

GRANT APPLICATION SUMMARY SHEET

Grant Name: Lebanon Pike Multimodal Access 23-26

Department: NDOT

Grantor: TENNESSEE DEPARTMENT OF TRANSPORTATION

**Pass-Through Grantor
(If applicable):**

Total Applied For: \$1,125,000.00

Metro Cash Match: \$125,000.00

Department Contact: Casey Hopkins
880-1676

Status: NEW

Program Description:

For the construction phase of Lebanon Pike sidewalk project. The project will involve the construction of an 8-foot sidewalk and a 4-foot grass furnishing zone with curb and gutter on the north side of Lebanon Pike. In addition, signal upgrades will be made at the intersections of McGavock Pike and Graylynn Drive that will also include ADA ramps, crosswalk markings, and pedestrian signals.

Plan for continuation of services upon grant expiration:

N/A

**APPROVED AS TO AVAILABILITY
OF FUNDS:**

DocuSigned by:
Kelly Flannery 7/18/2022
Director of Finance Date ^{DS} TE

**APPROVED AS TO FORM AND
LEGALITY:**

DocuSigned by:
Tara Ladd 7/18/2022
Metropolitan Attorney Date ^{DS} TE

**APPROVED AS TO RISK AND
INSURANCE:**

DocuSigned by:
Balogun Cobb 7/18/2022
Director of Risk Management Date ^{DS} TE
Services

DocuSigned by:
John Cooper 7/18/2022
Metropolitan Mayor Date ^{DS} TE
(This application is contingent upon approval of the application by the Metropolitan Council.)

Grants Tracking Form

Part One

Pre-Application <input type="radio"/>		Application <input checked="" type="radio"/>		Award Acceptance <input type="radio"/>		Contract Amendment <input type="radio"/>	
Department	Dept. No.	Contact				Phone	Fax
NDOT	042	Casey Hopkins				880-1676	
Grant Name:		Lebanon Pike Multimodal Access 23-26					
Grantor:		TENNESSEE DEPARTMENT OF TRANSPORTATION				Other:	
Grant Period From:		01/01/23	(applications only) Anticipated Application Date:		06/01/22		
Grant Period To:		01/01/26	(applications only) Application Deadline:		07/19/22		
Funding Type:	STATE	Multi-Department Grant		<input type="checkbox"/> If yes, list below.			
Pass-Thru:	Select Pass-Thru --- >	Outside Consultant Project:		<input type="checkbox"/>			
Award Type:	COMPETITIVE	Total Award:		\$1,125,000.00			
Status:	NEW	Metro Cash Match:		\$125,000.00			
Metro Category:	New Initiative	Metro In-Kind Match:					
CFDA #	n/a	Is Council approval required?		<input checked="" type="checkbox"/>			
Project Description:		Applic. Submitted Electronically?		<input checked="" type="checkbox"/>			
<p>For the construction phase of Lebanon Pike sidewalk project. The project will involve the construction of an 8-foot sidewalk and a 4-foot grass furnishing zone with curb and gutter on the north side of Lebanon Pike. In addition, signal upgrades will be made at the intersections of McGavock Pike and Graylynn Drive that will also include ADA ramps, crosswalk markings, and pedestrian signals.</p>							
Plan for continuation of service after expiration of grant/Budgetary Impact:							
N/A							
How is Match Determined?							
Fixed Amount of \$	\$125,000.00	or	10.0%	% of Grant	Other: <input type="checkbox"/>		
Explanation for "Other" means of determining match:							
For this Metro FY, how much of the required local Metro cash match:							
Is already in department budget?		Yes	Fund		42021	Business Unit	42409021
Is not budgeted?			Proposed Source of Match:		FY21 CSP		
(Indicate Match Amount & Source for Remaining Grant Years in Budget Below)							
Other:							
Number of FTEs the grant will fund:		0.00	Actual number of positions added:		0.00		
Departmental Indirect Cost Rate		18.83%	Indirect Cost of Grant to Metro:		\$0.00		
*Indirect Costs allowed? <input type="radio"/> Yes <input checked="" type="radio"/> No		% Allow.	0.00%	Ind. Cost Requested from Grantor:		\$0.00	in budget
*(If "No", please attach documentation from the grantor that indirect costs are not allowable. See Instructions)							
Draw down allowable? <input type="checkbox"/>							
Metro or Community-based Partners:							

Part Two

Grant Budget										
Budget Year	Metro Fiscal Year	Federal Grantor	State Grantor	Other Grantor	Local Match Cash	Match Source (Fund, BU)	Local Match In-Kind	Total Grant Each Year	Indirect Cost to Metro	Ind. Cost Neg. from Grantor
Yr 1	FY24	\$0.00	\$562,500.00	\$0.00	\$62,500.00	42021 42409021	\$0.00	\$625,000.00	\$117,687.50	\$0.00
Yr 2	FY25	\$0.00	\$562,500.00	\$0.00	\$62,500.00	42021 42409021	\$0.00	\$625,000.00	\$117,687.50	\$0.00
Yr 3	FY__									
Yr 4	FY__									
Yr 5	FY__									
Total		\$0.00	\$1,125,000.00	\$0.00	\$125,000.00	42021 42409021	\$0.00	\$1,250,000.00	\$0.00	\$0.00
Date Awarded:					Tot. Awarded:		Contract#:			
(or) Date Denied:					Reason:					
(or) Date Withdrawn:					Reason:					

Contact: trinity.weathersby@nashville.gov
vaughn.wilson@nashville.gov

GCP Rec'd
07/15/22

GCP Approved
07/15/22

VW

Application Requirements:

1. Prior to completing this application, please refer to the [TDOT Multimodal Access Grant Guidelines](#).
2. Project applications MUST be complete and contain all supporting materials as outlined in the application.
3. Projects MUST include a letter of support from the corresponding Rural Planning Organization (RPO) or Metropolitan Planning Organization (MPO).
4. Project applications MUST be accompanied by a letter of support from the county or municipal Mayor or legislative body which includes a written acknowledgment of the applicable local match and future maintenance responsibility. The Multimodal Access Grant is a reimbursable program. Local governments are required to pay 100% of all project invoices up-front prior to seeking reimbursement of the applicable state share.

Contact Name: Casey Hopkins
Phone: (615) 880-1676

Title: Program Manager - Grants Coordinator
Email: casey.hopkins@nashville.gov

Tennessee Metropolitan Planning Organization (MPO) or Rural Planning Organization (RPO):

Select the applicable organization from one of the following dropdown lists:

A map of Tennessee's planning organizations can be found at:

<https://www.tn.gov/tdot/long-range-planning-home/longrange-oct/transportation-long-range-planning-metropolitan-planning-organization-mpo-c.html>.

(Do not select from both lists.)

MPO: Nashville MPO

OR

RPO:

Project Title and Description:

Provide a project title (to reference this project in future documents) and a clear and concise project description.

Lebanon Pike Sidewalk Improvement Project - The project will construct a sidewalk and a grass furnishing zone with curb/gutter on Lebanon Pike. Signal upgrades will be made at the intersections of McGavock Pike and Graylynn Drive including ADA ramps, crosswalk markings, and pedestrian signals.

Funding Breakdown:

Projects located in economically Distressed or At-Risk counties may be match-funded by TDOT up to 95% of the project value, up to a maximum project value of \$1,250,000 and therefore a maximum match of \$1,187,000. Projects located in all other counties of Tennessee may be match funded at 90% of the project value, up to a maximum project value of \$1,250,000 and therefore a maximum match value of \$1,125,000.

To determine if you are in a distressed or at-risk county, please refer to the following webpage:
<https://tnecd.com/research-and-data/publications/>.

Is your local governmental agency located in a distressed or at-risk county based on the current TNECD determination?

☐ Yes ☒ No

A. State Funding Amount	\$1,125,000.00
B. Local Funding Amount	\$125,000.00
C. Total Project Funding Amount	\$1,250,000.00

TDOT Partial Funding Consideration:

TDOT may consider funding only a portion of the proposed project. What reduced scope or project limits would the applicant choose if the proposal had to be revised in this way?

This project has completed Survey, PE, Design, NEPA phases. The project will finish ROW phase by July 2023. It would be difficult to phase out a construction project with the limits being less than a half mile, specifically 0.43 miles. NDOT has programmed \$1,800,000 for the construction phase in the TIP. If we were not awarded the full asking amount for the construction phase, we would look for other means of funding through STBG or Local dollars.

Budget Template:

Attach completed budget template here:

https://tdot.intelligrants.com/_Upload/13198_1144943-2022MultimodalAccessApplication-LebanonPikeSW-supplementalfunding.xlsx

BUDGET TEMPLATE
Estimated Project Costs

Line items can be added or deleted as needed for any stage; however this is the format in which the budget must be submitted.

ITEM DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	TOTAL	90% STATE FUNDS	10% LOCAL EXPENSE (Cash Match Only, Non-Reimbursible)
Preliminary Engineering/Design/Environmental (PE)						
Environmental Documentation, Preliminary Design Plans (Up to 80% of total PE budget)						
Local, State and Federal Permits						
ROW/Construction Plans, Specs. & Estimate (PS&E) Document Completion						
SUBTOTAL						

Right-of-Way/Utilities (ROW)						
ROW Acquisition						
License Agreements, Easements, Recording Fees						
Utility Relocation/Certifications*						
SUBTOTAL						

Site Preparation & Demolition						
CONSTRUCTION STAKES, LINES AND GRADES	LS	1	\$ 44,520.00	\$ 44,520.00	\$ 40,068.00	\$ 4,452.00
CLEARING AND GRUBBING	LS	1	\$ 38,160.00	\$ 38,160.00	\$ 34,344.00	\$ 3,816.00
ROAD & DRAINAGE EXCAVATION (UNCLASSIFIED)	CY	2301	\$ 24.17	\$ 55,615.17	\$ 50,053.65	\$ 5,561.52
BORROW EXCAVATION (UNCLASSIFIED)	CY	100	\$ 49.61	\$ 4,961.00	\$ 4,464.90	\$ 496.10
FURNISHING & SPREADING TOPSOIL	CY	315	\$ 32.25	\$ 10,158.75	\$ 9,142.88	\$ 1,015.88
SEDIMENT REMOVAL	CY	89	\$ 13.97	\$ 1,243.33	\$ 1,119.00	\$ 124.33
Roadway Items						
CURB INLET PROTECTION (TYPE 4)	EACH	16	\$ 193.50	\$ 3,096.00	\$ 2,786.40	\$ 309.60
CATCH BASIN PROTECTION (TYPE D)	EACH	11	\$ 328.52	\$ 3,613.72	\$ 3,252.35	\$ 361.37
MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	1600	\$ 33.85	\$ 54,160.00	\$ 48,744.00	\$ 5,416.00
ASP. CONC. MIX (PG70-22) (BPMB-HM) GR. A-S	TON	77	\$ 228.96	\$ 17,629.92	\$ 15,866.93	\$ 1,762.99
ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING A	TON	99	\$ 222.60	\$ 22,037.40	\$ 19,833.66	\$ 2,203.74
ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING B-M2	TON	216	\$ 178.08	\$ 38,465.28	\$ 34,618.75	\$ 3,846.53
BITUMINOUS MATERIAL FOR PRIME COAT (PC)	TON	1	\$ 1,526.40	\$ 1,526.40	\$ 1,373.76	\$ 152.64
AGGREGATE FOR COVER MATERIAL (PC)	TON	4	\$ 95.40	\$ 381.60	\$ 343.44	\$ 38.16
BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	1	\$ 2,162.40	\$ 2,162.40	\$ 1,946.16	\$ 216.24
SAW CUTTING ASPHALT PAVEMENT	L.F.	2930	\$ 6.36	\$ 18,634.80	\$ 16,771.32	\$ 1,863.48
ACS MIX (PG70-22) GRADING D	TON	133	\$ 222.60	\$ 29,605.80	\$ 26,645.22	\$ 2,960.58
CLASS A CONCRETE (ROADWAY)	C.Y.	3.5	\$ 1,272.56	\$ 4,453.96	\$ 4,008.56	\$ 445.40
STEEL BAR REINFORCEMENT (ROADWAY)	LB	54	\$ 1.53	\$ 82.62	\$ 74.36	\$ 8.26
BOX TUBE SAFETY RAIL	L.F.	461	\$ 224.19	\$ 103,351.59	\$ 93,016.43	\$ 10,335.16
RETAINING WALL (MCGAVOCK PK. STA 100+74.21 LT. TO S.R. 24 STA 13+22.96 LT)	S.F.	1744	\$ 136.84	\$ 238,648.96	\$ 214,784.06	\$ 23,864.90
RETAINING WALL (S.R. 24 STA 17+24.87)	S.F.	39	\$ 136.84	\$ 5,336.76	\$ 4,803.08	\$ 533.68
RETAINING WALL (S.R. 24 STA 18+82.81 TO STA 20+38.01')	S.F.	758	\$ 136.84	\$ 103,724.72	\$ 93,352.25	\$ 10,372.47
RETAINING WALL (S.R. 24 STA 20+63.05 TO STA 21+30.60')	S.F.	15	\$ 136.84	\$ 2,052.60	\$ 1,847.34	\$ 205.26
RETAINING WALL (S.R. 24 STA 21+65.10)	S.F.	159	\$ 136.84	\$ 21,757.56	\$ 19,581.80	\$ 2,175.76
18" CONCRETE PIPE CULVERT (CLASS III)	L.F.	619	\$ 96.62	\$ 59,807.78	\$ 53,827.00	\$ 5,980.78
24" CONCRETE PIPE CULVERT (CLASS III)	L.F.	400	\$ 137.53	\$ 55,012.00	\$ 49,510.80	\$ 5,501.20
CAPPING EXISTING CATCHBASIN	EACH	2	\$ 1,621.80	\$ 3,243.60	\$ 2,919.24	\$ 324.36
CATCH BASINS, TYPE 12, 0' - 4' DEPTH	EACH	1	\$ 4,932.98	\$ 4,932.98	\$ 4,439.68	\$ 493.30
CATCH BASINS, TYPE 12, > 4' - 8' DEPTH	EACH	6	\$ 5,979.90	\$ 35,879.40	\$ 32,291.46	\$ 3,587.94
CATCH BASINS, TYPE 14, 0' - 4' DEPTH	EACH	1	\$ 8,883.65	\$ 8,883.65	\$ 7,995.29	\$ 888.37
CATCH BASINS, TYPE 14, > 4' - 8' DEPTH	EACH	3	\$ 9,855.51	\$ 29,566.53	\$ 26,609.88	\$ 2,956.65
CATCH BASINS, TYPE 14, 8' - 12' DEPTH	EACH	1	\$ 15,687.56	\$ 15,687.56	\$ 14,118.80	\$ 1,568.76
CATCH BASINS, TYPE 42, 0' - 4' DEPTH	EACH	1	\$ 5,478.57	\$ 5,478.57	\$ 4,930.71	\$ 547.86
CATCH BASINS, TYPE 42, > 4' - 8' DEPTH	EACH	1	\$ 6,137.03	\$ 6,137.03	\$ 5,523.33	\$ 613.70
CATCH BASINS, TYPE 42, > 8' - 12' DEPTH	EACH	2	\$ 8,359.16	\$ 16,718.32	\$ 15,046.49	\$ 1,671.83
CONCRETE SIDEWALK (4")	S.F.	10973	\$ 8.13	\$ 89,210.49	\$ 80,289.44	\$ 8,921.05
CONCRETE DRIVEWAY	S.F.	3756	\$ 13.33	\$ 50,067.48	\$ 45,060.73	\$ 5,006.75
CONCRETE CURB RAMP (RETROFIT)	S.F.	24	\$ 50.40	\$ 1,209.60	\$ 1,088.64	\$ 120.96
CONCRETE CURB RAMP	S.F.	888	\$ 29.65	\$ 26,329.20	\$ 23,696.28	\$ 2,632.92

CONCRETE CURB	C.Y.	27	\$ 739.25	\$ 19,959.75	\$ 17,963.78	\$ 1,995.98
CONCRETE COMBINED CURB & GUTTER	C.Y.	159	\$ 530.54	\$ 84,355.86	\$ 75,920.27	\$ 8,435.59
PORTABLE BARRIER RAIL DELINEATOR	EACH	17	\$ 14.45	\$ 245.65	\$ 221.09	\$ 24.57
GUARDRAIL REMOVED	L.F.	68	\$ 6.36	\$ 432.48	\$ 389.23	\$ 43.25
AGGREGATE UNDERDRAINS (WITH PIPE)	L.F.	2285	\$ 8.89	\$ 20,313.65	\$ 18,282.29	\$ 2,031.37
TRAFFIC CONTROL	LS	1	\$ 63,600.00	\$ 63,600.00	\$ 57,240.00	\$ 6,360.00
INTERCONNECTED PORTABLE BARRIER RAIL	L.F.	160	\$ 67.42	\$ 10,787.20	\$ 9,708.48	\$ 1,078.72
FLEXIBLE DRUMS (CHANNELIZING)	EACH	269	\$ 36.90	\$ 9,926.10	\$ 8,933.49	\$ 992.61
SIGNS (CONSTRUCTION)	S.F.	412	\$ 9.95	\$ 4,099.40	\$ 3,689.46	\$ 409.94
TEMPORARY BARRICADES (TYPE III)	L.F.	48	\$ 24.68	\$ 1,184.64	\$ 1,066.18	\$ 118.46
CHANGEABLE MESSAGE SIGN UNIT	EACH	2	\$ 8,785.34	\$ 17,570.68	\$ 15,813.61	\$ 1,757.07
RESET GROUND MOUNTED SIGN	EACH	8	\$ 540.60	\$ 4,324.80	\$ 3,892.32	\$ 432.48
PLASTIC PAVEMENT MARKING (CROSS-WALK)	L.F.	334	\$ 13.62	\$ 4,549.08	\$ 4,094.17	\$ 454.91
PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	12	\$ 31.80	\$ 381.60	\$ 343.44	\$ 38.16
MOBILIZATION	LS	1	\$ 95,151.13	\$ 95,151.13	\$ 85,636.02	\$ 9,515.11
TEMPORARY SEDIMENT TUBE 8IN	L.F.	1870	\$ 5.16	\$ 9,649.20	\$ 8,684.28	\$ 964.92
TEMPORARY SEEDING (WITH MULCH)	UNIT	9	\$ 95.40	\$ 858.60	\$ 772.74	\$ 85.86
WATER (SEEDING & SODDING)	M.G.	3	\$ 56.36	\$ 169.08	\$ 152.17	\$ 16.91
SODDING (NEW SOD)	S.Y.	1887	\$ 10.18	\$ 19,209.66	\$ 17,288.69	\$ 1,920.97
Signal Items						
STREET NAME SIGN (SUSPENDED 0.100IN THICK)	S.F.	128	\$ 46.50	\$ 5,952.00	\$ 5,356.80	\$ 595.20
SUSPENDED FLAT SHEET ALUMINUM SIGN (0.100" THICK)	EACH	6	\$ 633.46	\$ 3,800.76	\$ 3,420.68	\$ 380.08
REMOVAL OF SIGNAL EQUIPMENT	EACH	2	\$ 2,832.64	\$ 5,665.28	\$ 5,098.75	\$ 566.53
SIGNAL HEAD ASSEMBLY (130 WITH BACKPLATE)	EACH	6	\$ 1,156.27	\$ 6,937.62	\$ 6,243.86	\$ 693.76
SIGNAL HEAD ASSEMBLY (150 A2H WITH BACKPLATE)	EACH	11	\$ 1,687.61	\$ 18,563.71	\$ 16,707.34	\$ 1,856.37
SIGNAL HEAD ASSEMBLY (140 A1 WITH BACKPLATE)	EACH	2	\$ 1,190.87	\$ 2,381.74	\$ 2,143.57	\$ 238.17
INSTALL PULL BOX (TYPE B)	EACH	9	\$ 1,001.88	\$ 9,016.92	\$ 8,115.23	\$ 901.69
MODIFY EXISTING ELECTRICAL SERVICE CONNECTION	EACH	2	\$ 2,055.27	\$ 4,110.54	\$ 3,699.49	\$ 411.05
SIGNAL CABLE - 5 CONDUCTOR	L.F.	262	\$ 2.12	\$ 555.44	\$ 499.90	\$ 55.54
SIGNAL CABLE - 7 CONDUCTOR	L.F.	2079	\$ 2.54	\$ 5,280.66	\$ 4,752.59	\$ 528.07
SIGNAL CABLE - 9 CONDUCTOR	L.F.	724	\$ 3.13	\$ 2,266.12	\$ 2,039.51	\$ 226.61
CONDUIT 2" DIAMETER (PVC)	L.F.	91	\$ 16.83	\$ 1,531.53	\$ 1,378.38	\$ 153.15
CONDUIT 3" DIAMETER (PVC)	L.F.	60	\$ 18.99	\$ 1,139.40	\$ 1,025.46	\$ 113.94
CONDUIT 3" DIAMETER (JACK AND BORE)	L.F.	400	\$ 50.88	\$ 20,352.00	\$ 18,316.80	\$ 2,035.20
VEHICLE DETECTOR (RADAR)	EACH	2	\$ 12,456.98	\$ 24,913.96	\$ 22,422.56	\$ 2,491.40
CABINET (EIGHT PHASE BASE MOUNTED)	EACH	2	\$ 24,668.44	\$ 49,336.88	\$ 44,403.19	\$ 4,933.69
CONTROLLER (ATC)	EACH	2	\$ 7,256.16	\$ 14,512.32	\$ 13,061.09	\$ 1,451.23
PEDESTAL POLE (PEDESTRIAN, 10')	EACH	4	\$ 2,824.87	\$ 11,299.48	\$ 10,169.53	\$ 1,129.95
CANTILEVER SIGNAL SUPPORT (2 @ 35' & 45')	EACH	1	\$ 33,583.34	\$ 33,583.34	\$ 30,225.01	\$ 3,358.33
CANTILEVER SIGNAL SUPPORT (1 ARM @ 45')	EACH	1	\$ 23,917.42	\$ 23,917.42	\$ 21,525.68	\$ 2,391.74
CANTILEVER SIGNAL SUPPORT (1 ARM @ 50')	EACH	2	\$ 23,226.72	\$ 46,453.44	\$ 41,808.10	\$ 4,645.34
CANTILEVER SIGNAL SUPPORT (2 @ 30' & 50')	EACH	1	\$ 33,583.34	\$ 33,583.34	\$ 30,225.01	\$ 3,358.33
CANTILEVER SIGNAL SUPPORT (1 ARM @ 65')	EACH	1	\$ 52,728.56	\$ 52,728.56	\$ 47,455.70	\$ 5,272.86
COUNTDOWN PED SGNL HEAD W/AUDIBLE PUSH BUTTON & 15IN SIGN	EACH	10	\$ 1,999.36	\$ 19,993.60	\$ 17,994.24	\$ 1,999.36
SUBTOTAL of Itemized Quantities Above				\$ 1,998,189.15	\$ 1,798,370.24	\$ 199,818.92
Administration Costs						
CONST. Survey & Layout		of CONST		\$ 1,998,189.15	\$ 1,798,370.24	\$ 199,818.92
Construction Contingency		of CONST		\$ 399,637.83	\$ 359,674.05	\$ 39,963.78
		of CONST		\$ 359,674.05	\$ 323,706.64	\$ 35,967.41
***Construction Engineering Inspection (CEI) and Contract/Construction Administration						
TOTAL of Reimbursible Construction Expenses					\$ 2,481,750.93	\$ 275,750.11
GRAND TOTAL ALL PROJECT COSTS				\$ 2,757,501.03		

*Relocation of utilities is eligible for reimbursement only if the relocation is caused by this project and the utilities to be relocated are on private property. Relocation of utilities within public RoW is expected to be at the expense of the utility

**Landscaping is limited to 25% of eligible and reimbursable construction costs and must be necessary to this project only.

*** The CEI Estimate may not exceed 20% but can be decreased by the local agency at their discretion depending on the scope and complexity of the project. If contract administrative duties are performed by a private firm other than the contracted CEI consultant, this firm must still be TDOT pre-qualified and a copy of the fully executed contract between them and the CEI consultant must be sent to TDOT electronically. Costs for contract administration must be included within the CEI budget.

Compatibility Report for Part II 2018 Multimodal Access
Application - Budget.xls
Run on 5/14/2018 11:01

If the workbook is saved in an earlier file format or opened in an earlier version of Microsoft Excel, the listed features will not be available.

Significant loss of functionality

**# of
occurrences**

Version

Any effects on this object will be removed. Any text that overflows the boundaries of this graphic will appear truncated.

1

[Sheet1'!A1:L](#)
[96](#)

Excel
97-2003

Minor loss of fidelity

Some cells or styles in this workbook contain formatting that is not supported by the selected file format. These formats will be converted to the closest format available.

4

Excel
97-2003

Project Information

Project Route/Termini:

Provide the project route and termini. The project location should run from west to east or south to north and must include local/state/federal road names. You must include a color-coded map that clearly delineates the project (with road names, a north arrow and the project location highlighted) and any photographs, plans, drawings and applicable preliminary sketches. Please provide a photograph of each of the termini.

SR24 from McGavock Pike to Old Lebanon Pike

Project Description

Provide the project title, project description, and the scope of all work to be performed.

Lebanon Pike Sidewalk Improvement - The project will involve the construction of an 8-foot sidewalk and a 4-foot grass furnishing zone with curb and gutter on the north side of Lebanon Pike. In addition, signal upgrades will be made at the intersections of McGavock Pike and Graylynn Drive that will also include ADA ramps, crosswalk markings, and pedestrian signals. This project has federal and local funding that has been programmed in the TIP. The construction phase will be locally managed and monitored by TDOT Local Programs Development Office. The project will have a length of 4,106 linear feet and cross-section of 8-foot sidewalk with 4-foot furnishing zone. Project phases completed include: Environmental/NEPA: Completed; Design: Completed; Right-of-Way Acquisition: In progress; Utility Coordination: In progress; Estimated Construction Duration: 12 months.

The project is currently managed by TDOT local programs (PIN.121729.00). After ROW we will be ready for construction and have programmed \$1,800,000 for construction in the TIP.

Attach map(s) here:

https://tdot.intelligrants.com/_Upload/13293_1144948-LebanonPikeMap.pdf

https://tdot.intelligrants.com/_Upload/13293_1144945-LebanonPikeLocationMap.pdf

Attach photo(s) and/or artist rendering(s) here:

https://tdot.intelligrants.com/_Upload/13293_1144949-LebanonMcGavockAerial.jpg

https://tdot.intelligrants.com/_Upload/13293_1144946-LebanonOldLebanonAerial.jpg

Project Eligibility

Please select only those eligibilities that apply. Please Refer to TDOT's Multimodal Project Scoping Manual and Multimodal Design Guidelines for guidance regarding facility type and minimum design requirements. Projects must be located along a State Route.

[☒] Sidewalks

Project Information

- ☒ Pedestrian crossing improvements
- ☐ Bicycle facilities
- ☐ Multi-use path within the transportation corridor. Minimum 10 feet wide
- ☐ Transit stop amenities
- ☐ Complete streets, road diet, or traffic calming measures
- ☒ Accessibility improvements that address requirements of the Americans with Disabilities Act
- ☐ Pedestrian-scale lighting
- ☐ Other (Please explain):

Purpose and NeedSafety:

Provide any crash history and a brief description of how the proposed project will improve safety for pedestrians, cyclists, transit users, and other transportation users.

Safety is our priority, with Nashville committed to Vision Zero and the need to eliminate traffic fatalities and severe injuries in the city. Traffic deaths are a crisis. In 2019, over 33,000 people were killed in traffic crashes in the United States. Nashville has a higher fatality rate than identified peer cities and the 2019 U.S. national average (11 people per 100,000). The data shows that walking in Nashville can be dangerous. Pedestrians include people walking for transportation, recreation, and people using mobility devices such as wheelchairs. 1 in 4 pedestrians are killed or seriously injured when involved in a traffic collision. While only 3% of people identify

as walking as their main form of transportation, 17% of all traffic deaths or serious injuries are pedestrians (this number includes both commuting and non-commuting trips). The number of people killed or severely injured while walking has generally increased since 2014. Based on current projections, that number will increase to an estimated 96 people per year by 2026. Safety analyses were conducted to review bicycle and pedestrian involved crashes and fatalities from 2015-2019 in Davidson County. The 2022 WalknBike update, and the Vision Zero Action Plan produced similar safety analyses and resulted in similar findings: pedestrians are highly vulnerable, especially on high-speed arterial roadways, many of which are controlled by the state. In summary, nationally, hit-and-run collisions have increased at a rate of about 7% each year since 2009. In Nashville, people walking, especially at night, are at greater risk for being involved in a serious hit-and-run collision. On average, there are 84 pedestrian-involved hit-and-runs each year in Nashville. As of August 2021, there have been 75 pedestrian hit-and-runs in 2021. Furthermore, 150 people walking have been killed and over 248 people walking have been injured in Nashville. Our safety analyses conclude that 80% of all pedestrian fatalities occur on wide, state controlled arterial roads. Arterial roads have speed limits over 30 mph, multiple lanes, few safe crosswalks, and high transit ridership, therefore the need of these facilities is critical. The COVID-19 pandemic has impacted the pedestrian environment and most vulnerable communities. Safety experts have suggested that drivers are more reckless because there are fewer drivers on the road due to government shutdowns. In the U.S., fatality rates increased 30% in the second quarter, reversing a three-year downward trend in road fatalities. According to crash data Lebanon Pike/State Route 24 from 2016-2022 has reported 310 crashes. Of the 310 crashes, 116 injuries were reported, and no fatalities. Of the 310 crashes, 51 were reported during low light visibility times either dusk or dark-lighted. 237 crashes occurred during the daylight and 22 had no visibility data.

Project Information

Connectivity:

Provide a brief description of how the proposed project will increase connectivity among a mixture of land uses and activity centers such as transit, residential neighborhoods, low-income housing, medical facilities, schools, retail, parks, employment centers, etc. Proposals that will expand and create new network connectivity will generally rank higher than proposals that do not include these aspects, such as rehabilitation-only proposals.

The construction of the recommended improvements for Lebanon Pike will provide a safer means of travel for pedestrians and motorists along Lebanon Pike to commercial destinations such as offices, shopping, and restaurants. With sidewalks along Lebanon Pike, residents of the adjacent communities will have a better opportunity to visit parks, access the Music City Star commuter rail line, and transit stops. With the planned improvements and the development along Lebanon Pike, we can be proactive by building additional infrastructure and increasing mobility for pedestrians and motorists. The improvements along Lebanon Pike can promote increased use of sidewalk networks of adjacent communities.

Additionally, due to safer at home conditions, more people than ever started to walk in their neighborhoods. As we continue to transition towards a post-covid reality, people who have been stuck inside are desperate for some fresh air and have begun regularly walking through neighborhoods. As the pandemic continues to lessen, transit ridership numbers have been increasing. Providing a pedestrian connection to the WeGo Star is a local and regional connection. The WeGo Star is Middle Tennessee's only fixed guideway transit service, providing riders with reliable travel times along the 32-mile route between Lebanon and Riverfront Station in Downtown. The WeGo Star is a service of the Regional Transportation Authority of Middle Tennessee (RTA), which also offers commuter bus service in six additional regional travel. In addition to its role in carrying the Downtown workforce, the Star is often deployed to serve large scale special events such as the New Year's Eve celebration downtown, 4th of July Fireworks, and Tennessee Titans home football games. These trains often "sell-out," carrying 400-700 people per trip. The attractiveness of the Star (despite its limited service) comes from two characteristics lacking in the regional bus corridors: (1) reliable travel time, since it does not operate in mixed traffic on the regional highway network as RTA Regional Buses must; and (2) well located, "purpose-built" park and ride lots that provide a safe and convenient location for commuters to park and catch the train. To continue, the Donelson Station serves as a Park & Ride area for transit customers to park free and either ride the WeGo 6, carpool, vanpool, or take the WeGo Star.

Better transit and more sidewalk connectivity are increasingly sought by retired Baby Boomers when driving themselves is no longer feasible. Similarly, Millennials and Gen Z desire living within walking and biking distance of work and school. They support frequent transit service much more than previous generations. Gen Xers increasingly look to better walkability and safe access to parks and community centers as their families grow and mature. This generation is expected to take the mantle of the aging Baby Boomers as they open businesses, raise families, shaping political decisions moving forward.

This project will provide connectivity to projects in our WalknBike work plan including Lebanon Pike at JB Estille Dr. and Old Lebanon Pike and Lebanon Pike at McGavock Pike and Fairway Dr. Once complete, this sidewalk connection will provide a pedestrian path to the Donelson Library.

This project connects the surrounding neighborhoods to the businesses on Lebanon Pike including banks, grocery stores, restaurants, and pharmacies. In addition, the connection to Old Lebanon Pike gives access to Donelson Plaza with current businesses, restaurants, and to the vocational beauty school, The Salon

Project Information

Professional Academy. Donelson Plaza is under revitalization with the library as its centerpiece, the 1960s shopping center is being transformed into a modern town center, extending its life for decades to come as a hub of the Donelson Community.

Local Priority:

Is this project identified in state and/or local plans (including, but not limited to, Bicycle and Pedestrian Master Plan, Long Range Transportation Plan, Corridor Study, etc.)?

(☒) Yes () No

If yes, please list the name of plan(s) and upload relevant pages.

Name of Plan	Upload
Downtown Donelson Urban Design Overlay	https://tdot.intelligrants.com/_Upload/13293_1144965-DowntownDonelsonUDO.pdf

Has your municipality completed all requirements for ADA compliance?

If a municipality is unsure of its ADA program status, please visit

<https://www.tn.gov/tdot/government/g/ada-office0/local-agency-ada-self-evaluation--transition-plan--and-self-certification-status.html>. Email TDOT.ADA@TN.gov with questions or to provide them with an updated status.

(☒) Yes () No () Not Applicable

Does your municipality have a Private/Public Sidewalk Installation Program, New Development Sidewalk standard, Annual budgeted Sidewalk Project List or Complete Streets Ordinance?

(☒) Yes () No

If yes, upload relevant pages or document below.

https://tdot.intelligrants.com/_Upload/13293_1144968-WalknBike_Plan_WorkPlan22-24.pdf

Has there been any public involvement meetings regarding this project?

(☒) Yes () No

If yes, please provide a brief explanation.

The most recent community meeting for this project was for the Right-of-Way phase and provide the Public with up-to-date Information on the Lebanon Pike Sidewalk Improvement Project. The meeting discussed information on the Right of Way Acquisition Process and solicit comments and concerns regarding this Project.

Attach supporting documentation (i.e., meeting minutes) here:

https://tdot.intelligrants.com/_Upload/13293_1144972-LebanonPikeRightofWayMeetingPresentation.pdf

Project Readiness:

Project Information

Has any level of environmental or preliminary engineering work been completed?

☒ Yes ☐ No

If yes, please provide a brief explanation.

The project has NEPA certification as of 8/28/2017. The certificate is attached via miscellaneous supporting documentation.

Will this project include any ROW acquisition?

☒ Yes ☐ No

Do you foresee any potential environmental impact commitments for this project?

☐ Yes ☒ No

Does the project include a pedestrian/bike bridge or tunnel or impact an existing TDOT structure?

☐ Yes ☒ No

Will any part of the project impact a rail line?

☐ Yes ☒ No

Will the project move any utilities? If yes, identify all relevant types of utilities to be moved:

☒ Yes ☐ No

☐ Utilities in public right of way, identified in public revitalization plan

☒ Utilities in public right of way, not identified in public revitalization plan

☐ Utilities outside of public right of way

If potential challenges to project readiness have been identified, please describe proposed mitigation strategies.

We do not anticipate any major challenges to project readiness. The Right of way phase has started and is expected to end July 2023. We have built in inflation to our construction budget to accurately depict the costs of construction in 2023 (FY24).

If the applicant has received a Multimodal Access, Transportation Alternatives Program, or Congestion Mitigation and Air Quality grant since 2014, please describe the status of the awarded project.

125509.00 is in the process of closing out.

130748.00 is in the project initiation phase.

130753.00 has had a kickoff meeting.

128602.00 is in Survey and ROW stage.

132612.00 is in project initiation phase.

Economic Impact:

How will this project aid economic development in the community or serve economically disadvantaged populations?

Project Information

Aforementioned, completing this project provides multimodal transportation to the core business district of Donelson. Economically, this project aids business by providing safe access for consumers. This project provides a connection to Donelson station, which provides alternatives to vehicle commuting and access to neighboring Wilson County.

As the World continues to navigate the economic burdens from the pandemic, compounding crises in different parts of the world caused by geopolitical, economic, and natural causes have caused shortfalls in the world supply chain. More and more people are feeling the effects of food security, fuel increase, and inflation. Having safe, multimodal transportation options can relieve the burden of costs elsewhere.

TDOT is in the process of identifying how best to address growing congestion and mobility needs within the five-County Middle Tennessee Region (Davidson, Rutherford, Sumner, Williamson, and Wilson counties). There are a number of project concepts that relate to better managing our roadways along major commuting corridors like Lebanon Pike that serve and support Nashville-Davidson County as well as the region's surrounding counties.

Applicants are required to include local letters of support with their project application.

Attach letters of support here:

https://tdot.intelligrants.com/_Upload/13293_1144987-3686_001.pdf

Attach miscellaneous supporting documentation here:

https://tdot.intelligrants.com/_Upload/13293_1144988-CEapproved08_28_2017.pdf

https://tdot.intelligrants.com/_Upload/13293_1144988_2-121729.00ROWNTP.pdf

https://tdot.intelligrants.com/_Upload/13293_1144988_3-TIPpageLebanonPike.pdf

Figure B

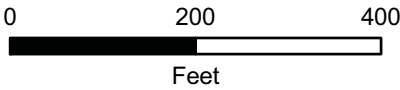
Lebanon Pike Sidewalk Pedestrian Enhancement

Legend

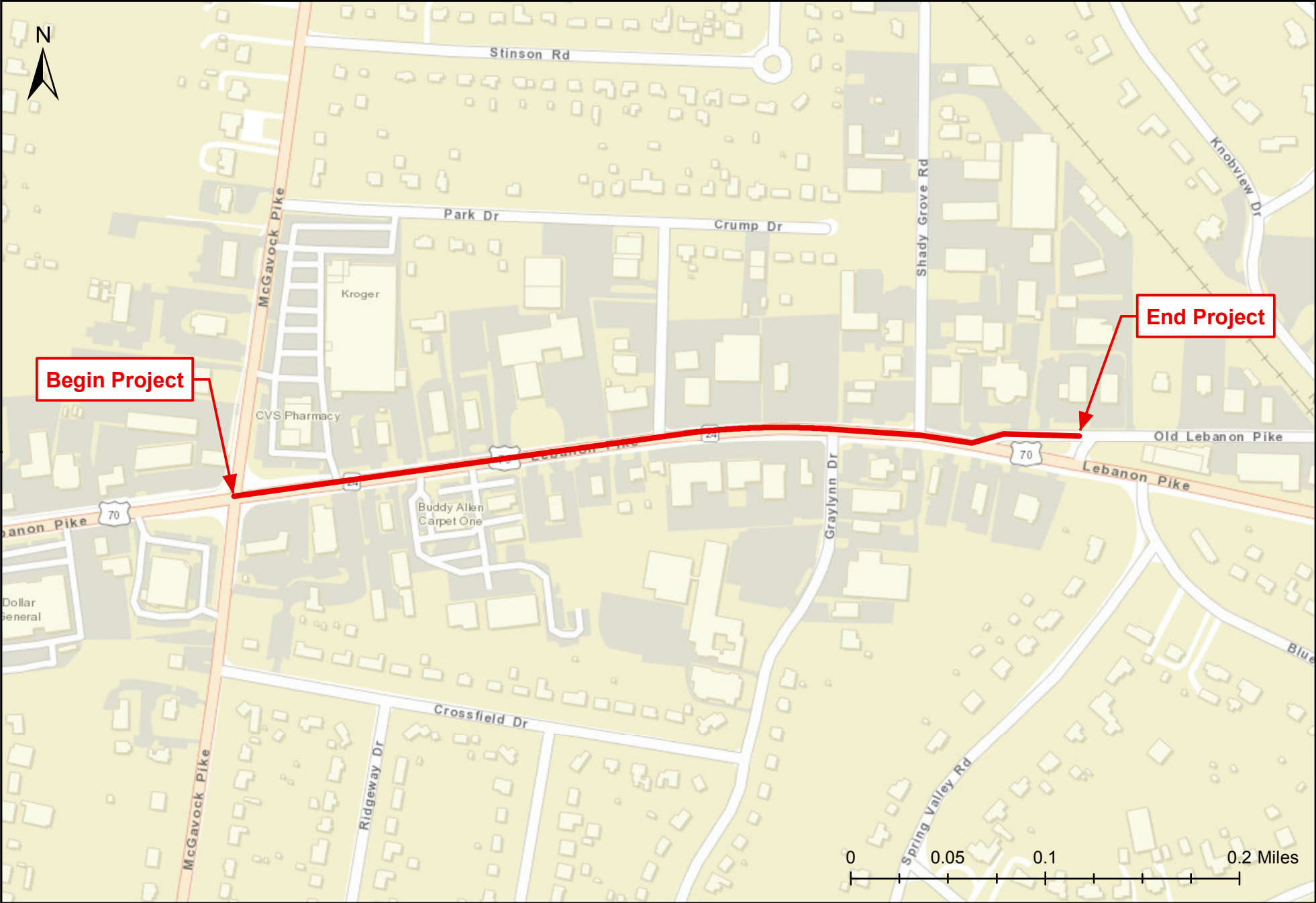
- Planned Sidewalk
- Existing Sidewalk
- Schools



Created On: 9-10-09



Project Location Map
Lebanon Pike Sidewalk Project
Nashville, Davidson County







Downtown Donelson Urban Design Overlay

prepared by
Metropolitan Nashville Planning Department
Attachment to ordinance nos. BL2009-552 and BL.2009-560
effective November 23, 2009
and as amended by ordinance no.BL2010-799
effective February 1, 2011
and as amended by ordinance no. BL2019-1749
effective August 23, 2019



Acknowledgements

The staff of the Metropolitan Planning Department thanks the many stakeholders who participated in the development of the Downtown Donelson Urban Design Overlay. Their thoughtful engagement on the vision of this area has been invaluable.

Special thanks are due to the following persons and entities:

McGavock High School for generously hosting our community meetings,

Councilmember Phil Claiborne for facilitation and participation,


 The Planning Department does not discriminate on the basis of age, race, sex, color, national origin, religion or disability in access to, or operation of, its programs, services, and activities, or in its hiring or employment practices. For ADA inquiries, contact Josie Bass, ADA Compliance Coordinator, at 862-7150 or e-mail her at josie.bass@nashville.gov. For Title VI inquiries contact Shirley Sims-Saldana or Denise Hopgood of Human Relations at 880-3370. For all employment-related inquiries call 862-6640.

Table of Contents

Section I: Introduction	5
Introduction and How to Use This Document	6
Location and Site Description	7
UDO Boundary and Subdistricts	9
Section II: Development Standards	11
Application of the Standards	12
Modifications and Process	13
Bulk Standards Calculations	14
Bulk Standards	16
Parking and Access Standards	19
Landscaping, Buffering and Screening Standards	20
Signage Standards	22
Development Incentives	25
Transfer of Development Rights	28
Section III: Transportation	33
Street Network Plan	34
Access management	35
Sidewalk Standards	36
Section IV: Community Planning	37
Process	38
Concept Plan	39
Goals and Objectives	40
Appendix: UDO Properties and Owners	57

This page left intentionally blank

Section I: Introduction

Introduction
How to Use This Document
Location and Site Description
UDO Boundary and Subdistricts

Section I: Introduction

Introduction and How to Use This Document

Introduction

The Downtown Donelson Urban Design Overlay (UDO) is the culmination of a public participatory planning process for the area along Lebanon Pike between Briley Parkway and Stewarts Ferry Pike. This document represents a vision for the redevelopment of an aging community center into a pedestrian-friendly, mixed-use downtown for the Donelson community. This vision will be realized through strategies that evolved during a series of public meetings where issues were raised and design concepts were presented for input and discussion.

How to Use this Document

This document establishes the development standards that vary from the underlying base zone district standards for the properties in the Downtown Donelson UDO. All provisions described below as regulatory in nature have the same force and effect as, but are variations from, the standards set forth for the base zone districts in the zoning regulations of the Metro Code. Any final development construction plans submitted for approval under the UDO will be reviewed for adherence to these provisions and to the provisions of the base zoning that are not varied by the UDO.

This document is intended to guide future development in a manner that addresses the important issues raised and discussed during community meetings. A portion of the objectives and illustrations in the main body of this document pertain to areas for which Metropolitan Government exercises final authority over design, construction and operation of facilities, such as public rights-of-way and stormwater detention and conveyance. The incorporation of these standards into any final development construction plans will depend on Metropolitan Government review for consistency with policies, laws, and related standards of various departments.

The UDO is organized by **Subdistricts**. Subdistricts are smaller districts within the larger UDO area that are envisioned to have unique character and development standards. Additionally, development standards vary based on the type of street a property fronts. The two **Street Types** in the Donelson UDO are “Arterial, automobile oriented,” and “Primary, pedestrian oriented.”

The **Regulating Plan** is the guiding map for the implementation of the UDO. The regulating plan shows the subdistricts and street types that govern the development standards for each property.

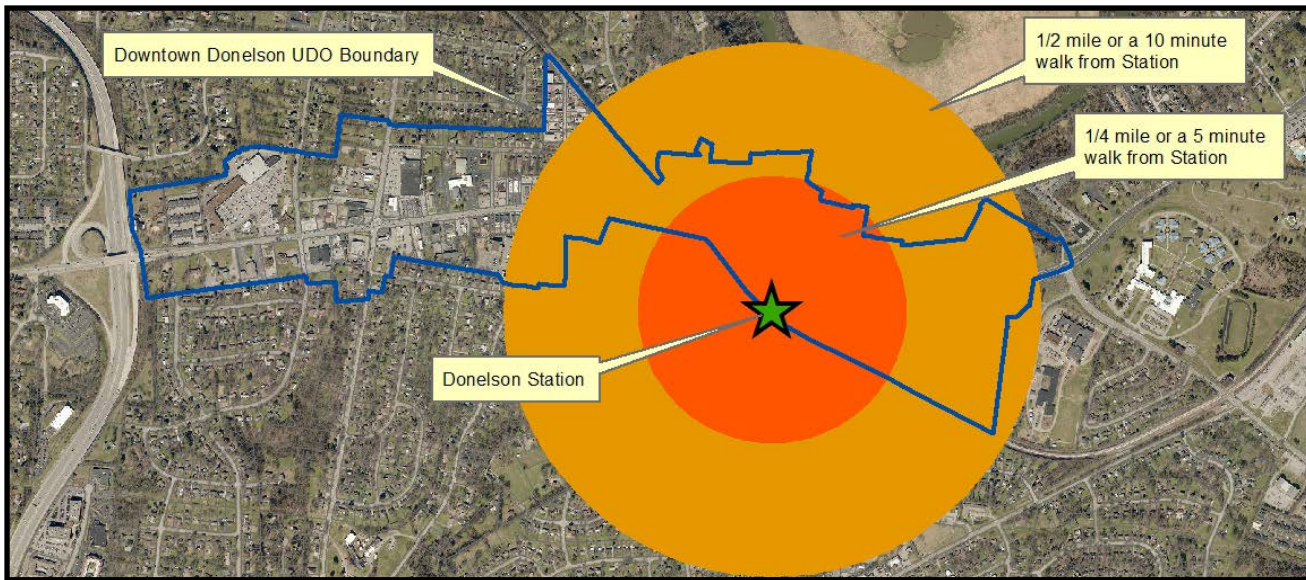
The **Development Standards** are the applicable bulk standards; landscaping, parking and signage regulations that govern development in the UDO area.

The **Goals and Objectives** are the result of the public planning process. They address multiple issues related to development including: buildings and lots, parks and open space, vehicular circulation, bicycle and pedestrian circulation, signage, transit, access, parking, landscaping and buffering. Development scenarios for portions of the UDO area illustrate the design intent of the UDO. The scenarios are simply one illustrative example of how development according to the design standards may occur.

The **Appendix** is dedicated to the methods by which the UDO was developed. It includes an overview of the study area inventory and analysis, a brief description of the charrette process, and a list of participants.

Section I: Introduction

Location and Site Description



Walk radius from Donelson Station

The Lebanon Pike corridor between Briley Parkway and Stewarts Ferry Pike is the geographic and historical center of Donelson and is the focus of the study area for the Downtown Donelson Urban Design Overlay. This corridor has provided the community with many of its services, institutions and shopping. Although there has been some recent redevelopment of the properties surrounding the intersection of McGavock Pike and Lebanon Pike, the rest of the corridor is experiencing vacancies and worsening property maintenance and appearance.

Donelson began its development shortly after World War II. Its adjacency to Nashville's airport led to much of its early growth. It is also the site of an early example of what would later be called a shopping center or "strip mall," Donelson Plaza. Donelson is now an example of an early postwar suburb with a stock of mostly half-century old, red brick, detached ranch-style homes.

The Lebanon Pike Corridor has suffered from a lack of new private investment. Originally zoned and configured to serve the anticipated growth of the post-war years, commercial strip corridors

initially offered businesses an opportunity to locate outside of downtowns. Subsequent development of regional malls and newer, more convenient developments located near interstates has led to an economic marginalization of the commercial strip. As new investment and existing business moved elsewhere, vacancy, poor maintenance, and out-of-date character became more prevalent in the area. The conveniences and cost savings once offered by the strip have been outweighed by the unattractive appearance of the corridor and its lack of integration with its community due to the dominance of the automobile, the lack of sidewalks and the generally unpleasant pedestrian environment.

The community has a station on the commuter rail line, which began operation in September, 2006. The commuter rail station provides for unique opportunities for Donelson to reevaluate its development potential.

Section I: Introduction

The Downtown Donelson UDO envisions mixed use, compact, transit oriented development. However, the existing base zoning expressly prohibits that vision. The Lebanon Pike Corridor is largely zoned Commercial Service (CS) and Commercial Limited (CL): the most auto-oriented zoning districts in Davidson County. The commercial base zoning not only ensures an auto-dominated development pattern, with large buildings at the back of lots abutting single-family homes and prohibiting buildings close to the street, but it also prohibits residential development.

The UDO is a tool that requires specific design standards for development in a designated area. UDOs can modify zoning standards such as setbacks, building height, floor area ratio, and parking. The only standard a UDO can not modify is the land uses allowed by the base zoning district.

In order to create the mixed use environment envisioned by the UDO, without changing the base zoning, an amendment to Section 17.16.030. F of the Metro Zoning Ordinance, Adaptive Residential Development, is necessary. This section allows residential development in the commercial zoning districts, such as CL and CS, without a zone change. This provision was previously allowed only within the Urban Services District, but the amendment to the section will extend this allowance to commercially zoned properties within the Donelson UDO.

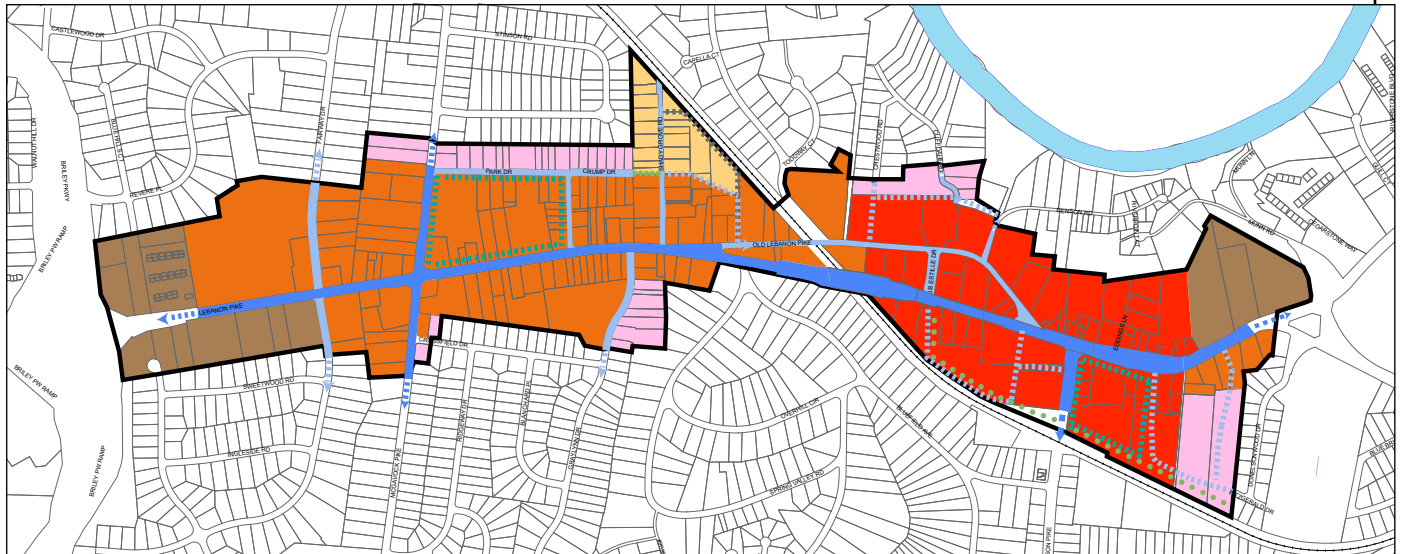
Additionally, there are some properties that will be unable to develop to the full potential that the UDO envisions due to the restrictions of the existing base zoning. In these cases, a rezoning will be required to take full advantage of the provisions offered by the UDO. In previous UDO applications that were paired with a rezoning, the Planning Department and the Metro Council sponsored these requests and there were no fees to the property owners. In light of this, one of the incentives offered by this

UDO is that rezoning fees will be waived for those properties that choose to rezone to a district that permits uses and density consistent with the goals of the UDO.

There is one property within the UDO boundary zoned Specific Plan (SP). As long as the SP is in place, SP will be the controlling zoning, however, is that SP is removed then the UDO will apply.

Section I: Introduction

UDO Boundary and Subdistricts



Downtown Donelson UDO Regulating Plan

The **Regulating Plan** is the guiding map for the implementation of the UDO. The regulating plan shows the subdistricts and street types that govern the development standards for each property.

- | | |
|--|--|
| <p>Subdistrict 1 - Transit Oriented Development
Create Transit Oriented Development (TOD); development that is within a 5 minute walk or ¼ mile in distance to the Donelson Music City Star Station, includes a mixture of commercial, residential, and office land uses, and coordinates with other modes of transportation (bike, pedestrian and vehicular).</p> <p>Subdistrict 2 - Mixed-Use Corridor
Enhance the suburban character of Lebanon Pike while promoting sustainable development and design through landscaping, access management and transit, signage, and mixed use development standards and an enhanced pedestrian environment.</p> <p>Subdistrict 3 - Commercial / Industrial Support
Enhance this area by providing infrastructure that would support light industrial and office land uses.</p> <p>Subdistrict 4 - Residential and Civic Corridor
Create development along the Lebanon Pike corridor that preserves the residential and civic character of these areas, while providing additional housing choice that accommodates various lifestyle options of residents in the area.</p> | <p>Subdistrict 5 - Residential/Office Neighborhood
Include office land uses along with mixed housing that provides additional housing choice and creates transitions between intense commercial land uses, and less intense residential neighborhoods.</p> <p>Primary Street</p> <p>Arterial Street</p> <p>Proposed Primary Street</p> <p>Proposed Arterial Street</p> <p>Proposed Pedestrian Connection</p> <p>Proposed Service Lane or Alley</p> <p>Future connections should be provided in the area at the time of redevelopment</p> |
|--|--|

This page left intentionally blank

Section II: Development Standards

Application of the Standards
Bulk Standards
Architectural Treatment Standards
Parking and Access
Landscaping, Buffering and Screening
Signage
Development Incentives

Section II: Development Standards

Application of the Standards

Compliance Provisions

Properties within the Downtown Donelson UDO boundary may choose whether or not to utilize the standards and incentives of the UDO, until such time as the following changes take place.

- Property ownership changes after the effective date of this ordinance (date), including change in majority interest or transfer of ownership due to death of previous owner.
- The base zoning is changed by request of the property owner.
- A property owner uses any of the development incentives provided as part of this UDO.

Following the changes listed above, compliance with the UDO standards shall be required as follows:

1. Full compliance with the Development Standards shall be required when:

- New Development
- When a new structure is built on a lot with multiple structures, the new structure shall be in compliance with all the Development Standards.

Signage

Notwithstanding the above, the Signage Standards of the UDO shall apply when a sign permit is required, including the replacement of a sign panel, according to the following provisions.

- New signs shall comply with all Signage Standards.
- Existing single tenant signs shall be brought into compliance when a change to the sign requires a sign permit. Panel replacement is allowed, but may not increase the degree of non-conformity. Any change to the cabinet or sign structure will require the sign to come into compliance.
- Existing multi-tenant signs may be permitted for individual tenant panel replacement without achieving full compliance as to size, location, and illumination. but may not increase degree of non-conformity.
- Notwithstanding the above, a non-conforming

sign damaged by any involuntary means may be reconstructed to its pre-damage condition.

- If a non-conforming use is changed to a conforming use a non-conforming sign shall be brought into compliance as well.

Applicability

Base district standards that are not varied by the provisions set forth in the Downtown Donelson Urban Design Overlay shall apply as applicable to all property within the UDO boundary.

Section II: Development Standards

Modifications and Process

Process

Final site plans shall be submitted in the future for any development within the boundary of the UDO. Final site plans shall consist of a detailed set of construction plans that fully demonstrate compliance with the UDO and shall specifically describe the nature and scope of development to serve as the basis for the issuance of permits by the Codes Department and all other applicable Metro departments. Following the approval of the final site plan, a final subdivision plat may be required to establish lots, rights of way and easements. Bonding for these improvements will be required.

Prior to applying for a building permit, applicants shall submit to the Planning Department complete sets of final construction documents, including site plan and landscape plan, for review and approval prior to the issuance of permits.

Applicants are encouraged to work with Metro Planning staff early in the design and development process. Where obvious physical constraints exist on a site within the UDO, Planning staff will review alternative design solutions that achieve the intent of the UDO for that subdistrict. Where a single use or function spans more than one subdistrict, Planning staff will work with the applicant to determine alternative solutions that achieve the design intent of the UDO.

Modifications

The natural landscape and man-made environment may present difficulties in compliance with some standards of the Downtown Donelson UDO. Based on site-specific issues, modifications to the standards may be necessary. The condition shall be unique to the subject property and generally not prevalent to other properties in the general area, or have been created by the previous actions of any person having an interest in the property after the effective date of this ordinance.

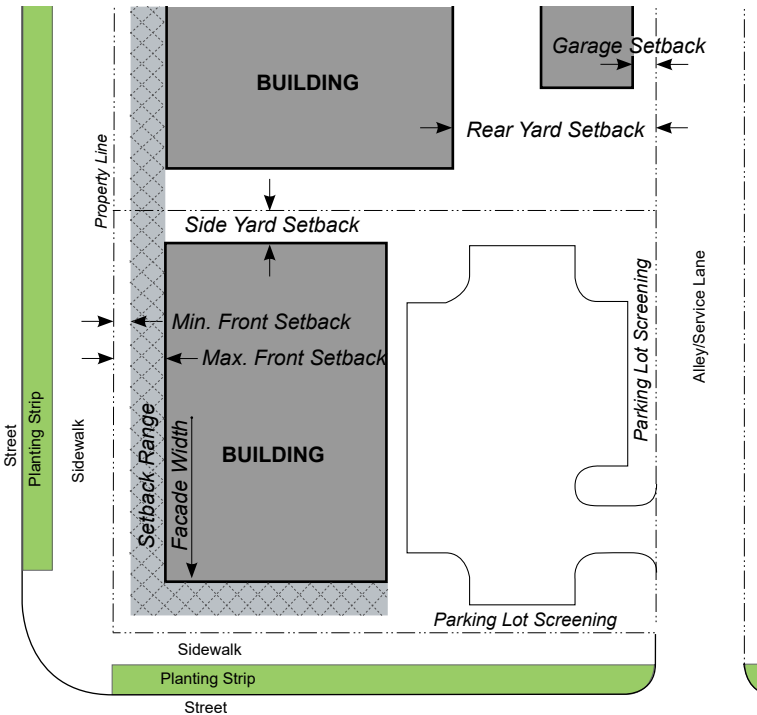
Any standard within the UDO (with the exception of

number of stories) may be modified, insofar as the intent of the standard is being met; the modification results in better urban design for the neighborhood as a whole; and the modification does not impede or burden existing or future development of adjacent properties. Modifications to the number of stories shall not be permitted.

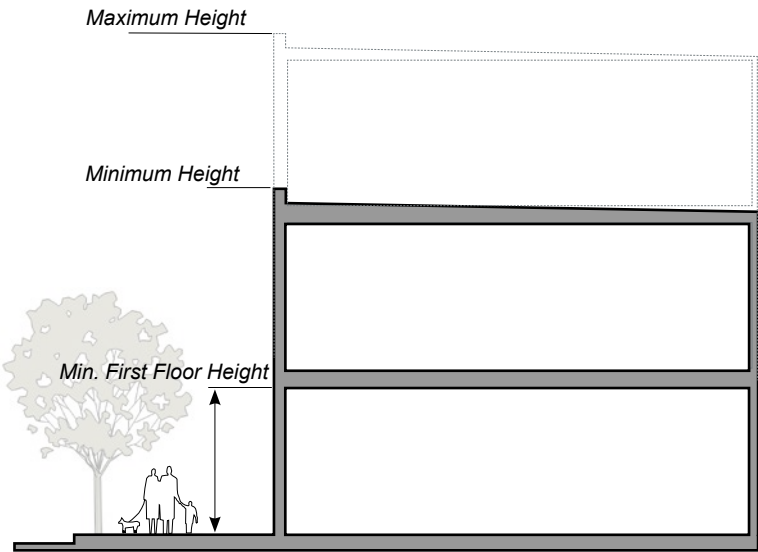
Modifications shall be approved by Planning Commission or its designee. Minor modifications – deviations of 20 percent or less – may be approved by the Planning Commission's designee. Any determination made by the Planning Commission's designee may be appealed to the Planning Commission within seven business days. Major modifications – deviations of 21 percent or more – may be approved by the Planning Commission.

Section II: Development Standards

Bulk Standards Calculations



Building Plan



Building Section

Calculations

Setbacks

Setbacks are determined by the ground floor use – residential or non-residential.

A setback range (minimum and maximum) is indicated for the front yard setbacks. The setback shall be measured from the back of the sidewalk at the property line. If additional sidewalk is dedicated by the property owner, the setback shall be measured from the back of the new sidewalk. The front yard setback shall also apply to secondary street frontages.

Height

Building height (in feet and stories) shall be measured from the average grade along the front property line to the top of eave or parapet. When buildings are set back from the back of sidewalk more than 20 feet, height shall be measured from the average grade at the building setback. No uses shall be exempt from counting towards building height.

Façade Width

The façade width is the length of building façade that is built within the setback range. The required façade width is determined by the Subdistrict and street type. **The façade width standard shall only apply to the predominant street frontage and no minimum shall be required along a secondary frontage** if the property is located at the intersection of two or more streets.

This page left intentionally blank

Section II: Development Standards

Bulk Standards Table

	Subdistrict 1	Subdistrict 2	Subdistrict 3
Front Yard Set Back Primary Street ¹	Residential: 10' min - 20' max Non-Residential: 0' min - 10' max	Residential: 10' min - 20' max Non-Residential: 0' min - 10' max	5' min - 15' max ⁶
Front Yard Set Back Arterial Street ¹	0' min - 52' max	0' min - 52' max	N/A
Minimum Facade Width ²	45% of the lot width must be occupied by building	45% of the lot width shall be occupied by building for building setbacks up to 15' 60% when the building is setback greater than 15'	45% of the lot width must be occupied by building
Side Yard Setback ^{3,4}	Residential: 5' min Non-Residential: 0' min	Residential: 5' min Non-Residential: 0' min	0' min
Rear Yard Setback ⁵	General: 5' min Townhouse: 20' min for primary structure	General: 5' min Townhouse: 20' min for primary structure	5' min
Maximum Building Height	5 stories in 75 feet	3 stories in 45 feet	3 stories in 45 feet
Floor Area Ratio	1.0	1.0	1.0
Impervious Surface Ratio	.80	.80	.80
First Floor Height Non-Residential	14' min	14' min	14' min
Glazing Requirements Non-Residential	40% of the first floor facing a street, 25% on upper floors	40% of the first floor facing a street, 25% on upper floors	N/A

¹ EXCEPTIONS TO FRONT YARD SETBACK: Exceptions may be made for recesses up to 5' for articulation of the building façade and up to 20' for outdoor seating areas.

² EXCEPTIONS FAÇADE WIDTH: Exceptions may be made for automobile convenience uses. See Building Type Standards

³ EXCEPTIONS TO SIDE YARD SETBACKS: Side yards with street frontage, on corner lots, shall follow front yard setbacks.

⁴ EXCEPTIONS TO SIDE YARD SETBACKS: Setbacks between townhouse units may be 0 ft.

⁵ REAR YARD SETBACK EXCEPTION: Development abutting property with an existing residential use shall comply with the applicable buffer yard standards of Title 17.

⁶ SETBACKS SUBDISTRICT 3: A minimum of 1 row of evergreen shrubs are required between the building and the street to screen any facades that do not incorporate glazing.

⁷ REAR SETBACK FOR PROPERTIES ON GRAYLYNN DR: The rear setback shall be located at the western edge of the required stream buffer. The intent of this is that the back portion of the properties (addressed as 191,193 and 195 Graylynn Drive) on the east side of the stream, abutting Bluefields, remain undeveloped and kept as a green space buffer.

Section II: Development Standards

Bulk Standards Table

	Subdistrict 4	Subdistrict 5
Front Yard Set Back Primary Street ¹	15' min - 25' max	15' min - 25' max
Front Yard Set Back Arterial Street ¹	20' min - 100' max	15' min - 52' max
Minimum Facade Width ²	45% of the lot width must be occupied by building	25% of the lot width must be occupied by building
Side Yard Setback ^{3,4}	5' min	5' min
Rear Yard Setback ⁵	20' min for primary structure	20' min for primary structure ⁷
Maximum Building Height	3 stories in 45 feet	3 stories in 45 feet
Floor Area Ratio	.70	.70
Impervious Surface Ratio	.70	.70
First Floor Height Non-Residential	14' min	N/A
Glazing Requirements Non-Residential	N/A	N/A

¹ EXCEPTIONS TO FRONT YARD SETBACK: Exceptions may be made for recesses up to 5' for articulation of the building façade and up to 20' for outdoor seating areas.

² EXCEPTIONS FAÇADE WIDTH: Exceptions may be made for automobile convenience uses. See Building Type Standards

³ EXCEPTIONS TO SIDE YARD SETBACKS: Side yards with street frontage, on corner lots, shall follow front yard setbacks.

⁴ EXCEPTIONS TO SIDE YARD SETBACKS: Setbacks between townhouse units may be 0 ft.

⁵ REAR YARD SETBACK EXCEPTION: Development abutting property with an existing residential use shall comply with the applicable buffer yard standards of Title 17.

⁶ SETBACKS SUBDISTRICT 3: A minimum of 1 row of evergreen shrubs are required between the building and the street to screen any facades that do not incorporate glazing.

⁷ REAR SETBACK FOR PROPERTIES ON GRAYLYNN DR: The rear setback shall be located at the western edge of the required stream buffer. The intent of this is that the back portion of the properties (addressed as 191,193 and 195 Graylynn Drive) on the east side of the stream, abutting Bluefields, remain undeveloped and kept as a green space buffer.

Section II: Development Standards

Architectural and Building Type Standards

Architectural Treatment Standards

Permitted Attachments

- Balconies, stoops, porches, entry stairs and bay windows shall not encroach more than 10 feet into the front setback.
- Porches shall be a minimum of 6 feet deep.
- Stoops and balconies shall be a minimum of 4 feet deep.
- Awnings, and canopies may encroach beyond the front setback, but shall not encroach into the public right-of-way without proper permitting from Metro Public Works.

Glazing

Minimum glazing requirements shall be calculated based on the façade area as follows:

- Non-Residential first floor: façade area shall be measured from the average grade at the sidewalk to the height of the first floor or eave for single story buildings.

Materials

- Prohibited materials: plywood, plastics, unfinished concrete block, full metal buildings (except in subdistrict 3), Vinyl siding, E.I.F.S. (on ground floor).
- Primary façade materials shall change along a horizontal plane.

Primary Pedestrian Entrance

- All Buildings fronting a public street shall have a minimum of one pedestrian entrance along the street for every 150 feet of frontage.

Standards Specific to Building Types

Townhouse Courts, Cottage Courts, and Courtyard Flats

Housing types that front a central open space.

- The width of the central open space, measured between buildings, shall be no less than the height of the buildings, measured to the bottom of the eave or top of parapet.

- Dwellings shall abut a minimum of two sides of the central open space.
- Parking, driveways, and detention areas shall not be located within central open space. Modifications may be permitted for stormwater elements such as rain gardens and bioswales when incorporated as a design element.
- Parking adjacent to the open space shall be separated from it by landscaping or an architectural screen.
- Interior sidewalks, a minimum of 4 feet wide, shall connect all units to the sidewalks at the street frontage and the parking areas.

Automobile Convenience Use Standards

- The minimum façade width may be waved for automobile convenience uses provided:
 - The building be placed at the back of sidewalk with no parking in front, and
 - Lots at the intersection of two streets are required to place the building at the intersection corner of the lot.

Fence and Wall Standards

- Fences and walls within the front setback and in front of buildings on side property lines facing public streets shall be constructed of wood, brick, masonry, vinyl, ornamental iron or aluminum, or similar materials. Chain-link shall not be permitted within the front setback or between buildings and the street.
- Fences and walls constructed between the principal building and the street (excluding alleys) shall not exceed 4 feet in height.
- Fences and walls along rear lot lines, behind a principal building, and along side property lines not fronting a street shall not exceed 10 feet in height.
- All fences shall be installed so that the finished side faces outward; all bracing shall be on the inside of the fence.

Section II: Development Standards

Parking and Access

Driveway Access

Vehicular access shall be permitted as follows:

Subdistrict 1, 2

- One driveway per street frontage if no other options are available, 2 per street frontage for parcels with 400 or more feet of frontage, joint use provisions required

Subdistrict 3

- According to Title 17.20.160 and 17.20.170

Subdistrict 4

- One driveway per street frontage if no other options are available, 2 per street frontage for parcels with 400 or more feet of frontage, joint use provisions required

Subdistrict 5,

- According to Title 17.20.160 and 17.20.170

Cross Access and Joint Access

- Cross Access, which allows for vehicles to pass between properties without entering the public right-of-way, and/or Joint Access, a shared driveway providing access to multiple properties via one driveway, shall be required when possible. An easement agreement which provides for the mutual right of ingress and egress for both property owners shall be recorded with the Register of Deeds and be referenced on a final plat and in any restrictive covenants, and shall be referenced on the site plan.

Parking Requirements

- The parking provisions of Title 17.20.030, that are applicable in the Urban Zoning Overlay District, shall be applicable to this UDO.

Parking Structures

- The ground floor of any parking structure that fronts an at-grade public street (excluding alleys) and is within Subdistrict 1 shall be set behind a liner building with a minimum width of 15 feet so the parking structure is not visible from the street except at the point of a driveway access.

Parking Location

- Parking shall be located behind, beside or beneath buildings on all lots except as follows:
- One row of parking is permitted between the building and the street in Subdistricts 1 and 2 along Arterial streets only.

Parking Setback

- Garages shall be set back from alleys and streets 5 feet or greater than 15 feet.
- Parking areas that accommodate 4 or more vehicles shall be separated from buildings a minimum of 4 feet in order to accommodate a sidewalk or planting area.

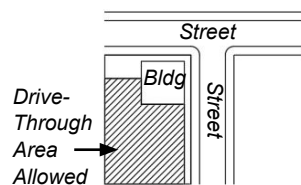
Parking Lot Lighting

- Lighting shall be appropriate in function and scale for both the pedestrian and the vehicle. Lighting that minimizes light trespass, pollution, and uplight shall be utilized.

Drive-throughs

- Drive-throughs should be located to the rear or side of buildings. If drive-throughs are located to the side of buildings, their width shall be limited to two lanes and they shall be recessed from the front façade by a minimum of 20 feet. In no case shall the drive-through portion of the building be located at the corner of two public streets.

For corner location reference:



Vehicular Drop-offs

- Vehicular drop-off areas shall be articulated through paving patterns, bollards, landscaping and other means that limit the impact of conflicts between vehicles and pedestrians.

Bicycle Parking

- Per zoning code.

Section II: Development Standards

Landscaping, Buffering and Screening Standards

The following requirements shall apply to all surface parking lots that have five or more parking spaces or are larger than seven hundred and fifty square feet.

Parking areas adjacent to public streets

- Parking areas and driveways shall be separated from the edge of the right-of-way by a perimeter landscape strip a minimum of 5 feet wide and planted with a continuous row of evergreen shrubs.
 - All shrubs shall be a minimum of 2 feet, 6 inches in height at the time of installation, forming a hedge that provides screening year-round.
 - Vegetation shall be maintained at a maximum height of 3 feet.
- Landscape strips that include a fence or wall may be reduced to 3 feet wide and the evergreen shrubs may be deleted in lieu of other natural or plant material.
 - The wall shall be between 2 feet, 6 inches and 3 feet, 6 inches in height.
 - The fence shall be a minimum of 3 feet in height and a maximum of 4 feet in height. The fence shall be a minimum of 30 percent transparent.
 - Fences and walls shall be placed at the back of the planting strip, so any vegetation is between the wall or fence and the sidewalk.
- A minimum of one tree shall be preserved or planted in the perimeter strip for each 35 feet of parking area perimeter, or portion thereof.
 - If perimeter strips are less than 4 feet wide the required trees shall be planted interior to the parking area
 - Trees shall be a minimum of 2 inches in caliper and 6 feet in height at planting.

Parking areas adjacent to side or rear property lines

- The standards of the Metro Code for Side Property Lines, 17.24.150 B shall apply.

Interior Landscaping Requirements

- The standards of the Metro Code for Interior Planting, 17.24.160 shall apply.

Section II: Development Standards

Landscaping, Buffering and Screening Standards

Mechanical, Service, and Loading Screening

- All outdoor equipment, such as HVAC and ventilation systems, utility meters and panels shall be placed on the roof, to the rear or side of buildings, or otherwise visually screened from the street. Mechanical equipment shall not be allowed along the frontage of a public street. Mechanical equipment on the roof shall be screened from abutting streets with parapets or other types of visual screening.
- All solid waste, recycling, and yard trash containers, including grease containers, and loading docks shall be located in parking areas, or at the side, or rear of a building, furthest from the public sidewalk. They shall be screened to minimize sound and visibility from residences, abutting sidewalks and streets, excluding service lanes.

Screening methods include: Vegetative Material as well as Fences and Walls.

- Vegetative materials shall be planted in two rows in staggered fashion.
- All trees shall be evergreen with a minimum height at time of planting of at least six feet above the root ball.
- All shrubs shall be evergreen with the minimum height and spacing necessary to fully screen the item intended for screening (but no less than thirty inches in height) at the time of planting.
- Vegetative material shall be located immediately adjacent to the element being screened in a planting area a minimum of four feet wide.
- Fences and walls shall be a minimum of 2 feet higher than the element being screened. Gates may be required to fully screen elements that face public streets.

** In order to properly locate and screen mechanical equipment, approval may be required from Metro Water Services, NES, and/or Metro Codes.

Section II: Development Standards

Signage Standards

The Signage standards of the Downtown Donelson UDO allow each business to communicate its message clearly without clashing with the envisioned visual character of the area. The goal is to encourage attractive, appropriate-scaled signage that contributes positively to the surroundings, provides businesses with sufficient visibility to the community, and reduce visual clutter.

General Standards

- In addition to the standards of this section, signage shall comply with the following sections of Chapter 17.32 Sign Regulations: Exempt signs 17.32.040; Prohibited signs 17.32.050; Permitted On-premise Temporary signs 17.32.060; Computations 17.32.160; and Sign Procedure 17.40.490.
- Refer to Title 17.04.060 for the definition of “sign.”
- Signs shall not obscure architectural features or details including, but not limited to, fenestration details, doors, or cornices.
- Signs may encroach into the public right-of-way with proper permitting through Metro government.
- Buildings shall not be painted to act as signs.

Common Signage Plan For New Development

- When more than one use or business is located on a lot a common signage plan shall be required. The common signage plan shall establish an allowable area of signage and the general sign locations for existing and future tenants with regard both to on-premises building and on-premises ground signs. The common signage plan shall be approved prior to issuance of any permits.
- For a development existing prior to the effective date of the ordinance codified in this chapter, a common signage plan may be submitted. The common signage plan shall indicate existing nonconforming signs as well as the amount and location of on-premises signage to be allocated to each tenant under the new plan.
- A common signage plan that measures the façade of the larger building as opposed to individual tenant spaces can benefit tenants by slightly increasing the permitted size of individual signs.

Prohibited Signs

- Billboards

Exempt signs

- Every restaurant use is permitted one Menu sign to be placed on the building adjacent to the main entry.

Illumination

- Externally lit signage using uplighting, downlighting, or backlighting techniques shall be permitted.
- Internal lighting shall be permitted to illuminate letters and logotype only. Sign backgrounds shall be opaque.
- Portions of signs with manual changeable copy may be internally lit to illuminate the background of the sign.
- Transformers and other mechanical equipment related to sign illumination shall be located inside the building or otherwise concealed from public view. Exposed raceways must be colored to match the sign or the building wall that the sign is attached to.
- External lighting sources shall be directed and shielded so that they do not produce glare off the site or on any object other than the sign.

Section II: Development Standards

Signage Standards: Sign Types

Permitted Sign Types

Building Signs

- Building Mounted Flush: a sign attached to a wall that does not project more than 1 foot.
- Building Mounted Projecting: a sign attached to a wall that projects outward from the wall more than twelve inches; or a sign suspended from any structure that constitutes a covering or shelter such as a canopy, portico or marquee. Usually, though not always, the face of a projecting sign will be perpendicular to or form a wide angle with the surface to which it is attached.
- Building Mounted Projecting – 2nd Story and Above
- Awning Sign: application of words, lettering and/or logos to an awning.
- Window Sign: any sign attached to or directly applied on a window or glass door; intended for viewing from the exterior.

Ground Signs

- Monument Sign: a low profile sign with a base that is at least 3' wide or 50% of the maximum width of the sign (whichever is greater).
- Hanging Sign: a ground sign with one vertical post, and one or two arms from which a sign hangs. The sign is intended for buildings with a deep Build-to zone and should be placed perpendicular to the sidewalk.
- Pillar Sign: a ground sign with two vertical posts constructed of masonry materials. The entire sign area shall be contained between the posts.
- Manual Changeable Copy Sign: Signs on which alphabetic, pictographic, or symbolic informational content can be changed or altered by manual means.

Permitted Sign Standards

- Each multi-tenant principal building may display not more than two on-premises building signs with a combined sign area of not more than thirty-two square feet.
- In addition to the principal building signage each occupant of a multiple occupancy complex may display on-premises building signs on any exterior viewed portion of the complex that is part of the occupant's unit. The total sign area shall not exceed fifteen percent of the facade area for the public entry side. Occupants may divide the permitted signage among the applicable facades, but a signage bonus is not given for an additional entry.
- Each single-tenant building may display on-premise building signs with a total sign area that shall not exceed fifteen percent of the facade area for the public entry side or a maximum of 128 square feet, whichever is greater. Occupants may divide their permitted signage over all façades of the building, but a signage bonus is not given for an additional entry.
- Where the owner of a multi-tenant building so chooses, a common signage plan in accordance with the standards for a single occupancy building may be submitted.
- Ground signs shall be permitted according to the Ground Signs Table.
- Facade area shall be calculated by multiplying the façade length by the ground floor height or eave for single story buildings.

Section II: Development Standards

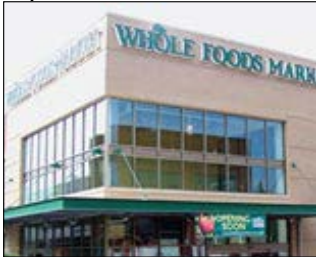
Signage Standards: Sign Types

Building Signs

Ground Signs

Building Sign Flush

Monument Signs



Building Sign Projecting

Hanging Signs

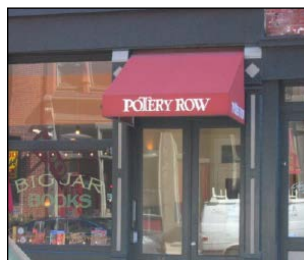


Window Signs

Pillar Signs



Awning Signs



Section II: Development Standards

Signage Standards: Tables

Non-Residential Uses: Building Signage Table

Building Signs Table				
Sign Types	Maximum Display Surface area of Individual Signs	Maximum Height	Maximum Projection	Specifications
Wall Mounted Flush	5% of the building façade on the public entry side or a maximum of 64 square feet for single story buildings and 84 square feet for multi story buildings, whichever is less.	Below top of parapet/eave	N/A	
Wall Mounted Projecting ¹	12 square feet	Below 2 nd story FFE or below top of parapet/eave, which ever is less.	6 feet from building façade	Minimum clearance is 8 feet or 10 feet within 10 feet of the right-of-way.
Wall Mounted Projecting – Second story and above	5% of the building façade on the public entry side or 64 square feet, whichever is less.	Below top of parapet/eave		
Awning	40% of all visible faces	N/A	N/A	On 1st story only On a maximum of 2 awning faces
Window	15% of window	N/A	N/A	On 1st and 2nd story only

Non-Residential Uses: Ground Signage Table

Ground Signs Table ²					
Sign Types	Permitted	Maximum Display Surface Area of Individual Signs	Maximum Height	Minimum Setback from Side Property Line	Specifications
Monument ³	1 per street frontage	48 square feet	2'6" for any part of a sign within 15 feet of a driveway, 7 feet otherwise	3 feet	
Hanging	1 for buildings with a set back greater than 15'	6 square feet	6 feet		The maximum projection shall be 2 feet 6 inches
Freestanding Pillar Signs ³ (Subdistricts 1 and 2 only)	1 per arterial street frontage for buildings with a set back greater than 15'	42 square feet	15 feet		No more than 20% of the height of the sign area shall extend above the top of the pillars.

Residential Uses: Signage Table

Sign Types	Permitted	Maximum Sign Area	Maximum Height	Maximum Projection	Specifications
Building Sign					
Buildings with 3 to 15 units	1 flush building sign per street facade	6 square feet	Below 2nd story Finished Floor Elevation (FFE) or below top of parapet/eave	N/A	Signs shall not contain internal lighting.
Buildings with 16 or more units		20 square feet			
Ground sign					
Monument	1 per street frontage	20 square feet	5 feet in height or 2'6" within 15 feet of a driveway.	N/A	A low wall may be substituted for a Monument sign.

¹ Any sign that encroaches on a public right-of-way must meet Metropolitan Government's current clearance standards and the encroachment must first be approved under the mandatory referral process.

² Maximum of one Ground Sign permitted per street frontage for non-residential development.

³ Properties with greater than 400 feet of frontage shall be permitted two ground signs. Signs on a single property shall be spaced a minimum of 100 feet apart.

Section II: Development Standards

Development Incentives

The following development incentives are only available to development that fully complies with the Development Standards of the UDO.

Maximum Floor Area

A development using incentives shall be allowed to exceed the maximum FAR for the cumulative floor space that results from the use of eligible incentives.

Adaptive Reuse

The provisions of Title 17.16.030 F, Adaptive Residential Development, shall apply to non-residential zoned property within the Downtown Donelson UDO and enables the inclusion of residential uses within a development. The Development Standards of the UDO shall apply.

Floor Area Exemption for Residential Use

In all non-residential districts that permit with conditions a residential use, no floor area ratio shall apply to those portions of a non-residential building converted to a residential use or new residential construction, provided a minimum of 25% of the gross floor area is devoted to residential use, as explicitly shown on the approved final site plan.

Parking Structure Floor Area Ratio (FAR) Exemption

To encourage the provision of consolidated parking facilities, the floor area of a parking garage may be excluded as floor area for the purpose of calculating floor area ratio. To be eligible for a FAR exemption the parking structure shall be lined with a minimum of 15 feet of active use.

Increased Setback

The front yard setback shall be increased 5 feet for any site proposed to be developed with the area between the property line and the front building wall designed as an outdoor dining courtyard, as explicitly shown on the approved final site plan.

Direct Street Frontage Access

To encourage land use activity with direct pedestrian access to a street or plaza frontage, leasable space that is located at street level with a minimum depth of 20 feet shall be excluded as floor area for the purpose of calculating floor area ratio.

Design Plan Street Dedication:

In exchange for property dedicated to provide streets and pedestrian connections delineated in the Street Network Plan, development rights are retained by the property owner making the dedication. These rights may be used, at the owner's option, on abutting property at the rate of 3 square feet of floor area for every 1 square foot of right-of-way dedicated to public street standards.

Low Impact Development (LID) Stormwater Management

A property owner electing to use Low Impact Development techniques for stormwater management that is incorporated as a design element into the site and is approved by Metro Stormwater as an LID project, shall derive the following increase in building floor area based on each square foot of LID provided:

- Residential: (6) six square feet
- All other: (3) three square feet

Section II: Development Standards

Development Incentives

Plaza and Transportation Floor Area Ratio (FAR) Bonus

For properties located within the UDO area, a floor area development bonus is offered in return for the design and construction of qualified* pedestrian plazas or contribution of money to the development of consolidated open space within the UDO district.

a) Plaza Bonus: A property owner electing to construct a public pedestrian plaza shall derive the following increase in building floor area based on each square foot of plaza space provided:

- Residential: 6 square feet
- All other: 3 square feet

*In order to be qualified, each plaza shall meet the following standards:

- The plaza shall comply with Americans with Disabilities Act Standards.
- The plaza shall have a minimum area of 1,000 square feet.
- The plaza shall be accessible to the public through a secured public easement.
- The plaza shall be a contiguous area with a maximum length to width ratio of 3:1.
- At least one edge of the plaza shall abut the front property line.
- When feasible, pedestrian linkages shall be provided to plazas located on adjacent lots.
- Any edges not abutting the front property line, another plaza, or an access way shall be fronted by leasable space with an entrance from the plaza.

b) Transit Stop Bonus: A property owner electing to construct a transit stop that is incorporated into the design of a building and is a Metropolitan Transit Authority approved stop and shelter shall derive the following increase in building floor area and/or reduction in parking using the following formula:

30% reduction in required parking

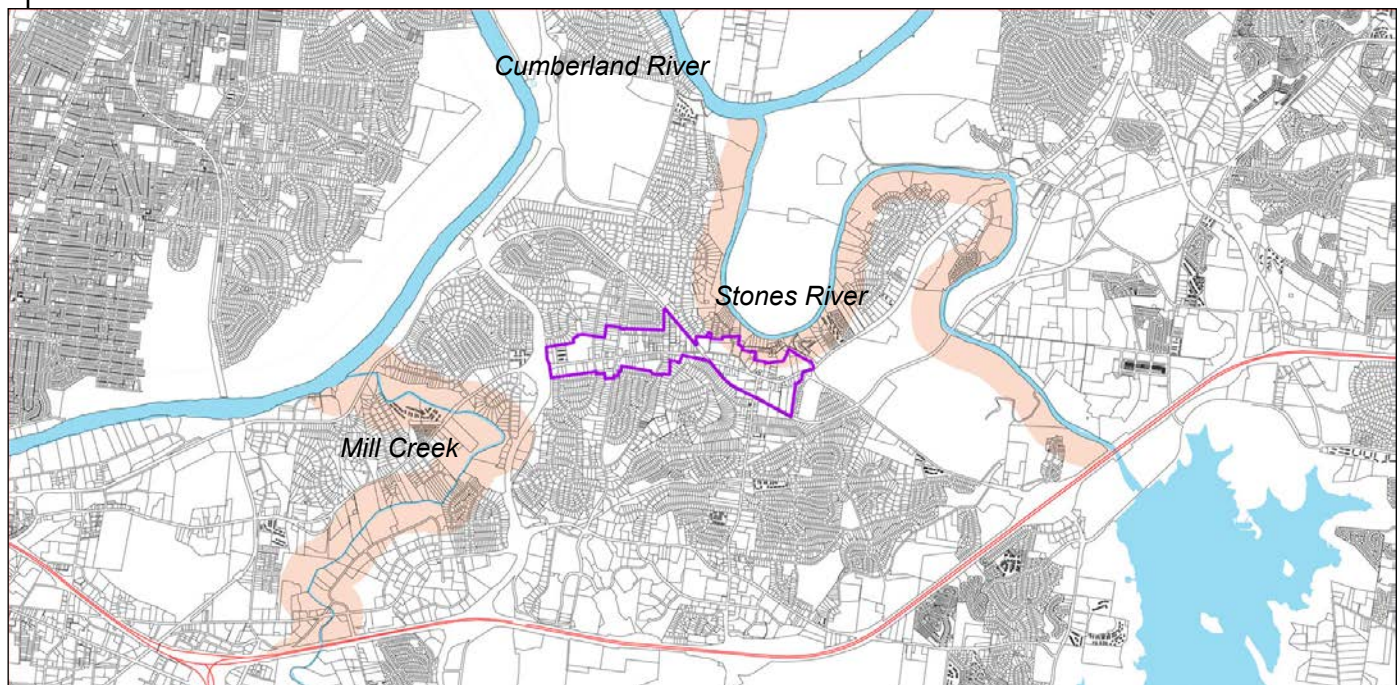
OR

An increase in floor area based on a reduction of 30% of parking spaces multiplied by 340 square feet.

Example: 30% of 100 parking spaces = $30 \times 340 = 10,200$ square feet.

Section II: Development Standards

Development Incentive: Transfer of Development Rights



Transfer of Development Rights Regulating Map

- | | |
|---|--|
|  Sending Sites |  Receiving Site - Donelson UDO Boundary |
|  Interstate 40 |  Major Water Bodies |

Purpose

The transfer of development rights provisions established by this section are intended to enhance Nashville and Davidson County's Greenway System and to protect sites along the western bank of the Stones River and both banks of Mill Creek in Donelson, while allowing owners of these sites to realize the value of their development entitlements. This is accomplished by permitting property owners in "sending sites", land preservation districts defined in this section, to transfer, through sale or donation, all or part of the property's unused development rights (undeveloped square footage) to a "receiving site" as defined in this section as property located with the Downtown Donelson Urban Design Overlay District, which is an area intended for higher-intensity development. The transfer of development rights provisions are established

pursuant to the authority contained in Sections 13-7-101 and 13-7-201 of the Tennessee Code Annotated.

Definitions and Regulations

Transfer of development rights between sites is allowed as follows:

- **Development Rights.** The "development rights" of a property within a land preservation district is defined as the square footage permitted for a property as calculated based on the floor area ratio (FAR) rights for non-residential development or density in dwelling units for residential development granted under the property's existing base zoning district. The only development rights available for transfer through sale or donation are the rights for the undeveloped square footage or dwelling units on the property.

Section II: Development Standards

Development Incentive: Transfer of Development Rights

- Sending Sites.
 - Location Criteria. Sites within the Stones River and Mill Creek land preservation districts as defined in the Downtown Donelson Urban Design Overlay may transfer development rights. These districts are the western bank of the Stones River, from the Cumberland River on the north to I-40 on the south; and both banks of Mill Creek, from Cumberland River on the north to I-40 on the south. The districts include land within 1,000 feet of the top of bank of each water body.
 - ii. Preservation Criteria. In order to transfer FAR or density, the area must contain floodway and floodway buffer in addition to lands containing development rights, and if located on Mill Creek must also include the dedication to the Metro Government of Nashville and Davidson County of a greenway conservation, and public access easement that includes the floodway plus a corridor at least 75 feet in width, measured from the outer edge of the floodway, and a permanent conservation easement for the property. In cases where the maximum cross-slope of the land included in the easement is greater than 15 percent, the greenway conservation easement width shall be extended to include an area at least 25 feet in width where a cross-slope of 15 percent or less exists, to enable an ADA accessible trail acceptable to the Greenways Commission Director to be constructed.
 - Conversion Table. The Conversion Table is to be used as guide when transferring density from residential districts to non-residential districts:
- Receiving Sites.
 - Location. Development rights may be transferred, through sale or donation, to any site located within the boundary of the Downtown Donelson Urban Design Overlay District. It is the applicant's responsibility to confirm with the metropolitan planning department, at the time of application, that the proposed receiving site is within the approved receiving site area as defined in this section.
 - Eligibility.
 - 1) Receiving sites eligible for donation of development rights. The Metropolitan Government of Nashville, the State of Tennessee or a not-for-profit conservation or preservation organization shall only be a receiving site through donation of development rights. A sending site property owner may donate all or part of the sending site development rights to these entities.
 - 2) Receiving sites eligible for purchase of development rights. Individual property owners or their designees may purchase development rights or receive the development rights as a donation. A sending site property owner may transfer all or part of the development rights to another individual or entity by donation or sale only with the consent of both the sending and receiving sites' property owners and through negotiations in a free marketplace.

Conversion Table

Type of Development in Sending Site	Square Footage Conversion to Determine Square Footage for Receiving Site
Single-family	2,500 square feet per unit
Multi-family Unit	Properties yielding less than 75 units -2000 square feet per unit Properties yielding more than 75 units -750 square feet per unit
Commercial	One square foot for one square foot

Section II: Development Standards

Development Incentive: Transfer of Development Rights

- **Transfer Procedure.** The procedure for a transfer of development rights must meet the following criteria:
 - **Application.** The owners or developers of the sending site and those of the receiving site shall file an application for transfer of development rights with the metropolitan planning department. The application shall be on a form provided by, and available from, the metropolitan planning commission and may be a joint application for both the sending site and the receiving site. A fee may be assessed to the application. At a minimum, the application shall include a warranty statement indicating that by requesting this transfer of development rights, the property owner of a sending site agrees that they, and anyone henceforth in the chain of title, will not apply for additional floor area ratio, square footage or any other form of density for the property in the future.
 - **Recording the Transfer of Development Rights from Sending Sites.** The conveyances of development rights from the sending site shall be in writing in an instrument that shall be signed by the owner of the sending site and shall be submitted to the metropolitan planning department for approval with the requirements of this section and such approval entered in writing on the document by the director or by another designee of the metropolitan planning department. At that point, the instrument shall be recorded in the office of the register of deeds subject to fees as set out in 8-21-1001 of Tennessee Code Annotated, with an additional copy of the instrument provided to the metropolitan planning commission for record purposes. This instrument should include the total square footage initially allowed, per FAR in the applicable zoning district for the property, the square footage transferred from the property, and the square footage of development that remains for the property. Once the transfer of development rights from the sending site is complete, this site shall forfeit any future claim for additional floor area ratio, square footage or other opportunity for increased intensity of development including, but not limited to, any type of rezoning. A statement to this effect shall be required on any subsequent deeds for the property.
 - **Recording the Transfer of Development Rights to Receiving Sites.** Whenever transferred development rights are allocated to a receiving property, such allocation shall not become effective until the transferred development rights are noticed in writing in an instrument that shall be signed by the owner of the receiving site and shall be submitted to the metropolitan planning department for approval with the requirements of this section and such approval entered in writing on the document by the director or by another designee of the Metropolitan Planning Department. At that point, the instrument shall be recorded in the office of the register of deeds subject to fees as set out in 8-21-1001 of Tennessee Code Annotated, with an additional copy of the instrument provided to the Metropolitan Planning Commission for record purposes. The instrument shall include the total square footage permitted on the receiving site per the FAR of its base zoning district and the additional square footage permitted through transfer of development rights. If the receiving site is subdivided or consolidated in the future, the transferred development rights shall be noted on any future plat(s), deed(s) or other relevant instrument used or referenced in any such subdivision or consolidation. A statement including this same information shall be noted on any future deeds for the property.

Section II: Development Standards

Development Incentive: Transfer of Development Rights

- Other Development Standards. For receiving sites, the transferred development rights (square footage) shall be in addition to those currently permitted by the receiving site's base zoning district and the Downtown Donelson Urban Design Overlay District. All other applicable development standards, including, but not limited to, building heights, sky exposure planes, and building setbacks shall continue to apply to both the sending site and the receiving site when a transfer of development rights occurs. The assessment of whether the transferred development rights can be accommodated within the receiving site's existing applicable development standards is the responsibility of the owners of the receiving sites.
- Conflict with Provisions of Zoning Code or Downtown Donelson Urban Design Overlay District. If there is a conflict between the development rights considered in this section and any other part of the Zoning Code or the Downtown Donelson Urban Design Overlay District, the Zoning Code or the Downtown Donelson Urban Design Overlay District shall prevail.

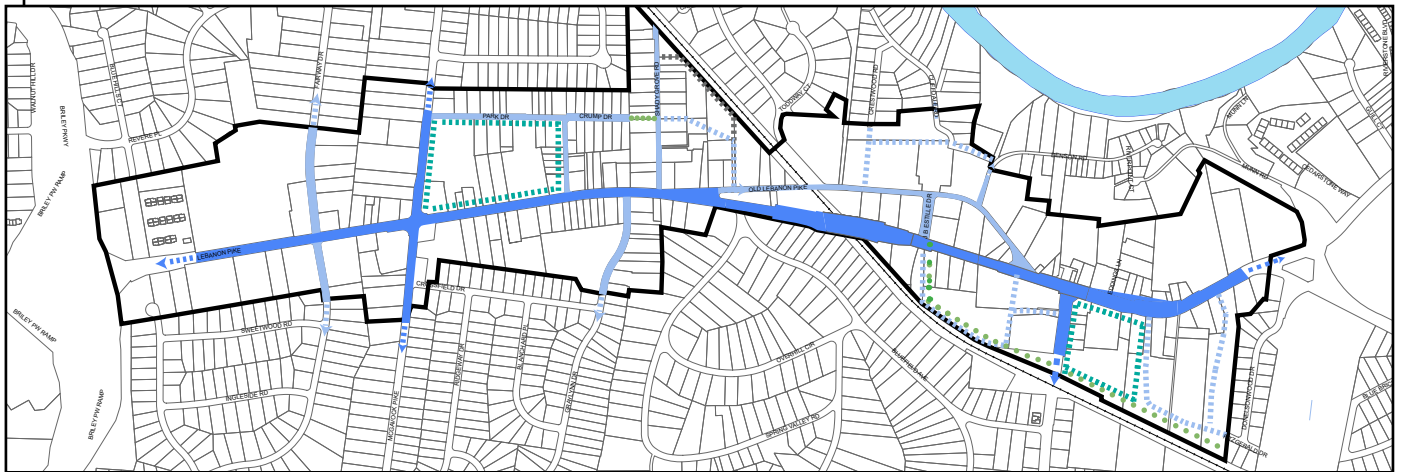
This page left intentionally blank

Section III: Transportation

Street Network Plan
Access Management
Sidewalk Standards

Section III: Transportation

Street Network Plan



Primary Street

Arterial Street

Proposed Primary Street

Proposed Arterial Street

Proposed Pedestrian Connection

Proposed Service Lane or Alley

Future connections should be provided in the area at the time of redevelopment

New Streets

New streets and alleys and street and alley upgrades shall be constructed as property develops within the UDO boundary in accordance with the Street Network Plan. The connection of Crump Drive to Shady Grove Road shall initially be pedestrian-only; further development of the connection as a working street connection may be considered if land uses along Shady Grove Road transition from light industrial to mixed-use or residential, both of which are more compatible with the mixed-housing district on the north side of Crump Drive.

Given the constraints of natural features, public utilities, difficulties in land assemblage, and reuse of existing structures, alternative scenarios to these network improvements may be considered, however, the applicant will be required to consider how their alternate scenario will affect the remaining properties within the UDO boundary and will be reviewed according to this standard.

Intersections

Intersections should provide adequate levels of service while facilitating both pedestrian and vehicular movement. Intersections should be designed with minimum curb radii to slow traffic and to reduce pedestrian crossing distances while accommodating safe vehicular movement.

Alleys and Service Lanes

Alleys or rear service lanes are required within the UDO, providing an opportunity to put garages and parking at the rear of buildings, allowing porches and pedestrian entries to front the street. Parking in the rear will keep the fronts of buildings from being dominated by garage doors or parking lots and the pedestrian travelways from being compromised by curb cuts. Adequate sight distance should be provided where alleys intersect streets.

On-street parking

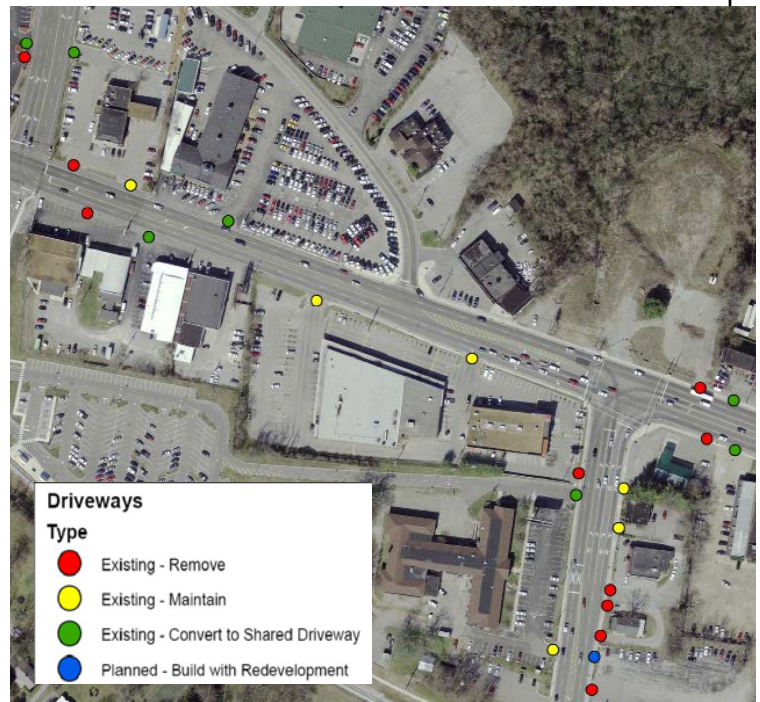
On-street parking may be considered along primary streets, pending pavement width and adequate space for travel lanes; arterial streets do not have on-street parking. Parking spaces along the street help reduce the number of required off-street spaces. On-street parking also provides convenient access for guests and patrons, creates a buffer between automobiles and pedestrians, and tends to slow the flow of through traffic. Bulb-outs that include street trees shall be installed wherever feasible to define permanent on-street parking and reduce the width of pavement that pedestrians must cross at intersections.

Section III: Transportation

Access Management

Access management regulates access point spacing and turn options, creating a safer, more predictable environment for drivers, pedestrians and bicyclists. Ideal driveway spacing for 40-45 miles per hour street is every 250-300 feet. Considering these findings, plus a standard of 300 foot spacing to prevent right-turn overlaps on a 40-45 mph road, a driveway spacing average standard of 200 feet is applicable.

Currently, many developments on Lebanon and McGavock Pike have their own driveway curb cuts and, in some instances two curb cuts. Some developments have a continuous curb cut along the entire property frontage, providing access to head-in parking. This situation is challenging for several reasons. The presence of curb cuts and the associated lack of cross access between properties make it necessary for cars to enter and exit the roadway to access each individual development along these streets, disrupting and slowing the flow of traffic. The head-in parking in front of other developments also causes traffic problems and can be a danger when parked cars have to back into traffic. Driveways need to be consolidated to accommodate more intense development without impacting the safety and congestion of traffic on arterial streets. Additionally, multiple curb cuts cause conflict between automobile traffic entering and exiting the roadway and pedestrian traffic along the sidewalk. The accompanying graphic illustrates the conceptual consolidation of driveways that should be considered through the development review process.



Donelson UDO Access Standards

Subdistrict 1, 2

- One driveway per street frontage if no other options are available, 2 driveways per street frontage for parcels with 400 or more feet of frontage, joint use provisions required

Subdistrict 3

- According to Title 17.20.160 and 17.20.170

Subdistrict 4

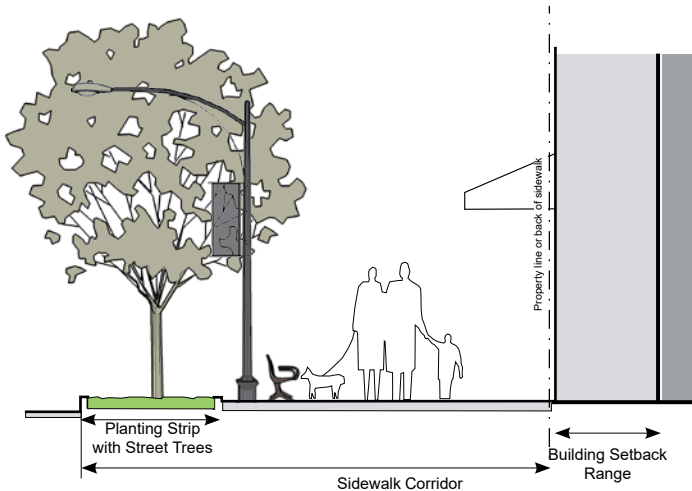
- One driveway per street frontage if no other options are available, 2 driveways per street frontage for parcels with 400 or more feet of frontage, joint use provisions required

Subdistrict 5

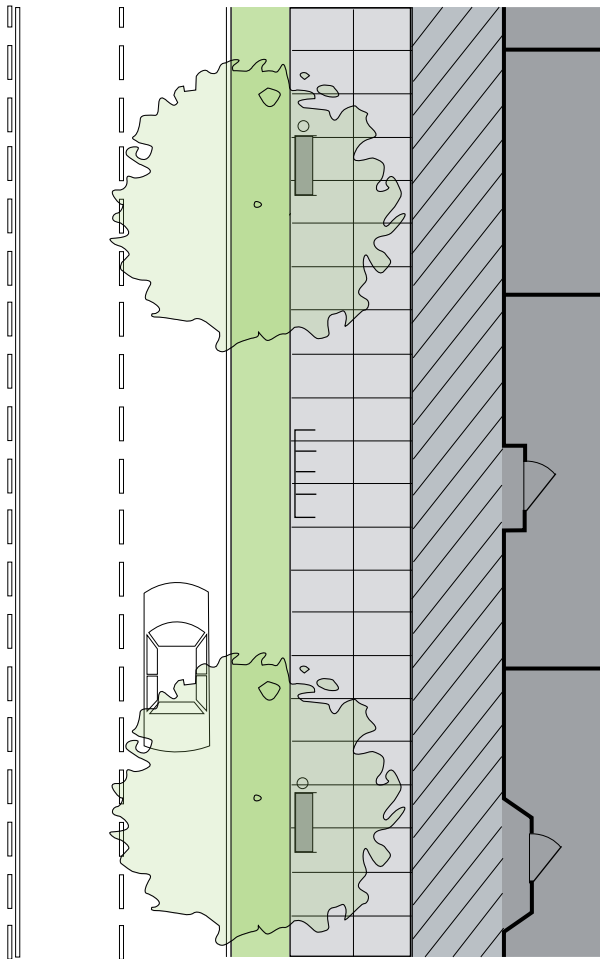
- According to Title 17.20.160 and 17.20.170

Section III: Transportation

Sidewalk Corridor



Arterial Street - Sidewalk Corridor Section with on-street parking



Arterial Street - Sidewalk corridor plan with on planting strip

New sidewalk construction, sidewalk improvements, and the installation of street trees shall be completed as property develops in accordance with the application standards.

Sidewalk Corridor

The Sidewalk Corridor is the portion of the public right-of-way located between the edge of motor vehicle, bicycle and/or parking lanes and the back of the sidewalk. The primary function of a Sidewalk Corridor is to provide a safe, comfortable, and convenient route for walking that is separated from vehicle movements. A Sidewalk Corridor may also accommodate other functions or fixtures, such as utility poles, street trees, planting strips, and street furniture.

The diagram shows the desired sidewalk corridor for Lebanon Pike. Sidewalks shall be designed based upon Metro's Major and Collector Street Plan and in accordance with the standards of Metro Public Works. Sidewalks on existing primary and arterial streets within Subdistrict 1 should provide at minimum a four foot green zone with street trees and an eight foot sidewalk.

Street Trees

Street trees are required along all streets. Tree species should remain consistent along a given block, and should be chosen for drought tolerance and their ability to create an effective canopy. Street trees shall be maintained by the adjacent property owner.

- On all streets, street trees shall be installed at an average spacing of 35 feet.
- Trees planting areas shall be no less than a 40 square feet at sidewalk grade. Tree grates are permitted and shall be modular to allow for removal as the tree grows.
- Tree species shall be chosen from the *Urban Forestry Recommended and Prohibited Tree and Shrub List*.
- At planting all trees shall be a minimum of 2 caliper inches and 6 feet in height.

Section IV: Community Planning

Process
Concept Plan
Subdistrict Goals and Objectives
Development Scenarios

Section IV: Community Planning

Planning Process

The Downtown Donelson UDO was crafted over a series of community meetings beginning in February, 2009 and ending in September, 2009. The community meetings were divided into two phases, Community Visioning and Urban Design Overlay Development.

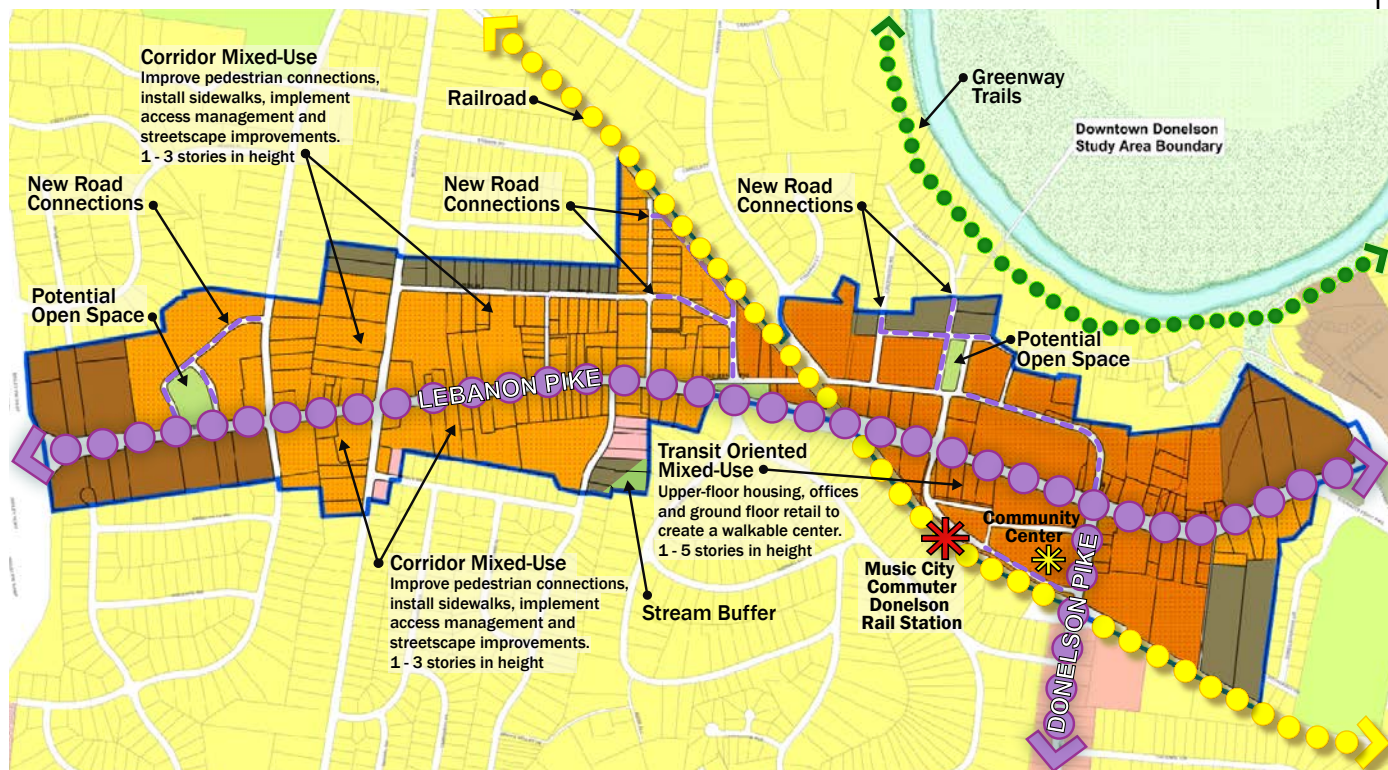
On February 9, 2009, the Community Visioning series was kicked off with a Visioning Workshop held at McGavock High School. The workshop was attended by approximately 150 participants. Planning Staff began the meeting by presenting an overview of the community planning process, principles of sustainable community development, a brief explanation of transit oriented development and an inventory of existing site conditions. The participants then divided into nine groups and Planning Staff served as a facilitator for each group. The information received from each group was distilled into a Concept Plan, laying the foundation for the rest of the process.

The later meetings in the Community Visioning series focused on refining the Concept Plan, choosing a System Strategy for implementing the Vision, and determining the Final Vision. The Concept Plan laid out the broad vision for the future of what Donelson should become. The Systems Strategies discussed focused on Planning Implementation tools available to ensure the vision, and guest speakers provided information on economic development tools that may be available. The idea of Transfer of Development Rights was introduced by a guest speaker as an economic development tool and a way to preserve the banks of Mill Creek and the Cumberland River in the area directly surrounding Donelson. The Planning tools discussed were Specific Plan (SP) and Urban Design Overlay (UDO). The participants and staff agreed that the Urban Design Overlay would be the best fit for the Donelson Community.

The Urban Design Overlay Development series of community meetings began with a two day Informal Work Session, where Planning Staff met on-site at the Donelson Fifty Forward Center and members of the community were invited to come by to discuss their concerns and desires with staff individually. Then four community meetings were held to focus on the standards for the different design districts of the UDO. The final meeting was held in late August, 2009, and focused on how the UDO would be triggered with new development, and the development incentives available to properties developing according to the standards of the UDO.

Section IV: Community Planning

Concept Plan



Downtown Donelson Concept Plan from 2009



Participants at the Donelson UDO Visioning Meeting

Section IV: Community Planning

Goals and Objectives: Subdistrict 1



TOD development



Train station

Create Transit Oriented Development (TOD); development that is within a 5 minute walk or ¼ mile in distance to the Donelson Station, includes a mixture of commercial, residential, and office land uses, and that coordinates with other modes of transportation (bike, pedestrian and vehicular).

Objectives: Buildings and Lots

- Create a transit-oriented mixed use district around the train station by promoting transit-oriented principles intended to integrate land use and transit and by providing greater density than the community average, a mix of uses, and a quality pedestrian environment around a defined center.
- Buildings may be a minimum of 1 story and a maximum of 5 stories in height.
- Design vertical mixed use buildings to accommodate active ground floor uses, such as retail and entertainment, while providing office and residential uses on upper floors.
- Place buildings on lots with shallow setbacks and with orientation to the street

Artists Rendering of Transit Oriented Development around the Donelson train station and Donelson Plaza



Section IV: Community Planning

Goals and Objectives: Subdistrict 1

or to the Donelson Station to increase active uses on the street, create a street wall, and to create pedestrian scaled spaces.

- Construct buildings of high quality building materials that require little maintenance in order to demonstrate sustained quality and a sense of permanence.
- Place overhead utilities on taller poles, in alleys, or underground as properties redevelop.

Objectives: Parks and Open Space

- Design necessary detention and water quality areas as amenities by providing seating, walkways, and landscaping.
- Provide hardscaped plazas within Subdistrict 1 that provide opportunities for seating and that may be accessed by pedestrians from nearby development.
- Enhance the open space adjacent to the Donelson Station and the senior center as a focal point.
- Create opportunities for open space near mixed housing areas north of Old Lebanon Pike, and design it as a focal point at the intersection of Cliffdale Road and Old Lebanon Pike.

Objectives: Vehicular Circulation

- Create new street connections near Crestwood Drive and Cliffdale Road to improve vehicular circulation and create a block structure.
- Realign Old Lebanon Pike with Donelson Pike to provide greater vehicular circulation and access to and from the Donelson Station.
- Accommodate other forms of vehicular travel along Lebanon Pike by including bike lanes and bus transit stops.

Objectives: Bicycle and Pedestrian Circulation

- Provide pedestrian facilities to and from individual development and to the Donelson Station, including sidewalks and crosswalks in parking lots through such means as markings, textured pavement, and other walkways and landscaping.
- Install crosswalks with pedestrian signalization at intersections.
- Construct appropriately-designed sidewalks where gaps exist or where sidewalks are deficient in the current sidewalk system.
- Install benches, seat walls, trash receptacles, and other pedestrian amenities to enhance the pedestrian environment.
- Provide bike lanes and/or bike routes that connect the Donelson Station and Transit Oriented Development (TOD) to neighborhoods and to greenway routes.
- Place pedestrian scaled lighting along the street near sidewalks and at major pedestrian crossing areas.



Public park



Main street development



Properly marked crosswalk



Coordinated signage

Section IV: Community Planning

Goals and Objectives: Subdistrict 1



Incorporated public transit



Parking located behind buildings



Mixed-use development



Pedestrian oriented street

Objectives: Signage

- Create signage that is appropriately scaled for motorists, as well as for pedestrians and bicyclists. Place and illuminate signs in a manner that is appropriate for creating and maintaining a pedestrian environment; building mounted, awning, or monument signs are appropriate.
- Develop a signage program that creates guidelines for signage to be used in the public right-of-way that establishes an identity for the area and directs visitors to important community and transportation facilities (the Donelson Star Train Station for example).
- Consolidate signs where there are multiple businesses on one property. Monument signs are appropriate for this purpose.
- Use signs to clearly convey a message. Design signs with simple, straightforward shapes. Use lettering styles that are simple, easy to read, and in proportion with the rest of the sign.

Objectives: Transit

- Make transit stops focal points as properties redevelop into mixed-use destinations; locate transit stops in areas that are accessible, visible and well lit.
- Make transit stops more accessible from neighborhoods and new development through new street and sidewalk connections.

Objectives: Access

- Limit multiple curb cuts and access points by developing minimum spacing standards for curb cuts, restricting the width of access points, and requiring shared access points for adjacent parcels or groups of parcels. Doing so increases internal circulation within existing and future developments and minimizes interruptions to the sidewalk network.
- Access for development should be from alleys and local side streets.
- Encourage shared access and cross-access between adjacent properties owners.

Objectives: Parking

- Locate parking to the rear or sides of buildings as appropriate.
- Provide cross-access between parking areas to minimize street curb cuts and adjacent access points.
- Separate parking areas from buildings with landscaping to avoid parking areas directly abutting buildings.
- Encourage on-street parking along appropriate streets to lessen the need for off-street parking, provide easy access to businesses, create a buffer between moving traffic and pedestrians, and to serve as a traffic calming device that slows vehicles.

Section IV: Community Planning

Goals and Objectives: Subdistrict 1

- Design parking structures with first floor active uses, liner buildings, or articulated facades. Integrate retail uses on the ground floors of parking structures serving buildings along Lebanon Pike to minimize the visual impact of parking structures and to add life to the street. If retail uses are not appropriate, locate parking structures below or behind buildings and include landscaping to lessen their visual impact.
- Develop shared parking plans for developments with different peak parking demands and operating hours to minimize the total amount of parking spaces needed.



Street trees



Screened parking

Objectives: Landscaping and Buffering

- Include long-term maintenance provisions in landscaping and tree planting projects.
- Plant trees, shrubs, and groundcover in order to break up large expanses of paving, to divide masses of parked cars, and to screen surface parking lots from view.
- Screen utilities, meter boxes, heating and cooling units, and other building systems that are visible from a public right-of-way.
- Plant street trees along Lebanon Pike as properties redevelop. Street trees help to contribute to the community's character, provide a clear separation between pedestrians and automobiles, soften the visual impact of development, provide a greater sense of comfort for pedestrians, diminish traffic noise, and screen unwanted views.

Artists Rendering of Lebanon Pike and new signage marking the entrance to the Donelson Station



Section IV: Community Planning

Goals and Objectives: Subdistrict 2



Buildings with parking in front



Suburban scale with urban placement



Screened parking

Enhance the suburban character of Lebanon Pike while promoting sustainable development and a higher standard of design through landscaping, access management and transit, signage, and mixed use development standards and an enhanced pedestrian environment. Development closer to Subdistrict 1 should transition between intense transit oriented development to less intense suburban corridor.

Objectives: Buildings and Lots

- Create a unique sense of place by preserving existing locally-owned small businesses and encouraging construction of new buildings of the appropriate scale, with proper orientation to the street and architectural detailing.
- Buildings may be a minimum of 1 story and a maximum of 3 stories in height. Design vertical mixed use buildings to accommodate active ground floor uses, such as retail and entertainment, while providing office and residential uses on upper floors; a mixture of uses should be provided on the same lot or block face if they are not provided in a vertically mixed use building form.
- Place buildings on lots with shallow to moderate setbacks and with orientation to the street to increase active uses on the street, create a street wall, and to create pedestrian scaled spaces. On larger lots with multiple buildings, orient buildings onto private streets and drives rather than onto parking.
- Construct buildings of high quality building materials that require little maintenance in order to demonstrate sustained quality and a sense of permanence.
- Create appropriate transitions between more intense uses along Lebanon Pike and less intense residential development through building design that considers scale, massing and orientation; use landscaping as a transition or buffer where necessary.
- Articulate building facades to break up large blank walls along streets.
- Place overhead utilities on taller poles, in alleys, or underground as properties redevelop.

Objectives: Parks and Open Space

- Design necessary detention and water quality areas as amenities by providing seating, walkways, and landscaping.
- Provide courtyards that combine hardscaped features with landscaped features, where opportunities for seating are provided, and that may be accessed by pedestrians from nearby development.
- Preserve and enhance the existing open space and gateway feature at the intersection of Old Lebanon Pike and Lebanon Pike.

Objectives: Vehicular Circulation

- Accommodate other forms of vehicular travel along Lebanon Pike by including bike lanes and or/bike routes and bus transit stops.
- Redesign Old Lebanon Pike to prioritize the existing north-south access to and from Lebanon Pike eliminating the Old Lebanon Pike and Lebanon Pike split.
- Limit multiple curb cuts and access points by developing minimum spacing

Section IV: Community Planning

Goals and Objectives: Subdistrict 2

standards for curb cuts for properties restricting the width of access points, requiring shared access drives for adjacent parcels or groups of parcels and increasing internal circulation within existing and future developments.

Objectives: Bicycle and Pedestrian Circulation

- Accommodate non-vehicular travel along Lebanon Pike and Old Lebanon Pike by including sidewalks and crosswalks.
- Install crosswalks with pedestrian signalization at intersections.
- Provide bike lanes and bike routes along Lebanon Pike and Old Lebanon Pike that connect to adjacent neighborhoods and to greenway routes.
- Provide pedestrian facilities to and from individual development including sidewalks and crosswalks in parking lots through such means as markings, textured pavement, and other walkways and landscaping.
- Construct appropriately-designed sidewalks where gaps exist or where sidewalks are deficient in the current sidewalk system.
- Install benches, seat walls, trash receptacles, and other pedestrian amenities to enhance the pedestrian environment.
- Place pedestrian lighting along the street near sidewalks, and at major pedestrian crossing areas to enhance safety.



Sidewalk and crosswalk

Artists Rendering of mixed use development in Subdistrict 2. Development along Lebanon Pike is either built at the back of sidewalk or behind one row of parking. Residential and office uses along Park Drive helps transition from the intense commercial uses of the corridor to the single-family residential neighborhood to the north. Shared parking for a grocery store, church and residential uses helps reduce the overall amount of space devoted to parking.



Section IV: Community Planning

Goals and Objectives: Subdistrict 2



Multi-tenant monument sign



Transit stop



Pedestrian amenities



One row of parking in front of building

Objectives: Signage

- Create signage that is appropriate in scale for motorists, as well as for pedestrians and bicyclists. Place and illuminate signs in a manner that is appropriate for creating and maintaining a pedestrian environment; building mounted, awning, or monument signs are appropriate.
- Consolidate signs where there are multiple businesses on one property. Monument signs are appropriate for this purpose.
- Use signs to clearly convey a message. Design signs with simple, straightforward shapes. Use lettering styles that are simple, easy to read, and in proportion with the rest of the sign.

Objectives: Transit

- Make transit stops focal points as properties redevelop into mixed-use destinations; locate transit stops in areas that are accessible, visible and well lit.
- Make transit stops more accessible from neighborhoods and new development through new street and sidewalk connections.

Objectives: Access

- Develop access management guidelines for Lebanon Pike in order to make pedestrian and bicycle travel safer, improve the appearance of the corridor, reduce traffic delay and congestion, and improve roadway safety conditions.
- Limit multiple curb cuts and access points by developing minimum spacing standards for curb cuts, restricting the width of access points, and requiring shared access points for adjacent parcels or groups of parcels. Doing so increases internal circulation within existing and future developments and minimizes interruptions to the sidewalk network.
- Reduce the number of individual curb cuts by requiring cross access among adjacent parking lots allowing vehicles to circulate between buildings without having to re-enter Lebanon Pike and Old Lebanon Pike.
- Access for development should be from alleys and local side streets. Improve existing access drives with paving and appropriate lighting.
- Encourage shared access and cross-access between adjacent properties owners.

Objectives: Parking

- Locate parking to the rear or sides of buildings as appropriate. Where development constraints exist along Lebanon Pike, allow one-row of parking in front of buildings, but screen parking in order to minimize direct views from the corridor.
- Provide cross-access between parking areas to minimize street curb cuts and adjacent access points.
- Separate parking areas from buildings with landscaping to avoid parking areas directly abutting buildings.
- Develop shared parking plans for developments with different peak parking demands and operating hours to minimize the total amount of parking spaces needed.

Section IV: Community Planning

Goals and Objectives: Subdistrict 2

- Encourage on-street parking along appropriate streets to lessen the need for off-street parking, provide easy access to businesses, create a buffer between moving traffic and pedestrians, and to serve as a traffic calming device that slows vehicles.

Objectives: Landscaping and Buffering

- Use landscaping in the form of trees and shrubs to screen surface parking lots, vehicles, utilities, meter boxes, heating and cooling units, and other building systems that are visible from a public right-of-way.
- Plant trees, shrubs, and groundcovers, in order to break up large expanses of paving and to divide masses of parked cars into smaller groups of parking areas in off-street parking lots and to screen surface parking lots from view.
- Screen utilities, meter boxes, heating and cooling units, and other building systems that are visible from a public right-of-way.
- Include long-term maintenance provisions in landscaping and tree planting projects.
- Plant street trees along Lebanon Pike and Old Lebanon Pike as properties redevelop. Street trees help to contribute to the community's character, provide a clear separation between pedestrians and automobiles, soften the visual impact of development, provide a greater sense of comfort for pedestrians, diminish traffic noise, screen unwanted views, reduce glare, absorbs heat, and filters air pollution and dust.



Parking lot landscaping

Artists Rendering of Development along Lebanon Pike in Subdistrict 2. Buildings are setback behind one row of parking. Sidewalk improvements, street trees and monument signage create a pedestrian friendly, walkable environment.



Section IV: Community Planning

Goals and Objectives: Subdistrict 3



Office building



Industrial building



Industrial building with landscaping



Industrial or office building

Enhance this area by providing infrastructure that would support light industrial and office land uses.

Objectives: Buildings and Lots

- Create a unique sense of place by preserving existing locally-owned small businesses and encouraging construction of new buildings of the appropriate scale, with proper orientation to the street and architectural detailing.
- Buildings may be a minimum of 1 story and a maximum of 3 stories in height.
- Place buildings on lots with moderate setbacks and with orientation to the street to increase active uses on the street, create a street wall, and to create pedestrian scaled spaces. On larger lots with multiple buildings, orient buildings onto private streets and drives rather than onto parking.
- Construct buildings of high quality building materials that require little maintenance in order to demonstrate sustained quality and a sense of permanence.
- Create appropriate transitions between more intense light industrial and office uses and less intense residential development through building design that considers scale, massing and orientation; use landscaping as a transition or buffer where necessary.
- Articulate building facades to break up large blank walls along streets.
- Place overhead utilities on taller poles, in alleys, or underground as properties redevelop.

Objectives: Parks and Open Space

- Design necessary detention and water quality areas as amenities by providing seating, walkways, and landscaping.

Objectives: Vehicular Circulation

- Create pedestrian walkways that would connect Crump Drive to Shady Grove Road. If future development along Shady Grove Road converts from the current light industrial uses to more mixed use and residential uses, transition the pedestrian connection to a street connection.
- Create street connections that would connect Shady Grove Road to Lebanon Pike.
- Limit multiple curb cuts and access points by developing minimum spacing standards for curb cuts for properties restricting the width of access points, requiring shared access drives for adjacent parcels or groups of parcels and increasing internal circulation within existing and future developments.

Section IV: Community Planning

Goals and Objectives: Subdistrict 3

Objectives: Bicycle and Pedestrian Circulation

- Provide pedestrian facilities to and from individual development including sidewalks and crosswalks in parking lots through such means as markings, textured pavement, and other walkways and landscaping.
- Construct appropriately-designed sidewalks on new streets or where gaps exist along existing streets.
- Provide bike lanes and bike routes along new and existing streets that connect to adjacent neighborhoods and to greenway routes.
- Place lighting along the street near sidewalks, and at major pedestrian crossing areas to enhance safety.

Objectives: Signage

- Create signage that is appropriately scaled for motorists, as well as for pedestrians and bicyclists. Place and illuminate signs in a manner that is appropriate for creating and maintaining a pedestrian environment; building mounted, awning, or monument signs are appropriate.
- Consolidate signs where there are multiple businesses on one property. Monument signs are appropriate for this purpose.
- Use signs to clearly convey a message. Design signs with simple, straightforward shapes. Use lettering styles that are simple, easy to read, and in proportion with the rest of the sign.

Objectives: Transit

- Make transit stops more accessible from neighborhoods and new development through new street and sidewalk connections.

Objectives: Access

- Limit the width of access points to minimize interruptions to the sidewalk network.
- Encourage shared access and cross-access between adjacent properties owners.

Objectives: Parking

- Locate parking to the rear or sides of buildings as appropriate. Where development constraints exist allow one-row of parking if front of buildings, but screen parking in order to minimize direct views from the street.
- Separate parking areas from buildings with landscaping to avoid parking areas directly abutting buildings.

Objectives: Landscaping and Buffering

- Use landscaping in the form of trees and shrubs to screen surface parking lots, vehicles, utilities, meter boxes, heating and cooling units, and other building systems that are visible from a public right-of-way.
- Plant trees, shrubs, and groundcovers, in order to break up large expanses of paving and to divide masses of parked cars into smaller groups of parking areas in off-street parking lots.



Signage



Screening



Landscaping

Section IV: Community Planning

Goals and Objectives: Subdistrict 4

Create development along the Lebanon Pike corridor that preserves the residential and civic character of these areas, while providing additional housing choice that accommodates various lifestyle options of residents in the area.

Objectives: Buildings and Lots

- Place buildings on lots with moderate setbacks and with orientation to the street to place activity on the street, but yet maintain a separation between the street and private residences with porches, stoops, or front yards.
- Buildings may be a minimum of 1 story and a maximum of 3 stories in height.
- Civic and public benefit building placement may vary from other buildings in order to show prominence. Buildings however should be visible from the street and have entrances that orient to the street. Parking should remain behind or beside the building to preserve open space in front of the building or to frame the street with the building.
- Construct buildings of high quality building materials that require little maintenance in order to demonstrate sustained quality and a sense of permanence.
- Place overhead utilities on taller poles, in alleys, or underground as properties redevelop.

Objectives: Parks and Open Space

- Design necessary detention and water quality areas as amenities by providing seating, walkways, and landscaping.
- Provide meaningful open space as an integral part of residential designs.

Objectives: Vehicular Circulation

- Limit multiple curb cuts and access points by developing minimum spacing standards for curb cuts for properties restricting the width of access points, requiring shared access drives for adjacent parcels or groups of parcels and increasing internal circulation within existing and future developments.
- Accommodate other forms of vehicular travel by including bike lanes and or/ bike routes and bus transit stops.

Objectives: Bicycle and Pedestrian Circulation

- Construct appropriately-designed sidewalks where gaps exist or where sidewalks are deficient in the current sidewalk system.
- Install crosswalks with pedestrian signalization at intersections.
- Install benches, seat walls, trash receptacles, and other pedestrian amenities to enhance the pedestrian environment.
- Provide bike lanes and bike routes that connect residential development to facilities along Lebanon Pike.
- Place pedestrian scaled lighting along the street near sidewalks, and at major pedestrian crossing areas.

Objectives: Signage

- Create signage that is appropriate in scale for motorists, as well as for



Residential building type



Civic building type



Common open space



Bike and pedestrian path

Section IV: Community Planning

Goals and Objectives: Subdistrict 4

pedestrians and bicyclists. Place and illuminate signs in a manner that is appropriate for creating and maintaining a pedestrian environment; building mounted, awning, or monument signs are appropriate.

- Use signs to clearly convey a message. Design signs with simple, straightforward shapes. Use lettering styles that are simple, easy to read, and in proportion with the rest of the sign.

Objectives: Transit

- Make transit stops focal points when planning of residential development along Lebanon Pike; locate transit stops in areas that are accessible, visible and well lit.
- Make transit stops more accessible from neighborhoods and new development through new street and sidewalk connections.

Objectives: Access

- Reduce the number of individual curb cuts by requiring cross access among adjacent parking lots allowing vehicles to circulate between buildings without having to re-renter the street. In most cases, frontage roads to access residential development oriented to Lebanon Pike are appropriate.
- Access for development should be from alleys and local side streets.
- Limit the width of parking accesses from local streets to minimize interruptions to the sidewalk network.
- Encourage shared access and cross-access between adjacent properties owners.

Objectives: Parking

- Locate parking to the rear or sides of buildings as appropriate.
- Encourage shared parking.
- Separate parking areas from buildings with landscaping to avoid parking areas directly abutting buildings.
- Encourage on-street parking along appropriate streets to lessen the need for off-street parking, provide easy access to businesses, create a buffer between moving traffic and pedestrians, and to serve as a traffic calming device.

Objectives: Landscaping and Buffering

- Use landscaping in the form of trees and shrubs to screen surface parking lots, utilities, meter boxes, heating and cooling units, and other building systems that are visible from a public right-of-way.
- Plant trees, shrubs, and groundcover, in order to break up large expanses of paving and to divide masses of parked cars into smaller groups of parking areas in off-street parking lots.
- Include long-term maintenance provisions in landscaping projects.
- Plant street trees along Lebanon Pike as properties redevelop. Street trees help to contribute to the community's character, provide a clear separation between pedestrians and automobiles, soften the visual impact of development, provide a greater sense of comfort for pedestrians, diminish traffic noise, screen unwanted views, reduce glare, absorbs heat, and filters air pollution and dust.



Signage



Rear access parking garages



Real located parking and screening



Landscaping and screening

Section IV: Community Planning

Goals and Objectives: Subdistrict 5



Residential building type



Mixed use building type



Open space



Sidewalk and street trees

Create development in the form of mixed housing that provides additional housing choice for the various lifestyle options of residents in the area. Include office land uses along with the mixed housing to create transitions between intense commercial land uses, and less intense residential neighborhoods.

Objectives: Buildings and Lots

- Place buildings on lots with moderate setbacks and with orientation to the street to place activity on the street, but yet maintain a separation between the street and private residences.
- Buildings may be a minimum of 1 story and a maximum of 3 stories in height. Create appropriate transitions between more intense office uses and less intense residential development through building design that considers scale, massing and orientation; use landscaping as a transition or buffer where necessary.
- Construct buildings of high quality building materials that require little maintenance in order to demonstrate sustained quality and a sense of permanence.
- Place overhead utilities on taller poles, in alleys, or underground as properties redevelop.

Objectives: Parks and Open Space

- Design necessary detention and water quality areas as amenities by providing seating, walkways, and landscaping.
- Provide meaningful open space as an integral part of residential designs.

Objectives: Vehicular Circulation

- Limit multiple curb cuts and access points by developing minimum spacing standards for curb cuts for properties restricting the width of access points, requiring shared access drives for adjacent parcels or groups of parcels and increasing internal circulation within existing and future developments.
- Accommodate other forms of vehicular travel by including bike lanes and or/ bike routes and bus transit stops.

Objectives: Bicycle and Pedestrian Circulation

- Construct appropriately-designed sidewalks where gaps exist or where sidewalks are deficient in the current sidewalk system.
- Install benches, seat walls, trash receptacles, and other pedestrian amenities to enhance the pedestrian environment.
- Provide bike lanes and bike routes that connect residential development to facilities along Lebanon Pike.
- Place pedestrian scaled lighting at transit stops, along the street near sidewalks, and at major pedestrian crossing areas.

Section IV: Community Planning

Goals and Objectives: Subdistrict 5

Objectives: Signage

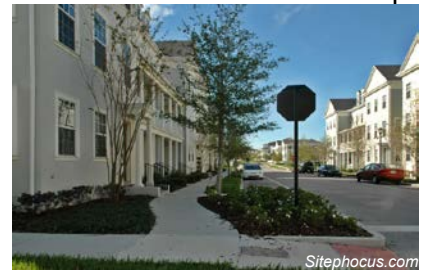
- Create signage that is appropriate in scale for motorists, as well as for pedestrians and bicyclists. Place and illuminate signs in a manner that is appropriate for creating and maintaining a pedestrian environment; building mounted, awning, or monument signs are appropriate.
- Use signs to clearly convey a message. Design signs with simple, straightforward shapes. Use lettering styles that are simple, easy to read, and in proportion with the rest of the sign.
- Create signs for office land uses that are not intrusive to adjacent residential land uses.



Appropriately scaled signage

Objectives: Transit

- Make transit stops focal points in the planning of residential development along Lebanon Pike; locate transit stops in areas that are accessible, visible and well lit.
- Make transit stops more accessible from neighborhoods and new development through new street and sidewalk connections.



Sidewalks

Artists Rendering of Development along Park Drive, Crump Drive and Shady Grove Road. The UDO envisions a diversity of residential building types in Subdistrict 5 from single-family homes to town houses to stacked flat buildings.



Section IV: Community Planning

Goals and Objectives: Subdistrict 5

Objectives: Access

- Reduce the number of individual curb cuts by requiring cross access among adjacent parking lots allowing vehicles to circulate between buildings without having to re-enter new or existing streets. In most cases, frontage roads to access residential development oriented to Lebanon Pike are appropriate.
- Access for development should be from alleys and local side streets.
- Limit the width of parking accesses from local streets to minimize interruptions to the sidewalk network.
- Encourage shared access and cross-access between adjacent properties owners.



On-street parking on primary streets



Parking garages accessed from rear

Artists Rendering of Development along Park Drive, Crump Drive and Shady Grove Road. The UDO envisions a diversity of residential building types in Subdistrict 5 from single-family homes to town houses to stacked flat buildings. The Mixed-use development in Subdistricts 3, along Shady Grove Road, is intended to remain industrial and commercial in character with added pedestrian amenities such as sidewalks, landscaping and street trees.



Section IV: Community Planning

Goals and Objectives: Subdistrict 5

Objectives: Parking

- Locate parking to the rear or sides of buildings as appropriate. Where development constraints exist for office land uses, allow one-row of parking if front of buildings, but screen parking in order to minimize direct views from the Lebanon Pike corridor or less prominent side streets.
- Provide cross-access between parking areas to minimize street curb cuts and adjacent access points.
- Separate parking areas from buildings with landscaping to avoid parking areas directly abutting buildings.
- Encourage on-street parking along appropriate streets to lessen the need for off-street parking, provide easy access to businesses, create a buffer between moving traffic and pedestrians, and to serve as a traffic calming device that slows vehicles.

Objectives: Landscaping and Buffering

- Use landscaping in the form of trees and shrubs to screen surface parking lots, vehicles, utilities, meter boxes, heating and cooling units, and other building systems that are visible from a public right-of-way.
- Plant trees, shrubs, groundcovers, in order to break up large expanses of paving and to divide masses of parked cars into smaller groups of parking areas in off-street parking lots.
- Include long-term maintenance provisions in landscaping and tree planting projects.
- Plant street trees along prominent corridors in as properties redevelop. Street trees help to contribute to the community's character, provide a clear separation between pedestrians and automobiles, soften the visual impact of development, provide a greater sense of comfort for pedestrians, diminish traffic noise, screen unwanted views, reduce glare, absorbs heat, and filters air pollution and dust.



Landscaping provides separation from Street



Mechanical screening

This page left intentionally blank

Appendix I

Urban Design Overlay

Properties and Owners

Appendix I: Properties and Owners

PARCEL NUMBER	OWNER	PROPERTY ADDRESS	MAILING ADDRESS			
08416016300	MRMR PROPERTIES, LLC	203 SHADY GROVE RD	214 SHADY GROVE RD	NASHVILLE	TN	37214
09504024300	WOODSON, GRADY JR. ET UX	0 PARK DR	P O BOX 117	BURNS	TN	37029
09601000200	DBA, LLC	2630 OLD LEBANON PIKE	2630 OLD LEBANON RD	NASHVILLE	TN	37214
09504002800	COLE WG NASHVILLE TN DST	2421 LEBANON PIKE	104 WILMOT RD MS # 1435 C/O RE TAX DEPT	DEERFIELD	IL	60015
09504024800	DURNIN, GEORGE	204 C MCGAVOCK PIKE	2408 LEBANON PK	NASHVILLE	TN	37214
09601004400	RINALDO GROUP, L.P.	0 LEBANON PIKE	225 CLARENDON AV	NASHVILLE	TN	37205
09601001200	HILLIARD LENA J	2734 LEBANON PIKE	115 WOODMONT BV, #523	NASHVILLE	TN	37205
09601001500	HARWELL, JOHN A., SR. & JOHN A., JR.	2742 A OLD LEBANON PIKE	5337 WEBER RD	HERMITAGE	TN	37076
09601002800	SMITH, WILLIAM ALBERT	2768 LEBANON PIKE	2956 MCGAVOCK PK	NASHVILLE	TN	37214
08416016200	CLAIRDAY AND ASSOCIATES	200 SHADY GROVE RD	200 SHADY GROVE RD	NASHVILLE	TN	37214
08416021700	BASS, SAMMY V.	202 A SHADY GROVE RD	202 A SHADY GROVE RD	NASHVILLE	TN	37214
08416016100	SCRAGG, R. SCOTT & PAUL R.	204 SHADY GROVE RD	2619 BLUEFIELD AV	NASHVILLE	TN	37214
08416016000	MRMR PROPERTIES, LLC	206 SHADY GROVE RD	214 SHADY GROVE RD	NASHVILLE	TN	37214
08416016500	REESE, JOSEPH S. & MARGARET C.	205 SHADY GROVE RD	205 SHADY GROVE RD	NASHVILLE	TN	37214
08416021500	NASHVILLE YMCA	207 SHADY GROVE RD	1000 CHURCH ST	NASHVILLE	TN	37203
08416015900	MRMR PROPERTIES, LLC	210 SHADY GROVE RD	214 SHADY GROVE RD	NASHVILLE	TN	37214
08416002100	HOPE, MARIA YVONNE	140 MCGAVOCK PIKE	140 MCGAVOCK PK	NASHVILLE	TN	37214
08416016700	MANSON, MARVIN C. & VICKI C.	211 SHADY GROVE RD	211 SHADY GROVE RD	NASHVILLE	TN	37214
08416015800	MRMR PROPERTIES, LLC	214 SHADY GROVE RD	214 SHADY GROVE RD	NASHVILLE	TN	37214
08416016800	CORAZZA, RICHARD ET UX	213 SHADY GROVE RD	2517 LINCOYA CT	NASHVILLE	TN	37214
08416002200	JENNINGS, GREG	142 MCGAVOCK PIKE	142 MCGAVOCK PIKE	NASHVILLE	TN	37214
08416010600	HJL, L.P.	129 MCGAVOCK PIKE	P O BOX 140510	NASHVILLE	TN	37214
08416010700	STARTUP, WILLIAM R. SR. ETUX	2506 PARK DR	2506 PARK DR	NASHVILLE	TN	37214
08416010800	GRAVES, ERIC T. & LISA S.	2510 PARK DR	2510 PARK DR	NASHVILLE	TN	37214
08416010900	FIRST BAPTIST CHURCH OF DONELSON, INC.	2512 PARK DR	2526 LEBANON RD	NASHVILLE	TN	37214
08416011000	FIRST BAPTIST CHURCH OF DONELSON	2514 PARK DR	2526 LEBANON RD	NASHVILLE	TN	37214
08416011100	FIRST BAPTIST CHURCH OF DONELSON	2516 PARK DR	2526 LEBANON RD	NASHVILLE	TN	37214
08416017500	GODDARD, THOMAS L. ET UX	2622 A OLD LEBANON PIKE	2505 TIMWOOD DR	NASHVILLE	TN	37214
08416011200	FIRST BAPTIST CHURCH OF DONELSON	2518 PARK DR	2526 LEBANON RD	NASHVILLE	TN	37214
08416011500	FIRST BAPTIST CHURCH OF DONELSON	2526 PARK DR	2526 LEBANON RD	NASHVILLE	TN	37214
08416011600	MALONE, HARVEY E. JR. ET UX	2528 PARK DR	235 DOWNEYMEADE DR	NASHVILLE	TN	37214
08416011700	RYMAN, TIMOTHY G.	2530 PARK DR	2530 PARK DR	NASHVILLE	TN	37214
08416011800	HALE, ELLIS & FRANCES J.	2534 PARK DR	259 WILOWEN DR	NASHVILLE	TN	37210
08416013500	HALE, ELLIS & FRANCES J.	2600 CRUMP DR	259 WILOWEN DR	NASHVILLE	TN	37210
08416013600	JACKSON, RHONDA & TIDWELL, MICHAEL	2602 CRUMP DR	2602 CRUMP DR	NASHVILLE	TN	37214
08416013700	WILLIAMSON, JOHN & MOSHER, SUSANNAH	2604 CRUMP DR	2604 CRUMP DR	NASHVILLE	TN	37214
08416013800	GRAVES, LUCAS & DALE, AMBER	2606 CRUMP DR	2606 CRUMP DR	NASHVILLE	TN	37214
08416013900	MAHAN, CHRISTOPHER S.	2608 CRUMP DR	2608 CRUMP DR	NASHVILLE	TN	37214
08416014000	GRAVES, ERIC T. & LISA S.	2610 CRUMP DR	2610 CRUMP DR	NASHVILLE	TN	37214
08416014100	GRAVES, ERIC T. & LISA S.	2612 CRUMP DR	2612 CRUMP DR	NASHVILLE	TN	37214
08416010500	HJL, LP	131 MCGAVOCK PIKE	P O BOX 140510	NASHVILLE	TN	37214
08416015700	MANSON, MARVIN C. & VICKI C.	216 SHADY GROVE RD	162 STONERS GLEN DR	HERMITAGE	TN	37076
08416016900	ALLEN, J. GARY	217 SHADY GROVE RD	9521 CENTRAL PK	MT JULIET	TN	37122
08416002300	JONES, ARTHUR, JR. ET UX	144 MCGAVOCK PIKE	2617 PENNINGTON BEND RD	NASHVILLE	TN	37214
08416017000	ALLEN, J. GARY	219 SHADY GROVE RD	9521 CENTRAL PK	MT JULIET	TN	37122
08416015600	OAKLEY, BARRY N. & KELLY G.	218 SHADY GROVE RD	3536 CENTRAL PK STE 103	HERMITAGE	TN	37076
08416002400	KROGER LIMITED PARTNERSHIP I	150 MCGAVOCK PIKE	2620 ELM HILL PK	NASHVILLE	TN	37214
09503001300	BOYLE SHADY GROVE SOUTHWEST, LLC ET AL	2296 LEBANON PIKE	5900 POPLAR AV STE 100	MEMPHIS	TN	38119
08416011900	KROGER LIMITED PARTNERSHIP I	141 MCGAVOCK PIKE	2620 ELM HILL PK C/O REAL ESTATE DEPT	NASHVILLE	TN	37214

Appendix I: Properties and Owners

08416015500	OAKLEY, BARRY N. & KELLY G.	220 SHADY GROVE RD	3536 CENTRAL PK STE 103	HERMITAGE	TN	37076
08416012100	FIRST BAPTIST CHURCH OF DONELSON	2511 PARK DR	2526 LEBANON RD	NASHVILLE	TN	37214
08416012200	FIRST BAPTIST CHURCH OF DONELSON	2515 PARK DR	2526 LEBANON PK	NASHVILLE	TN	37214
08416012400	BOND, LOY CLARK ETUX	2519 PARK DR	2519 PARK DR	NASHVILLE	TN	37214
09504001100	DONELSON BAPT. CHURCH	2540 LEBANON PIKE	2526 LEBANON RD	NASHVILLE	TN	37214
08416012900	PARK DRIVE ASSOCIATES, LLC	2527 PARK DR	2531 PARK DR	NASHVILLE	TN	37214
08416013000	PARK DRIVE ASSOCIATES, LLC	2531 PARK DR	2531 PARK DR	NASHVILLE	TN	37214
09504002500	HELMS, LEXIE(LE)& RUTHVEN, JOYA H. ET AL	2624 OLD LEBANON PIKE	7548 HWY 49 E	SPRINGFIELD	TN	37172
08513004900	LEE, LAURA E.	231 CLIFFDALE RD	231 CLIFFDALE RD	NASHVILLE	TN	37214
08415016200	OLDHAM, JAMES A. ET UX	113 FAIRWAY DR	113 FAIRWAY DR	NASHVILLE	TN	37214
08513004600	LLC	220 CLIFFDALE RD	REL MGMT	LAKE WORTH	FL	33461
08416014800	BROWN, JUDITH LORENE	2601 CRUMP DR	2804 BLUE BRICK DR	NASHVILLE	TN	37214
08416014700	WEBER, FRANK V.,JR. & HOBBS, JOSEPH E.	2603 CRUMP DR	123 KAREN RD	MT JULIET	TN	37122
08416014600	WHITLOCK, PARKER	2605 CRUMP DR	6752 BETHESDA ARNO RD	THOMPSONS STATION	TN	37179
08416014500	HOBBS & SONS, L.P.	2607 CRUMP DR	2607 CRUMP DR	NASHVILLE	TN	37214
08416015400	MRMR PROPERTIES, LLC	222 SHADY GROVE RD	214 SHADY GROVE RD	NASHVILLE	TN	37214
08416017100	WILLIAMS, FRED O. ET UX	221 SHADY GROVE RD	3213 KNOBVIEW DR	NASHVILLE	TN	37214
08416014400	GREGORY, BARRY	2609 CRUMP DR	511 HIGH AV	KNOXVILLE	TN	37920
08416021400	PETTY, MICHAEL S.	229 SHADY GROVE RD	500 LAKE VALLEY CT	FRANKLIN	TN	37069
08416014300	GRAVES, ERIC T. & LISA S.	2611 CRUMP DR	114 WALNUT HILL	NASHVILLE	TN	37214
08416014200	CRABB, RONALD D.	2613 CRUMP DR	2745 ANDERSON RD	NASHVILLE	TN	37217
08513004800	PAPUCHIS, STEVE & CAROL	235 CLIFFDALE RD	209 ROLLING MILL RD	OLD HICKORY	TN	37138
08416015200	OAKLEY ENTERPRISES	226 SHADY GROVE RD	3536 CENTRAL PK STE 103	HERMITAGE	TN	37076
08416021200	PETTY, MICHAEL S.	223 SHADY GROVE RD	500 LAKE VALLEY CT	FRANKLIN	TN	37069
08416002700	MCDONALD'S CORPORATION	154 MCGAVOCK PIKE	P O BOX 182571	COLUMBUS	OH	43218
08415016300	HJL, L.P.	117 FAIRWAY DR	P O BOX 140510	NASHVILLE	TN	37214
08416021300	HUFFMAN, LEROY W. ET UX	225 SHADY GROVE RD	606 HAYES LN	MT JULIET	TN	37122
08416021100	PETTY, MICHAEL S.	2540 PARK DR	500 LAKE VALLEY CT	FRANKLIN	TN	37069
08416012500	BOND, LOY CLARK ETUX	0 PARK DR	2519 PARK DR	NASHVILLE	TN	37214
08416017600	US COMMUNITY CREDIT UNION	2622 OLD LEBANON PIKE	125 8TH AV S	NASHVILLE	TN	37202
09504002200	GAITHER, JAMES W. JR. ETUX	2616 OLD LEBANON PIKE	7169 BAY COVE TRAIL	NASHVILLE	TN	37221
08513004700	RAD PHYSICS, LLC	239 CLIFFDALE RD	239 CLIFFDALE RD	NASHVILLE	TN	37214
08416013100	FIRST BAPT CHURCH OF DONELSON TRS	2535 PARK DR	2526 LEBANON RD	NASHVILLE	TN	37214
09601000300	DONELSON BUILDING OWNER, LLC	2710 OLD LEBANON PIKE	2328 10TH AV N STE 401 C/O IN REL MGMT	LAKE WORTH	FL	33461
09504002600	HELMS, LEXIE C.(LE)& A.M.& S.E.,JR.	2626 OLD LEBANON PIKE	7548 HWY 49 E	SPRINGFIELD	TN	37172
09504001000	FIRST BAPTIST CHURCH OF DONELSON	2520 LEBANON PIKE	2526 LEBANON RD	NASHVILLE	TN	37214
08416015001	DENNIS, JEFFERY & JAN R.	2604 B LEBANON PIKE	66 BALLENGER RD	CARTHAGE	TN	37030
08416021200	PETTY, MICHAEL S.	227 SHADY GROVE RD	500 LAKE VALLEY CT	FRANKLIN	TN	37069
09504000900	EVANS, JOHN W., JR. & FRANCES M. ET AL	2518 LEBANON PIKE	2518 LEBANON PK	NASHVILLE	TN	37214
09504023600	MTCM ENTERPRISES, INC	2604 LEBANON PIKE	7215 KINGSTON PK	KNOXVILLE	TN	37919
09504001500	MTCM ENTERPRISES, INC	2602 LEBANON PIKE	7215 KINGSTON PK	KNOXVILLE	TN	37919
09504002100	SCHLABACH, RICHARD A. & LINDA	2614 OLD LEBANON PIKE	2614 OLD LEBANON PIKE	NASHVILLE	TN	37214
08416013200	FIRST BAPTIST CHURCH OF DONELSON TRS	2537 PARK DR	2526 LEBANON RD	NASHVILLE	TN	37214
09504001501	MTCM ENTERPRISES, INC	2602 A LEBANON PIKE	7215 KINGSTON PK	KNOXVILLE	TN	37919
08416020600	MTCM ENTERPRISES, INC	0 PARK DR	7215 KINGSTON PK	KNOXVILLE	TN	37919
095030A07200CO	DOOLEY, RICHARD N.	0 LEBANON PIKE	2258 LEBANON PK 1 A	NASHVILLE	TN	37214
08416015000	METRO FIRE FIGHTERS ASSOC.-LOCAL 763	2550 PARK DR	2550 PARK DR	NASHVILLE	TN	37214
09601000400	DONELSON BUILDING OWNER, LLC	2720 OLD LEBANON PIKE	2328 10TH AV N STE 401 C/O IN REL MGMT	LAKE WORTH	FL	33461
08415016400	HJL, L.P.	119 FAIRWAY DR	P O BOX 140510	NASHVILLE	TN	37214
09601000100	KELLEY, GEORGE BRIDGES & MARION WILLIAMS, TRS.	2628 OLD LEBANON PIKE	2200 LEBANON RD	NASHVILLE	TN	37214
08416013300	HOLLADAY, JERI	2539 PARK DR	146 SADDLE TREE DR	HERMITAGE	TN	37076

Appendix I: Properties and Owners

09504000100	BELLSOUTH TELECOMMUNICATIONS, INC.	156 MCGAVOCK PIKE	333 COMMERCE ST #102	NASHVILLE	TN	37201
09504001600	WONG, AH-SHEW & CHE-MING	2606 LEBANON PIKE	2317 DUNDEE LN	NASHVILLE	TN	37214
09504001700	SHELLY, KEITH	2608 LEBANON PIKE	2608 LEBANON PIKE	NASHVILLE	TN	37214
09504002300	R. B. BRANCH PROPERTIES, INC.	2618 OLD LEBANON PIKE	280 WARFIELD DR	CLARKSVILLE	TN	37040
09504002400	YOUNG, CHRISTOPHER L.	2620 OLD LEBANON PIKE	2620 OLD LEBANON PIKE	NASHVILLE	TN	37214
08416015100	LAMPKIN, S. L.	228 SHADY GROVE RD	228 SHADY GROVE RD	NASHVILLE	TN	37214
09504001900	PETTY, MICHAEL S.	2612 LEBANON PIKE	500 LAKE VALLEY CT	FRANKLIN	TN	37069
08416013400	WOODSON, GRADY JR.	2541 PARK DR	P O BOX 117 C/O JUANITA P WOODSON	BURNS	TN	37029
09503001700	HJL, L.P.	121 FAIRWAY DR	P O BOX 140510	NASHVILLE	TN	37214
09601002000	HURT, SUSAN	111 BENSON RD	111 BENSON RD	NASHVILLE	TN	37214
09504001400	LOUNSBURY, DONALD E. & DONNA J.	2600 LEBANON PIKE	1621 BROOKVALLEY CR	MOUNT JULIET	TN	37122
09503001600	METRO GOVT F FIRE HALL	2394 LEBANON PIKE	222 3RD AV N STE 701	NASHVILLE	TN	37201
09504001300	FREEMAN & WOODSON	2540 LEBANON PIKE	202 DONELSON PK C/O TREVA CARTER	NASHVILLE	TN	37214
09504001800	GODDARD, THOMAS LEE ET UX	2610 LEBANON PIKE	2622 OLD LEBANON RD	NASHVILLE	TN	37214
09504000200	HJL, L.P.	162 C MCGAVOCK PIKE	P O BOX 140510	NASHVILLE	TN	37214
09504000600	INLAND AMERICAN NASHVILLE DONELSON, LLC	2500 LEBANON PIKE	2901 BUTTERFIELD RD	OAK BROOK	IL	60523
09602000500	BOWEN, ARTHUR, JR. & PASCHALL, EDDINE B. ET AL	2850 LEBANON PIKE	1915 BERKSHIRE DR	NASHVILLE	TN	37216
09601001700	HARWELL, JOHN A.	2738 OLD LEBANON PIKE	5341 WEBER RD	HERMITAGE	TN	37076
09504024100	INLAND AMERICAN NASHVILLE DONELSON, LLC	2512 LEBANON PIKE	2901 BUTTERFIELD RD	OAK BROOK	IL	60523
09503001900	RICHMOND, CLIFFORD O.	2412 LEBANON PIKE	3807 HILDALE DR	NASHVILLE	TN	37215
09504000800	INLAND AMERICAN NASHVILLE DONELSON, LLC	2510 LEBANON PIKE	2901 BUTTERFIELD RD	OAK BROOK	IL	60523
09503002100	DURNIN, GEORGE V. ET UX	0 LEBANON PIKE	2408 LEBANON PK	NASHVILLE	TN	37214
09504000300	DURNIN, GEORGE V. ET UX	2414 LEBANON PIKE	2408 LEBANON PK	NASHVILLE	TN	37214
09504000400	HJL, L.P.	2416 LEBANON PIKE	P O BOX 140510	NASHVILLE	TN	37214
09504000500	HJL, L.P.	2428 B LEBANON PIKE	P O BOX 140510	NASHVILLE	TN	37214
09601000700	RAHIMI, MOHAMMAD H. & ZARY	2633 LEBANON PIKE	1601 OLD HICKORY BV	NASHVILLE	TN	37207
09504002700	FARRAR SERVICE CO, INC.	2620 LEBANON PIKE	2600 NOLENSVILLE RD	NASHVILLE	TN	37211
09601000600	RAHIMI, MOHAMMAD H. & ZARY	2631 OLD LEBANON PIKE	1601 OLD HICKORY BV	NASHVILLE	TN	37207
09503002200	DURNIN, GEORGE V. ET UX	0 LEBANON PIKE	2408 LEBANON PK	NASHVILLE	TN	37214
09504005800	CROWELL, J. FRANK JR.	2615 A LEBANON PIKE	P O BOX 40101	NASHVILLE	TN	37204
09504005900	CROTZER & FINCH, LLC	2611 LEBANON PIKE	2803 LEALTO CT	NASHVILLE	TN	37214
09504005700	CROTZER & FINCH, LLC	2611 LEBANON PIKE	2803 LEALTO CT	NASHVILLE	TN	37214
09504018500	GREENE COUNTY BANK	2621 LEBANON PIKE	100 N MAIN ST	GREENEVILLE	TN	37744
09504005600	MCCULLARS, DONALD ET UX	2607 LEBANON PIKE	2607 LEBANON PIKE	NASHVILLE	TN	37214
09504005500	LITSEY BUILDING PARTNERS	2605 LEBANON PIKE	4992 TULIP GROVE LN	HERMITAGE	TN	37076
09504005400	WATKINS, DAVID C., JR. & KAREN R.	2547 LEBANON PIKE	6060 VANDERBILT RD 1325 S 4TH ST C/O JEREMY CURRAN	OLD HICKORY	TN	37138
09504018600	B. J. REAL ESTATE, LLC	2623 LEBANON PIKE	1325 S 4TH ST C/O JEREMY CURRAN	LOUISVILLE	KY	40208
09504005300	BECKER, RAYMOND H. ET UX	2545 LEBANON PIKE	2543 LEBANON RD	NASHVILLE	TN	37214
09504024600	MRS. WINNER'S, L.P.	2625 LEBANON PIKE	5995 BARFIELD RD	ATLANTA	GA	30328
09504005200	PETTY, MICHAEL S.	2541 LEBANON PIKE	500 LAKE VALLEY CT	FRANKLIN	TN	37069
09504005100	ELROD, CARLENE C.	2539 LEBANON PIKE	P O BOX 2667	FLORENCE	AL	35630
09503012400	C & H PROPERTIES, LLC	2400 LEBANON PIKE	1503-B MEMORIAL BV	SPRINGFIELD	TN	37172
09504024500	FFCA ACQUISITION CORP.	2629 LEBANON PIKE	17207 N PERIMETER DR	SCOTTSDALE	AZ	85255
09504005000	ELROD, CARLENE C.	2535 LEBANON PIKE	204 W TUSCALOOSA ST	FLORENCE	AL	35630
09504004900	BROWN, EDWIN M. & JESSIE A.	2527 LEBANON PIKE	200 POND DR	GALLATIN	TN	37066
09504018700	U.S.RESTAURANT PROPERTIES OPERATING, L.P	2633 LEBANON PIKE	930 W 1ST ST STE 303 C/O BRAZOS TX GRP	FT WORTH	TX	76102
09504023700	DOAK, JOHN G., SR. ET UX	2525 LEBANON PIKE	2525 LEBANON PIKE	NASHVILLE	TN	37214
09504024400	ALLEN, J. GARY ET UX	2525 C LEBANON PIKE	2517 LEBANON RD	NASHVILLE	TN	37214
09504004600	ALLEN, J. GARY ET UX	2517 LEBANON PIKE	2517 LEBANON PIKE	NASHVILLE	TN	37214
09504004400	ALLEN, J. GARY ET UX	2515 LEBANON PIKE	2517 LEBANON PIKE	NASHVILLE	TN	37214
09601001000	HARWELL MOTOR COMPANY, INC.	2730 LEBANON PIKE	5341 WEBER RD	HERMITAGE	TN	37076
09504018300	WOOD, ERIC K., SR.	113 GRAYLYNN DR	4424 ENCHANTED CR	NASHVILLE	TN	37218

Appendix I: Properties and Owners

09504004300	OAKLEY ENTERPRISES, LP	2509 LEBANON PIKE	3536 CENTRAL PK # 103	HERMITAGE	TN	37076
09504004100	INLAND AMERICAN ST		919 E MAIN ST 14TH FL C/O			
09504004000	PORTFOLIO, LLC	2503 LEBANON PIKE	SUNTRUST BANK	RICHMOND	VA	23219
09504006000	HOBBS & SONS, LP	2501 A LEBANON PIKE	2607 CRUMP DR	NASHVILLE	TN	37214
095040018200	CHURCH	190 GRAYLYNN DR	190 GRAYLYNN DR	NASHVILLE	TN	37214
	JOH PROPERTIES, LLC	115 GRAYLYNN DR	2206 CRAIGMEADE CR	NASHVILLE	TN	37214
09503010400	TAYLOR, CAROLINE C.B. &		7102 CINNEERCE WAT C/O			
09601001100	CLUCKER, P. T.	2417 LEBANON PIKE	MAPCO EXPRESS INC	BRENTWOOD	TN	37027
09503010500	HILLIARD LENA J	2732 LEBANON PIKE	115 WOODMONT BV APT-523	NASHVILLE	TN	37205
	JORDAN PROPERTIES, INC.	2401 LEBANON PIKE	P O BOX 150865	NASHVILLE	TN	37215
09601002500	OSBORNE BROS. PARTNERSHIP	0 EDDINGS LN	201 EDDING LN	NASHVILLE	TN	37214
09503004700	DONELSON CHRISTIAN CHURCH	2319 LEBANON PIKE	2319 LEBANON RD	NASHVILLE	TN	37214
09504018100	WASSEL, PAULETTE NORFLEET	119 GRAYLYNN DR	6699 OWEN HILL RD	COLLEGE GROVE	TN	37046
09504004500	MCWHIRTERSVILLE LODGE 375 F & AM, TRS.	2515 LEBANON PIKE	2515 LEBANON PK C/O JOHN H FUQUA	NASHVILLE	TN	37214
09503004500	METRO GOV'T L DONELSON BRANCH	2315 LEBANON PIKE	222 3RD AV N STE 701	NASHVILLE	TN	37201
09504018000	WOODSON, TERRY & SHIRLEY	187 GRAYLYNN DR	865 ROBERTSTON RD	NASHVILLE	TN	37220
09503004300	LEBANON ROAD CHURCH OF CHRIST, TRS.	2307 LEBANON PIKE	2307 LEBANON PK	NASHVILLE	TN	37214
09503004000	TRS.	2301 LEBANON PIKE	2301 LEBANON PIKE	NASHVILLE	TN	37214
09504007800	JORDAN, GERALD	2500 CROSSFIELD DR	BOX 13331, RT 1	BELL BUCKLE	TN	37020
09504017900	PEEK, LEISA S.	189 GRAYLYNN DR	189 GRAYLYNN DR	NASHVILLE	TN	37214
09503003900	BINKLEY, GARY S. & BAGGETT, MAC ELLIOTT, SR.	2303 LEBANON PIKE	2303 LEBANON PK	NASHVILLE	TN	37214
09601002600	OSBORNE, JOHN E. & KENNETH C. & TIMOTHY E.	201 EDDINGS LN	201 EDDINGS LN	NASHVILLE	TN	37214
09503003800	BINKLEY, GARY S. & BAGGETT, MAC ELLIOTT, SR.	2303 LEBANON PIKE	2303 LEBANON PK	NASHVILLE	TN	37214
09601004600	DAL-MAC PROPERTIES, LLC	2707 LEBANON PIKE	P O BOX 148458	NASHVILLE	TN	37214
09504024200	PITCHFORD, DANIEL L. & AMY D.	191 GRAYLYNN DR	191 GRAYLYNN DR	NASHVILLE	TN	37214
09601004700	DAL-MAC PROPERTIES, LLC	2717 LEBANON PIKE	P O BOX 148458	NASHVILLE	TN	37214
09601004800	KIDD, BARRY C. & CANDE W.	2721 LEBANON PIKE	2721 LEBANON PK	NASHVILLE	TN	37214
09504003100	HJL, L. P.	206 MCGAVOCK PIKE	P O BOX 140510	NASHVILLE	TN	37214
09504017800	PITCHFORD, DANIEL L. & AMY D.	193 GRAYLYNN DR	193 GRAYLYNN DR	NASHVILLE	TN	37214
09601004900	GOLIATH, LLC	2725 LEBANON PIKE	2725 LEBANON PK	NASHVILLE	TN	37214
09503010600	JORDAN PROPERTIES, INC.	0 FAIRWAY DR	P O BOX 150865	NASHVILLE	TN	37215
09504003200	HJL L. P.	208 MCGAVOCK PIKE	P O BOX 140510	NASHVILLE	TN	37214
09504009200	HJL, L.P.	2501 CROSSFIELD DR	P O BOX 140510	NASHVILLE	TN	37214
09504003300	HJL, L.P.	212 MCGAVOCK PIKE	P O BOX 140510	NASHVILLE	TN	37214
09602001400	WAUFORD ASSOCIATES, INC.	2835 LEBANON PIKE	2835 LEBANON RD	NASHVILLE	TN	37214
09602001100	ROBINSON, PENNY L.	2831 LEBANON PIKE	1627 SHELL RD	GOODLETTSV	TN	37072
09601007300	BARRETT, M. LEE, JR.	2831 B LEBANON PIKE	976 MURFREESBORO RD	NASHVILLE	TN	37217
09602000400	ANDREW PRICE MEMORIAL METHODIST CH.	2846 LEBANON PIKE	2846 LEBANON PIKE	NASHVILLE	TN	37214
09601005600	SENIOR CITIZENS INC.	108 DONELSON PIKE	112 DONELSON PK	NASHVILLE	TN	37214
09602001200	WAUFORD ASSOCIATES, INC.	0 LEBANON PIKE	2835 LEBANON RD	NASHVILLE	TN	37214
09602001300	WAUFORD ASSOCIATES, INC.	2833 LEBANON PIKE	2835 LEBANON RD	NASHVILLE	TN	37214
09602001500	HOLLINGSWORTH FAMILY LTD PARTNERSHIP	2837 LEBANON PIKE	P O BOX 29	SPRINGFIELD	TN	37172
09602001600	HOLLINGSWORTH FAMILY LTD PARTNERSHIP	2839 LEBANON PIKE	P O BOX 29	SPRINGFIELD	TN	37172
09601006000	YOUNG, ROY W.	113 DONELSON PIKE	113 DONELSON PIKE	NASHVILLE	TN	37214
09601007400	CBC LIMITED	117 DONELSON PIKE	820 FESSLEERS PW STE 107	NASHVILLE	TN	37210
09601001600	HARWELL, JOHN ALLEN & JOHN ALLEN JR.	2740 OLD LEBANON PIKE	5337 WEBER RD	HERMITAGE	TN	37076
09601002501	RESURRECTED PEOPLES CHURCH, INT.	210 EDDINGS LN	P O BOX 148228	NASHVILLE	TN	37214
09601002700	OSBORNE, JOHN E., KENNETH C. & TIMOTHY E.	0 LEBANON PIKE	201 EDDING LN	NASHVILLE	TN	37214
09601003000	MCFARLAND, SAM ETUX	2750 LEBANON PIKE	501 UNION ST STE 404 C/O GUARDIANSHIP	NASHVILLE	TN	37219
09601003100	MCFARLAND, SAM ETUX	2754 LEBANON PIKE	501 UNION ST STE 404 C/O GUARDIANSHIP	NASHVILLE	TN	37219

Appendix I: Properties and Owners

09601003700	RESURRECTED PEOPLES CHURCH, INT.	0 LEBANON PIKE	P O BOX 148228	NASHVILLE	TN	37214
09601003800	HIBBS, JAMES R. ET UX	2812 LEBANON PIKE	213 GRAEME DR	NASHVILLE	TN	37214
09601004100	LILLARD, AARON & GRACIE	2824 LEBANON PIKE	3781 THE GREAT DR	ATLANTA	GA	30349
09601006100	MURPHY, JAMES THOMAS, SR.	109 DONELSON PIKE	109 DONELSON PK	NASHVILLE	TN	37214
09601006200	THURMAN, C. Y. ET UX	107 DONELSON PIKE	P O BOX 140258	NASHVILLE	TN	37214
09601006201	HUMPHRIES, LEROY J. & BEAM, BEVERLY S.	2801 LEBANON PIKE	P O BOX 148755	NASHVILLE	TN	37214
09601006700	HUMPHRIES, L.J. & BEAM, J.A. III ET UX	2815 LEBANON PIKE	P O BOX 148755	NASHVILLE	TN	37214
09601006900	TUNE-HARDISON JOINT VENTURE	2821 LEBANON PIKE	2821 LEBANON RD., STE 102	NASHVILLE	TN	37214
09601007100	YOUNG'S PROPERTIES, L.L.C.	2829 LEBANON PIKE	169 SHUTE CR	OLD HICKORY	TN	37138
09601014100	YSI-HART	2825 LEBANON PIKE	460 E SWEDES FORD RD STE 3000	WAYNE	PA	19087
09601015600	REGIONAL TRANSPORTATION AUTHORITY (RTA)	104 DONELSON PIKE	501 UNION STREET 6TH FLOOR	NASHVILLE	TN	37219
09602000200	SHAW, JAMES L.	2836 LEBANON PIKE	2966 MC GAVOCK PK	NASHVILLE	TN	37214
09602000300	LANE, L. W., JR. & RUTH E.	2842 LEBANON PIKE	2840 LEBANON PK	NASHVILLE	TN	37214
09601001300	HARWELL, JOHN A.	2736 LEBANON PIKE	5341 WEBBER RD	HERMITAGE	TN	37076
09601002900	HARWELL, JOHN A., SR. & JOHN A., JR.	2748 OLD LEBANON PIKE	5337 WEBER RD	HERMITAGE	TN	37076
09601003900	RESURRECTED PEOPLES CHURCH, INT.	2816 LEBANON PIKE	P O BOX 148228	NASHVILLE	TN	37214
09601004000	HARWELL, JOHN ALLEN, SR.	2818 LEBANON PIKE	5341 WEBER RD	HERMITAGE	TN	37076
09601004200	HARWELL, JOHN ALLEN, SR.	2820 LEBANON PIKE	5341 WEBER RD	HERMITAGE	TN	37076
09601004300	HARWELL, JOHN ALLEN, SR.	2826 LEBANON PIKE	5341 WEBER RD	HERMITAGE	TN	37076
09601005200	H. G. HILL REALTY CO.	2731 LEBANON PIKE	3011 ARMORY DR., SUITE 130	NASHVILLE	TN	37204
09601006800	YOUNG, WILLIAM B. ET UX	2819 LEBANON PIKE	2819 LEBANON PIKE	NASHVILLE	TN	37214
09602000100	HARWELL, JOHN ALLEN, SR.	2830 LEBANON PIKE	5341 WEBER RD	HERMITAGE	TN	37076
09602007100	HARWELL, JOHN ALLEN, SR.	2832 A LEBANON PIKE	5341 WEBER RD	HERMITAGE	TN	37076
09601003600	OSBORNE BROS. PARTNERSHIP	2808 LEBANON PIKE	201 EDDING LN	NASHVILLE	TN	37214
09601003200	SMITH, WILLIAM ALBERT	2760 LEBANON PIKE	2956 MCGAVOCK PK	NASHVILLE	TN	37214
09601003300	SMITH, WILLIAM ALBERT	0 LEBANON PIKE	2956 MCGAVOCK PK	NASHVILLE	TN	37214
09601003400	SMITH, WILLIAM ALBERT	2764 LEBANON PIKE	2956 MCGAVOCK PK	NASHVILLE	TN	37214
09601003500	SMITH, WILLIAM ALBERT	2800 LEBANON PIKE	2956 MCGAVOCK PK	NASHVILLE	TN	37214
09503002600	DYE, GARRY D. & MARILEE H.	100 BLUE HILLS DR	100 BLUE HILLS DR	NASHVILLE	TN	37214
09503012200	DURNIN, GEORGE V.	2408 LEBANON PIKE	2408 LEBANON PIKE	NASHVILLE	TN	37214
09503012500	SMITH, ANITA DAWN	2254 LEBANON PIKE	P O BOX 24916	NASHVILLE	TN	37202
09503012900	CHITWOOD, CAROLE & SMITH, GREGORY E. & PEGGY M.	0 LEBANON PIKE	2250 LEBANON PK	NASHVILLE	TN	37214
09503000800	CHITWOOD, CAROLE & SMITH, GREGORY E. & PEGGY M.	2250 LEBANON PIKE	2250 LEBANON PK	NASHVILLE	TN	37214
095030A06100CO	KING, CYNTHIA M.	2258 LEBANON PIKE	2258 LEBANON PK., UNIT 61	NASHVILLE	TN	37214
095030A06000CO	LAZIMI, SIVAN	2258 LEBANON PIKE	2258 LEBANON PK UNIT 60	NASHVILLE	TN	37214
095030A05100CO	BURNETT, NANCY A.	2258 LEBANON PIKE	2258 LEBANON PK # 51	NASHVILLE	TN	37214
095030A05000CO	QOSE, DRITAN & JETA	2258 LEBANON PIKE	2258 LEBANON PK # 50	NASHVILLE	TN	37214
095030A04300CO	MYSJA, ENVER & LUMTURI	2258 LEBANON PIKE	2258 LEBANON PK # 43	NASHVILLE	TN	37214
095030A06200CO	FRIDGANT, ANITA	2258 LEBANON PIKE	2258 LEBANON PIKE 62	NASHVILLE	TN	37214
095030A05900CO	WORMSLEY, R. M. & CARMICAL, J. R.	2258 LEBANON PIKE	2258 LEBANON PK., UNIT 59	NASHVILLE	TN	37214
095030A05200CO	POPE, DOROTHY M.	2258 LEBANON PIKE	4507 PARKWOOD LN W	NICEVILLE	FL	32578
095030A04900CO	POTKA, ZYBE	2258 LEBANON PIKE	P O BOX 148195	NASHVILLE	TN	37214
095030A04400CO	MCNULTY, GEORGE T. & MARION P.	2258 LEBANON PIKE	2258 LEBANON PK., UNIT 44	NASHVILLE	TN	37214
095030A03900CO	BRATCHER, BILLIE JEAN	2258 LEBANON PIKE	2258 LEBANON PIKE 39	NASHVILLE	TN	37214
095030A03200CO	MITCHELL, HAROLD L.	2258 LEBANON PIKE	2258 LEBANON PIKE 32	NASHVILLE	TN	37214
095030A03100CO	BOGATINOFF, SCOTT T.	2258 LEBANON PIKE	2258 LEBANON PIKE 31	NASHVILLE	TN	37214
095030A02400CO	REDMOND, AVA	2258 LEBANON PIKE	2258 LEBANON PK., UNIT 24	NASHVILLE	TN	37214
095030A03800CO	NOE, THOMAS K. & DONNA S.	2258 LEBANON PIKE	2258 LEBANON PK # 27	NASHVILLE	TN	37214
095030A02300CO	BEHRENS, MARY	2258 LEBANON PIKE	2258 LEBANON PK., UNIT 23	NASHVILLE	TN	37214
095030A01800CO	NOE, THOMAS K. & DONNA S.	2258 LEBANON PIKE	2258 LEBANON RD # 27	NASHVILLE	TN	37214
095030A03300CO	LONG, MELBA K.	2258 LEBANON PIKE	2258 LEBANON PIKE 33	NASHVILLE	TN	37214
095030A03000CO	LUSK, JAMES W.	2258 LEBANON PIKE	2258 LEBANON PK # 30	NASHVILLE	TN	37214
095030A02500CO	LUSK, SHIRLEY LAVERNE	2258 LEBANON PIKE	2258 LEBANON PK # 25	NASHVILLE	TN	37214
095030A02200CO	SMITH, RUBEN W. TRUST	2258 LEBANON PIKE	P O BOX 17069	HUNTSVILLE	AL	35810
095030A01900CO	REAGAN, GEORGIA ANN	2258 LEBANON PIKE	2258 LEBANON PK., UNIT 19	NASHVILLE	TN	37214

Appendix I: Properties and Owners

095030A00300CO	BOGATINOFF, PETER F. & STEPHANIE J., CO-TRS.	2258 LEBANON PIKE	P O BOX 158555	NASHVILLE	TN	37215
095030A00800CO	N.	2258 LEBANON PIKE	2258 LEBANON PIKE 8	NASHVILLE	TN	37214
095030A00700CO	NOE, THOMAS K. & DONNA S.	2258 LEBANON PIKE	2258 LEBANON PK # 27	NASHVILLE	TN	37214
095030A01400CO	STARKEY, GRACE M.	2258 LEBANON PIKE	2258 LEBANON PIKE 14	NASHVILLE	TN	37214
095030A00100CO	CARRIGAN, EDWIN KEITH	2258 LEBANON PIKE	2258 LEBANON PK # 1	NASHVILLE	TN	37214
095030A00900CO	RICHMOND, DOROTHY	2258 LEBANON PIKE	2258 LEBANON PK #9	NASHVILLE	TN	37214
095030A00600CO	BROWN, ANNETTE DUNCAN	2258 LEBANON PIKE	2428 DUNDEE LN	NASHVILLE	TN	37214
095030A01500CO	CRAFTON, AUBREY L. & LUDIE ANN	2258 LEBANON PIKE	2258 LEBANON PIKE 15	NASHVILLE	TN	37214
08513001700	DONELSON CHURCH OF CHRIST, TRS.	2706 OLD LEBANON PIKE	2706 OLD LEBANON RD	NASHVILLE	TN	37214
09601000800	MATANA CO, LLC	2733 OLD LEBANON PIKE	2409 VALLEY BROOK RD	NASHVILLE	TN	37215
09601000900	COMMERCE UNION BANK	2720 LEBANON PIKE	P O BOX 2818 C/O BURR WOLFF	ALPHARETTA	GA	30023
09601014200	FIRST BANK & TRUST	2725 OLD LEBANON PIKE	919 E MAIN ST 14TH FL C/O SUNTRUST BANK	RICHMOND	VA	23219
09601014300	WONG, PING KLONG LING	2719 OLD LEBANON PIKE	2134 DEARBORN DR	NASHVILLE	TN	37214
09601014400	FLETCHER, GARY RUSSELL	2715 OLD LEBANON PIKE	2715 OLD LEBANON PK	NASHVILLE	TN	37214
09601014500	MPD COMMUNITY CREDIT UNION	2711 OLD LEBANON PIKE	305 GAY ST STE 105	NASHVILLE	TN	37201
09602001700	BARRETT, M. LEE, JR.	2841 LEBANON PIKE	976 1/2 MURFREESBORO PK	NASHVILLE	TN	37217
09602001900	GEARON, R. BARRY	2843 A LEBANON PIKE	5036 FRANKLIN RD	NASHVILLE	TN	37220
09602006900	CUMBERLAND PARTNERS	2845 LEBANON PIKE	320 DOUGLAS BEND RD	GALLATIN	TN	37066
09601004500	RINALDO GROUP, L.P.	2703 LEBANON PIKE	225 CLARENDON AV	NASHVILLE	TN	37205
09601015700	REGIONAL TRANSPORTATION AUTHORITY	0 LEBANON PIKE	501 UNION ST 6TH FL	NASHVILLE	TN	37219
095030A05700CO	DALY, CHARLES E.	2258 LEBANON PIKE	2258 LEBANON PIKE 57	NASHVILLE	TN	37214
095030A04800CO	POTKA, ZYBE	2258 LEBANON PIKE	P O BOX 148195	NASHVILLE	TN	37214
095030A05800CO	SHOCKNEY, DON & SHIRLEY	2258 LEBANON PIKE	404 HOLLYDALE DR	NASHVILLE	TN	37217
095030A04700CO	NOE, THOMAS K. & DONNA S.	2258 LEBANON PIKE	2258 LEBANON PK # 27	NASHVILLE	TN	37214
095030A04200CO	STREET, VIRGINIA S. & THOMAS R. JR.	2258 LEBANON PIKE	2258 LEBANON PIKE 42	NASHVILLE	TN	37214
095030A07100CO	HAGAR, SUZZANNE	2258 LEBANON PIKE	2258 LEBANON PK # 71	NASHVILLE	TN	37214
095030A06500CO	FILHART, ROBERT E.	2258 LEBANON PIKE	2258 LEBANON RD # 65	NASHVILLE	TN	37214
095030A06400CO	SMITH, JOHN PATRICK	2258 LEBANON PIKE	2258 LEBANON PK # 64	NASHVILLE	TN	37214
095030A05500CO	PAPA, MONIKA	2258 LEBANON PIKE	2258 LEBANON RD # 55	NASHVILLE	TN	37214
095030A05400CO	JENKINS, JOHN A.	2258 LEBANON PIKE	2258 LEBANON PK, UNIT 54	NASHVILLE	TN	37214
095030A04500CO	MILLER, LISA	2258 LEBANON PIKE	2258 LEBANON PIKE 45	NASHVILLE	TN	37214
095030A07000CO	NOE, THOMAS K. & DONNA S.	2258 LEBANON PIKE	2258 LEBANON PK #27	NASHVILLE	TN	37214
095030A06600CO	JETNOR BEQIR, L.L.C.	2258 LEBANON PIKE	P O BOX 148195	NASHVILLE	TN	37214
095030A06300CO	JETNOR BEQIR, LLC	2258 LEBANON PIKE	P O BOX 148195	NASHVILLE	TN	37214
095030A05600CO	MECO, ARIAN R. & LUTFIJE	2258 LEBANON PIKE	2258 LEBANON PK # 56	NASHVILLE	TN	37214
095030A05300CO	PAPA, LUCIE & PERIKLI & DIONIS N.	2258 LEBANON PIKE	2258 LEBANON PIKE 53	NASHVILLE	TN	37214
095030A04600CO	DERVISHI, SAIMIR	2258 LEBANON PIKE	2258 LEBANON PK # 26	NASHVILLE	TN	37214
095030A04100CO	NOE, THOMAS K. & DONNA S.	2258 LEBANON PIKE	2258 LEBANON PK # 27	NASHVILLE	TN	37214
095030A03600CO	CAMPBELL, NELLIE R.	2258 LEBANON PIKE	2258 LEBANON RD UNIT 36	NASHVILLE	TN	37214
095030A03500CO	JETNOR BEQIR, LLC	2258 LEBANON PIKE	P O BOX 148195	NASHVILLE	TN	37214
095030A02800CO	DERVISHI, DASHAMIR & MERITA	2258 LEBANON PIKE	2258 LEBANON PK UNIT 28	NASHVILLE	TN	37214
095030A04000CO	SHOCKNEY, DON & LORENE	2258 LEBANON PIKE	404 HOLLYDALE DR	NASHVILLE	TN	37217
095030A02700CO	NOE, THOMAS K. & DONNA S.	2258 LEBANON PIKE	2258 LEBANON PK # 27	NASHVILLE	TN	37214
095030A02000CO	STANDLEY, H. J., JR.	2258 LEBANON PIKE	2258 LEBANON RD # 20	NASHVILLE	TN	37214
095030A03700CO	LATIMER, MEREDITH C.	2258 LEBANON PIKE	2258 LEBANON PIKE 37	NASHVILLE	TN	37214
095030A03400CO	NOE, THOMAS K. & DONNA S. ET UX	2258 LEBANON PIKE	2258 LEBANON PK # 27	NASHVILLE	TN	37214
095030A02900CO	LUSK, SHIRLEY LAVERNE	2258 LEBANON PIKE	2258 LEBANON PK # 29	NASHVILLE	TN	37214
095030A02600CO	DERVISHI, DASHAMIR & MERITA	2258 LEBANON PIKE	2258 LEBANON PK UNIT # 26	NASHVILLE	TN	37214
095030A02100CO	KAPEDANAS, NIKO & ALVERTA	2258 LEBANON PIKE	2258 LEBANON PK # 21	NASHVILLE	TN	37214
095030A00500CO	SULLIVAN, ESTELLE R.	2258 LEBANON PIKE	2258 LEBANON PK UNIT 5	NASHVILLE	TN	37214
095030A01200CO	JETNOR BEQIR, L.L.C.	2258 LEBANON PIKE	P O BOX 148195	NASHVILLE	TN	37214
095030A01100CO	JETNOR BEQIR, L.L.C.	2258 LEBANON PIKE	P O BOX 148195	NASHVILLE	TN	37214
095030A01600CO	JETNOR BEQIR, L.L.C.	2258 LEBANON PIKE	P O BOX 148195	NASHVILLE	TN	37214
095030A00400CO	LUSK, JAMES W. & SHIRLEY L.	2258 LEBANON PIKE	2258 LEBANON RD # 25	NASHVILLE	TN	37214
095030A01300CO	BOWERS, CHARLESETTA	2258 LEBANON PIKE	2258 LEBANON PK # 13	NASHVILLE	TN	37214

Appendix I: Properties and Owners

095030A01000CO	NOONAN, MARY A. & LEMOLLARI, VIKTOR	2258 LEBANON PIKE	2258 LEBANON PIKE 10	NASHVILLE	TN	37214
095030A01700CO	AGOLLI, FABJOLA	2258 LEBANON PIKE	3461 WHITE PINE DR	NASHVILLE	TN	37214
095030A00200CO	POTKA, ZYBE	2258 LEBANON PIKE	P O BOX 148195	NASHVILLE	TN	37214
095030A06800CO	NOE, THOMAS K. & DONNA S.	2258 LEBANON PIKE	2258 LEBANON PK # 27	NASHVILLE	TN	37214
08416002500	BERTUCA, THEODORE F.	146 MCGAVOCK PIKE	152 MCGAVOCK PK	NASHVILLE	TN	37214
095030A06900CO	DENNISTON, FLORENCE E.	2258 LEBANON PIKE	2258 LEBANON PIKE 69	NASHVILLE	TN	37214
095030A06700CO	FIGUEROA, OSCAR VASQUEZ	2258 LEBANON PIKE	804 BALLEycastle DR	SMYRNA	TN	37167

Appendix C: 2022-2024 Work Plan for Sidewalks

On Street	From Street	To Street	Phase	2022	2023	2024	Length/ Feet	Council District
Maplewood Trace	Dickerson Pk	Walton Ln	CON	CON			2,311	8
Brick Church Pk	Masonwood Dr	Village Trail	CON	CON			1,965	3
Annex Ave	Thunderbird Dr	Mall Entrance	CON	CON			1,994	20
Linbar Dr	Wallace Rd	Harding Place	ROW	ROW/ CON			1,627	30
Highway 70 S	Metro Park & Ride	Sawyer Brown Rd	ROW	ROW/ CON			2,566	22
Tusculum Rd	Nolensville Pk	Brewer Dr	ROW	ROW/ CON			4,526	27, 30
Brewer Dr	Green Leaf Dr	Nolensville Pk	ROW	ROW/ CON			1,523	27
Stewarts Ferry Pk	Tennessee School for the Blind	Stones River Greenway	ROW	ROW/ CON			1,134	14, 15
Hickory Plaza	Old Hickory Blvd	Ocala Dr	CON	DES/ CON			2,759	27
Andrew Jackson Pkwy	Highland View Dr	Lebanon Pk	CON	DES/ CON			4,055	11,12
Elm Hill Pk	Fessler's Ln	Spence Ln	CON	DES/ CON			6,361	19
E Thompson Ln*	Lawndale Dr	Murfreesboro Pk	DES	DES/ CON			340	16
Due West Ave*	Gallatin Pk	Coreland Dr	DES	DES/ CON			430	7
Foster Ave*	Glenrose Ave	Murfreesboro Pk	DES	DES/ CON			3,988	17
51st Ave N*	Charlotte	Alabama Ave	DES	DES/ CON			400	24
Craighead St*	Oceanside Dr	Bransford Ave	DES	DES/ CON			1,379	17
Highway 100*	Mid-Block	Collins Rd	DES	DES/ CON			261	35
Sawyer Brown Rd*	Todd Preis Dr	Highway 70 S	DES	DES/ CON			2,188	22
Cane Ridge Rd*	Old Franklin Rd	Bell Rd	DES	DES/ CON			6,773	32
Anderson Rd*	Bell Rd	Owendale Dr	DES	DES/ CON			3,200	29
Athens Way*	Rosa L Parks Blvd	430' N of Rosa L Parks Blvd	Planned	DES/ CON			368	02
Broadway	2nd Ave N	1st Ave N	CON	CON			270	19
W Hamilton Ave	West of Haynes Park Dr	Harold Prewett Dr	CON	CON			2,484	01
Herman St	12th Ave N	10th Ave N	CON	CON			1,318	19
Vailview Dr	Richmond Hill Dr	Parkwood Park Entrance	CON	CON			561	03

* *Newly identified Sidewalk project based on priority scoring*

CON = Construction

ROW= Right-of-Way

DES= Design

On Street	From Street	To Street	Phase	2022	2023	2024	Length/ Feet	Council District
Old Lebanon Pk	J B Estille Dr	Lebanon Pk	CON	CON			1,055	15
Delmas Ave	Cherokee Ave	Gallatin Pk	CON	CON			2,352	5
Old Harding Pk	Hicks Rd	Highway 70 S	CON	CON			1,432	22, 34
Kings Ln	Phipps Dr	West of Clarksville Pike	CON	CON			2,759	1
Forest Park Rd	Neelys Bend Rd	Lexington Garden Apartments	CON	CON			985	9
Hamilton Church Rd	Murfreesboro Pk	East of Hamilton Creek Apartments	CON	CON			1,922	29, 33
Riverside Rd	20th St	1502 Riverside Rd	CON	CON			2,281	11
Edge O Lake Dr	Deerpath Dr	Bell Rd	CON	CON			2,278	29
Bowling Ave	Forest Park Ave	Richmeade Pl	CON	CON			1,792	24
Davidson Rd	Hg Hill Middle School	Highway 70 S	CON	CON			2,009	23
Asheford Trce	Murphywood	West of Bentwood Dr	CON	CON			139	32
Eastland Ave	N 16th St	Setliff Pl	CON	CON			744	6
Murfreesboro Pk*	Rural Hill Rd	Dover Glen Dr	DES	DES/ ROW	CON		530	28, 29
Mt View Rd	Curtis Hollow Rd	Rural Hill Rd	CON	ROW	CON		746	32
Dickerson Pk	Donald St	Dellway Dr	ROW	ROW	CON		1,365	8
23rd Ave N	St Louis St	Cass St	ROW	ROW	CON		1,291	2, 21
Foster Ave	Thompson Ln	Whitsett Rd	ROW	ROW	CON		1,646	16
Murfreesboro Pk	Una Antioch Pk	British Woods Dr	ROW	ROW	CON		1,759	28, 29
15th Ave N/Hynes St	Alley 243	Alley 372	ROW	ROW	CON		715	19
Shadowbrook Trl	Existing Sidewalk	Cane Ridge Elementary	ROW	ROW	CON		363	32
Brewer Dr	McMurray Dr	Green Leaf Dr	ROW	ROW	CON		1,055	27
Winthorne Dr	Glenpark Dr	Briley Pkwy	ROW	ROW	CON		2,755	13
Blue Hole Rd	Henry Maxwell Elementary School	Blue Hole Way	ROW	ROW	CON		919	31, 32
Ordway Pl	N 16th St	N 17th St	ROW	ROW	CON		839	6
Lebanon Pk	Craigmeade Dr	Cottage Ln	ROW	ROW	CON		561	15
Stratford Ave	Oxford St	Kennedy Ave	ROW	ROW	CON		954	7

* **Newly identified Sidewalk project based on priority scoring**

CON = Construction

ROW= Right-of-Way

DES= Design

On Street	From Street	To Street	Phase	2022	2023	2024	Length/ Feet	Council District
McGavock Pk*	Gallatin Pk	Bronte Ave	DES	DES	CON		172	7
Dickerson Pk*	Homestead Rd	Briley Pkwy Ramp	DES	DES	CON		2,766	2, 3, 8
Charlotte Pk*	River Rd	Davidson Dr	DES	DES	CON		525	22, 35
Welshwood Dr*	Corning Dr	Nolensville Pk	DES	DES	CON		1,082	26
Murfreesboro Pk*	Crutchfield Ave	Spence Ln	DES	DES	CON		1,641	19, 16, 17
Myatt Dr*	N Dupont	Anderson Rd	DES	DES	CON		1,922	9
Gallatin Pk	120 Ft East of Cude Ln	Shepherd Hills Dr	CON		CON		648	10
Dickerson Pk	180' North of E Trinity Ln	Duke St	CON		CON		292	5
Vaulx Ln	Kirkwood Ave	Iverness Ave	CON		CON		1,297	17
Old Hickory Blvd	Bending Creek Dr	Mill Creek Greenway	CON		CON		1,322	31
Hillsboro Pk	Crestmoor Rd	Graybar Ln	DES	DES	ROW	CON	430	25
Harris St*	Gallatin Pk	Mid-Block	DES	DES	ROW	CON	508	09
Gallatin Pk*	Myatt Dr	Conference Dr	DES	DES	ROW	CON	2,225	10
Doverside Dr*	Eaglewood Ln	Oakview Dr	DES		DES	CON	206	3
Dickerson Pk*	Ewing Dr	Oak Valley Dr	DES		DES	ROW	512	2, 8
Murfreesboro Pk*	Kermit Dr	Bowwood Ct	Planned		DES	ROW	1,438	13, 16
Nolensville Pk*	Welch	Paragon Mills Rd	Planned		DES	ROW	266	26
Gallatin Pk*	One Mile Pkwy	Welworth St	Planned		DES	ROW	1,114	10
Harding Pl*	S Perimeter Park Dr	Antioch Pk	Planned		DES	ROW	1,273	13, 28
Brick Church Pk*	Richmond Hill Dr	Masonwood Dr	Planned		DES	ROW	1,801	3
Broadmoor Dr*	Dickerson Pk	Walton Ln	Planned			DES	1,837	8
Old Hickory Blvd*	Devon Valley Dr	Highway 70 S	Planned			DES	2,373	34
Myatt Dr*	500' S of Spring Branch Dr	Gallatin Pk	Planned			DES	1,625	10
Harrington Ave*	Gallatin Pk	Idlewood Ave	Planned			DES	866	9
Dickerson Pk*	Dellway Dr	Ben Allen Rd	Planned			DES	3,027	2, 8
Clifton Ave*	931 Aly	22nd Ave N	Planned			DES	400	21
Lebanon Pk*	J B Estille Dr	Old Lebanon Pk	DES			DES	592	15

* *Newly identified Sidewalk project based on priority scoring*

CON = Construction

ROW= Right-of-Way

DES= Design

Appendix D: 2022-2024 Work Plan for Bikeways

On Street	From Street	To Street	Phase	2022	2023	2024	Length/ Feet	Council District
Monroe St	Arthur Ave	1st Ave N Greenway	CON	CON			4,860	19
20th Ave S	Broadway	Charlotte Ave	Planned		DES	CON	3,190	19, 21
E Thompson Ln	Thompson Ln	Murfreesboro Rd	Planned		DES	CON	8,702	16
3rd Ave N/S	Broadway	Union St	Planned	CON			1,675	19
Jefferson St	Rosa L Parks Blvd	Cowan St	Planned	CON			4,687	5, 19
Jonquil Dr	Harding Pl	Paragon Mills Rd	Planned	CON			3,040	26
Heritage Dr/	Old Hickory Blvd	Nesbitt Ln	Planned	CON			4,511	8
Cahal Ave	Gallatin Pk	Porter Rd	Planned	CON			5,140	7
KVB/Shelby Ave	8th Ave S	S 14th St	Planned		DES	CON	12,219	6, 19
6th Ave S	Oak St	Lafayette St	DES	CON			2,281	17, 19
Lindsley Ave	2nd Ave S	Hermitage Ave	DES	CON			1,904	19
Lindsley Ave	3rd Ave S	2nd Ave S	DES	CON			331	19
Hillside Ave	Wedgewood Ave	Olympic St	DES	CON			3,483	17
Dr Martin Luther King Jr Blvd	George L Davis Blvd	Rosa L Parks Ave	DES	CON			2,371	19
12th Ave S	Ashwood Ave	11th Ave S	DES	DES/ CON			8,659	17, 18, 19
2nd Ave N	Gay St	Junior Gilliam Way	DES	DES/ CON			2,125	19
Demonbreun St	10th Ave S	1st Ave S	DES	DES	CON		3,239	19
Chestnut St	8th Ave S	Wharf Ave	DES	DES	CON		5,275	17
Edgehill Ave	16th Ave S	8th Ave S	DES	DES	CON		4,350	17, 19
Centennial Blvd/ Dr Walter S Davis Blvd	51st Ave N	Ed Temple Blvd	DES	DES	CON		9,664	20, 21
49th Ave N	Indiana Ave	Centennial Blvd	DES	DES	CON		2,177	20
Kentucky Ave	52nd Ave N	49th Ave N	DES	DES	CON		1,166	20
Georgia Ave	Delray Dr	52nd Ave N	DES	DES	CON		1,090	20
Indiana Ave	52nd Ave N	49th Ave N	DES	DES	CON		1,159	20
52nd Ave	Georgia Ave	Kentucky Ave	DES	DES	CON		1,653	20
Demonbreun St	Roundabout	14th Ave S	DES	DES	CON		1,013	19
East Nashville Backbones	3rd Ave N	S 11th St	Planned	DES	CON		17,154	6, 19

**Locations in close proximity to one another may be advanced as a single project.*

CON = Construction

ROW= Right-of-Way

DES= Design

On Street	From Street	To Street	Phase	2022	2023	2024	Length/ Feet	Council District
25th Ave N	West End Ave	Park Plaza	Planned		DES	CON	3,003	21
40th Ave N	Charlotte Ave	Dr Walter S Davis Blvd	Planned		DES	CON	4,746	21, 24
Patterson St	25th Ave N	McMillan St	Planned		DES	CON	5,483	21
31st Ave N	West End Ave	Park Plaza	Planned	DES/ CON			2,948	21
Long Blvd	Acklen Park Dr	31st Ave N	Planned		DES	CON	1,981	21
S 1st St/Davidson St	Seigenthaler Bridge	Shelby Park	Planned		DES	CON	7,907	6
Carroll St	Pedestrian Bridge	Charles E Davis Blvd	Planned		DES	CON	904	19
Gay St	Bank St	1st Ave N	Planned		DES	CON	1,004	19
Apache Ln/Bart Dr/Coleridge Dr	Tusculum Rd	Haywood Ln	Planned		DES	CON	12,957	30
Acklen Park Dr	Murphy Rd	Long Blvd	Planned		DES	CON	2,729	21, 24
Powell Ave	Sidco Dr	Thompson Ln	Planned		DES	CON	7,147	16
Sidco Dr	Harding Pl	Thompson Ln	Planned		DES	CON	11,159	16
Charles E Davis Blvd/Wharf Ave	Chestnut St	Carroll St	Planned		DES	CON	2,808	17, 19
3rd Ave S	Lindsley Ave	Korean Veterans Blvd	Planned		DES	CON	2,315	19
Clifton Ave	40th Ave N	Spruce St	Planned	DES/ CON			9,156	21
Union St	Church St	3rd Ave N	Planned		DES	CON	3,074	19
Cass St	18th Ave N	I-65	Planned		DES	CON	5,563	2, 21
18th Ave N/Kellow St/10th Ave N	Clarksville Pk	Rosa Parks Blvd	Planned		DES	CON	5,408	2
Heiman St/11th Ave N	Ed Temple Blvd	Arthur Ave	Planned		DES	CON	8,524	21
14th Ave N	Heiman St	Buchanan St	Planned		DES	CON	3,019	21
5th Ave N	James Robertson Pkwy	Hume St	Planned		DES	CON	4,766	19
County Hospital Rd	Briley Pkwy	Clarksville Pk	Planned		DES	CON	11,718	1, 2
21st Ave S	Broadway	Charlotte Ave	Planned		DES	CON	3,666	18,19, 21
Jefferson St	Ed Temple Blvd	Rosa L Parks Blvd	Planned	DES/ CON			8,925	19, 21
Shelby Ave	S 14th St	S 20th St	On hold				3,218	6
51st Ave N/ Nebraska Ave	Wyoming Ave	440 Greenway	On hold				8,058	24
Cross Timbers Dr	Highway 70 S	Sawyer Brown Rd	On hold				4,367	22

*Locations in close proximity to one another may be advanced as a single project.

CON = Construction

ROW= Right-of-Way

DES= Design

On Street	From Street	To Street	Phase	2022	2023	2024	Length/ Feet	Council District
Clarksville Pk	Ed Temple Blvd	Clay St	On hold				3,377	2, 21
Coley Davis Rd	Poplar Ridge Dr	Highway 70 S	On hold				9,270	22
Alabama Ave	51st Ave N	42nd Ave N	On hold				3,405	24
Colice Jeanne Rd	Baugh Rd	Highway 70 S	On hold				1,887	22
42nd Ave N	Murphy Rd	Alabama Ave	On hold				4,569	24
Fatherland St	S 5th St	S 19th St	On hold				8,179	6
S 14th St	Davidson St	Woodland St	On hold				5,209	6
N/S 17th St	Davidson St	Eastland Ave	On hold				8,156	6
Woodland St	S 11th St	S 17th St	On hold				3,026	6
Holly St	S 17th St	S 19th St	On hold				958	6
Lawrence Ave	12th Ave S	10th Ave S	On hold				870	17

**Locations in close proximity to one another may be advanced as a single project.*

CON = Construction

ROW= Right-of-Way

DES= Design

RIGHT OF WAY (ROW) MEETING FOR PROPERTY OWNERS

LEBANON PIKE (SR-24/US-70) SIDEWALK IMPROVEMENTS

FROM MCGAVOCK PIKE TO OLD LEBANON ROAD

MEETING PURPOSE

To Provide the Public with up-to-date Information on the Lebanon Pike Sidewalk Improvement Project

To Provide Information on the Right of Way Acquisition Process

To solicit comments and concerns regarding this Project.

PROJECT TEAM AND SPONSORS

Metro Nashville Council (District 15):

- **Council Member Jeff Syracuse**

Nashville Department of Transportation (NDOT):

- **Darrell Moore, Project Manager**

Tennessee Department of Transportation (TDOT):

- **John Lentz, TDOT Region 3 ROW Office**

HDR, Inc., Roadway Design Consultant:

- **Brian Reynolds, Roadway Design Project Manager**

ROW Acquisition Consultant Team:

- **AECOM – ROW Acquisition Project Manager (Patrick Murray)**
- **R&D Enterprises – ROW Negotiations (Debra Rheman)**
- **Randy Button & Associates – Appraisals (Randy Button)**
- **Boozer & Co. – Review Appraisals (Ted Boozer)**
- **Smith Sholar Milliken, PLLC. – Title Searches and ROW Closings (Trudy Milliken)**
- **Ragan-Smith – Right of Way Staking (Brandon Baxter)**

PROJECT INFORMATION

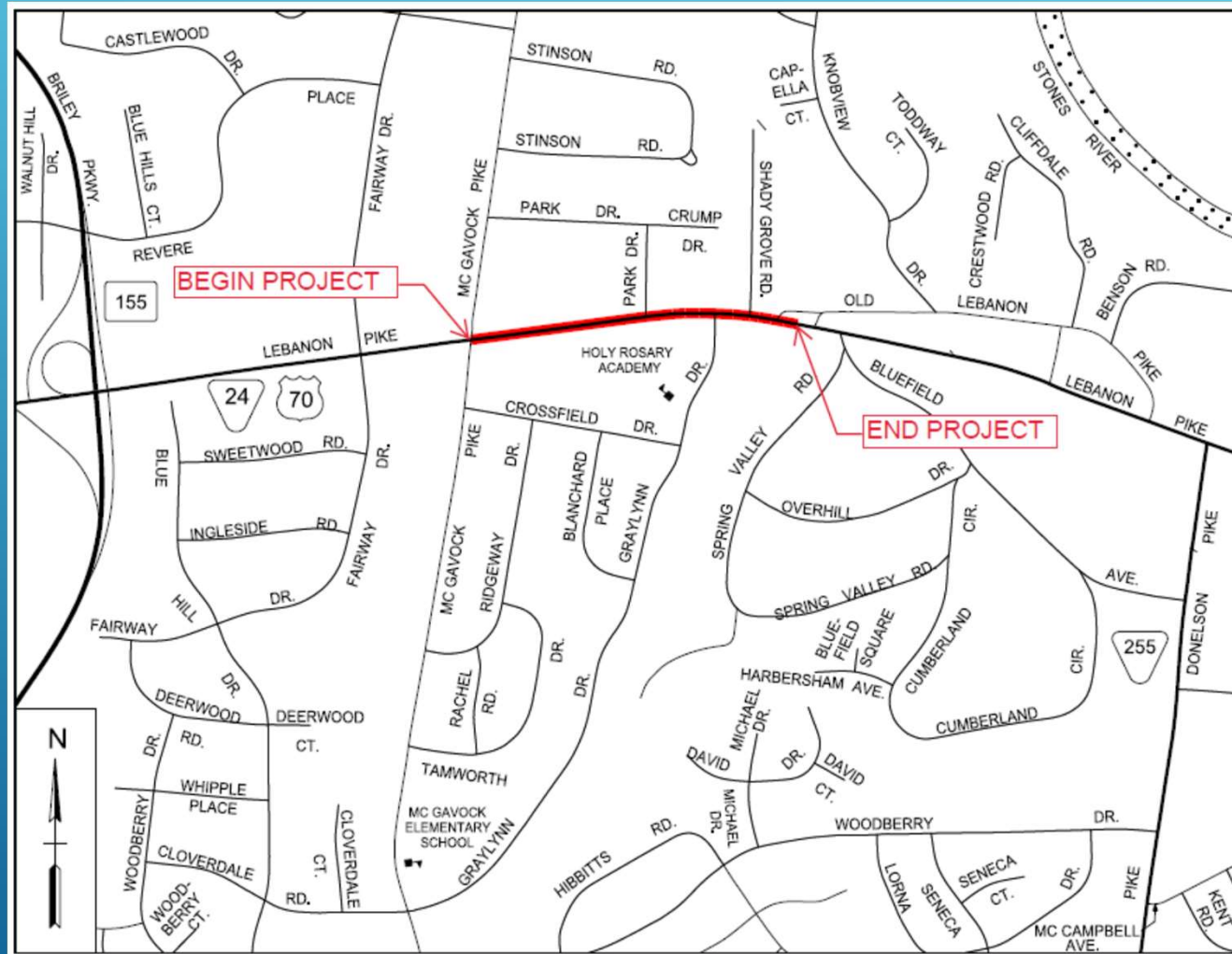
The project is a Nashville Department of Transportation (NDOT) project and managed by NDOT staff.

The project is partially funded by a grant from The Tennessee Department of Transportation (TDOT) and TDOT has oversight/approval responsibilities for all phases of project development

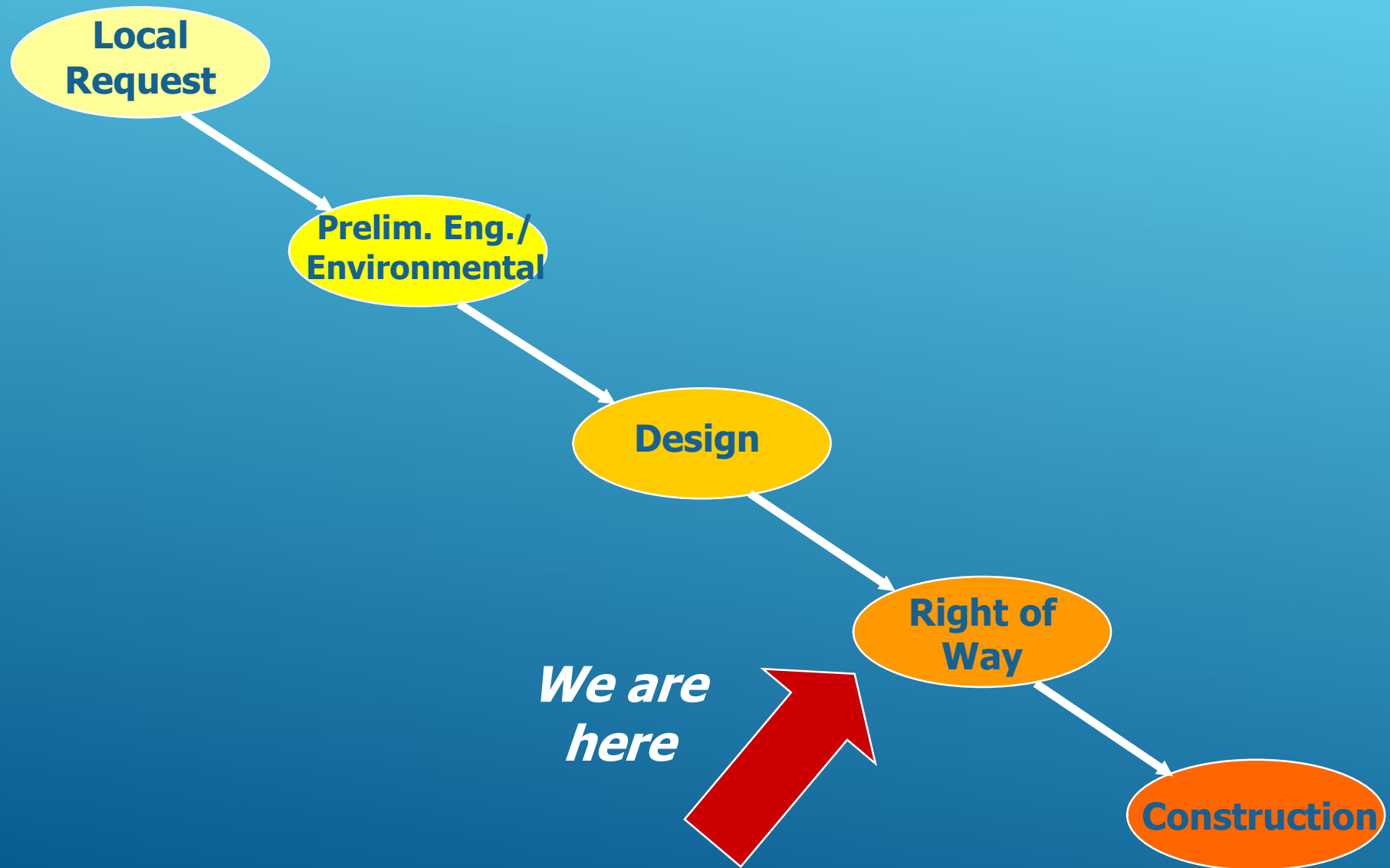
The project must be developed in accordance with TDOT's Local Government Guidelines for the Management of Federal and State-Funded Transportation Projects

The Roadway Design, ROW Acquisition, Environmental Studies and Construction must be to TDOT Standards

LOCATION MAP



PROJECT DEVELOPMENT PROCESS



ROW ACQUISITION PROCESS

NOTICE OF PROPOSED ACQUISITION (COMPLETE)

TITLE SEARCHES (COMPLETE)

ROW OWNER MEETING (UNDERWAY)

ROW STAKING FOR APPRAISALS

- **PROPOSED ROW AND EASEMENTS WILL BE IDENTIFIED**

PROPERTY APPRAISALS

- **PROPERTY OWNER WILL RECEIVE A NOTICE OF APPRAISAL**

REVIEW APPRAISALS

NEGOTIATIONS FOR ACQUISITION

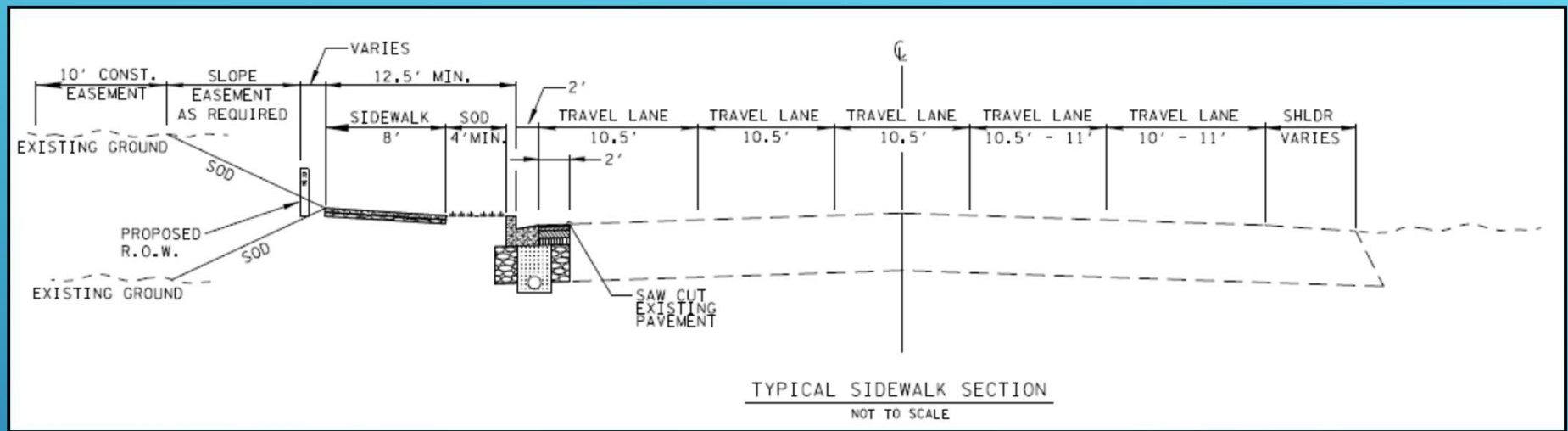
CLOSINGS

PROJECT PHOTO



**WORK WILL OCCUR ON THE NORTH SIDE OF LEBANON PIKE
(LEFT SIDE OF THE PICTURE)**

TYPICAL SECTION



Right of Way: Also known as Fee Simple. Any area within the proposed right of way will become the property of the State/City.

Slope Easement: These are permanent easements for construction and maintenance of slopes outside of the ROW line and remain the property of the Grantor(s) and may be used for any purpose desired, provide such use does not interfere with the use or maintenance of said slopes.

Construction Easement: Temporary easements used by the State of Tennessee, its contractors or assigns for a period of 3 or more years from and after the commencement of construction.

Permanent Drainage Easement (Not Shown): For construction and maintenance of drainage facilities and they remain the property of the Grantor(s) and may be used for any purpose desired, provide such use does not interfere with the use or maintenance of said facility.

PROJECT SCHEDULE

The project schedule is largely dependent on the availability of funding for construction.

The ROW must be acquired in accordance with TDOT standards. The acquisition from 19 ROW tracts could take as long as 18 months.

While the ROW is being acquired, coordination with utility companies, acquisition of Environmental Permits and final preparation of Roadway Design Plans will be happening.

Once certification of the ROW Acquisition, Utility Coordination, Environmental Coordination (Permits) and Construction Plans is obtained from TDOT, TDOT will provide a Notice to Proceed with Construction Phase

The Construction Phase involves development of the Bid Book (Proposal Contract), Contract Bidding and project Construction.

The entire Construction Phase could take up to 18 months with the physical construction lasting more than a year.

FREQUENTLY ASKED QUESTIONS

Q 1: What if I don't agree with the amount the State/City offers?

A 1: Additional negotiations, administrative settlement and eminent domain process.

Q 2: When will I get an offer?

A 2: Typically 2-3 weeks after negotiators receive final approved appraisals, they will schedule to meet with owner to present the initial offer.

Q 3: When will the highway construction begin and be completed?

A 3: The ROW acquisition process could take up to 18 months for acquisition from 19 ROW tracts. The entire construction phase could take 18 months.

FREQUENTLY ASKED QUESTIONS

Q 4: Who is responsible for damages to my property made during construction?

A 4: The contractor is responsible for damages to real or personal property including items such as cracks and flooding.

Q 5: When will I receive payment for the property you are acquiring?

A 5: It usually takes 45 to 60 days to secure a check after an agreement has been reached. A closing will be scheduled upon receipt of the check. In some cases, when a lender is involved, it can take longer.

TDOT ROW ACQUISITION BROCHURE (FRONT)

TDOT Right of Way Division Acquisition Section



REGION 1 Right-of-Way Office:

7345 Region Lane
Knoxville, TN 37914

Phone: (865) 594.2496 Fax: (865) 594.2495

REGION 2 Right-of-Way Office:

4005 Cromwell Road
P.O. Box 22368
Chattanooga, TN 37422

Phone: (423) 510.1100 Fax: (423) 510.1131

REGION 3 Right-of-Way Office:

6601 Centennial Blvd.
Nashville, TN 37243

Phone: (615) 350.4200 Fax: (615) 350.4410

REGION 4 Right-of-Way Office:

300 Benchmark Place
Jackson, TN 38301

Phone: (731) 935.0134 Fax: (731) 935.0135

HEADQUARTERS:

505 Deaderick Street
Suite 600-James K. Polk Bldg.
Nashville, TN 37243

Phone: (615) 741.3196 Fax: (615) 532.1548
www.tn.gov/tdot/section/row



Tennessee Department of Transportation
 Authorization No 401497 (Jan. 2011)
 500 copies. This public document was
 promulgated at a cost of \$0.15 per copy



Right of Way Division: Acquisition Process



Communication Accountability Consistency Integrity

TDOT RIGHT OF WAY PROCESS

OUR MISSION

The TDOT Right-Of-Way Division is charged with purchasing property needed to construct highway with as little impact as possible to property owners and the communities around them.

YOUR RIGHTS

The acquisition of property is like a private sale transaction. Due to the responsibility of protecting public interest, the state offers fair market value for each property acquisition.

Occasionally, agreements cannot be reached. When these situations develop, property owners, as well as the state, have well-defined rights under the law. These rights safeguard the owner from getting less than fair market value and protect state tax funds from unrealistic demands.

ACQUISITION PROCESS

APPRAISALS

Before property owners are contacted by the state for property purchases, licensed appraisers investigate the market for the type of property to be acquired. They personally check each home, place of business, and/or parcel of land that will be involved. Public records are

searched and recent area land sales are checked to establish a basis for valuing each property. This is based on fair market value; what a willing buyer would pay to a willing seller.

The appraiser then prepares an appraisal report that shows the development of fair market values assigned to the items involved. When only a portion of an entire property is to be acquired, the effect of the proposed acquisition on the remainder of the property is also considered by the appraiser. The appraisal report includes a signed certification that the appraiser has no present or intended future interest in the property appraised.

Often the state employs outside appraisers, especially if the nature of the property and its use is unique and a specialist is needed.

Typically staff appraisers review each appraisal and an amount is determined that the state will offer for the property. Sometimes an appraisal is not required if fair market value can easily be obtained and the valuation process is uncomplicated.

TDOT ROW ACQUISITION BROCHURE (BACK)

ACQUISITIONS

When acquisition of property begins, buyers for the state call on the property owners at a convenient time and explain the effects of the proposed project on the property. An approved written offer will be made to the owner at this time. Except in very unusual circumstances, the owner will receive payment in the amount of the state's offer prior to vacating his property. If during negotiations it is found that some item of value in the proposed acquisition or element of damage was overlooked, the appraisal, after due process, will be adjusted accordingly.

Payment is made to the property owner within a reasonable time, usually within 60 days after agreement is reached, thereby providing the property owner with funds to purchase new property or make adjustments to his remaining property.

RELOCATION ASSISTANCE

An occupant, either an owner or a tenant, who will be displaced by a highway project, is contacted by an agent for the state. An explanation is given on assistance and payments available under **The Relocation Assistance Program**. Ample notice and time for relocation will be given to the occupant.

EMINENT DOMAIN

The State strives to settle all acquisitions of property through fair negotiations and equitable procedures established by law. When an agreement cannot be reached between a property owner and the state for sale of property, right of way will be acquired through **Eminent Domain Proceedings**.

The state deposits with the Circuit Court Clerk an amount of money equal to the offer made by the state for the property and/or property rights required for the project.

The property owner may withdraw all funds deposited without influencing the amount of the final award to be determined by the court. If the award differs from the amount the owner has withdrawn from the court prior to final court action, necessary adjustments are made between the owner and the state after the court decision is rendered.

The State Attorney General's Office will work with the landowners and their legal representatives in an attempt to reach a settlement outside of court. If a settlement cannot be reached a jury of citizens will hear testimony prepared by both the property owner and the state. After deliberation, the jury determines just compensation due the property owner. The amount set by the court is binding to both parties, unless it can be shown that some part of the proceeding was in error.

RIGHT OF ENTRY

To meet project construction schedules, in some cases, the state may find it necessary to request the owner to allow construction to proceed on the property prior to either an amicable purchase agreement or Eminent Domain Proceedings. A "Right of Entry" given by the owner to the department for benefit of the public at large shall, in no manner, affect the legal rights of either the owner or the state. When such a right is voluntarily granted by the owner, the procedures outlined above would be followed as applicable to the case; however, the state would proceed with construction during the period of negotiations.

RIGHT OF CLEARANCE

The right of way acquired must be cleared of all structures. The property owner, therefore, is offered a fair market price for any and all structures located on the land acquired. Once an agreement is reached a deed executed, the structures, as well as the land, become the property of the state.

The state may dispose of the structures by advertising them for sale and removal, or including them in the roadway contract to be removed by the contractor.

Property owners who wish to retain and remove any pre-existing improvements may do so based on a predetermined value. This is called a salvage appraisal.

The agreement specifies the time allotted to the property owner to clear the improvements from the right of way.

Any structures or items of personal property, located on existing right of way, are considered an encroachment. The appraiser generally does not consider these structures when valuing the property, nor is compensation paid for items of personal property which are located on existing right of way. The state at its discretion may dispose of these items.

CONCLUSION

It is the Department's aim to secure right-of-way required for construction and maintenance of our highway system at a fair price under amicable conditions. Many questions may arise in your mind concerning the acquisition of your property that may not be explained in this pamphlet.

A list of the offices in charge of the Right-of-Way Acquisition for each of the State's Four Regions is shown on the back cover. Please feel free to ask any questions you wish of the Right-of-Way personnel located at these offices.

REPRESENTATIVES FROM NDOT, AND REPRESENTING NDOT, ARE PRESENT TO ANSWER YOUR QUESTIONS.

Regarding the Right of Way Acquisition process.

The Project's design features

Tentative Project schedules.

THE NASHVILLE DEPARTMENT OF TRANSPORTATION THANKS YOU FOR YOUR PARTICIPATION IN THE MEETING



CONTACT INFORMATION

Metro Council Member Jeff Syracuse
jeff.syracuse@nashville.gov

Darrell Moore, NDOT Project Manager
darrell.moore@nashville.gov

Patrick Murray (AECOM), Consultant ROW Project Manager
patrick.murray1@aecom.com



METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY

**JOHN COOPER
MAYOR**

OFFICE OF THE MAYOR
METROPOLITAN COURTHOUSE
NASHVILLE, TENNESSEE 37201
PHONE: (615) 862-6000
EMAIL: mayor@nashville.gov

July 7, 2022

Mr. Dan Pallme, Interim Director
Multimodal Transportation Resources Division
Tennessee Department of Transportation
Division of Multimodal Transportation Resources
505 Deaderick Street
James K. Polk Building, Suite 1200
Nashville, TN 37243

Mr. Pallme,

The Multimodal Access Grant (MMAG), a TDOT-administered program, annually awards state funds to support mobility infrastructure investments that address facility and service gaps along state routes in order to meet the mobility needs of pedestrians, bicyclists, and transit users.

NDOT's MMAG 2022 application is an innovative funding approach which requests state funding to construct new sidewalks and supporting safety improvements along the north side of Lebanon Pike (SR 24/US 70) from McGavock Pike to Old Lebanon Pike in Donelson. More specifically, NDOT proposes:

- The construction of an 8-foot sidewalk and a 4-foot grass furnishing zone with curb and gutter on the north side of Lebanon Pike.
- In addition, signal upgrades will be made at the intersections of McGavock Pike and Graylynn Drive that will also include ADA ramps, crosswalk markings, and pedestrian signals.

Safety is our priority, with Nashville committed to Vision Zero and the need to eliminate traffic fatalities and severe injuries in the city. Crash data on Lebanon Pike supports this intervention.

If awarded, NDOT's application will fund the construction of a safer means of travel for pedestrians and motorists along Lebanon Pike. More specifically,

- Helps implement the community's vision for the area to evolve into a more walkable and transit supportive street, as defined in the Downtown Donelson Urban Design Overlay. The overlay was first adopted by Metro Council in 2009 following extensive involvement from Donelson stakeholders.
- As the ROW phase is in process and design has been complete, the project is almost shovel-ready.

- Provides multimodal transportation to the core business district of Donelson. This project connects the new library, which is a large Metro investment in Donelson Plaza. This project provides a connection to Donelson station, which provides alternatives to vehicle commuting and access to Downtown and neighboring Wilson County.

I support NDOT's application for state funding to implement these important multimodal improvements that will address transportation needs on Lebanon Pike/SR 24.

Sincerely,

A handwritten signature in blue ink, appearing to read "John Cooper". The signature is fluid and cursive, with the first name "John" being more prominent than the last name "Cooper".

John Cooper
Mayor



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
ENVIRONMENTAL DIVISION
 SUITE 900 - JAMES K. POLK BUILDING
 505 DEADERICK STREET
 NASHVILLE, TENNESSEE 37243-0334
 (615) 741-3655

Streamlined Documentation Checklist (SDC) for NEPA Programmatic or C-List Categorical Exclusion (CE)

Memorandum

To: Klint Rommel
 Transportation Manager II
 TDOT Environmental Division
 Tennessee Department of Transportation
 505 Deaderick Street, Suite 900
 Nashville, Tennessee 37055

From: Darrell K. Moore
 Metro Nashville Public Works
 Capital Projects, Engineering Division
 720 South Fifth Street
 Nashville, TN 37206

Date: 08/28/2017

Project Information

Project Name: SR-24/US-70 (Lebanon Pike)
Project Termini: (Lebanon Pike), From McGavock Pike to Old Lebanon Pike in Nashville
City/Town/County: Donelson, Davidson County
MPO/RPO/TPO Area: Nashville Area MPO **PIN #:** 121729.00

	PE #	Right-of-Way #	Construction #
Federal	PE-N: STP-M-24(60)	STP-M-24(60)	STP-M-24(60)
	PE-D: STP-M-24(60)		
State	PE-N: 19LPLM-F0-127	19LPLM-F2-129	19LPLM-F3-130
	PE-D: 19LPLM-F1-128		

Per instruction from the Tennessee Department of Transportation (TDOT) Environmental Division, the following action is being proposed for processing as a:

☒ C-List CE
 ☐ Programmatic Categorical Exclusion (PCE)

Project Planning

- ☒ **The project is in an MPO/TPO**—The relevant Transportation Improvement Program (TIP) page is included in the attachment.
- ☐ **The project is in an RPO**—The relevant State Transportation Improvement Program (STIP) page is included in the attachment.

Project Description and Need

The project would extend new sidewalk along the north side of Lebanon Pike from McGavock Pike to Old Lebanon Pike (approximately 2,400 linear feet). The proposed 12-foot cross section would consist of an 8-foot sidewalk with a 4-foot furnishing zone or a similar 12-foot combination. In addition, reconstruction/modification of the existing traffic signals at Lebanon Pike and Graylynn Drive would be required, including handicap accessible pedestrian push buttons.

The project is in response to at least one pedestrian accident in the area and is an effort to provide better connectivity to several businesses in the area. In addition to new sidewalk, the project would also provide associated stormwater infrastructure, green zone, and pedestrian enhancements.

A project map is located in Attachment B.

Review Criteria

		NO	YES
1	Right-of-Way —Does the proposed action require the acquisition of more than 1.0 acre of right-of-way or permanent easements, or involve displacements of any commercial or residential occupants?	<input checked="" type="radio"/>	<input type="radio"/>
2	Access Control —Does the proposed action involve a change in access control?	<input checked="" type="radio"/>	<input type="radio"/>
3A	Streams/Wetlands —Does the proposed action require a U.S. Coast Guard construction permit or an Individual U.S. Army Corps of Engineers Section 404 permit?	<input checked="" type="radio"/>	<input type="radio"/>
3B	Streams/Wetlands —Does the proposed project involve impoundment (surface area of 10 acres or more), diversion, channel deepening, or other modification of a stream or other water body as regulated by the Fish and Wildlife Coordination Act?	<input checked="" type="radio"/>	<input type="radio"/>
4	Endangered Species —Does the proposed action result in adverse impacts to endangered species and/or critical habitat of species protected by the Endangered Species Act?	<input checked="" type="radio"/>	<input type="radio"/>
5	Floodplain/Floodway —Does the proposed action involve work encroaching on a regulatory floodway or work affecting the base floodplain (100-year flood) elevations of a water course or lake?	<input checked="" type="radio"/>	<input type="radio"/>
6A	Farmland —Does the project convert farmland to a transportation use?	<input checked="" type="radio"/>	<input type="radio"/>
6B	Farmland —If the answer to 6A is YES , do the total points on the Natural Resources Conservation Service Farmland Conversion Impact Rating Form equal 160 or greater?	<input checked="" type="radio"/>	<input type="radio"/>
7	Wild and Scenic Rivers —Does the proposed action involve construction in, across, or adjacent to a river designated as a component of the National System of Wild and Scenic Rivers?	<input checked="" type="radio"/>	<input type="radio"/>
8	Air Quality —Will an Air Quality Analysis be needed?	<input checked="" type="radio"/>	<input type="radio"/>
9	Noise —Is the proposed action a Type I highway project?	<input checked="" type="radio"/>	<input type="radio"/>

Error! Reference source not found.

Donelson, Davidson County

Page 3

Review Criteria		NO	YES
10	Section 4(f) —Does the proposed action involve the use of properties protected by Section 4(f) (49 USC 303)?	<input type="radio"/>	<input type="radio"/>
11	Section 6(f) —Does the proposed action involve use of park land funded through the Land and Water Conservation Fund Act of 1965?	<input type="radio"/>	<input type="radio"/>
12	Cultural Resources (Architectural/Historical and Archaeological) —Does the proposed action have a Section 106 Adverse Effect on properties eligible for or listed in the National Register of Historic Places?	<input type="radio"/>	<input type="radio"/>
13	Native American Coordination —If the project required Native American coordination, were any responses received from the tribes indicating possible controversy?	<input type="radio"/>	<input type="radio"/>
14	Hazardous Materials —Does the proposed action affect a known hazardous materials site?	<input type="radio"/>	<input type="radio"/>
15	Environmental Justice —Does the proposed action result in disproportionately high and adverse human health or environmental effects on minority or low-income populations as defined in the Environmental Justice Executive Order 12898 and in FHWA Order 6640.23A?	<input type="radio"/>	<input type="radio"/>
16	Other —Are there other issues of concern?	<input type="radio"/>	<input type="radio"/>

If the answer to all of the above questions is NO, the proposed action qualifies for processing as a Programmatic or C-List Categorical Exclusion. This checklist should now be completed, signed, and submitted to the TDOT Environmental Division for review and concurrence.

If the answer to any of the above questions is YES, contact the TDOT point of contact to confirm the level of environmental documentation. If the impacts/effects are not significant, the proposed action may still be processed as a Programmatic or C-List Categorical Exclusion.

Preparer's Certification

I hereby certify that I have read and understand the *Instructions for the Preparation of the NEPA Streamlined Documentation Checklist* (SDC) for Programmatic or C-List Categorical Exclusions. I also certify that I have read and understand the Programmatic Categorical Exclusion Agreement between the FHWA –Tennessee Division and TDOT.

This document has been reviewed for compliance with applicable federal, state, and local laws and regulations. It has been prepared in compliance with the Council on Environmental Quality Regulations for Implementing the Procedural Provision of the National Environmental Policy Act, 40 CFR 1500–1508, 23 CFR 771, and the Tennessee Environmental Procedures Manual. I hereby certify that I have read and understand 23 CFR 771.117(c).

This document has been prepared by experienced, technically competent, and knowledgeable professionals. I can attest to the document's quality, accuracy, and completeness. By signing this document I am further certifying that, to the best of my knowledge, it meets the criteria for a Programmatic or C-List Categorical Exclusion.

Prepared by:

**Birch,
Valerie D**

Digitally signed by
Birch, Valerie D
Date: 2017.08.29
16:28:24 -05'00'

06/28/2017

Valerie Birch
Environmental Planner
HDR
615-812-9894
Valerie.Birch@hdrinc.om

**Local Government
Representative Signature
(if prepared by local
government):**

mm/dd/yyyy

Name
Title
Organization
Phone
Email

FOR TDOT ENVIRONMENTAL DIVISION USE ONLY

☒ C-List CE

The TDOT Environmental Division has reviewed the proposed project for compliance with environmental laws and regulations. It is our determination that the project will not, either individually or cumulatively, have any significant environmental impacts. This project meets the criteria of a Categorical Exclusion under 23 CFR 771.117(c) and therefore does not normally require further NEPA approvals by FHWA.

☒ PCE

The TDOT Environmental Division has reviewed the proposed project for compliance with environmental laws and regulations. It is our determination that the project will not, either individually or cumulatively, have any significant environmental impacts. This project meets the criteria of a Categorical Exclusion under 23 CFR 771.117(d), and complies with the most recent Programmatic Categorical Exclusion Agreement executed by TDOT and FHWA.

Reviewed by:



08/28/2017

Drew Gaskins, AICP
Transportation Planner

Approved by:



Klint
Rommel

Digitally signed by Klint Rommel
DN: cn=Klint Rommel, o=Environmental Analysis
Office, ou=Transportation Department of Transportation,
email=Klint.rommel@dot.gov, c=US
Date: 2017.08.31 15:25:46 -0500

Klint Rommel
Transportation Manager II

cc: TDOT Local Programs Office
TDOT Environmental Division
TDOT Region III
Metro Nashville
Mr. John Kahle

Additional Information (Preparer to provide additional information as necessary)

1. Right-of-Way

The project would require the acquisition of 0.39 acres of ROW.

2. Access Control

Businesses in the project corridor would retain access to Lebanon Pike. The proposed project would not change access to any of the adjacent properties or to any of the proposed roadways to be improved.

3. Streams/Wetlands

Field reviews of the project area identified one wet weather conveyance located at the far eastern portion of the proposed project. No other significant resources were observed within or near the direct impact zone of the proposed project.

The full Ecology Report is included in Appendix 1.

4. Endangered Species

The project has been coordinated with all agencies with the responsibility of protecting endangered species in the project area. Below are summaries of all responses.

See Attachment C for copies of all coordination response letters.

- ☐ U.S. Fish and Wildlife Service (USFWS) Memorandum of Agreement applies. If marked, the following three checkboxes would not apply.
- ☒ The proposed project has been coordinated with the USFWS Endangered Species Office. The USFWS response dated 05/31/2017 stated "upon review of the information provided and our database, we would not anticipate impacts to any federally listed or proposed species as a result of the project. Also, our national wetland inventory maps do not indicate that any wetlands are present. Therefore, based on the best information available at this time, we believe that the requirements of section 7 of the Endangered Species Act (Act) of 1973, as amended, are fulfilled for all species that currently receive protection under the Act."
- ☒ The proposed project has been coordinated with the Tennessee Wildlife Resources Agency (TWRA). The TWRA response dated 08/31/2016 stated "it is our understanding that no streams will be crossed for this project. We have no objection to the proposed project and do not anticipate adverse impacts to state listed species under our authority due to the proposed project; provided that best management practices to address erosion and sediment are implemented and maintained during construction activities."
- ☒ The proposed project has been coordinated with the Tennessee Department of Environment and Conservation (TDEC), Division of Natural Areas. The TDEC response dated 02/22/2017 stated "based on the limited suitable habitat within the project area for listed plant species, we do not anticipate any impacts to rare, threatened, or endangered plant species from this project. No other conservation sites, natural areas, or scenic rivers were documented from the general area."

5. Floodplain/Floodway

The project is not in a FEMA floodway, floodplain, or study area, and is located on Flood Insurance Rate Map (FIRM) in Nashville and Davidson County, Panel 266 of 478, Map # 47037C0266H. A portion of the FEMA FIRM is included in Attachment D.

6. Farmland

The project does not convert farmland to a transportation use.

7. Wild and Scenic Rivers

The project does not involve a designated Wild and Scenic River.

8. Air Quality

The proposed project has been coordinated with the TDOT Air Quality and Noise section. Statements from the response, dated 02/16/2017, are provided below and included in Attachment E.

Transportation Conformity

"This project is located in Davidson County which is in attainment for all regulated criteria pollutants. Therefore, conformity does not apply to this project."

Mobile Source Air Toxics (MSATs)

"This project qualifies as a categorical exclusion under 23 CFR 771.117 and, therefore does not require an evaluation of MSATs per FHWA's 'Interim Guidance Update on Air Toxic Analysis in NEPA Documents' dated October 2016."

9. Noise

The proposed project has been coordinated with the TDOT Air Quality and Noise section. Statements from the response, dated 02/16/2017, are provided below and included in Attachment E.

"This project is Type III in accordance with the FHWA noise regulations in 23 CFR 772 and TDOT's noise policy; therefore, a noise study is not needed."

10. Section 4(f)

The proposed project does not include the use of lands protected by Section 4(f).

11. Section 6(f)

No Section 6(f) lands acquired or improved through the Land and Water Conservation Fund (LWCF) would be impacted from project implementation.

12. Cultural Resources (Architectural/Historical and Archaeological)

Separate Historical/Architectural and Archaeological Resources Reports were prepared for the proposed project. As proposed, no historical or archaeological resources that are listed or eligible for listing in the National Register of Historic Places (NRHP) would be adversely affected by this undertaking.

Record of coordination with the SHPO and TDOT Cultural Resources are included in Attachment F. The Historical/Architectural report and the Archaeological survey are included in Appendix 2.

Error! Reference source not found.

Donelson, Davidson County

Page 8

- ☐ Tennessee State Historic Preservation Office (SHPO)/TDOT Memorandum of Understanding applies. If marked, the checkbox below does not apply.
- ☒ The proposed project has been coordinated with the SHPO.

Historical/Architectural

The Historical/Architectural SHPO response dated 05/03/2017 stated "Considering the information provided, we concur that no historic properties eligible for listing in the National Register of Historic Places will be affected by this undertaking."

Archaeological

The Archaeological SHPO response dated 05/03/2017 stated "Considering the information provided, we find that no archaeological resources eligible for listing in the National Register of Historic Places will be affected by this undertaking."

Record of coordination with the SHPO and TDOT Cultural Resources are included in Attachment F. The Historical/Architectural report and the Archaeological survey are included in Appendix 2.

13. Native American Coordination

Native American consultation was initiated on 03/22/2017. A coordination letter was sent to the following tribes: The Absentee Shawnee Tribe of Oklahoma, The Cherokee Nation, Eastern Band of Cherokee Indians, Eastern Shawnee Tribe, Kialegee Tribal Town, Muscogee (Creek) Nation, Shawnee Tribe, and United Keetoowah Band of Cherokee Indians.

Two responses were received. The Muscogee (Creek) Nation "are unaware of any Muscogee cultural resources or sacred site" and the Cherokee Nation "does not foresee this project imparting impacts to Cherokee cultural resources at this time."

The United Keetoowah Band "recommend that a cultural resources inventory be completed prior to project implementation." After the historic and archaeology surveys were sent to them, they replied that they "agree with the report findings that the project will result in a finding of no effect to historic properties."

See Attachment G for record of Native American Coordination.

14. Hazardous Materials

The proposed project has been coordinated with the TDOT Hazardous Materials section. Statements from the response, dated 03/10/2017, are provided below and included in Attachment H. The Lebanon Pike Sidewalks Phase I Environmental Site Assessment (ESA) is included as Appendix 3.

"After reviewing the Lebanon Pike Sidewalks Phase I Environmental Site Assessment Report, it is unlikely that the three properties with "high indication" of potential impacts will actually impact the sidewalk project. The report is sufficient and meets all the requirements of a Phase I ESA. As the report recommends, a review of ROW sidewalk plans which include details on property acquisition, excavation depths, and any drainage features which will aid in determination of the scope for additional investigation."

Some remedial activities have already taken place at these properties and two have been redeveloped as Brownfields projects, which indicates any remaining contamination was within health risk exposure limits. If excavation depths are only deep enough to install sidewalks the potential to encounter contamination is minimal. However, collecting soil samples ahead of the work, to assess health risk and disposal/handling options is never a bad decision.

In the event hazardous substances/wastes are encountered within the right-of-way, their disposition shall be subject to all applicable regulations, including the applicable sections of the Federal Resource Conservation and Recovery Act, as amended; and the Comprehensive Environmental Response, Compensation, and Liability Act, as amended; and the Tennessee Hazardous Waste Management Act of 1983, as amended."

15. Environmental Justice

The proposed project does not have the potential to cause disproportionately high or adverse effects on low-income or minority populations.

16. Other

No other issues are involved.

Commitments

- ☐ Commitments are involved on the project and the list is found on the Environmental Commitments Green Sheet.
- ☒ Commitments are not involved on the project.

Error! Reference source not found.
Donelson, Davidson County
Page 10

Attachments

NOTICE TO PROCEED WITH THE FUNDED RIGHT-OF-WAY PHASE OF PROJECT DEVELOPMENT

PIN: 121729.00
FEDERAL PROJECT NUMBER: STP-M-24(60)
STATE PROJECT NUMBER: 19LPLM-F2-129
TERMINI / PROJECT NAME: SR-24 (US-70, Lebanon Pike), From
McGavock Pike to Old Lebanon Pike in
Nashville
CONTRACTEE: Nashville
REGION: 3

Effective Date: 08/04/2020

Date of Transmittal: 08/06/2020

This letter serves as the official Notice to Proceed for the Regional Right-of-Way office to coordinate with the Local Government to complete any necessary right-of-way (ROW) acquisition activities, and to certify that any ROW has been acquired in accordance with the federal Uniform Act. The Local Government shall perform this phase of project development in accordance with the Local Government Guidelines provided on the Local Program Development Office website at: https://www.tn.gov/content/dam/tn/tdot/programdevelopment/localprograms/documents-and-forms/LGG_Manual.pdf. Work may not be performed for any later phase of project development until an additional Notice to Proceed is issued. **Any work performed ahead of a Notice to Proceed's Effective Date will not be reimbursed.**

The Local Government may procure, in accordance with law, any necessary consulting firm(s) to perform all stages of right-of-way acquisition at this time. Please refer to Section 1.6 of the Local Government Guidelines for guidance on consultant selection, if needed. **All consultant solicitation advertisements shall be reviewed and approved by the Local Programs Development Office before publishing.** *Please provide a copy of the executed contract with the consultant and a copy of the notice sent to the consultant indicating the date work was authorized on the phase of the project mentioned above.* These documents should be emailed to Local.Programs@tn.gov. **Note: If the person in responsible charge for this phase of the project has changed since Form 3-1 (Project Initiation Form) was submitted, the Local Government shall resubmit Form 1-1 (Responsible Charge Form) at this time to the Local Programs Development Office at the email below.**

If you have questions or concerns regarding this matter please direct them to Simchah Edwards at 615-741-5314 in the Local Programs Development Office. You may contact us by email at: Local.Programs@tn.gov.

cc: Paul Degges
John Kahle
Jeff Hoge
Gary Palmer
David Goodman
Kimery Grant
Matt Burcham
Chasity Bell
David Layhew
Shane Hester
Becky White
Iraj Eghbali
Anna Emerson
File



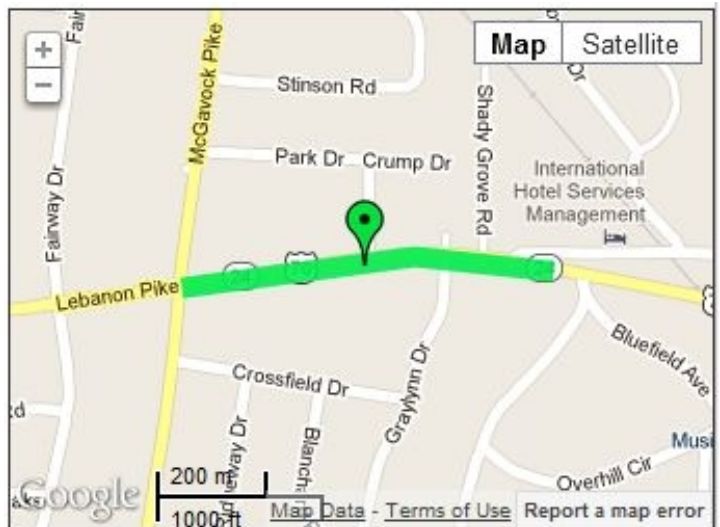
Transportation Improvement Program for FYs 2020-2023

Project Name	Lebanon Pike Sidewalk Improvements			TIP #	2014-16-002
Improvement Type	Sidewalks			Lead Agency	Metro Nashville
County	Davidson County	Length	0.50	Regional Plan ID	1416-103
Air Quality Status	Exempt	TDOT PIN	121729.00	Project Cost	\$3,800,000.00
Route	Lebanon Pike Sidewalk Improvements				
Location	From McGavock Pike to Old Lebanon Pike.				
Project Description	Construct a sidewalk, green zone, and pedestrian enhancements along the north side of Lebanon Pike.				

Fiscal Year	Type of Work	Funding Type	Total Funds	Federal Funds	State Funds	Local funds
2020	CONSTRUCTION	U-STBG	\$1,800,000.00	\$1,440,000.00	\$0.00	\$360,000.00
2020	ROW	U-STBG	\$1,300,000.00	\$1,040,000.00	\$0.00	\$260,000.00
TOTAL			\$3,100,000.00	\$2,480,000.00	\$0.00	\$620,000.00


REVISION HISTORY

PROJECT NOTES



APPLICATION FOR Multimodal Access Grant

METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY

DocuSigned by:

CCA6046554B9461

Diana W. Alarcon, Director
Nashville Department of Transportation
and Multimodal Infrastructure

7/15/2022

Date