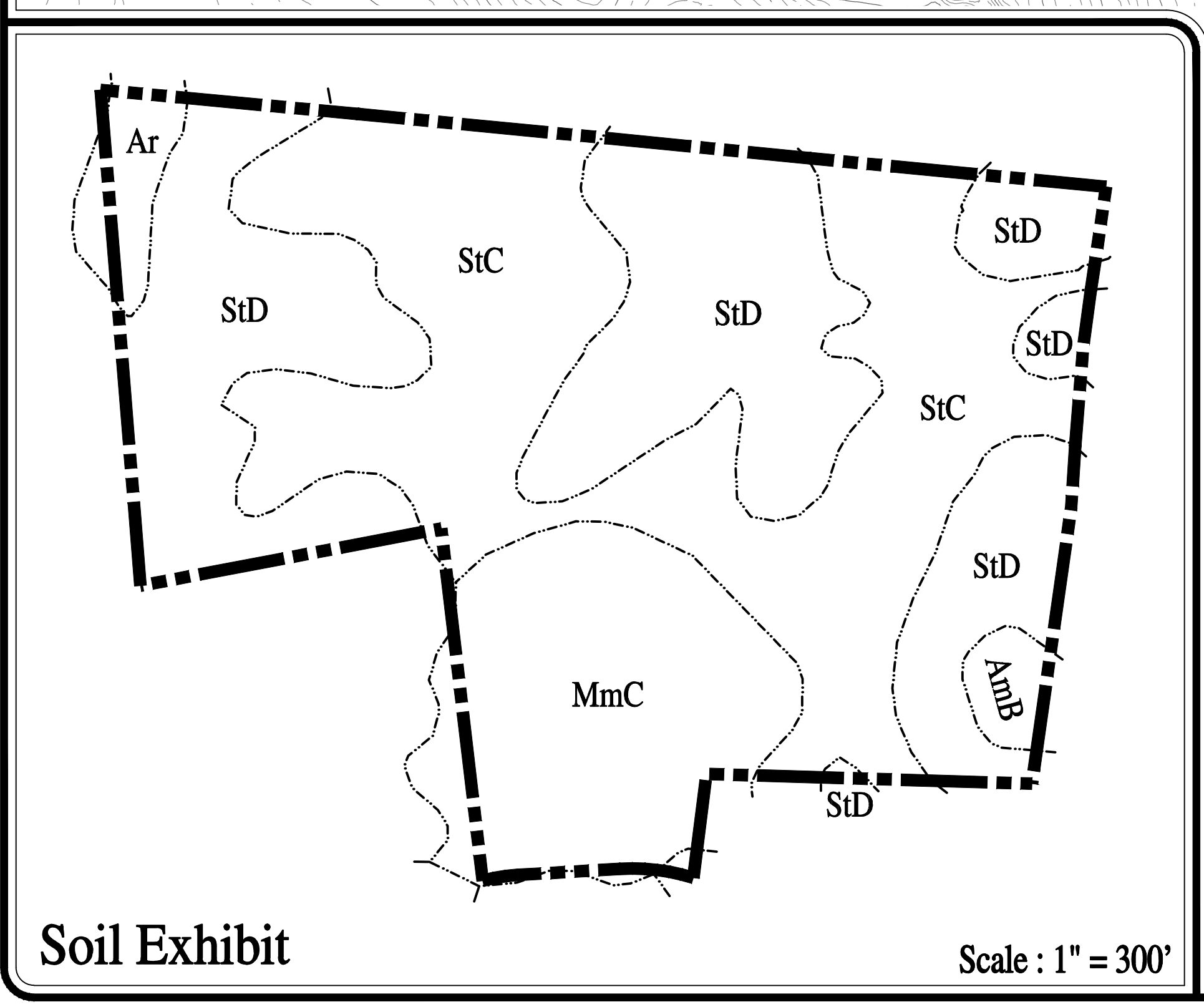
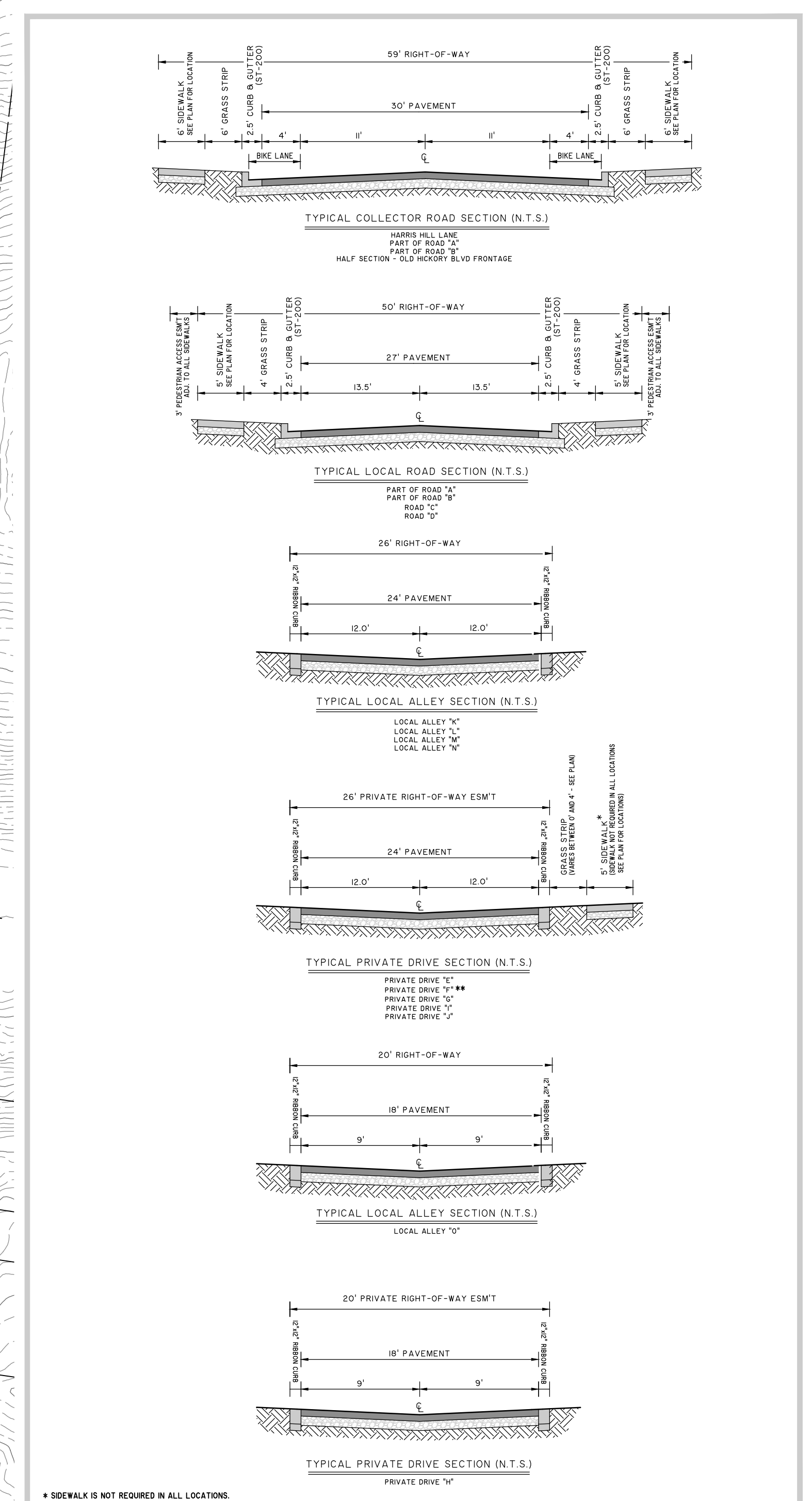
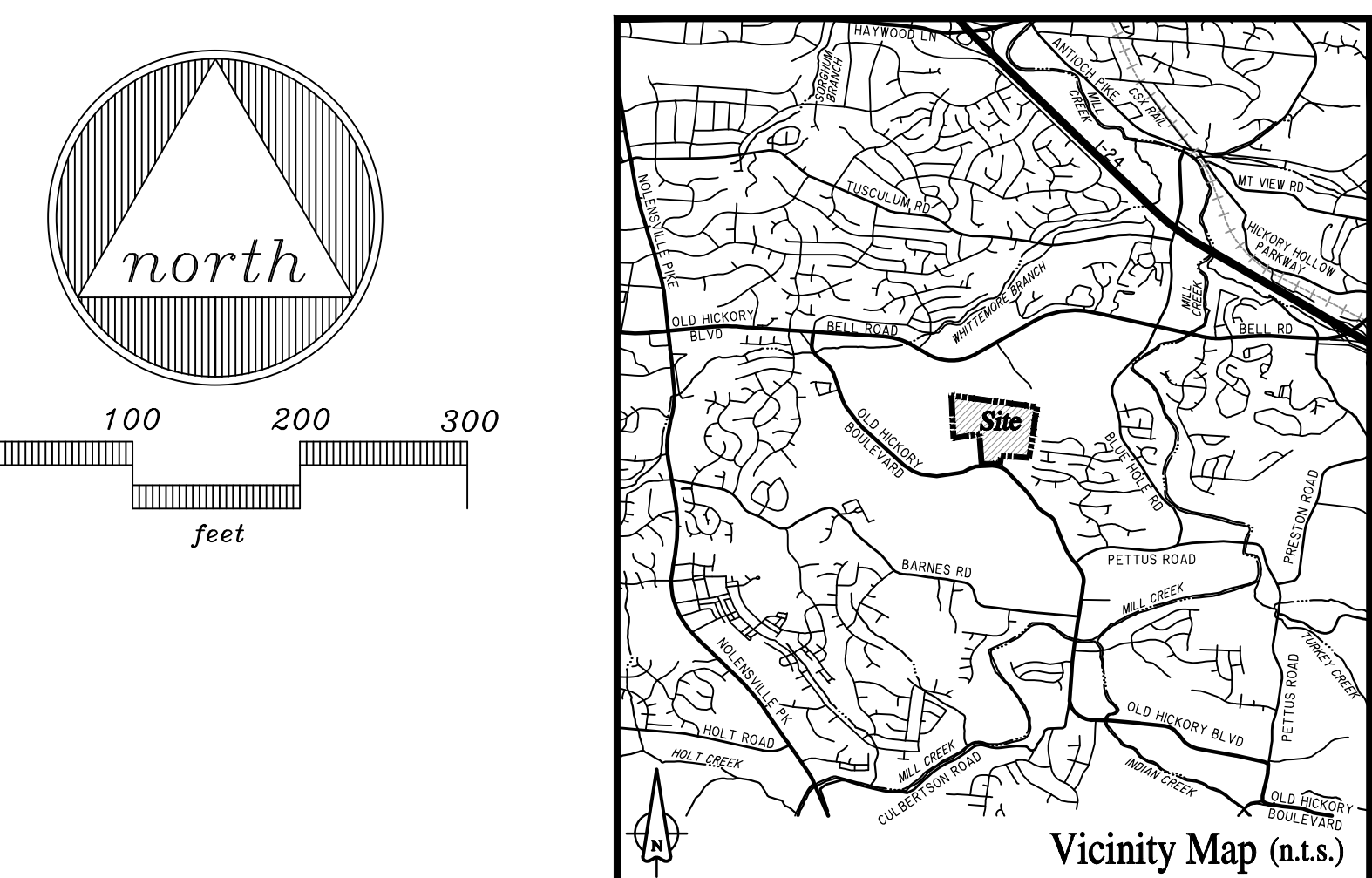
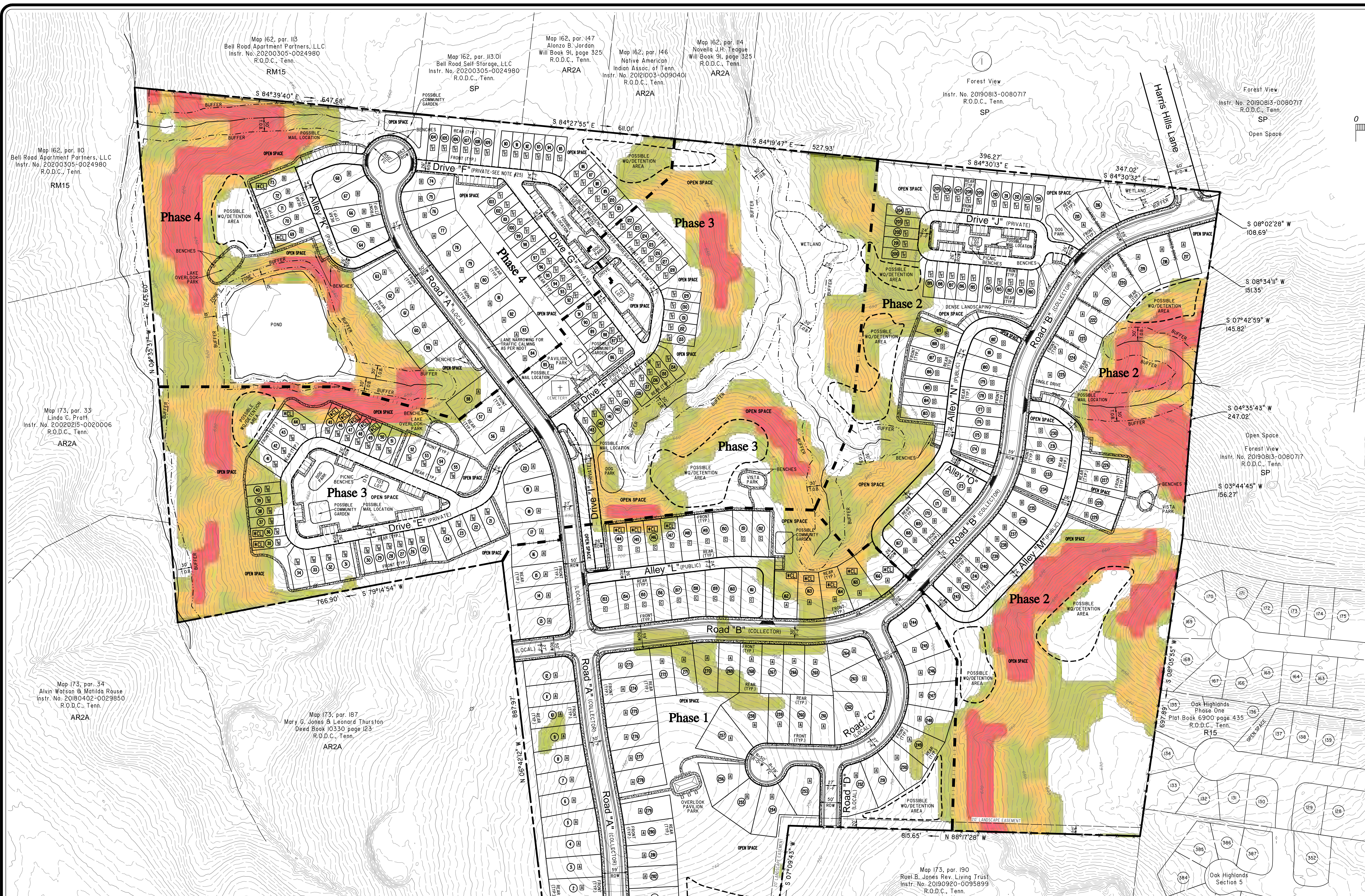
8" - S	SANITARY SEWER W/ MANHOLE
8" - W	WATER LINE
8" - F	FIRE HYDRANT
8" - V	GATE VALVE
8" - P	UTILITY POLE
8" - S	SIDEWALK
8" - A	ADA COMPLIANT HANDICAP RAMP
8" - F	FENCE
8" - F	FACE OF CURB TO FACE OF CURB
8" - R	RIGHT OF WAY
8" - P.U.D.E.	PUBLIC UTILITY AND DRAINAGE EASEMENT



#### GENERAL NOTES:

- THE PURPOSE OF THIS PLAN IS TO CREATE A 283 LOT/UNIT SINGLE-FAMILY RESIDENTIAL NEIGHBORHOOD.
- SITE CONTAINS 80.07 ACRES.
- SITE IS LOCATED ON PROPERTY MAP 173, PARCEL 36, R.O.D.C.
- EXISTING ZONING: AREA
- PROPOSED ZONING: SP
- EXISTING LAND USE POLICIES: T3-NEIGHBORHOOD EVOLVING (N) CO-CONSERVATION
- COUNCIL DISTRICT 31 - COUNCIL MEMBER MR. JOHN RUTHERFORD
- PROPERTY OWNER: DON CAMERON, III  
1503 COLUMBIA AVENUE  
FRANKLIN, TN 37064  
NSTR. NO. 2005012-040598, R.O.D.C.
- OWNER/DEVELOPER: CAMERON PROPERTIES  
1503 COLUMBIA AVENUE  
FRANKLIN, TN 37064  
CONTACT: MR. DON CAMERON  
(615) 791-0053
- BOUNDARY INFORMATION BASED ON EXISTING FINAL PLATS, PROPERTY MAPS, DEEDS, AND BOUNDARY SURVEY PERFORMED BY ANDERSON, DELK, EPPS & ASSOCIATES, INC.
- TOPOGRAPHIC CONTOURS TAKEN FROM USGS NATIONAL ELEVATION DATA SET.
- PROPERTY IS LOCATED IN ZONE "X" ON F.E.M.A. COMMUNITY PANELS NO. 47037C0 367H AND NO. 47037C0 399H, BOTH DATED APRIL 5, 2017.
- ANY EXCAVATION, FILL, OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO. 10-25-21 AND APPROVED BY THE METRO DEPARTMENT OF WATER SERVICES.
- METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED INGRESS AND EGRESS AT ALL TIMES IN ORDER TO MAINTAIN, REPAIR, REPLACE, AND INSPECT ANY STORMWATER FACILITIES WITHIN THE PROPERTY.
- ALL LOTS SHALL BE SERVED BY PUBLIC WATER AND SEWER.
- FOR ANY DEVELOPMENT STANDARDS, REGULATIONS, AND REQUIREMENTS NOT SPECIFICALLY SHOWN ON THE SP PLAN AND/OR INCLUDED AS A CONDITION OF COMMISSION APPROVAL, THE PROPERTY SHALL BE SUBJECT TO THE STANDARDS, REGULATIONS, AND REQUIREMENTS OF THE RM4 ZONING DISTRICT AT THE EFFECTIVE DATE OF THIS ORDINANCE, WHICH MUST BE SHOWN ON THE PLAN.
- SIZE DRIVEWAY CULVERTS PER THE DESIGN CRITERIA SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL (MINIMUM DRIVEWAY CULVERT SIZE IN METRO ROW IS 15')
- ALL SIDEWALKS SHALL BE CONSTRUCTED IN CONFORMANCE WITH METRO PUBLIC WORKS SIDEWALK STANDARDS.
- DETENTION / WATER QUALITY AREAS ARE TO BE DETERMINED WITH FINAL SITE DEVELOPMENT PLANS AND WILL MEET THE REQUIREMENTS SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL. AREAS ARE SUBJECT TO CHANGE WITH FINAL DESIGN.
- NO GRADING, STRIPPING, FILLING, OR OTHER DISTURBANCE OF THE NATURAL GROUND COVER SHALL TAKE PLACE PRIOR TO THE APPROVAL OF AN EROSION CONTROL PLAN.
- THE DEVELOPMENT OF THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS OF THE ADOPTED TREE ORDINANCE NO. 2008-329 (METRO CODE CHAPTER 17.04, ARTICLE 8, TREE PROTECTION AND REPLACEMENT PROCEDURES).
- SITE WILL BE DEVELOPED IN SEVERAL PHASES BASED ON MARKET DEMAND. THE SIZE, LOT COUNT, AND PHASE LINE LOCATIONS ARE SUBJECT TO CHANGE WITH FINAL DESIGN.
- ALL OPEN SPACE IS A PUBLIC UTILITY AND DRAINAGE EASEMENT.
- THE FINAL SITE PLAN/BUILDING PERMIT SITE PLAN SHALL DEPICT THE REQUIRED PUBLIC SIDEWALKS, ANY REQUIRED GRASS STRIP OR FRONTAGE ZONE, AND THE LOCATION OF ALL EXISTING AND PROPOSED VERTICAL OBSTRUCTIONS WITHIN THE REQUIRED SIDEWALK AND GRASS STRIP OR FRONTAGE ZONE. PRIOR TO THE ISSUANCE OF USE AND OCCUPANCY PERMITS, EXISTING VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUTSIDE OF THE REQUIRED SIDEWALK. VERTICAL OBSTRUCTIONS ARE ONLY PERMITTED WITHIN THE REQUIRED GRASS STRIP OR FRONTAGE ZONE.
- PRIVATE DRIVE "F" IS ALSO A PUBLIC EMERGENCY ACCESS, INGRESS, AND EGRESS EASEMENT.
- BUILDING FACADES SHALL BE CONSTRUCTED OF BRICK, BRICK VENER, STONE, CAST STONE, CEMENTITIOUS SIDING, GLASS, OR MATERIALS SUBSTANTIALLY SIMILAR IN FORM AND FUNCTION, UNLESS OTHERWISE APPROVED ON DETAILED BUILDING ELEVATIONS INCLUDED WITH THE PRELIMINARY SP.
- BUILDING FACADES SHALL BE CONSTRUCTED OF BRICK, BRICK VENER, STONE, CAST STONE, CEMENTITIOUS SIDING, GLASS, OR MATERIALS SUBSTANTIALLY SIMILAR IN FORM AND FUNCTION, UNLESS OTHERWISE APPROVED ON DETAILED BUILDING ELEVATIONS INCLUDED WITH THE PRELIMINARY SP.
- THE BUFFER ALONG WATERWAYS WILL BE AN AREA WHERE THE SURFACE IS LEFT IN A NATURAL STATE, AND IS NOT DISTURBED BY CONSTRUCTION ACTIVITY. THIS IS IN ACCORDANCE WITH THE STORMWATER MANAGEMENT MANUAL VOLUME 1 - REGULATIONS.
- DRAWING FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT. AS IT PERTAINS TO STORMWATER APPROVAL / COMMENTS ONLY. THE FINAL LOT COUNT AND DETAILS OF THE PLANS SHALL BE GOVERNED BY THE APPROPRIATE STORMWATER REGULATIONS AT THE TIME OF FINAL APPLICATION.
- PRIVATE CURB SIDE SOIL WASTE AND RECYCLING COLLECTION WILL BE PROVIDED. PRIVATE HAULER FOR THE DEVELOPMENT'S WASTE MANAGEMENT PLAN SHALL BE ESTABLISHED IN THE RECORDED HOA DOCUMENTS WITH BUILDING PERMIT APPROVALS.
- USPS CENTRALIZED MAILBOXES ARE TO BE INSTALLED AS PER USPS GUIDELINES. LOCATION TO BE COORDINATED WITH USPS TO FIND A SUITABLE LOCATION. VEHICLES SHOULD BE OUT OF ROADWAY WHEN ACCESSING MAILBOX CLUSTERS.
- FINAL CONSTRUCTION PLANS AND ROAD GRADES SHALL COMPLY WITH THE DESIGN REGULATIONS ESTABLISHED BY THE DEPARTMENT OF PUBLIC WORKS / NASHVILLE DEPARTMENT OF TRANSPORTATION. SLOPES ALONG ROADWAYS SHALL NOT EXCEED 3%.
- ALL FRONT LOADED UNITS LOCATED ALONG COLLECTION ROADS SHALL HAVE ACCESS FROM ADJACENT SIDE STREETS WHERE POSSIBLE OR HAVE SHARED DRIVEWAYS SHOWN ON THE PLAN. UNITS 110, 163-164, 215-224, 265-272, 274-277, AND 280-288 SHALL HAVE SHARED DRIVES. UNITS 12, 162, 166, 244, AND 264 SHALL HAVE DRIVEWAY ACCESS LOCATED ON AN ADJACENT SIDE STREET. UNITS 11, 165, 225, 273, 278, AND 279 SHALL HAVE A SINGLE DRIVE.
- TAPER LENGTH FOR LEFT HAND TURN LANE INTO THE SITE FROM OLD HICKORY BLVD SHALL MEET AASHTO STANDARDS FOR 35 MPH.
- CRITICAL LOTS. PRIOR TO THE ISSUANCE OF ANY BUILDING PERMIT FOR A LOT DESIGNATED AS A CRITICAL LOT, A CRITICAL LOT PLAN SHALL BE SUBMITTED TO THE METRO PLANNING DEPARTMENT IN CONFORMANCE WITH METRO SUBDIVISION REGULATIONS.





- Legend**
- = Slopes 15%-20%
  - = Slopes 20%-25%
  - = Slopes 25%+
  - = Critical Lot
- SLOPES TAKEN FROM METRO GIS MAP

**Road Section Details**

Case No. 2021SP-080-001  
**Preliminary Development Plan**  
*Cothorn Property*  
 Sheet 2 of 3

**Proposed SP Development**  
 31st Councilmanic District  
 Nashville, Davidson County, Tennessee

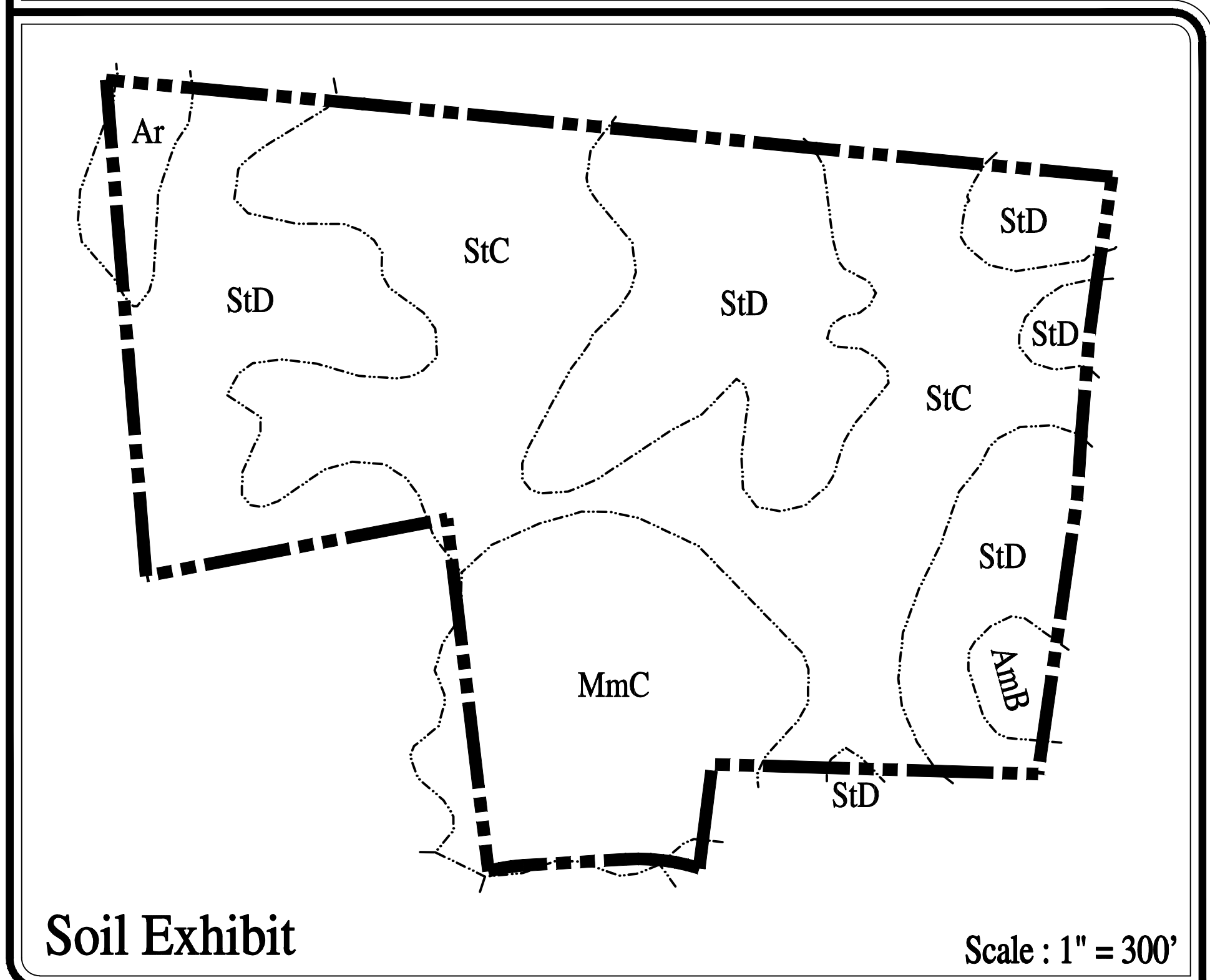
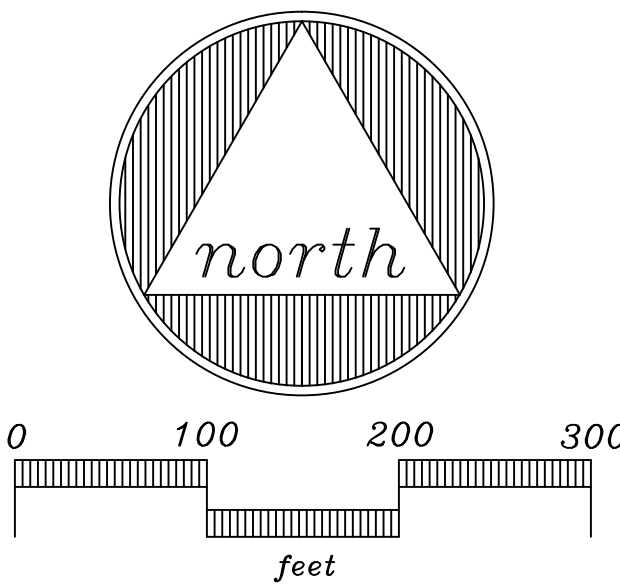
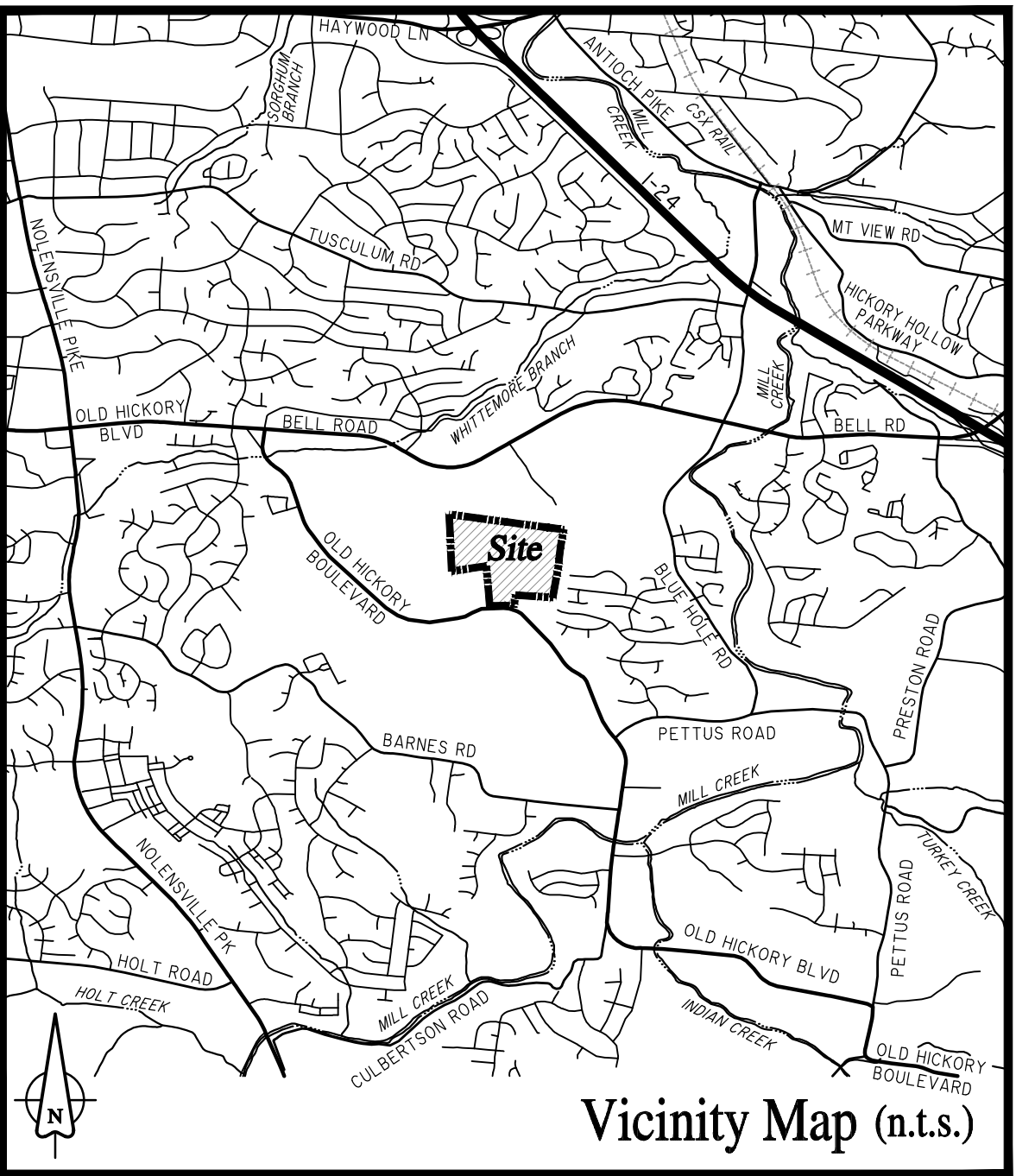
developer  
**Cameron Properties**  
 1503 Columbia Avenue  
 Franklin, TN 37064

Date: 06 - 21 - 21    Scale : 1" = 100'

**Anderson, Delk, Epps & Associates Inc.**  
 618 Grassmere Park Drive, Suite 4  
 Nashville, Tennessee 37211  
 (615) 331-0809

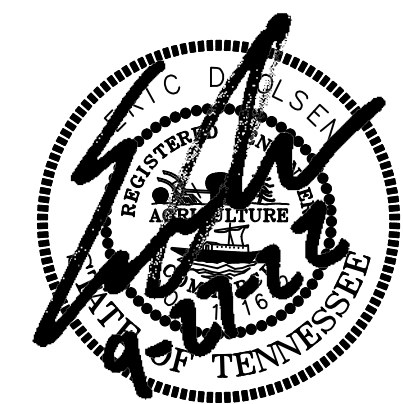
REVISED : 10-25-21 as per Metro Planning  
 11-23-21 as per Metro Planning  
 11-30-21 as per Metro Planning  
 12-02-21 as per Metro Planning  
 03-07-22 as per Community Meeting  
 04-26-22 as per Community Meeting





**NOTES :**

1. DRAWING IS FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT, AS IT PERTAINS TO STORMWATER APPROVAL / COMMENTS ONLY. THE FINAL LOT COUNT AND DETAILS OF THE PLANS SHALL BE GOVERNED BY THE APPROPRIATE STORMWATER REGULATIONS AT THE TIME OF FINAL APPLICATION. GRADE LINES SHOWN ARE PRELIMINARY AND SUBJECT TO CHANGE WITH FINAL DESIGN.
2. THE BUFFER ALONG WATERWAYS WILL BE AN AREA WHERE THE SURFACE IS LEFT IN A NATURAL STATE, AND IS NOT DISTURBED BY CONSTRUCTION ACTIVITY. THIS IS IN ACCORDANCE WITH THE STORMWATER MANAGEMENT MANUAL VOLUME 1 - REGULATIONS.
3. [HCL] DENOTES CRITICAL LOTS. PRIOR TO THE ISSUANCE OF ANY BUILDING PERMIT FOR A LOT DESIGNATED AS A CRITICAL LOT, A CRITICAL LOT PLAN SHALL BE SUBMITTED TO THE METRO PLANNING DEPARTMENT IN CONFORMANCE WITH METRO SUBDIVISION REGULATIONS.



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04-26-22 as per Community Meeting

Case No. 2021SP-080-001  
**Preliminary Development Plan**  
**Cothorn Property**  
Sheet 3 of 3

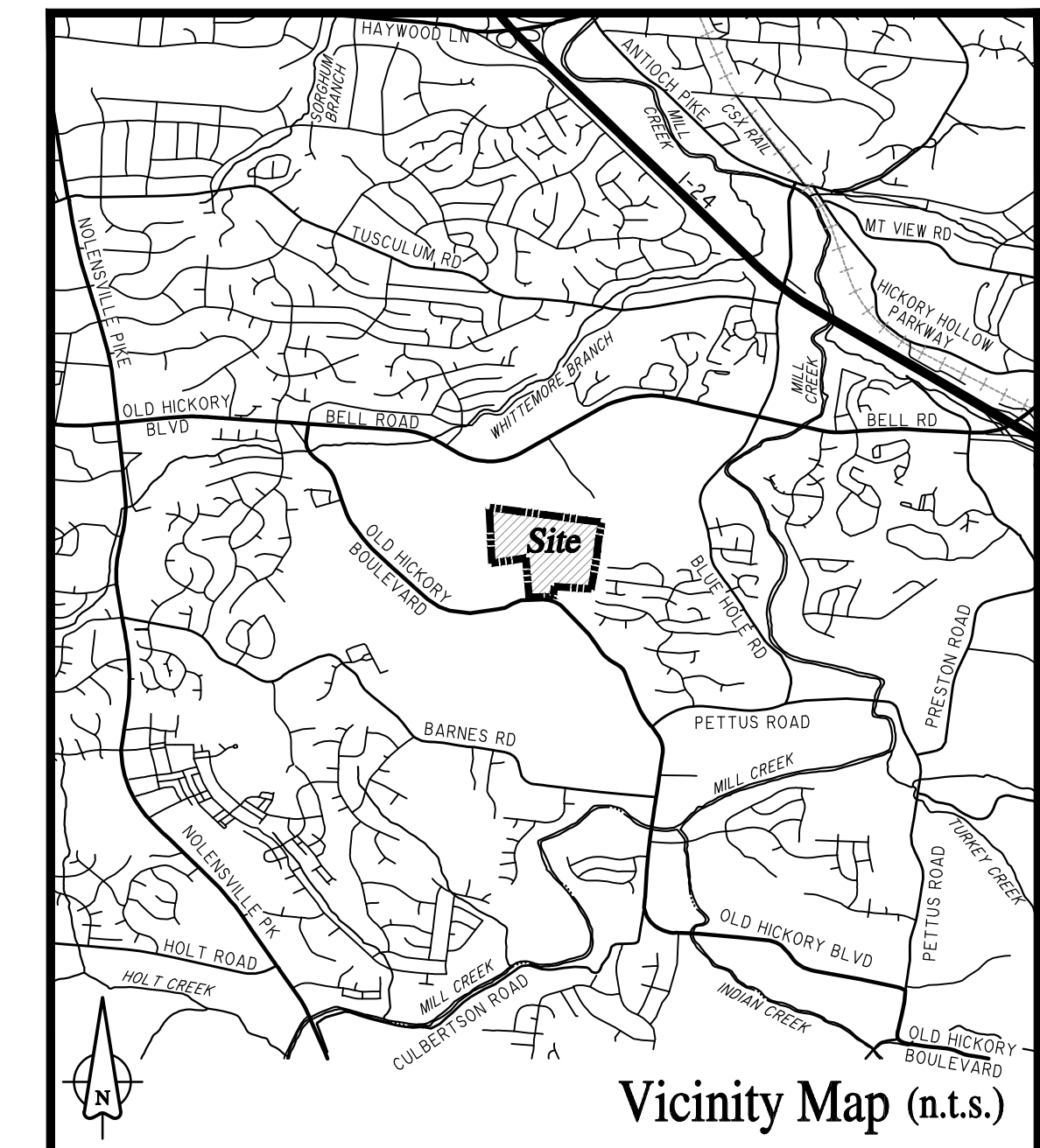
**Proposed SP Development**  
31st Councilmanic District  
Nashville, Davidson County, Tennessee

developer  
**Cameron Properties**  
1503 Columbia Avenue  
Franklin, TN 37064

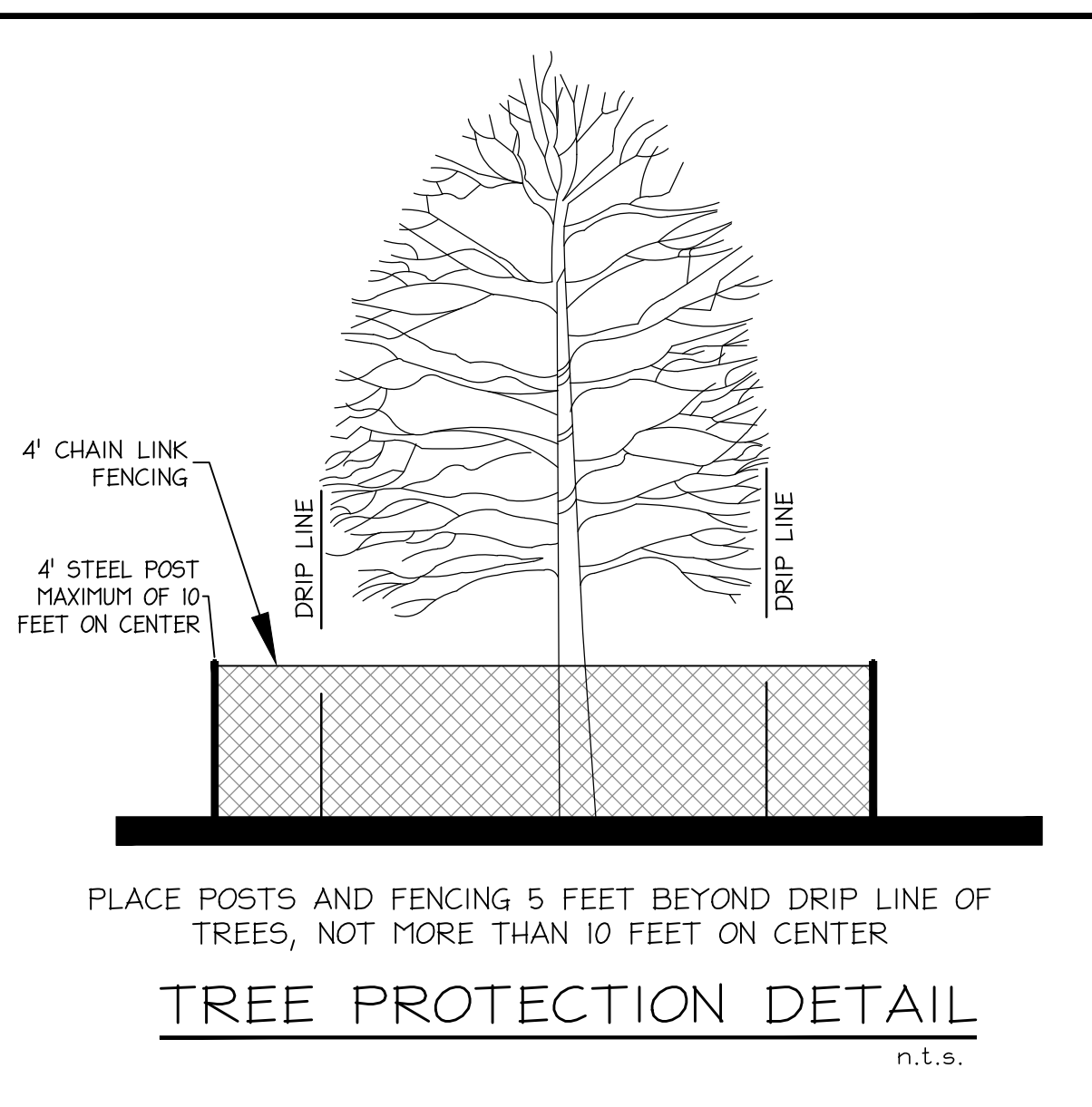
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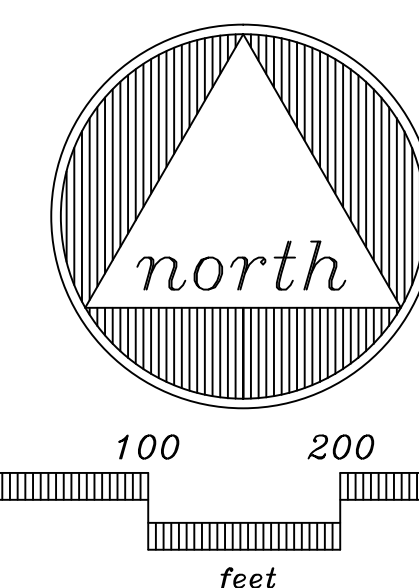
11. Street trees to be planted at 50 foot intervals along roadways (50' & 59' R-O-W). This includes Road A, B, C, D and P.



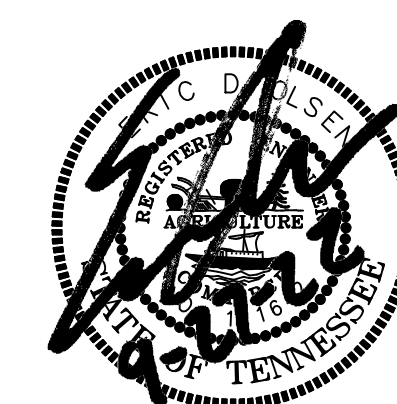
**Anderson, Delk, Epps & Associates Inc.**  
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Nashville, Tennessee 37211  
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EVERGREEN SCREENING:  
Cryptomeria japonica -- Cryptomeria  
Juniperus virginiana -- Eastern Red Cedar  
Magnolia grandiflora -- Southern Magnolia  
Pinus strobus -- White Pine  
Pinus taeda -- Loblolly Pine  
Thuja plicata 'Green Giant' -- Green Giant Arborvitae

Existing Trees 4 inches in caliper DBH and greater shall be the trees required above provided that they are in healthy condition at the time of inspection at completion of the project by the Landscape Architect. If there are 4.9 to 9.9 trees per 100' of Linear Foot after construction, 3.5 Canopy Trees and 1.4 Undersory Trees must be installed for each 100 linear feet (e.g. 3.9 existing trees remain, so no Canopy Trees and 1 Undersory Tree must be installed). If there are 10.0 or more trees per 100' of Linear Foot, the trees must be relatively uniformly distributed. (e.g. for 300 feet of typical Planting Area there are 14.7 trees clustered within a 50 foot area, those trees will count for 100 feet of the area). If there are 10.0 or more trees per 100' of Linear Foot, the Planting Area must have 3.5 Canopy Trees and 1.4 Undersory Trees installed per 100 feet as required above). If the area is heavily forested and planting shrubs is not feasible, then it is possible that a lesser number of trees, mostly shrubs, may be required.



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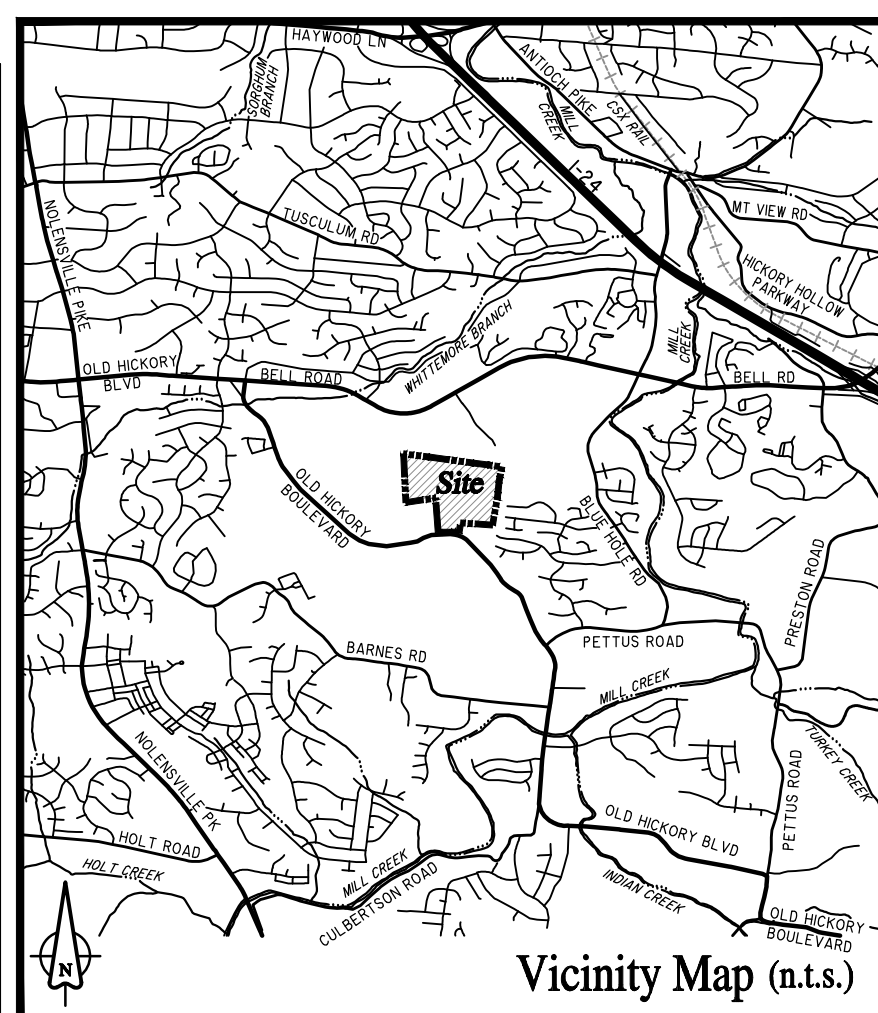






SP Development Summary

● COUNCIL DISTRICT:	31st
● COUNCIL MEMBER:	Mr. John Rutherford
● DEVELOPER:	Cameron Properties 1503 Columbia Avenue Franklin, TN 37064 (615) 791-0809 contact : Mr. Don R. Cameron, III
● OWNER:	Map 173, Parcel 36 Don Cameron, III 1503 Columbia Avenue Franklin, TN 37064 Instr. No. 2005121-0140598 Register's Office, Davidson Co., TN
● OVERLAY DISTRICT:	OV-AIR (Airport Overlay)
● SP NAME:	Cothorn Property
● SP NUMBER:	2021SP-080-001
● PLAN PREPARATION DATE:	06-21-21
REVISIONS:	10-25-21 : II-23-21 11-30-21 : 12-02-21 03-07-22 : 09-26-22
● SCALE:	1" = 100'
● SHEET NUMBER:	SHEET 1 OF 1
● PLANNER/SURVEYOR:	Anderson, Delk, Eggs & Associates, Inc. 618 Grassmere Park Drive, Suite 4 Nashville, TN 37211 phone : (615) 331-0809 fax : (615) 331-0100 e-mail : andersondelk@bellstate.net
● FEMA MAP NO.	47037C0387H, ZONE "X" (Dated 4/5/2017) 47037C0391H, ZONE "X" (Dated 4/5/2017)



Legend

- 8" = 1' S  
8" = 1' W  
SANITARY SEWER W/ MANHOLE  
WATER LINE  
FIRE HYDRANT  
GATE VALVE  
UTILITY POLE  
SIDEWALK  
ADA COMPLIANT HANDICAP RAMP  
FENCE  
X = FACE OF CURB TO FACE OF CURB  
F-C = FACE OF CURB  
ROW = RIGHT OF WAY  
P.U.D.E. = PUBLIC UTILITY AND DRAINAGE EASEMENT



Appropriate Residential Building Materials

SEE SP BOOK FOR ADDITIONAL INFORMATION

APPROPRIATE BUILDING CLADDING (FACADE WALL):

- BRICK VENEER
- PAINTED BRICK VENEER
- CULTURED STONE (ASHLAR PATTERN, DRystack PATTERN)
- CEMENTITIOUS AND FIBER CEMENT COMPOSITE SIDING (E.G. HARDIE, CERTAINTeed)
- APPLIED CEMENT STUCCO

APPROPRIATE WINDOWS AND DOORS:

- PRE-FINISHED ALUMINUM AND VINYL CLAD WOOD WINDOWS
- VINYL WINDOWS (UPON ARC REVIEW)
- CLEAR OR SLIGHTLY TINTED GLAZING
- TRUE DIVIDED LIGHTS
- SIMULATED DIVIDED LIGHTS
- SHUTTERS SIZED TO FIT WINDOW OPENING WHEN CLOSED

APPROPRIATE ROOFS:

- SLOPED ROOFS OF ASPHALT OR FIBERGLASS COMPOSITE SHINGLES
- STANDING SEAM METAL
- NATURAL OR SYNTHETIC SLATE
- SHINGLE-STYLED CONCRETE TILE

APPROPRIATE ARCHITECTURAL DETAILING:

- PAINTED OR STAINED WOOD
- HIGH DENSITY RIGID POLYURETHANE (HRP)
- SMOOTH SURFACE COMPOSITE OR CELLULAR PVC OR COMPARABLE IN STANDARD WOOD TRIM SIZES
- VINYL SOFFIT & FASCIA
- CAST STONE
- COPPER

Not Permitted Residential Building Materials

SEE SP BOOK FOR ADDITIONAL INFORMATION

INAPPROPRIATE BUILDING CLADDING (FACADE WALL):

- OVERSIZED BRICK - NON MODULAR
- EXTERIOR INSULATION FINISH SYSTEMS (EIFS)
- METAL SIDING
- VINYL SIDING
- CONCRETE MASONRY UNITS (CMU) OR SPLIT-FACED
- CONCRETE PANELS

INAPPROPRIATE WINDOWS AND DOORS:

- GLASS BLOCK (ON FRONTS)
- DARK TINTED, COLOR TINTED OR MIRROR FINISHED GLAZING
- SHUTTERS THAT ARE NOT SIZED TO FIT THE WINDOW OPENING WHEN CLOSED

INAPPROPRIATE ROOFS:

- SPANISH TILE
- WOOD SHINGLE

INAPPROPRIATE ARCHITECTURAL DETAILING:

- EXTRUDED POLYSTYRENE
- PVC

NOTE: ALL HOMES WILL BE FOR SALE

# EXHIBIT

Case No. 2021SP-080-001

## Preliminary Development Plan

# Cothorn Property

### Proposed SP Development

31st Councilmanic District  
Nashville, Davidson County, Tennessee

developer

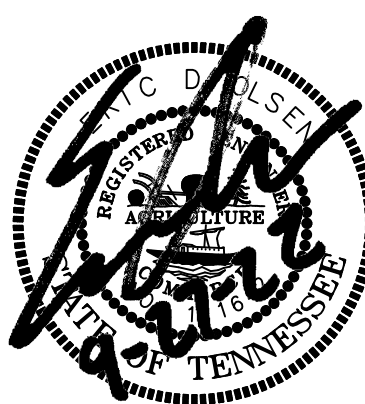
**Cameron Properties**

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Date: 06 - 21 - 21 Scale : 1" = 100'

**Anderson, Delk, Eggs & Associates Inc.**

618 Grassmere Park Drive, Suite 4  
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### Site Data

Total Site Area	80.07 ± Ac.
Developed Land Area Usage	Proposed Right-of-Way Dedication = 0.08 ± Ac. Proposed Right-of-Way (Roads and Alleys) and Private Drives = 10.5 ± Ac. Total Open Space = 43.3 ± Ac. Total Area of Lots = 26.2 ± Ac.
Open Space Usage	Total Open Space = 43.3 ± Ac. (54.1 % of Site) Stream Buffers = 7.5 ± Ac. Landscape Easements = 0.5 ± Ac. Estimated Detention / Water Quality Areas = 4.7 ± Ac. Usable Open Space = 30.6 ± Ac.
Number of Units / Lots Proposed	283 Units
Proposed Density	3.53 Units / Ac.

NOTE: THE DETENTION / WATER QUALITY AREAS ARE SUBJECT TO CHANGE AND ARE TO BE DETERMINED WITH FINAL SITE DEVELOPMENT PLANS. THEY WILL MEET THE REQUIREMENTS SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL.

Unit and Phase Data	PHASE 1	PHASE 2	PHASE 3	PHASE 4	TOTAL
ACREAGE	18.99 Acs	23.57 Acs	21.56 Acs	15.95 Acs	80.07 Acs
LOT / UNIT TYPE "A"	61	10	7	14	93 (33%)
LOT / UNIT TYPE "B"	0	20	0	13	33 (12%)
LOT / UNIT TYPE "C"	18	0	0	0	18 (6%)
LOT / UNIT TYPE "D"	0	21	0	0	21 (7%)
LOT / UNIT TYPE "E"	0	25	28	30	83 (29%)
LOT / UNIT TYPE "F"	0	0	35	0	35 (12%)
TOTAL NUMBER OF LOTS / UNITS	79	77	70	57	283

NOTE: THE SIZE, LOT COUNT, AND PHASE LINE LOCATIONS ARE SUBJECT TO CHANGE WITH FINAL DESIGN BASED ON MARKET DEMAND. ALL HOMES WILL BE FOR SALE.

### Bulk Standards (FOR ADDITIONAL BULK STANDARDS AND REGULATIONS, SEE SP BOOK)

BULK STANDARD	SINGLE-FAMILY LOT TYPE "A" (Detached Fronting Public R-O-W) (Lot Nos. 1-21, 97-144, 178-186, 193-195, 245-250, 259-277, 279-292)	SINGLE-FAMILY LOT TYPE "B" (Detached Rear Loaded Alley) (Lot Nos. 97-144, 178-186, 193-195, 245-250, 259-277, 279-292)	SINGLE-FAMILY LOT TYPE "C" (Detached Rear Loaded Alley) (Lot Nos. 145-162)	SINGLE-FAMILY LOT TYPE "D" (Rear Loaded Attached Townhome) (Lot Nos. 163-182)	SINGLE-FAMILY LOT TYPE "E" (Front Loaded Attached Townhome) (Lot Nos. 97-144, 178-186, 193-195, 245-250, 259-277, 279-292)	SINGLE-FAMILY LOT TYPE "F" (Rear Loaded Attached Townhome) (Lot Nos. 163-182)
Front Yard Setback from Property Line	20 ft.	5 ft. - 20 ft. Build-to-Line from Right-of-Way or Open Space	5 ft. - 15 ft. Build-to-Line from Right-of-Way or Open Space	5 ft. - 10 ft. Build-to-Line from Right-of-Way or Open Space	20 ft. from Right-of-Way	0 ft. - 10 ft. Build-to-Line from Open Space
Side Yard Setback from Property Line	3 ft. Minimum (Lot / Open Space) 5 ft. Minimum (ROW / Alley / Drive)	3 ft. Minimum (Lot / Open Space) 5 ft. Minimum (ROW / Alley / Drive)	3 ft. Minimum (Lot / Open Space) 5 ft. Minimum (ROW / Alley / Drive)	3 ft. Minimum (Lot / Open Space) 5 ft. Minimum (ROW / Alley / Drive)	10 ft. Minimum Between Buildings	10 ft. Minimum Between Buildings
Rear Yard Setback from Property Line	20 ft.	20 ft.	20 ft.	20 ft. from Right-of-Way	0 ft.	5 ft. or 20 ft. from Right-of-Way
Typical Lot Size	50 ± ft. x 110 ± ft.	32 ± ft. x 110 ± ft.	40 ± ft. x 105 ± ft.	30 ± ft. x 105 ± ft.	Front Varies x 80 ± ft.	Front Varies x 80 ± ft.
Minimum Lot Area	5,200 sq. ft.	3,400 sq. ft.	4,200 sq. ft.	3,100 sq. ft.	2,000 sq. ft.	2,000 sq. ft.
Building Height	Minimum: 1 Story Maximum: 3 Stories	Minimum: 1 Story Maximum: 3 Stories	Minimum: 1 Story Maximum: 3 Stories	Minimum: 1 Story Maximum: 3 Stories	Minimum: 1 Story Maximum: 3 Stories	Minimum: 1 Story Maximum: 3 Stories
Parking Requirements	2 On-site Spaces per Single-Family Home			2 On-site Spaces per Residential Unit		

### Right of Way, Drive, and Alley Information

SEE DETAILS ON PRELIMINARY SP PLAN SHEET 2 OF 2

PROPOSED COLLECTOR (PART OF ROAD "A" AND PART OF ROAD "B"):	59 FT. PAVEMENT WIDTH 30 FT.
PROPOSED LOCAL ROADS (PART OF ROAD "A", PART OF ROAD "B", AND ROADS "C" & "D"):	50 FT. PAVEMENT WIDTHS 27 FT.
HARRIS HILL LANE:	60 FT. PAVEMENT WIDTH 37 FT.
PROPOSED PUBLIC ALLEYS (ALLEYS "K", "L", "M", AND "N"):	26 FT. PAVEMENT WIDTHS 24 FT.
PROPOSED PRIVATE DRIVES (DRIVES "E", "F", "G", "H", AND "J"):	26 FT. PAVEMENT WIDTHS 24 FT.
PROPOSED PUBLIC ALLEYS (ALLEYS "O"):	20 FT. PAVEMENT WIDTHS 18 FT.
PROPOSED PRIVATE DRIVES (DRIVES "I"):	20 FT. PAVEMENT WIDTHS 18 FT.

### Parking Information

PARKING REQUIRED:	288 SPACES
SINGLE-FAMILY DETACHED HOMES 144 HOMES AT 2 SPACES / HOME	
SINGLE-FAMILY DETACHED TOWNHOMES 27 TOWNHOMES AT 2 SPACES / UNIT	42 SPACES
SINGLE-FAMILY ATTACHED TOWNHOMES 110 TOWNHOMES AT 2 SPACES / TOWNHOME (280)	236 SPACES
TOTAL PARKING REQUIRED	566 SPACES