

2nd & VAN BUREN

CASE NUMBER: 2022SP-013-001

PRELIMINARY SPECIFIC PLAN

SP NAME: 2nd & Van Buren

COUNCIL DISTRICT: District 19

COUNCIL REPRESENTATIVE: Freddie O'Connell

FEMA FIRM PANEL: 47037C241H, 04/15/2017

EXISTING ZONING: IR, Industrial Restrictive

OWNER: Toll Brothers Apartment Living
Yolanda Rodriguez, Chief Transactional Counsel, 215-938-8216

PLANNER: Barge Design Solutions
Kathryn Withers, AICP, 615-252-4213

CIVIL ENGINEER: Barge Design Solutions
Roy Hasle, PE, 615-252-4304

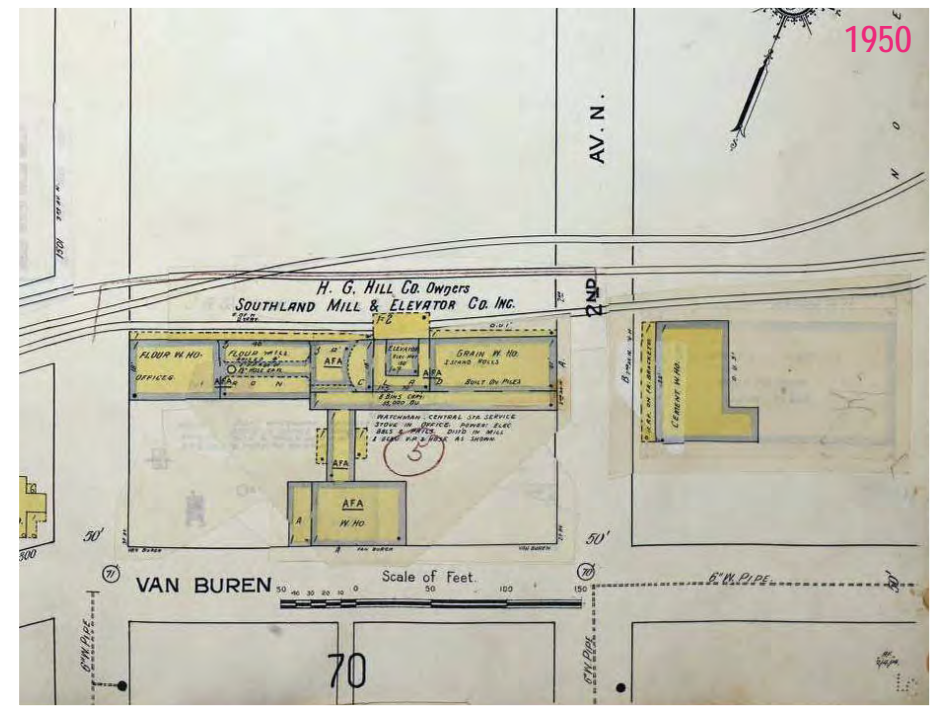
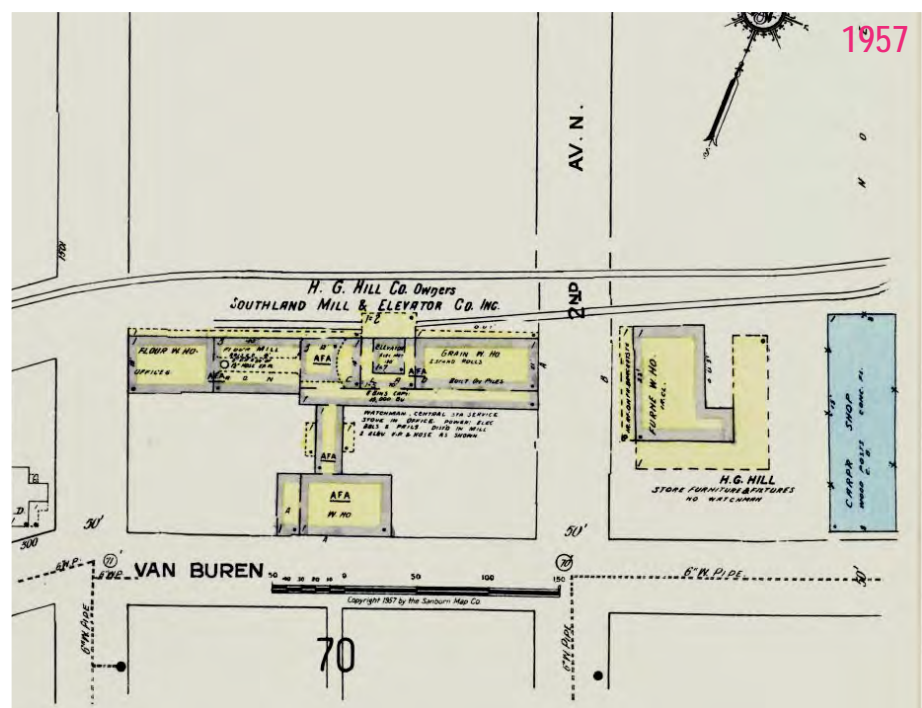
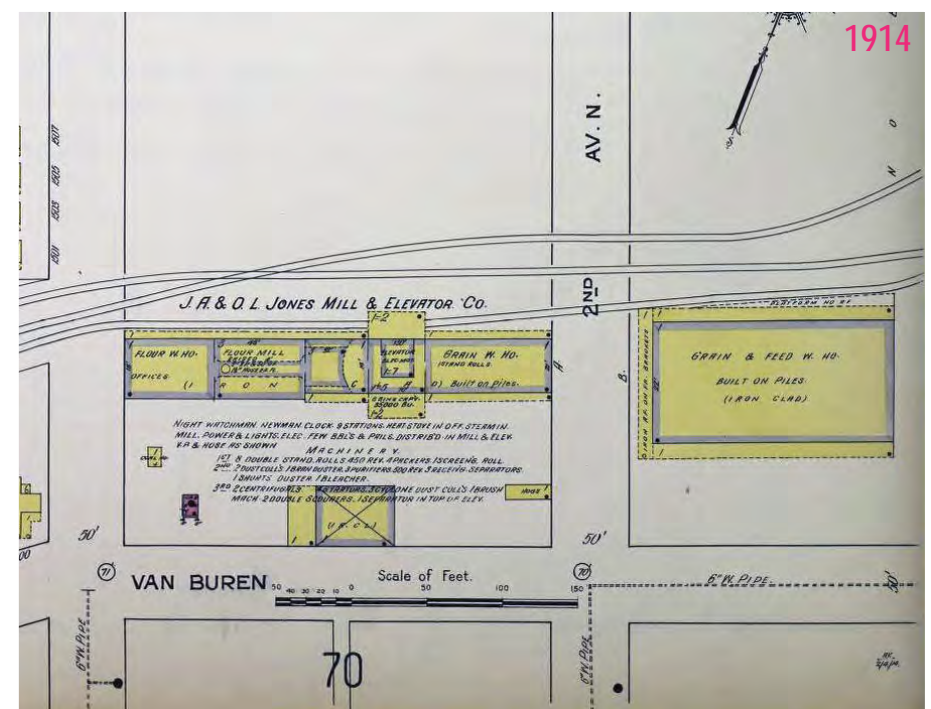
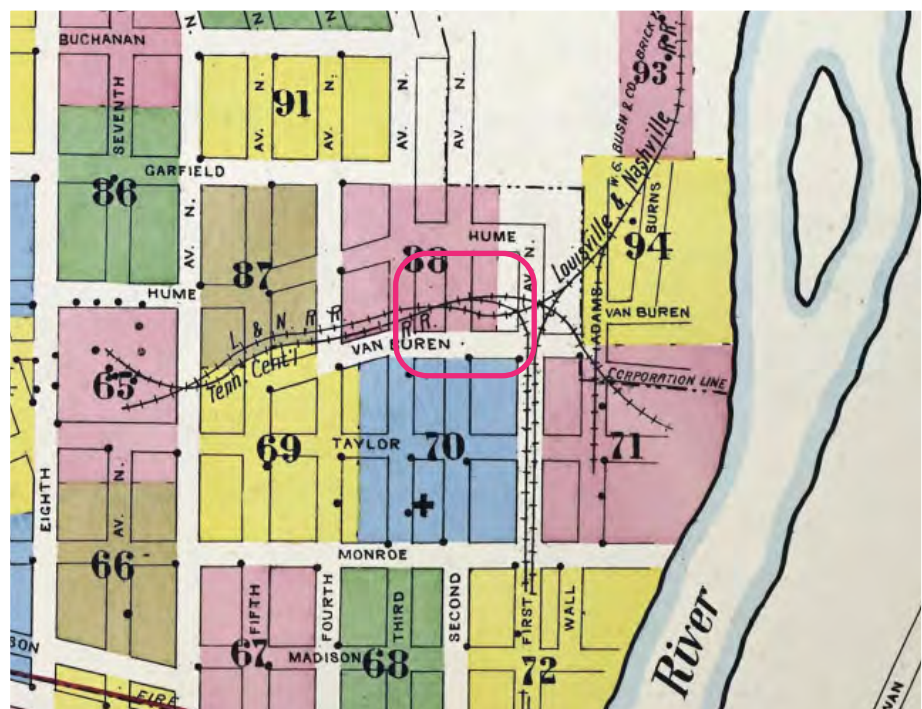
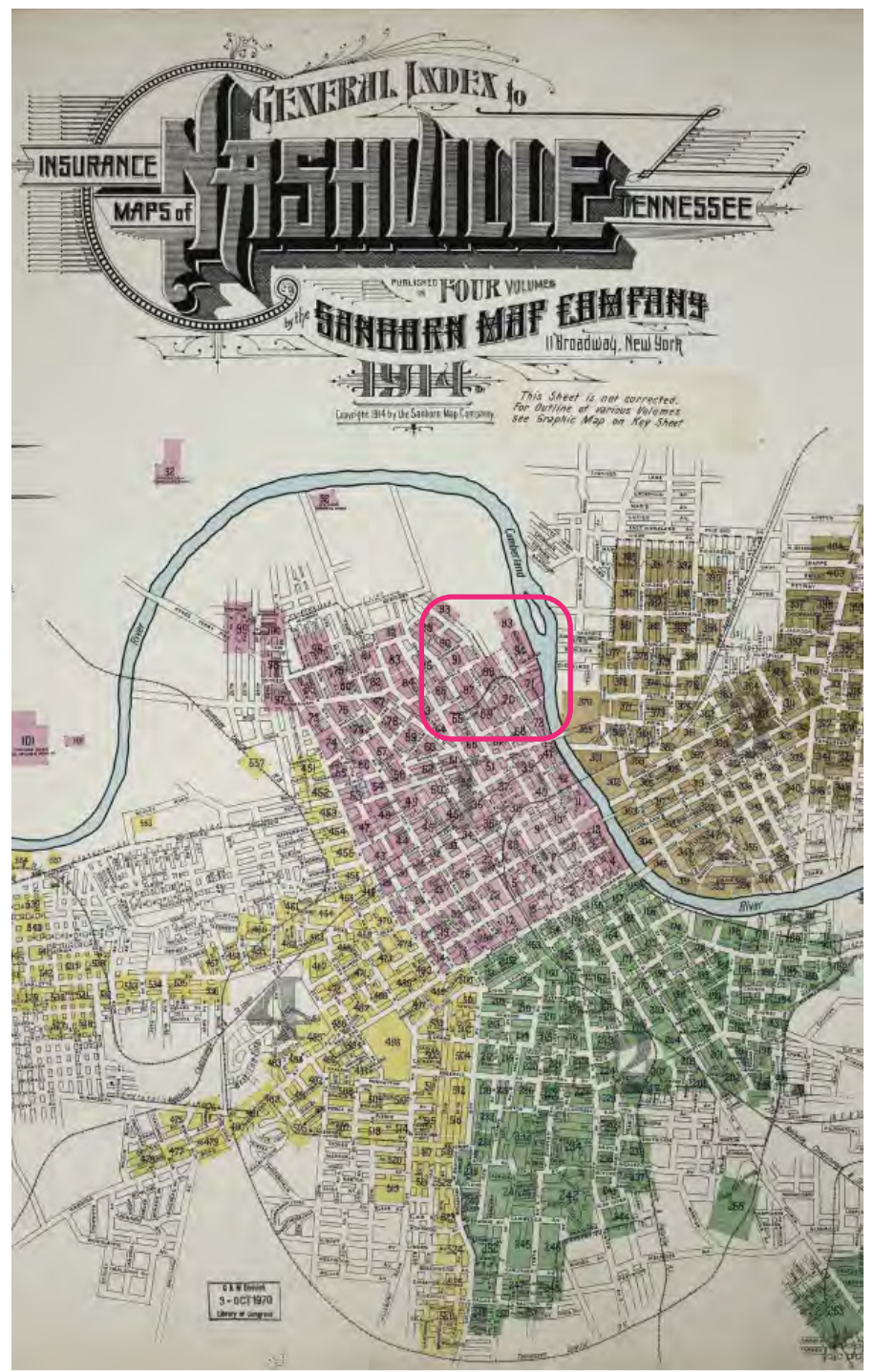
ARCHITECT: Brock Hudgins Architects
Ben Hudgins, AIA, 404-213-5271

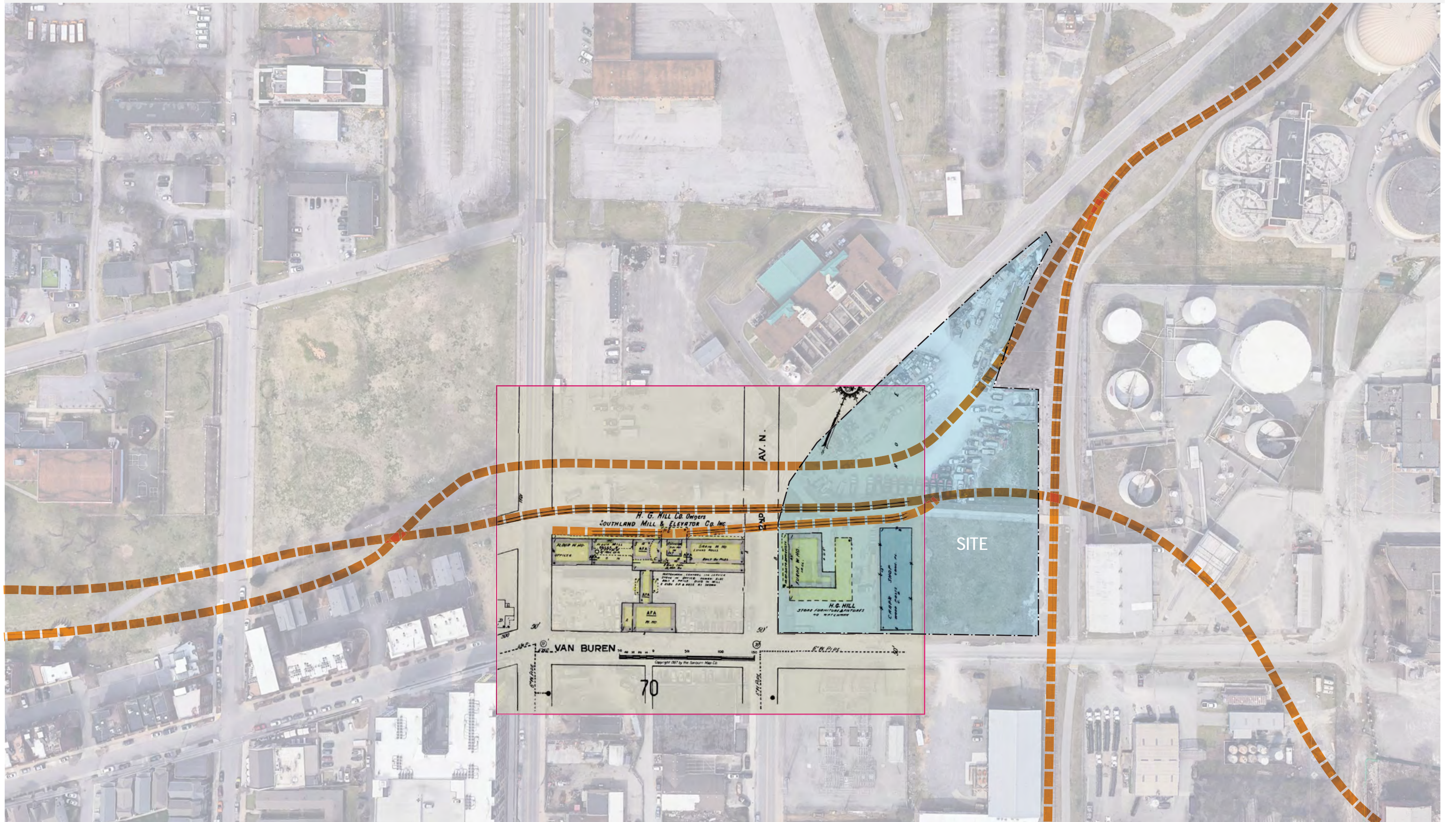
PLAN PREPARATION DATE: 01/26/2022; REVISED 05/09/2022

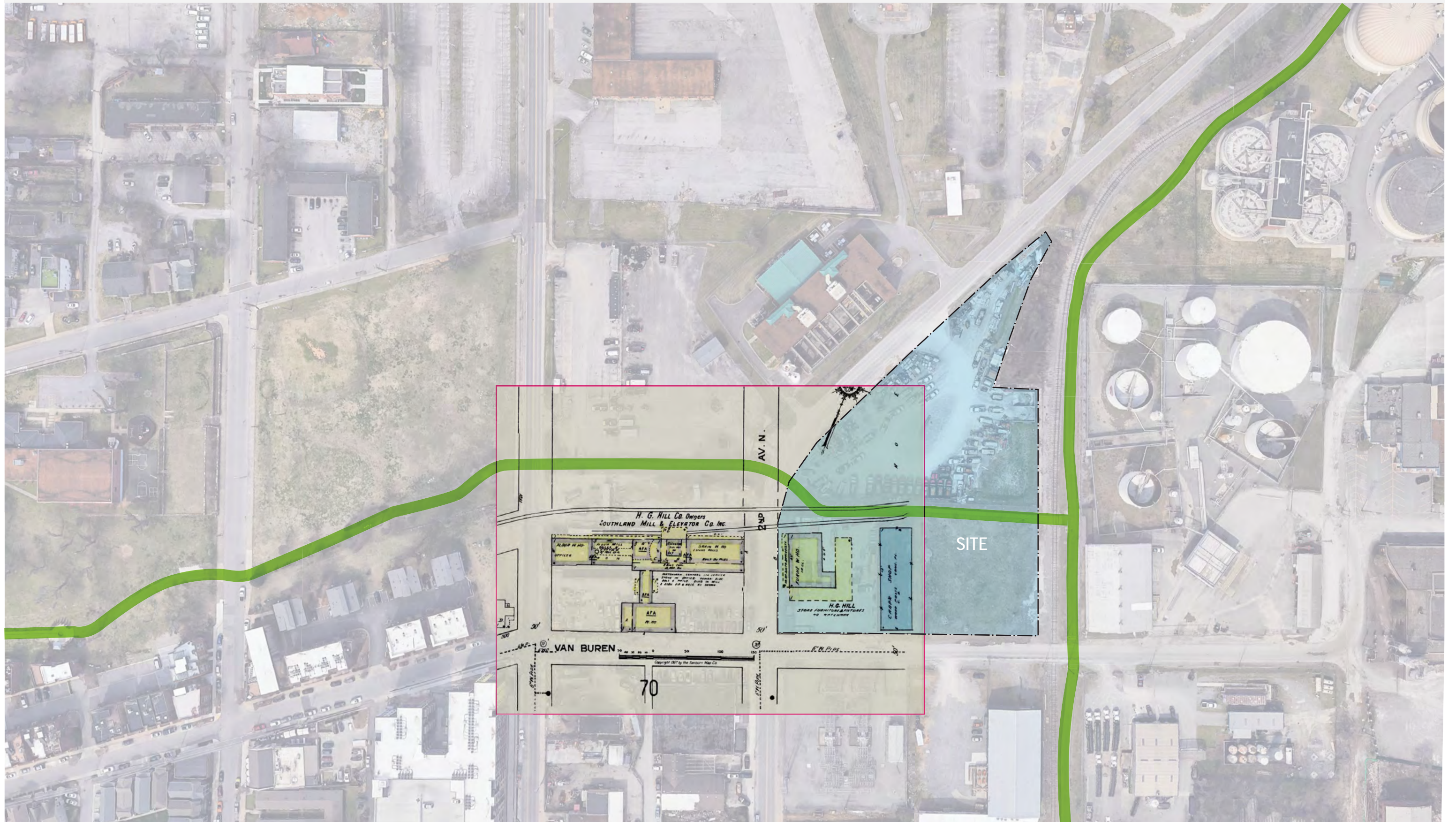
Toll Brothers
APARTMENT LIVING



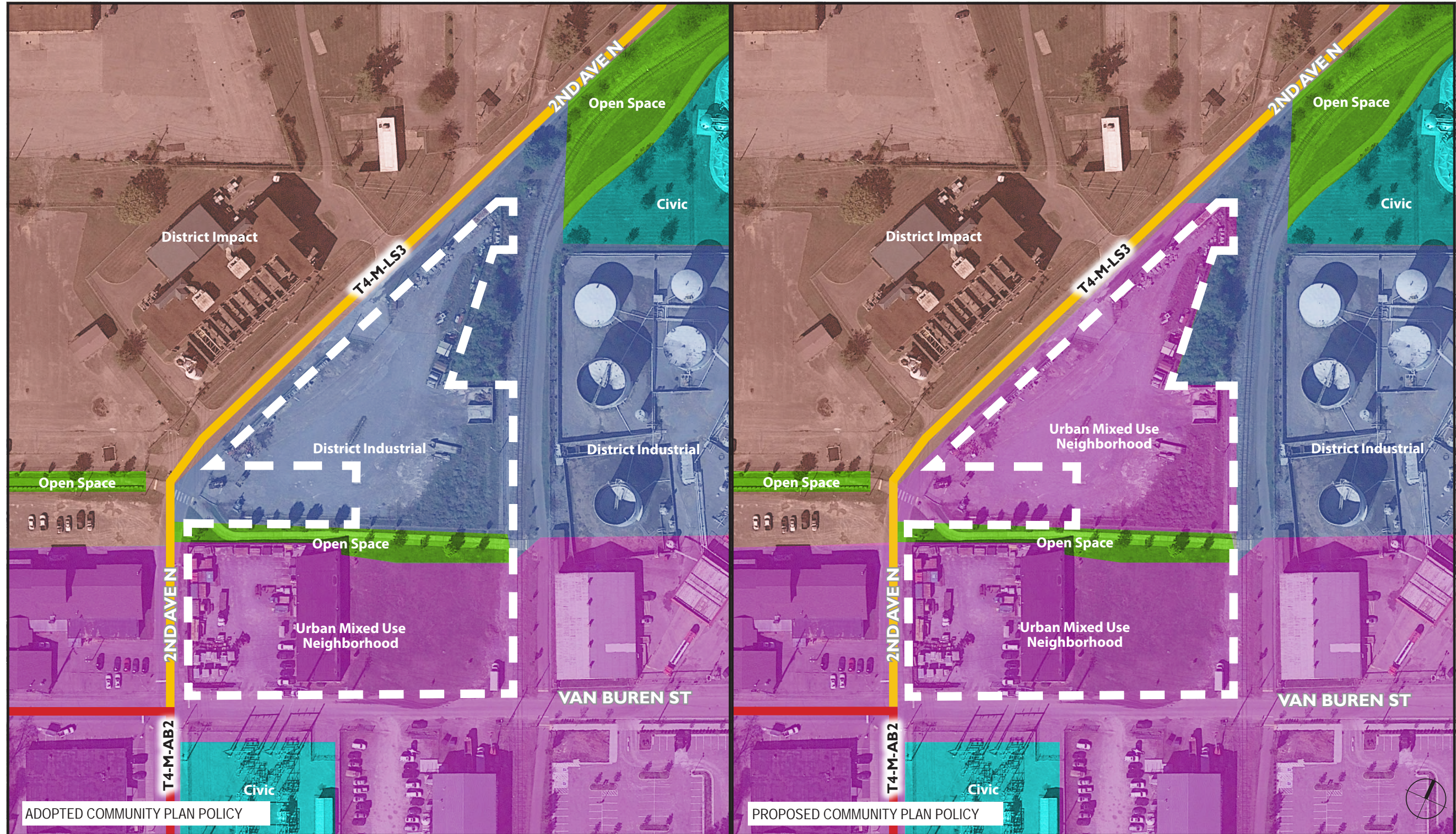
PARCEL HISTORY







Community Character Policy North Nashville Community Plan



Survey

NOTES

- THIS SURVEY MEETS THE REQUIREMENTS OF AN URBAN LAND SURVEY (CATEGORY 1), WITH A RATIO OF PRECISION OF THE UNADJUSTED SURVEY GREATER THAN 1:10,000 AS PER STANDARDS OF PRACTICE ADOPTED BY THE BOARD OF EXAMINERS FOR LAND SURVEYORS, STATE OF TENNESSEE.
- BEARINGS SHOWN ARE BASED ON THE TENNESSEE COORDINATE SYSTEM OF 1983 (NAD-1983), ELEVATIONS ARE BASED ON NAVD-1988, USING GEOID 2003 FOR ORTHOMETRIC HEIGHT DETERMINATION.
- PROPERTY IS ZONED IR (WITHIN OV-UZO OVERLAY DISTRICT.) ZONING INFORMATION SHOWN HEREON WAS TAKEN FROM METROPOLITAN PLANNING DEPARTMENT-ONLINE MAPPING: PARCEL REPORT, NO ZONING LETTER, OR ZONING REPORT WAS PROVIDED TO THIS SURVEYOR.
- BASED UPON A GRAPHIC SCALE A PORTION OF THIS PROPERTY IS LOCATED IN A FLOOD HAZARD AREA ACCORDING TO FEDERAL EMERGENCY MANAGEMENT AGENCY FLOOD INSURANCE RATE MAP COMMUNITY NO. 470040, PANEL NO. 0241 H, DATED APRIL 5, 2017. SHADED ZONE "X" - AREAS OF 0.2% ANNUAL CHANCE FLOOD; AREAS OF 1% ANNUAL CHANCE FLOOD WITH AVERAGE DEPTHS OF LESS THAN 1 FOOT OR WITH DRAINAGE AREAS LESS THAN 1 SQUARE MILE; AND AREAS PROTECTED BY LEVEES FROM 1% ANNUAL CHANCE FLOOD. NO ELEVATIONS OR CONTOURS WERE OBTAINED AS A PART OF THIS SURVEY.
- ABOVE GRADE AND UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM FIELD LOCATED VISIBLE APPURTENANCES, ATLAS MAPS AS MAY BE AVAILABLE FROM MUNICIPALITIES OR UTILITY COMPANIES, AND OTHER DRAWINGS. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES STATE THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION CONSULTED. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. AVAILABILITY AND COST OF SERVICE SHOULD BE CONFIRMED WITH THE APPROPRIATE UTILITY COMPANY. TO AVOID ANY POSSIBLE HAZARD OR CONFLICT, IT IS A REQUIREMENT OF THE UNDERGROUND UTILITY DAMAGE PREVENTION ACT, FOR ANYONE WHO ENGAGES IN EXCAVATION TO NOTIFY ALL KNOWN UTILITY OWNERS NO LESS THAN THREE (3) OR MORE THAN TEN (10) WORKING DAYS PRIOR TO THE DATE OF THEIR INTENT TO EXCAVATE. CALL TENNESSEE ONE CALL AT 1-800-351-1111.
- TITLE COMMITMENT NO'S: 01288-46936, DATED 1/29/2021 AT 8:00 A.M., AND 01288-46936A, DATED 2/19/2021 AT 8:00 A.M., PREPARED BY STEWART TITLE GUARANTY COMPANY WAS FURNISHED TO THIS SURVEYOR. TITLE COMMITMENT NO. TN-NAS-210198-COM, DATED 2/25/2021 AT 8:00 A.M., PREPARED BY CHICAGO TITLE INSURANCE COMPANY WAS FURNISHED TO THIS SURVEYOR.
- THIS PARCEL CONTAINS NO MARKED PARKING SPACES.
- THE BUILDING AREA SHOWN HEREON IS THE APPROXIMATE AREA OF THE BUILDING FOOTPRINT AS CALCULATED FROM FIELD LOCATED BUILDING CORNERS. NO ATTEMPT WAS MADE TO DETERMINE THE LEASABLE AREA.
- CONTOUR INTERVAL ONE-FOOT, CONTOURS WERE INTERPOLATED FROM RANDOM SHOTS TAKEN ON A 50-FOOT INTERVAL.

DEED REFERENCE

PARCELS 134, 135, 138, AND 139 BEING PROPERTY CONVEYED TO ROY GLENN GOODWIN BY DEED OF RECORD IN INSTRUMENT NO'S. 20140219-0014284, 20151230-0131107, AND 20190321-0025899 R.O.D.C.

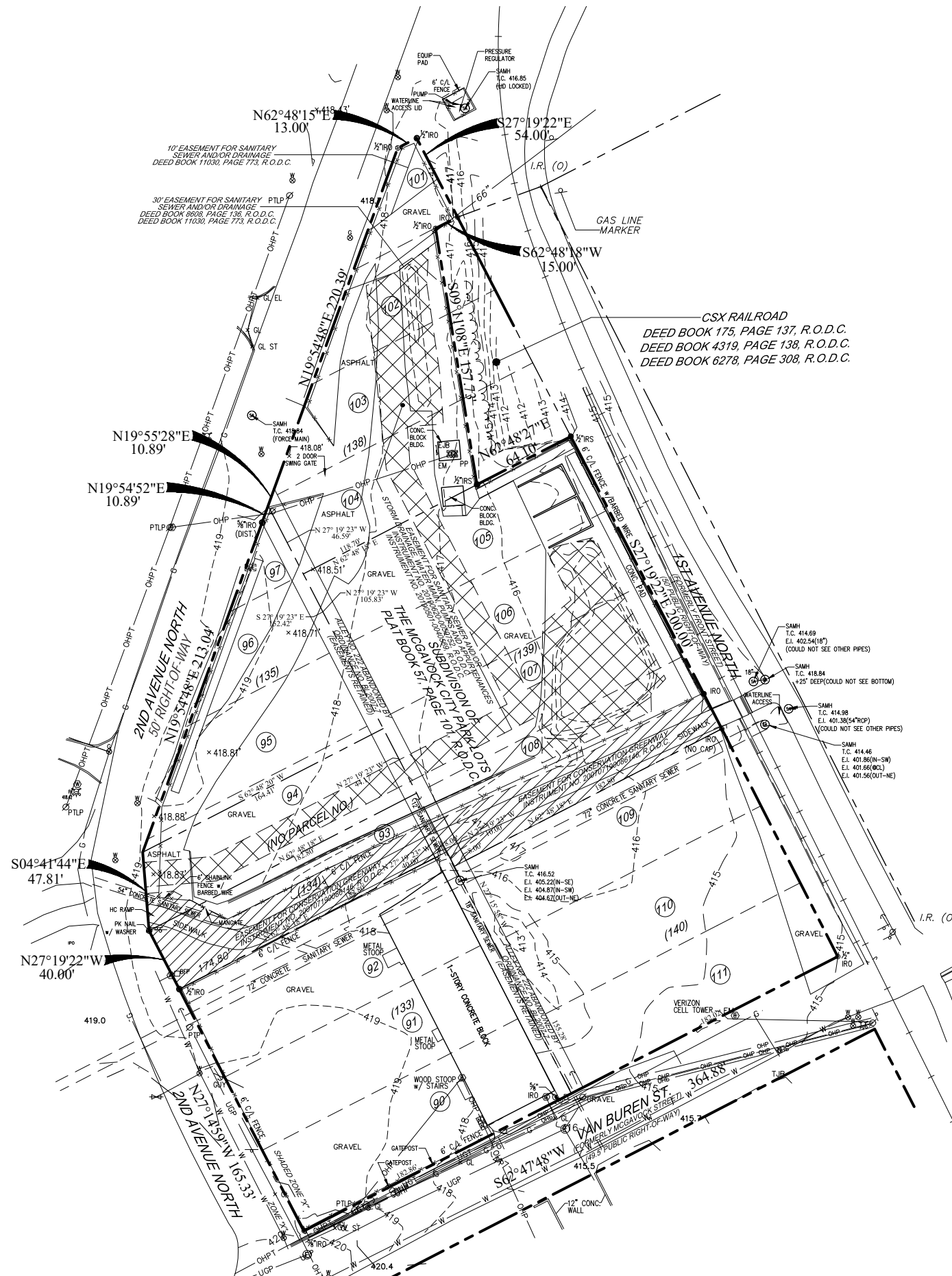
PARCELS 133, AND 140 BEING PROPERTY CONVEYED TO TOLL SOUTHEAST LP COMPANY, INC. BY DEED OF RECORD IN INSTRUMENT NO. 20200213-0016876, R.O.D.C.

PROPERTY MAP REFERENCE

BEING PARCELS 133, 134, 135, 138, 139, AND 140 AS SHOWN ON DAVIDSON COUNTY PROPERTY MAP NO. 82-05.

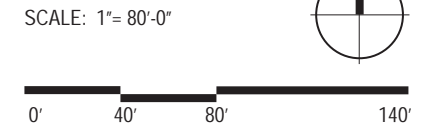
PLAT REFERENCE

BEING ALL OR PORTIONS OF LOT NOS. 90-97, AND 101-111 AS SHOWN ON THE SUBDIVISION OF THE MCGAVOCK CITY PARK LOTS OF RECORD IN PLAT BOOK 57, PAGE 101, R.O.D.C.

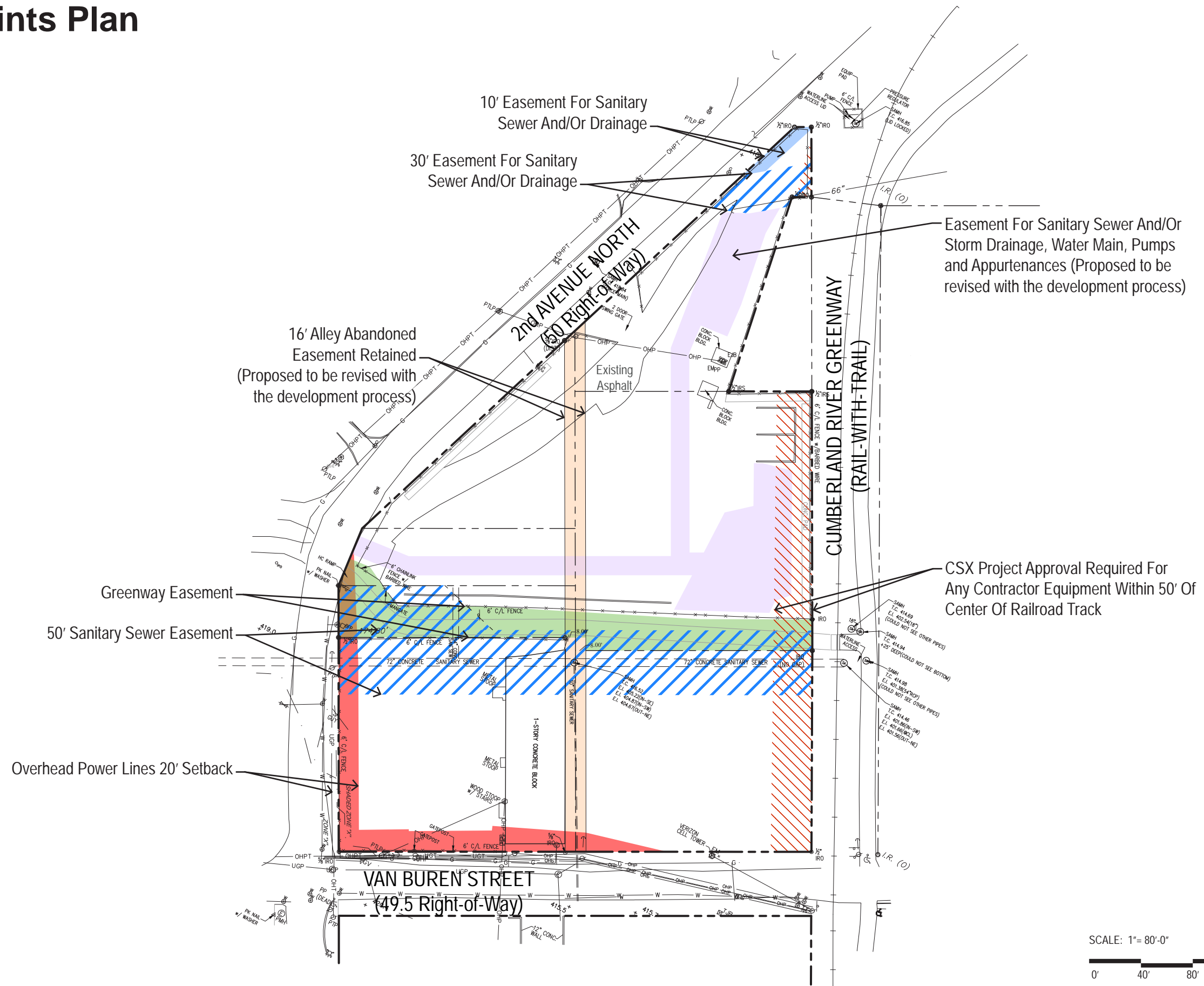


Parcel Summary:
 Parcel 133: 30,226 sq. ft.
 Parcel 134: 7,312 sq. ft.
 Lot 94 - Not included
 Parcel 135: 12,529 sq. ft.
 Parcel 138: 17,994 sq. ft.
 Parcel 139: 36,560 sq. ft.
 Parcel 140: 28,326 sq. ft.
 Total: 132,947 sq. ft (3.05 acres)

MAPPING SYMBOLS AND CODES		LEGEND	
	AIR CONDITIONER (AC)		POST INDICATOR VALVE (PIV)
	CLEANOUT		MANHOLE (MH)
	AREA DRAIN (AD)		CONCRETE MONUMENT OLD (MON(O))
	ELECTRIC JUNCTION BOX (EJB)		CONCRETE MONUMENT NEW (MON(N))
	ELECTRIC METER (EM)		P.K. NAIL OLD (PKO)
	ELECTRIC TRANSFORMER (ETF)		P.K. NAIL NEW (PKN)
	FINISHED FLOOR ELEVATION (F.F.E.)		POWER LIGHT POLE (PLP)
	FIRE HYDRANT (FH)		POWER MANHOLE (PMH)
	FLAG POLE (FP)		POWER POLE (PP)
	GAS METER (GM)		POWER TELEPHONE LIGHT POLE (PTLP)
	GUY POLE (GP)		POWER TELEPHONE POLE (PTP)
	GAS VALVE (GV)		RIGHT-OF-WAY MONUMENT (RWM)
	GUY WIRE (GW)		SPRINKLER HEAD/VALVE (SPH,SPV)
	IRON ROD FOUND (IRF)		SANITARY SEWER MANHOLE (SAMH)
	IRON ROD OLD (IRO)		SIGN (SN)
	IRON PIPE OLD (IPO)		STEAM MANHOLE (STMH)
	IRON SPIKE OLD (ISO)		STORMWATER MANHOLE (SWMH)
	LIGHT STANDARD METAL/WOOD (LS/LP)		TELEPHONE JUNCTION BOX (TJB)
	MAIL BOX (MB)		TELEPHONE LIGHT POLE (TLP)
	POST		TELEPHONE POLE (TP)
	WATER METER (WM)		TREE EVERGREEN
	WATER VALVE (WV)		



Existing Constraints Plan



2nd & VAN BUREN SPECIFIC PLAN

Development Standards

Fall Back Zoning	The standards of this SP shall follow RM100-A zoning district in the UZO except where the standards in this district are more specific.
Total Acreage (existing)	3.05
Total Acreage (after dedications)	2.85
Residential Units	303
MAX ISR¹	0.9
Maximum Building Height	7 stories, max of 100 feet
Build-to-zone²	0-15 feet
Side/Rear Setback	0 feet
Glazing³	15%
Foundations⁴	18 in. min. to 36 in. max.
Minimum Parking	Per UZO Parking Requirements
Allowable Uses	All uses in RM100-A
Prohibited Uses	Short-term Rental Property (STRP), Owner-Occupied and Short Term Rental Property (STRP) not Owner Occupied uses shall be prohibited
1 ISR calculation based on total acreage before dedications	
2 Build-to-Zone to be measured from the back of proposed sidewalks on public streets, private drives and garden courtyards. The setback along the greenway will be driven by the location of existing easements.	
3 Minimum glazing requirements shall be required on building facades facing public right-of-way or easements. Glazing calculations shall be measured from floor to floor.	
4 With the exception of non-residential portions of the building, accessible units, visible units, and topographically challenged units. Challenging site topography and other site constraints may result in higher or lower foundations at strategic locations.	

STANDARD NOTES

1. The purpose of this SP is to receive preliminary approval to permit the development of 303 units of multifamily development as shown.
2. For any development standard, regulations and requirements not specifically shown on the SP plan and/or included as a condition of Council approval, the property shall be subject to the standards, regulations, and requirements of MUG-A base zoning as of the date of the application request or application.
3. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its design based upon final architectural, engineering, or site design and actual site conditions. All modifications shall be consistent with the principles and further objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by the Metro Council that increase the permitted density and floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance.

FEMA NOTES

1. This parcel is located in a 0.2% annual chance flood hazard according to the Federal Emergency Management Agency Flood Insurance Rate Map Community Panel No. 47037C0241H April 5, 2017.

ARCHITECTURAL STANDARDS

1. Building facades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 15% glazing.
2. Windows shall be vertically oriented to a ratio of 1.5:1 or greater except for dormers
3. Building facades shall be constructed of brick, brick veneer, stone, cast stone, cementitious siding, glass, metal panel, or materials substantially similar in form and function, unless otherwise approved on detailed building elevations included with the preliminary SP.
4. Porches shall provide a minimum of six feet of depth.
5. A raised foundation of 18"- 36" is required for all residential structures.

NASHVILLE DEPARTMENT OF TRANSPORTATION (NDOT) NOTES

1. The final site plans and building permits shall depict the required public sidewalks, any required grass strip or frontage zone, and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Where feasible, vertical obstructions are only permitted within the required grass strip or frontage zone.
2. Roadway improvements that are direct result of this specific project as determined by the approved Traffic Improvement Study and NDOT shall be constructed.
3. Any new improvements within existing public right-of-way within the project site that are identified as necessary to meet the adopted roadway plans shall be dedicated.
4. The final construction drawings shall comply with the design regulations established by the NDOT. In effect at the time of the approval of the preliminary development plan or final development plan or building permit, as applicable. Final designs for Phase 1 and Phase 2 may vary based on field conditions.
5. The design of the public infrastructure is to be coordinated with the Final SPs. The roads and pedestrian infrastructure are to be designed and constructed per NDOT standards.
6. All construction within the right of way shall comply with ADA and NDOT Standards and Specifications.
7. Trash and recycling service is to be contracted between the developer/owner and a private hauler.
8. Bicycle parking shall be provided in accordance with section 17.20.135 of the Metro Zoning Code and will be identified in the Final SPs.

LANDSCAPE STANDARDS

1. The developer of this project shall comply with the requirements of the approved preliminary SP and Metro's adopted tree ordinance.
2. Street trees shall be provided, irrigated, and maintained by Owner along all street frontages at a minimum spacing average of 50 linear feet. All street trees placed within right of way shall count toward tree density unit credit outlined in Metro Zoning Code 17.24.
3. No landscape buffer requirement of 17.24 shall apply to this site.
4. Tree species from the Historic Germantown Neighborhood Association Arboretum List will be considered for inclusion in the planting plan.

FIRE MARSHAL NOTES

1. New commercial developments shall be protected by a fire hydrant that complies with the 2006 edition of NFPA 1 Table H.
2. No part of any building shall be more than 500 feet from a fire hydrant via a hard surface road. Metro Ordinance 095-1541 Sec. 1568.020 B.
3. All fire department access roads that shall be 20 feet minimum width and shall have an unobstructed vertical clearance of 13.5 feet.
4. If more than three stories above grade, Class I standpipe system shall be installed.
5. A fire hydrant shall be provided within 100 feet of the fire department connection.
6. Fire hydrants shall be in-service before any combustible material is brought on site.
7. Fire apparatus access roads shall be provided and maintained in accordance with the adopted fire code and standards.
8. Except as approved by the fire code official; fire apparatus access roads shall extend to within 200 feet of all portions of the facility and all portions of the exterior walls of the first story of the building as measured by an approved route around the exterior of the building or facility.
9. Buildings over 30 feet in height shall meet fire department aerial apparatus access requirements.
10. Dead end fire apparatus access roads in excess of 200 feet shall be provided with an approved fire apparatus turnaround (Per discussions with Metro Fire).
11. All points of the building shall be within 500 feet of a fire hydrant when measured via approved fire apparatus access route.
12. All buildings and/or developments are required to meet the fire-flow requirements listed in the adopted code prior to construction.
13. The maximum grade for fire apparatus access roads shall not exceed 10% without approval from the fire code official.
14. Gates across fire apparatus access roads shall comply with adopted code and standards.
15. Approval of a preliminary or final site plan is not an approval for building construction. Full and complete review of building plans is required prior to approval for construction and may require changes to the site.

NES NOTES

1. Where feasible, this development will be served with underground power and pad-mounted transformers.
2. New facilities will not be allowed to sit in or to pass through retention areas, including rain gardens, bioretention areas, bioswales, and the like. This includes primary duct between pad-mounted transformer equipment as well as service duct to a meter.

STORMWATER NOTES

See Grading and Utility Plans

FEDERAL COMPLIANCE

1. All development within the boundaries of this plan will meet the requirements of the Americans with Disabilities Act and the Fair Housing Act

SOILS

Per the USDA Web Soil Survey, the existing soils on site are classified as Ln - LINDELL-URBAN LAND COMPLEX (HYDROLOGIC SOIL GROUP C), slopes from 0-4%.

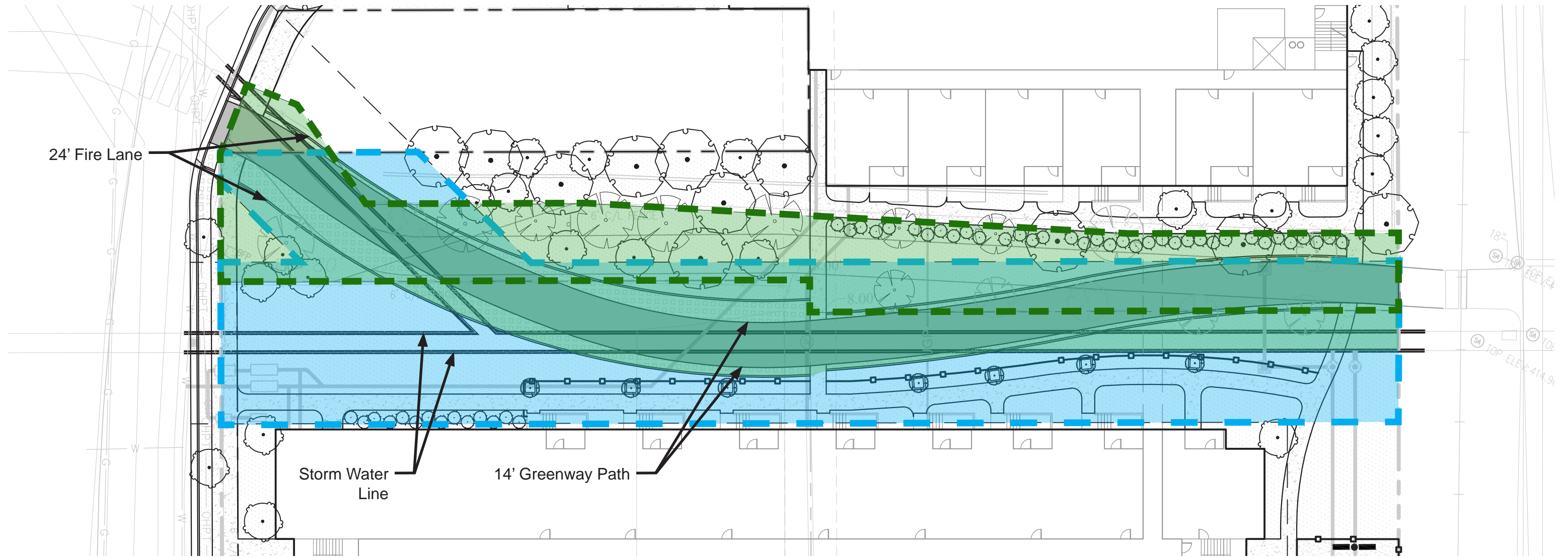
STREET LIGHTS

With the final SP and building permit plans, urban streetscape lighting fixtures will be used that are consistent with what is being used in Germantown (Holophane Washington LED).

Road Sections, Right-of-Way Dedications and Fire Access







Existing and Proposed Greenway and Storm Water Easement Comparison



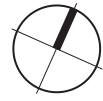
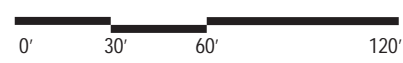
Greenway Character Imagery



LEGEND	
	25' Existing Storm Water Easement
	Existing Storm Water Easement Area: 20,666 sf
	Existing Greenway Easement Area: 10,005 sf
	Proposed Greenway Easement Area: 15,375 sf

Landscape Plan

SCALE: 1" = 60'-0"



Grass pavers for
for 26' wide fire
access with fire
lane signage

Canopy Trees, Typical

Right-of-way widths
(see cross sections)

Property Not
Included

Grass pavers
for 24' wide fire
access with fire
lane signage (not
to exceed 200')

Small flowering
trees under
overhead power
lines

VAN BUREN STREET

2nd AVENUE NORTH

CUMBERLAND RIVER GREENWAY
(RAIL-WITH-TRAIL)






38

4

4

50' Public Sewer
Easement
35' Public Sewer and
Drainage Easement
Dog Run
Foundation
Planting, Typical
Gas line prohibits
street trees along
Van Buren Street

LEGEND

-  Existing Tree
-  Proposed Canopy Tree
-  Proposed Small to Medium-sized Tree
-  Bioretention Pond
-  50 Bike Parking - Residential required/provided: 50 spaces



(A) Potted trees along Greenway



(B) Grasses along Greenway



(C) Walkways (pavers or concrete lined with pavers)



(D) Native plants

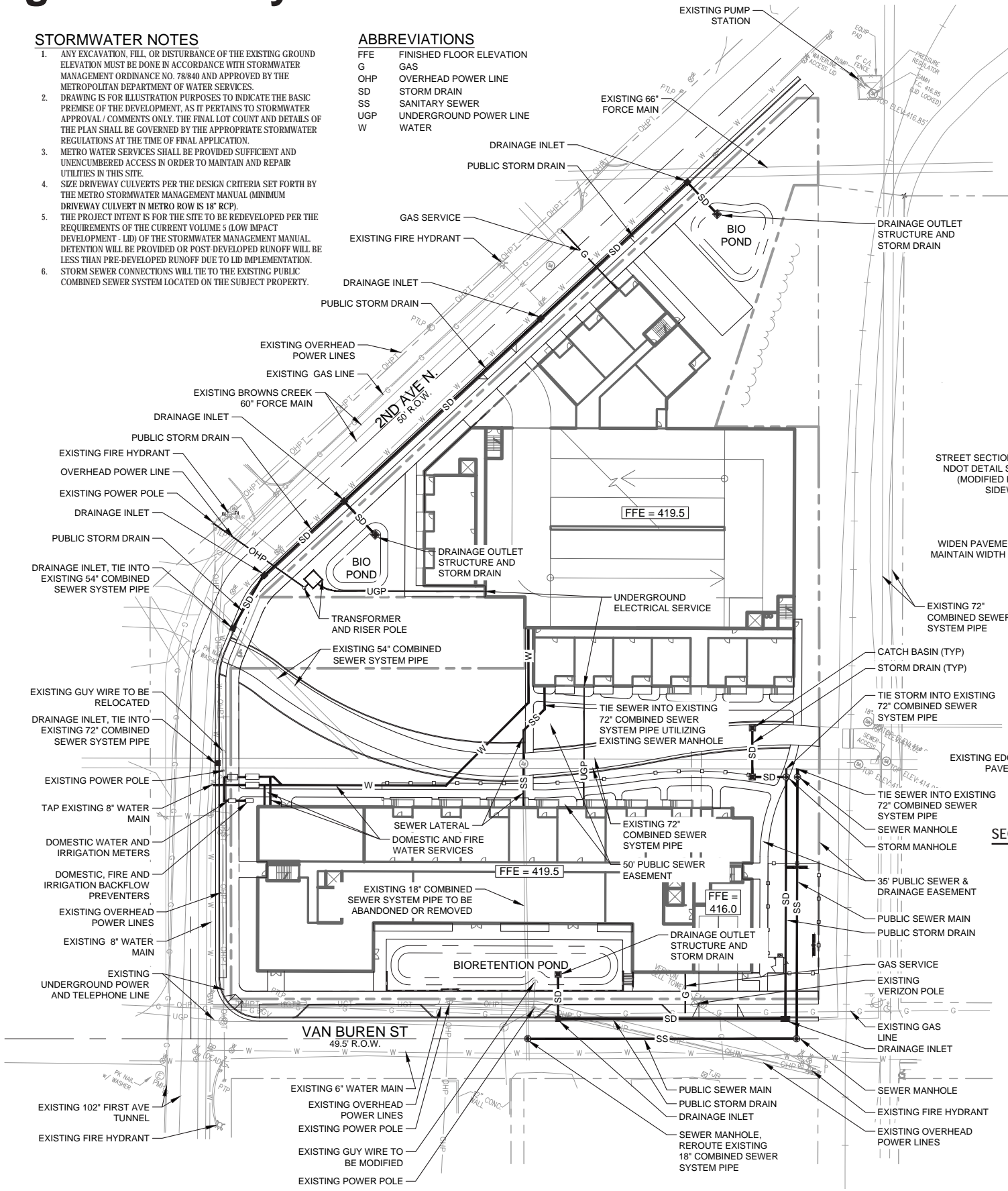
Drainage and Utility Plan

STORMWATER NOTES

1. ANY EXCAVATION, FILL, OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORMWATER MANAGEMENT ORDINANCE NO. 78840 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.
2. DRAWING IS FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT, AS IT PERTAINS TO STORMWATER APPROVAL / COMMENTS ONLY. THE FINAL LOT COUNT AND DETAILS OF THE PLAN SHALL BE GOVERNED BY THE APPROPRIATE STORMWATER REGULATIONS AT THE TIME OF FINAL APPLICATION.
3. METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED ACCESS IN ORDER TO MAINTAIN AND REPAIR UTILITIES IN THIS SITE.
4. SIZE DRIVEWAY CULVERTS PER THE DESIGN CRITERIA SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL (MINIMUM DRIVEWAY CULVERT IN METRO ROW IS 18" RCP).
5. THE PROJECT INTENT IS FOR THE SITE TO BE REDEVELOPED PER THE REQUIREMENTS OF THE CURRENT VOLUME 5 (LOW IMPACT DEVELOPMENT - LID) OF THE STORMWATER MANAGEMENT MANUAL. DETENTION WILL BE PROVIDED OR POST-DEVELOPED RUNOFF WILL BE LESS THAN PRE-DEVELOPED RUNOFF DUE TO LID IMPLEMENTATION.
6. STORM SEWER CONNECTIONS WILL TIE TO THE EXISTING PUBLIC COMBINED SEWER SYSTEM LOCATED ON THE SUBJECT PROPERTY.

ABBREVIATIONS

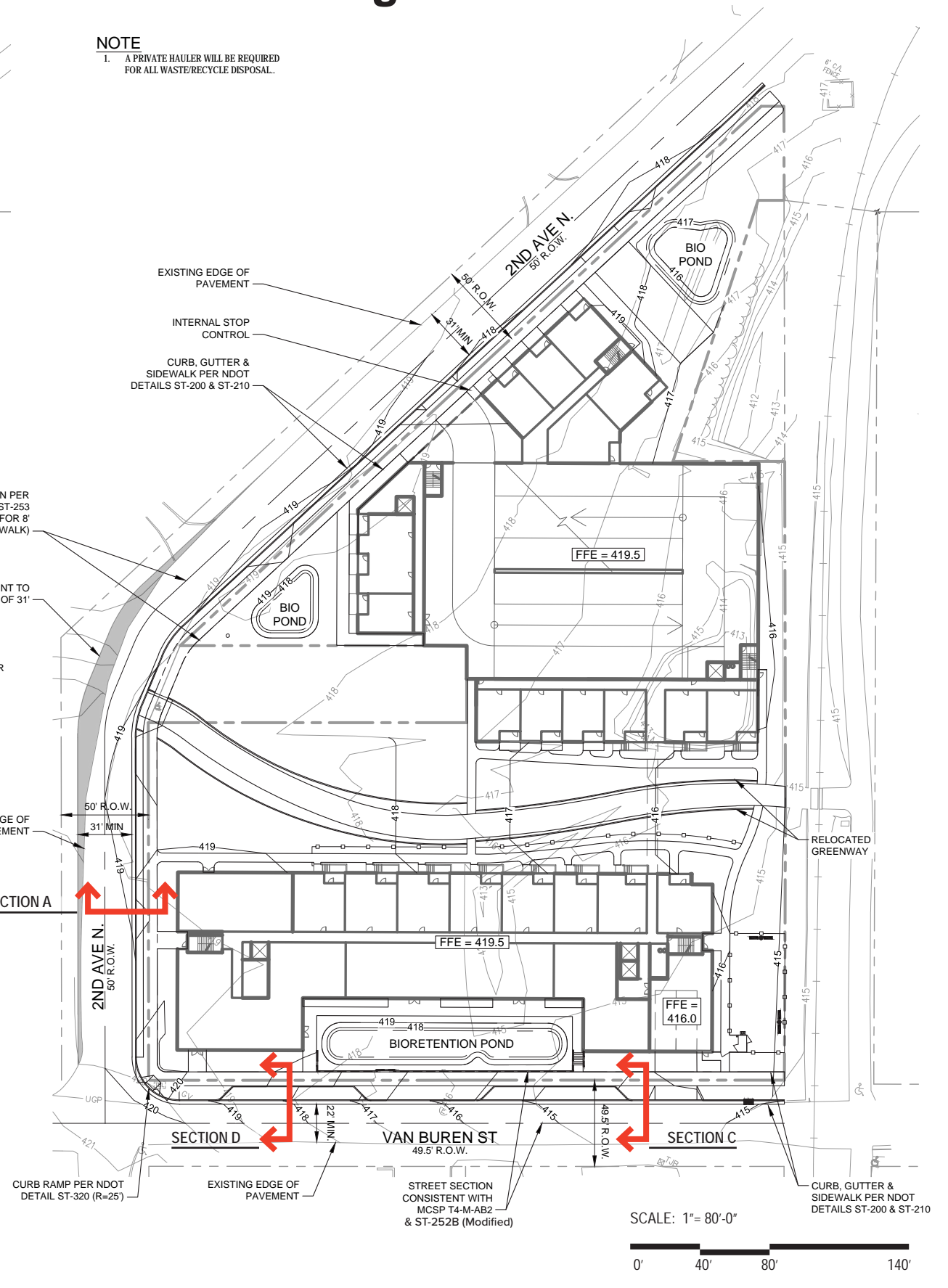
- | | |
|-----|--------------------------|
| FFE | FINISHED FLOOR ELEVATION |
| G | GAS |
| OHP | OVERHEAD POWER LINE |
| SD | STORM DRAIN |
| SS | SANITARY SEWER |
| UGP | UNDERGROUND POWER LINE |
| W | WATER |



Site and Grading Plan

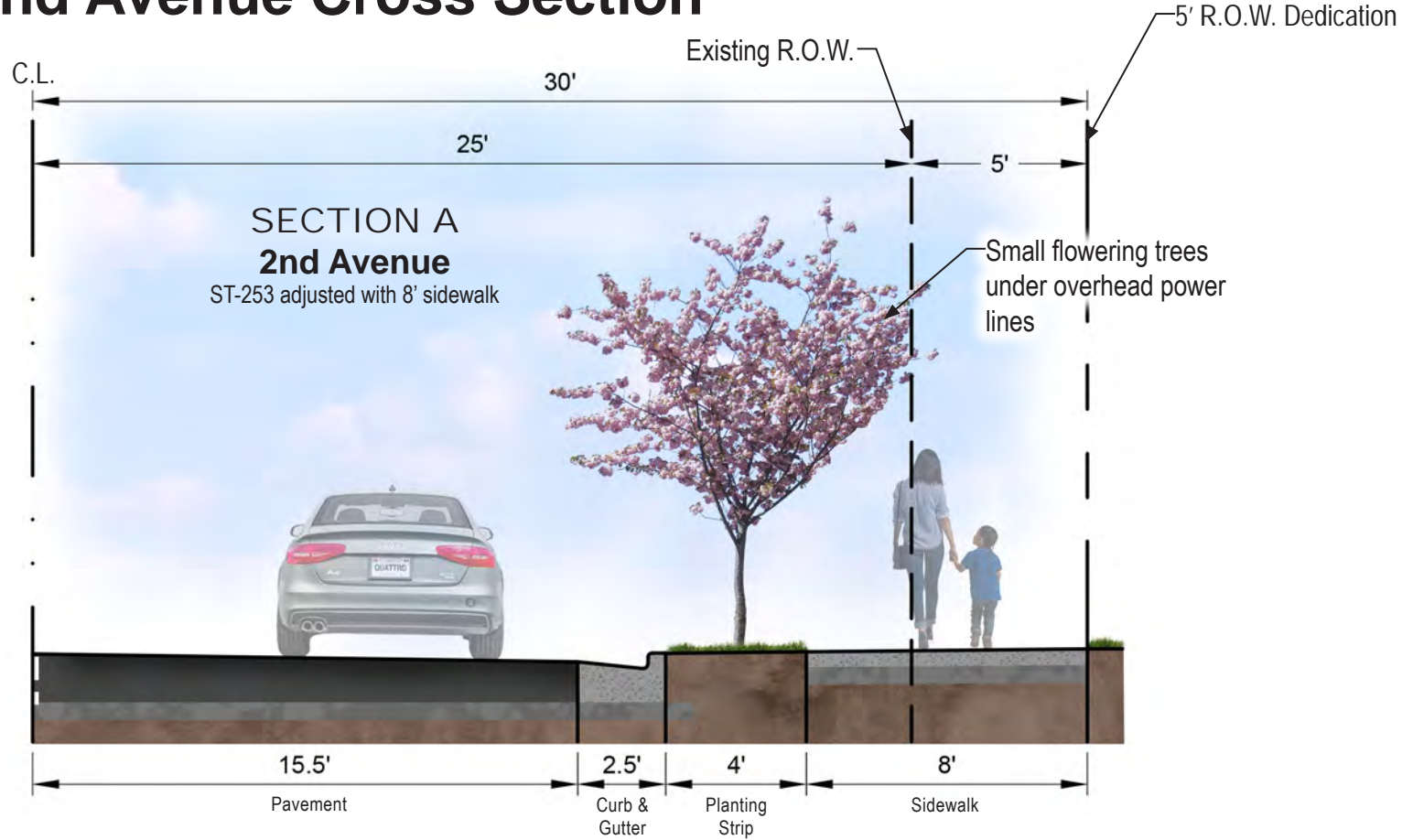
NOTE

1. A PRIVATE HAULER WILL BE REQUIRED FOR ALL WASTE/RECYCLE DISPOSAL.

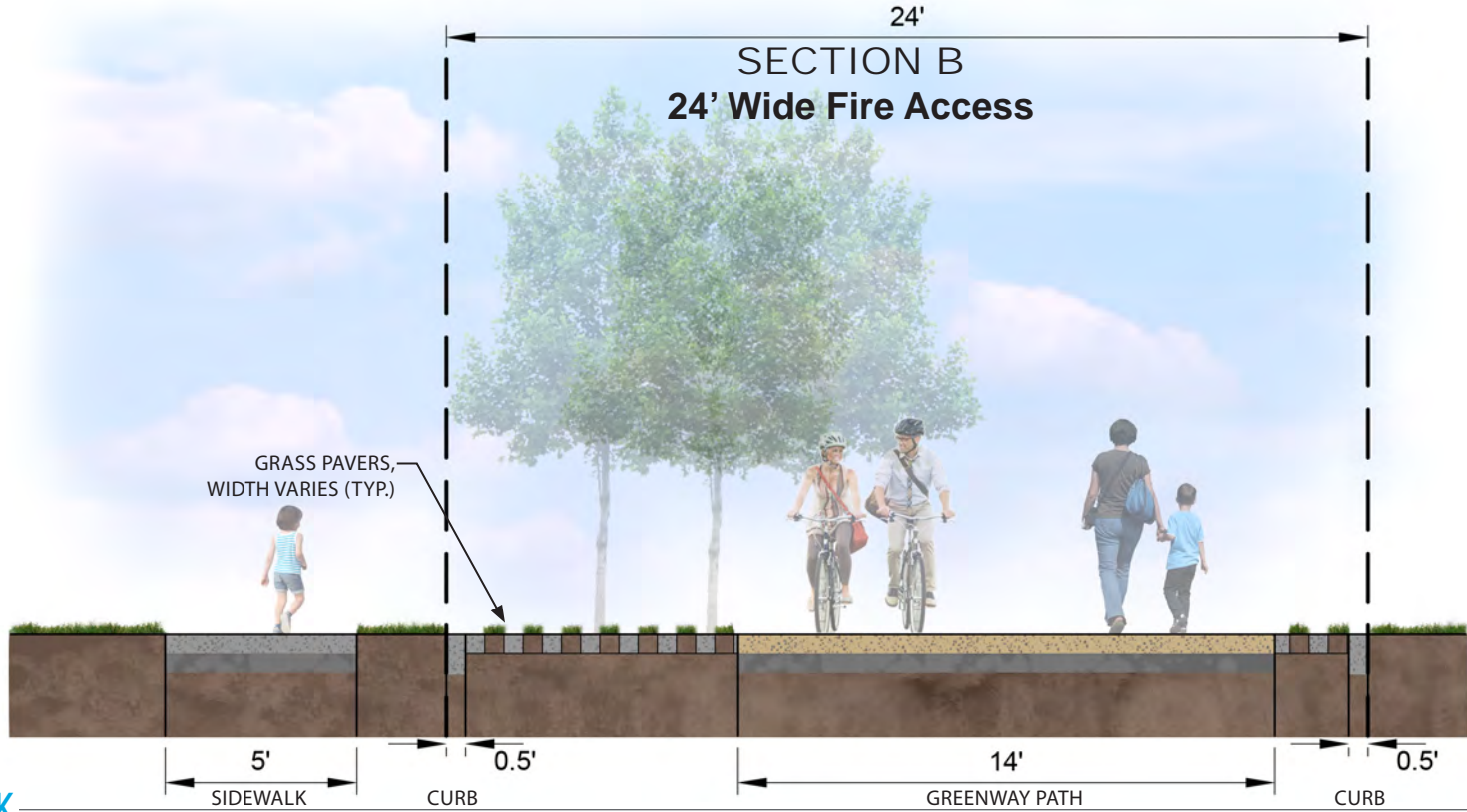


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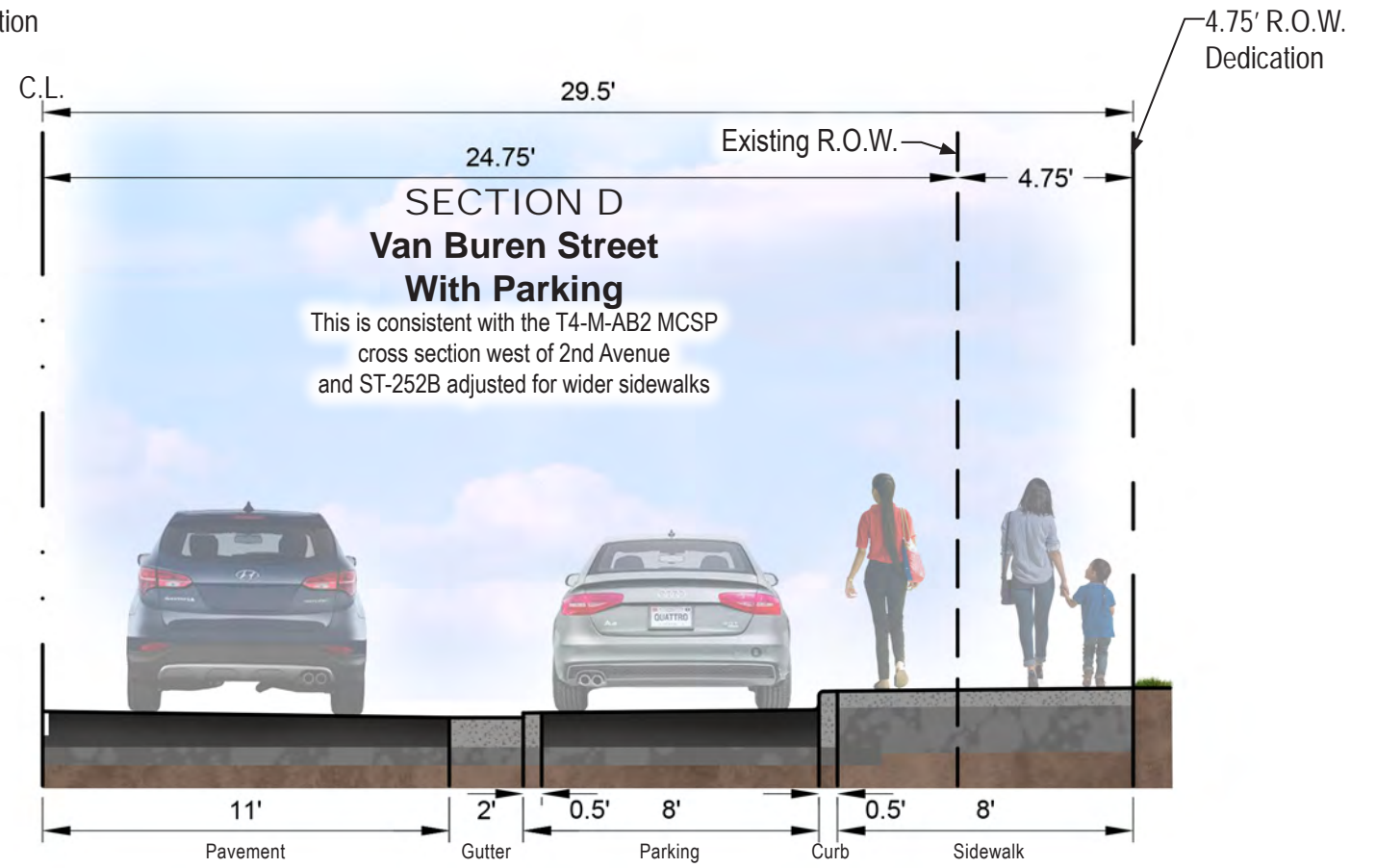
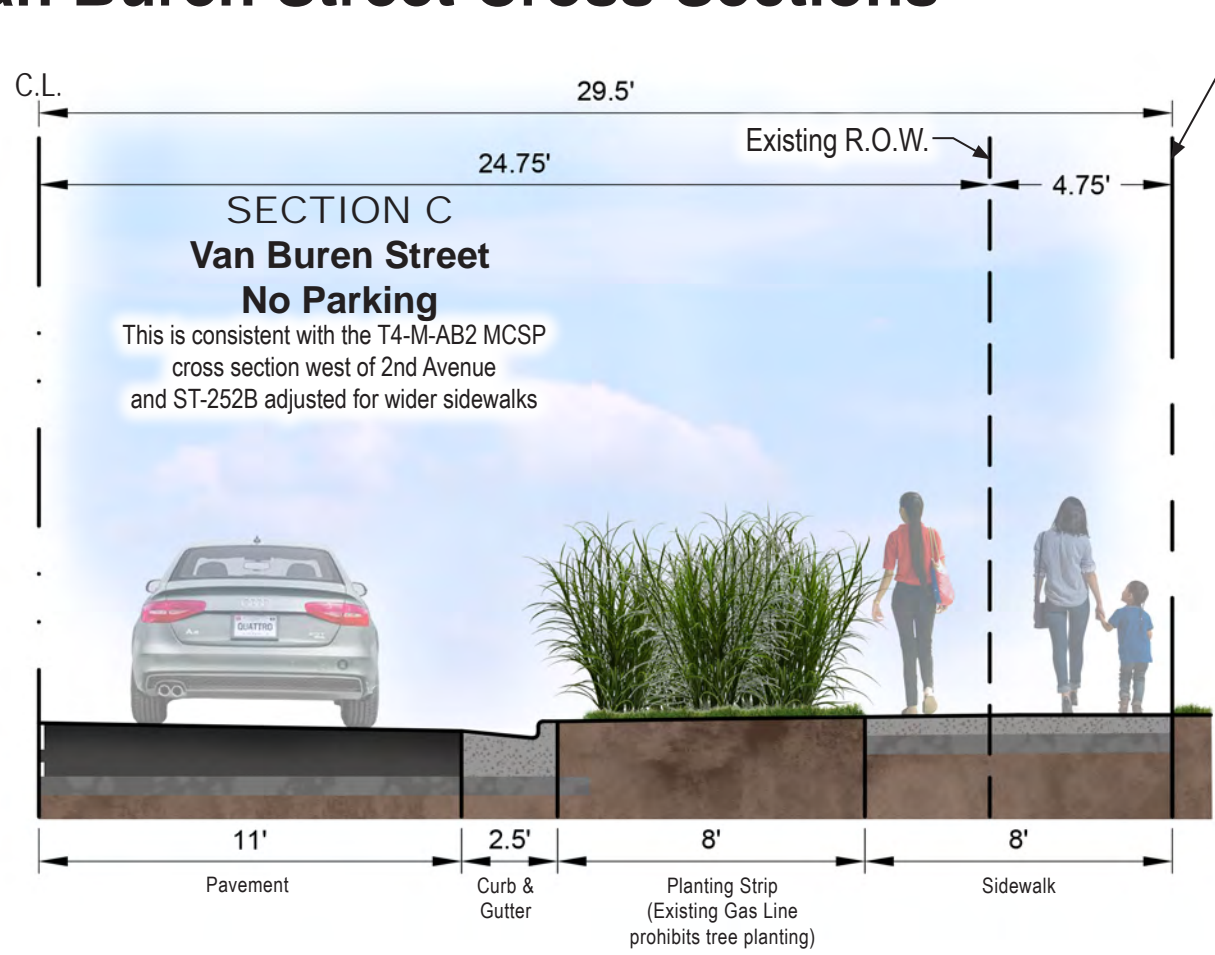
2nd Avenue Cross Section



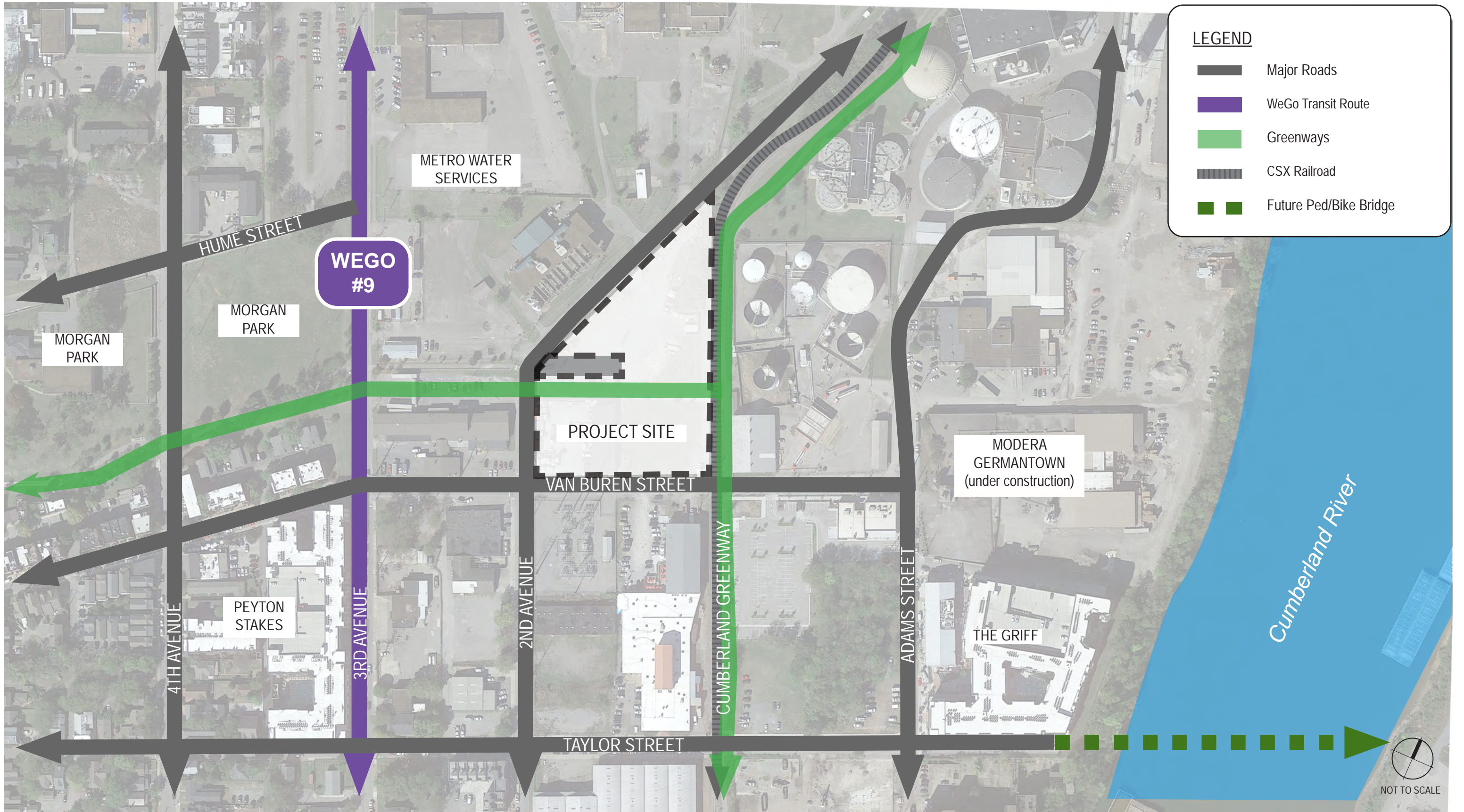
Greenway Cross Section



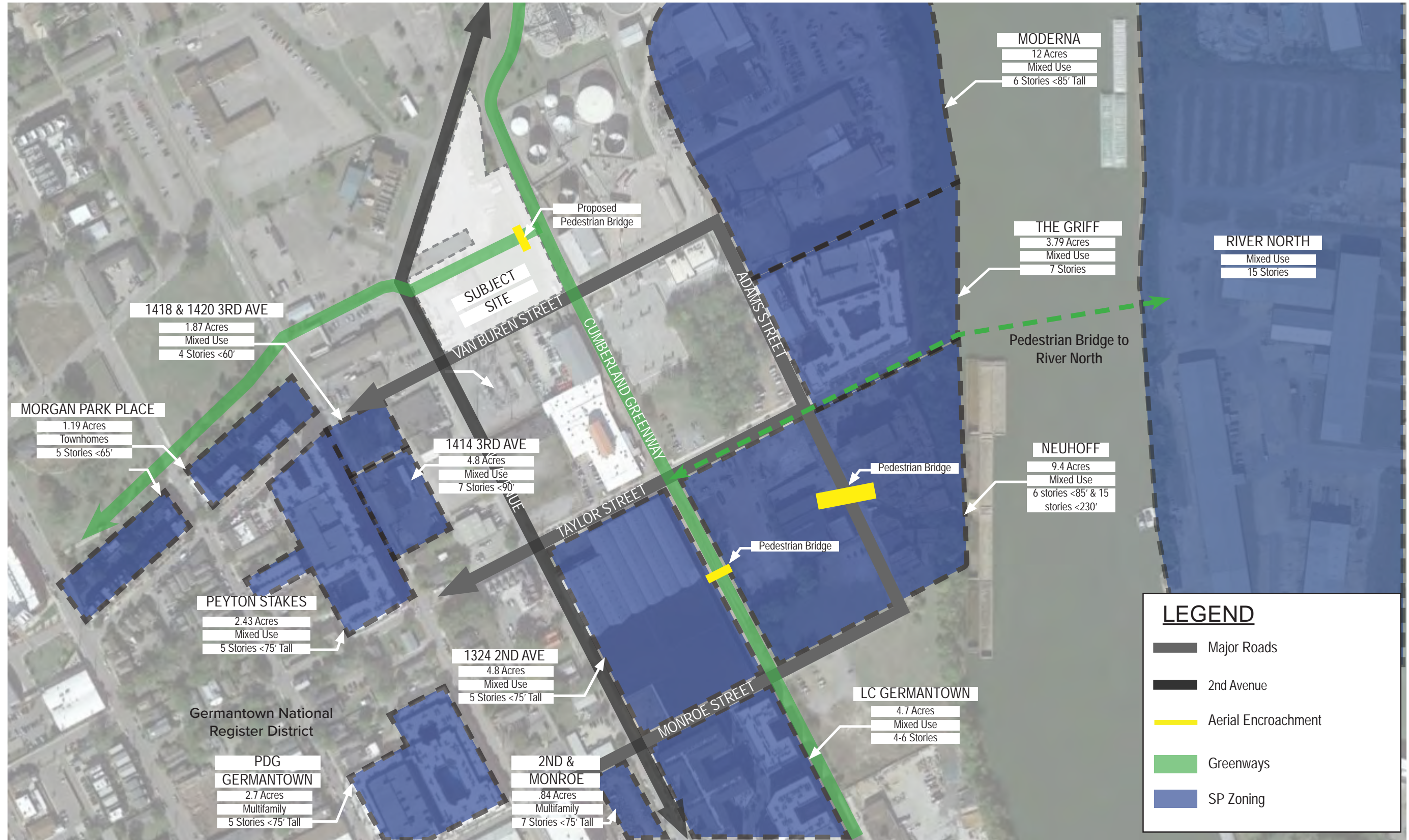
Van Buren Street Cross Sections



Mobility Map



Context Map



Additional Height Request

Community Plan Policy Language. T4 Mixed Use Neighborhood

Building Height – Mixed-use, non-residential, and multi-family buildings are generally up to five stories in height but may be taller in limited instances. The appropriate height is based on the building type, surrounding context, architectural elements and location within the neighborhood.

Consideration of taller heights is proposed based on the following factors for the subject site:

1. Proximity to other policy areas and the role of the building in transitioning between policies (see below for further details on transitions);

Response: This location, as well as the surrounding East Germantown neighborhood, is an appropriate place for additional height. The Nashville Next Growth and Preservation Concept Plan classifies these properties as being in a Transition or Infill Policy. This Transition and Infill policy is significant because it is adjacent to the Tier One Center that holds Downtown and Germantown. This designation is where the most intense development in the city is intended to be located, and the areas adjacent should provide opportunities, like housing, to support the Downtown.

In addition to being adjacent to the T6 Community Character policies of Downtown, this site is also in close proximity to River North, a T5 Mixed Use policy area that will soon be a center of activity with the development of the Oracle campus and will connect directly into East Germantown with a pedestrian bridge. Metro Center is a short distance away and is also experiencing intense mixed-use redevelopment, this site is connected directly to those employment opportunities by the greenway. Adjacent policies to the north and east include both District Industrial and District Impact policies on the northern boundary and do not lend themselves to requiring a dampening of intensity or massing as a transition.

2. Planned height of surrounding buildings and the impact on adjacent historic structures;

Response: This site is within one block of Moderna, approved for 6 stories, one block of Neuhoff, where both 15 story and 6 story heights have been approved and one block of a recent approval of 7 stories at 1414 3rd Avenue North, which is between the subject site and the Historic Germantown Neighborhood. Many developments have already been approved and constructed, such as 1414 3rd Avenue North or Peyton Stakes, that create a buffer and view shield between the subject site and the Historic Germantown Neighborhood.

3. Contribution that the building makes to the overall fabric of the Mixed-Use Neighborhood in terms of creating pedestrian-friendly streetscapes, plazas and open space, public art, innovative stormwater management techniques, etc.;

Response: While this development is primarily multi-family, care has been taken so that the ground level creates a pedestrian-friendly and active streetscape, varying from a storefront frontage for the leasing office and amenity center to the stoop fronts along the greenway and Second Avenue North. The project is providing improvement of the streetscape that exceeds the minimum requirements of the Major and Collector Street Plan (MCSP) (described in response to item 4 below).

This development removes the chain-link fences that currently border the greenway, widening the experience of the corridor and providing eyes on the greenway with stoop frontages facing it. In addition to enhancing the public greenway experience, the site utilizes low impact development stormwater management techniques to create additional greenspace space in the project, such as the courtyard along Van Buren and the nooks along Second Avenue North that create green relief in a neighborhood that currently is characterized by a high degree of impervious area. The project will also mitigate a stormwater management deficiency on Van Buren that causes ponding during rain events.

4. Relationship of the height of the building to the width of the street and sidewalks, with wider streets and sidewalks generally corresponding to taller building heights;

Response: The MCSP will require dedications and reconstruction on both Van Buren and Second Avenue North where curb, gutter, sidewalk and planting areas currently do not exist. Van Buren Street is classified as a local street; however, given the characteristics of the proposed development, the design team utilized the guidance of the block just to the west, classified as a T4-M-AB2, thus requiring an 8-foot sidewalk instead of the 5-foot local sidewalk. Additionally, on-street parking is provided, as well as green space between the back of the sidewalk and the building to provide additional relief and planting areas, as well as the courtyard along Van Buren with the building setback 42-feet from the back of the sidewalk. Second Avenue North is classified as a three-lane local street, T4-M-LS3. The design team felt that a wider 8-foot sidewalk was appropriate for this proposed development and that the addition of on-street parking would improve the urban neighborhood experience.

The experience along the greenway will be enhanced from the narrow, chain link fence lined condition that exists today. The greenway area will be improved by removing the fences, replacing with buildings that interact with the trail, supplemented with landscaping and other amenities and be available to the broader community, while maintained by the developer. Additionally, the design interpretations for Van Buren, Second Avenue North and the greenway widen the public realm and support the requested additional height.



Existing development creates a transition between the subject properties and the Historic Germantown Neighborhood. This image illustrates the intense urban character of the East Germantown Neighborhood and how desirable it is as a primarily residential neighborhood in close proximity to downtown.



Additional height on this site is consistent with the emerging character of the East Germantown Neighborhood and will provide housing opportunities in close proximity to the developing River North campus.

Additional Height Request cont.

5. Prominence of the intersection or street segment on which the building is located, with locations along or at intersections of arterial-boulevard streets being favored for taller buildings;

Response: The subject site is at the terminus of two arterial boulevard streets, and the frontage of Second Avenue North transitions to a local street classification, though it is a 3 lane cross-section, thus creating prominence for this site to be a point of activity interpreted through additional height.

6. Capacity of the block structure and rights-of-way to accommodate development intensity;

The surrounding area is developed in a grid pattern with quick access to major streets and larger greenway networks. The contributions of this project to improving local rights-of-way for all modes of transit – from vehicular to bike and pedestrian-- it is further discussed in item 4 above and supports the request for additional height.

7. Proximity to existing or planned transit, with increased height benefits for areas within 0.25 miles of a High Capacity Transit station;

Response: It is a short walk of 0.07 miles (one block) to Route #9 that runs from Downtown to MetroCenter. The consideration of the greenway as a mode of transit should also be considered. Once the pedestrian bridge across the Cumberland River is open and the Oracle campus is constructed, this site will provide additional housing opportunities that do not require a vehicular trip to work or to play. The city has made a tremendous investment in alternative transportation in this regard and similar investments of intense housing opportunities should be encouraged near the greenway to bolster use of these pathways in order to fully realize the impact of the city's investments.

8. Use of increased building setbacks and/or building stepbacks to mitigate increased building heights;

Response: As noted above, the irregular shape of the lot provides opportunities to set the building back and create pockets of elusive urban green space. A green courtyard has been provided at ground level along Van Buren. In addition to the ground level setbacks, a step back after 5th floor has been provided, as well as a change of building materials to minimize the experience of the height on the street below.

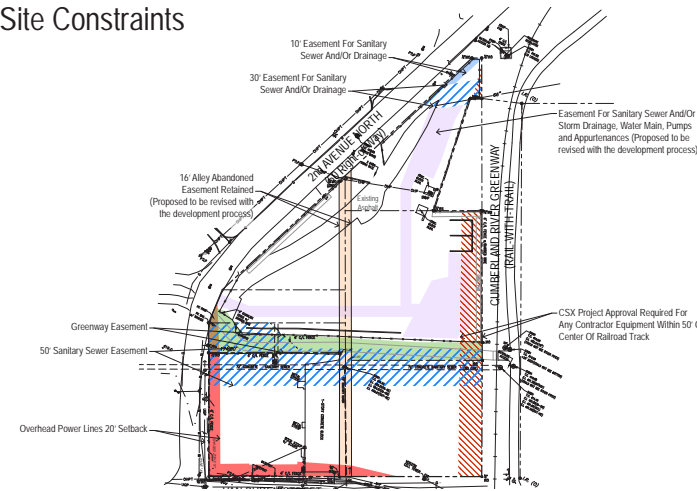
9. Topography and other unique site and locational characteristics;

Response: This site is severely constrained due to irregular shape, easements and overhead power lines as shown in the Constraints exhibit. The development is accommodating overhead powerlines, greenway easement, railroad easements, and sewer easements, which restrict buildable area though increase opportunities for green space. Due to the constraints that restrict the buildable footprint, additional height is appropriate to achieve similar entitlements that a regularly shaped lot would enjoy.

10. Ability to provide light and air between buildings and in the public realm of streets, sidewalks, internal walkways, multi-use paths, and open spaces.

Response: As has been noted in 2,3,4 and 8 above, the site is afforded unique opportunities due to its good fortune of being located along an existing greenway and its ability to turn constraints and irregular shape into opportunities that create a green, open, livable plan that will be a comfortable place for future residents to call home.

Site Constraints



The site contains a variety of constraints that have driven the design response. The unbuilt alley easement (where the right-of-way was abandoned but easements retained) and the stormwater easements shown in lavender can be abandoned or revised with new development, but the 50' sanitary sewer easement south of the greenway, the setbacks required from overhead power lines, the existing utility poles and the CSX setback are all constraints that the design must accommodate, restricting the maximization of the buildable footprint. Refer to the large scale site constraints exhibit in the front of the document for more information.



The existing greenway corridor is narrow and enclosed by a chain link fence.



The project will widen and activate both the trail and the greenway experience with enhancements and "eyes" on the corridor, improving the experience for the larger community.



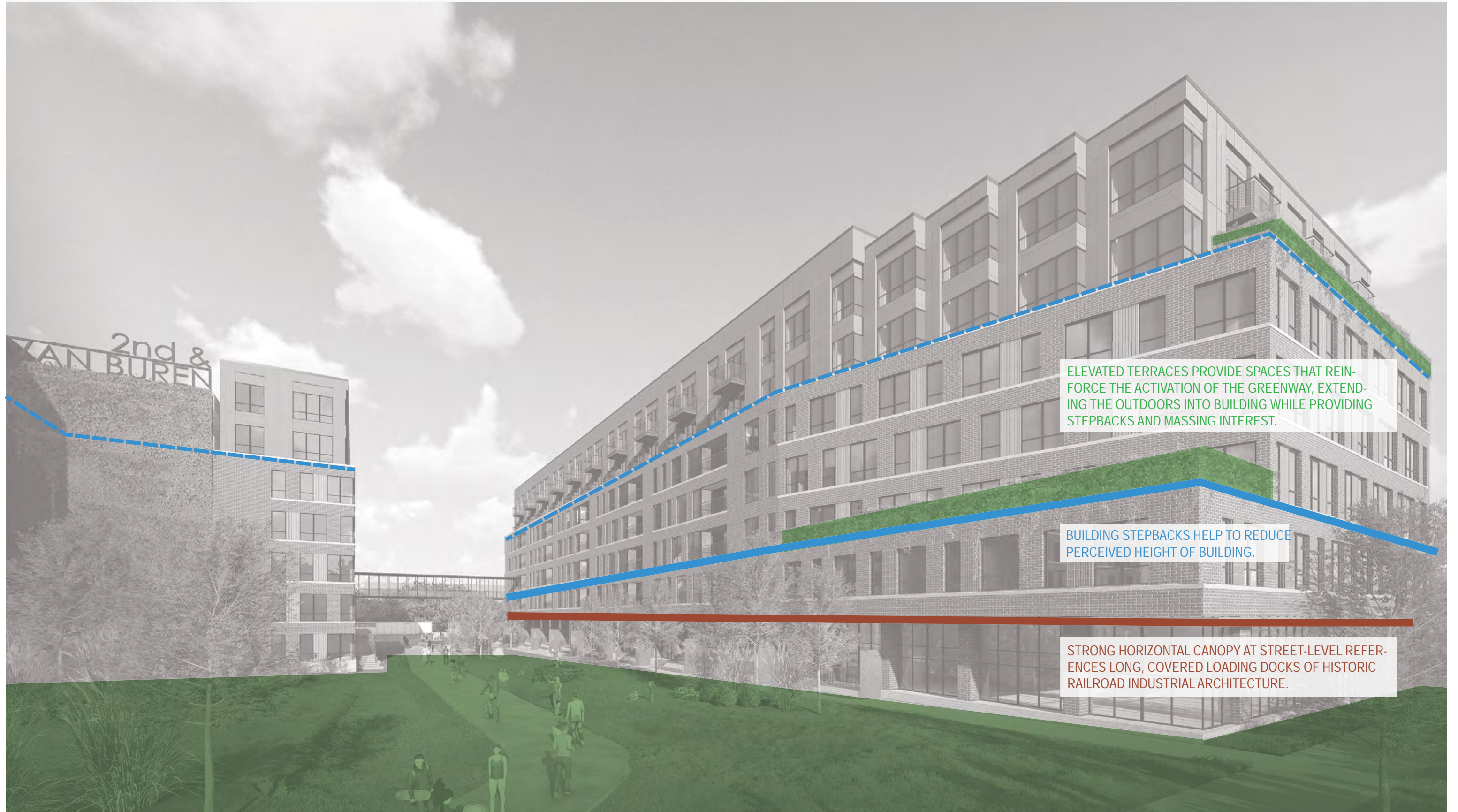
The Second Avenue frontage will provide on-street parking, planting zone and 8' sidewalk greatly improving the current view where none of these elements exist.



The Van Buren frontage will provide on-street parking, planting zone and 8' sidewalk and will also include a landscaped courtyard to lessen the impact of the NES substation shown.

HISTORICAL REFERENCES REIMAGINED WITH MODERN TECHNIQUES





ELEVATED TERRACES PROVIDE SPACES THAT REINFORCE THE ACTIVATION OF THE GREENWAY, EXTENDING THE OUTDOORS INTO BUILDING WHILE PROVIDING STEPBACKS AND MASSING INTEREST.

BUILDING STEPBACKS HELP TO REDUCE PERCEIVED HEIGHT OF BUILDING.

STRONG HORIZONTAL CANOPY AT STREET-LEVEL REFERENCES LONG, COVERED LOADING DOCKS OF HISTORIC RAILROAD INDUSTRIAL ARCHITECTURE.





CONCEPTUAL VIEW FROM THE 2ND AVENUE & VAN BUREN STREET



CONCEPTUAL VIEW FROM 1ST AVENUE





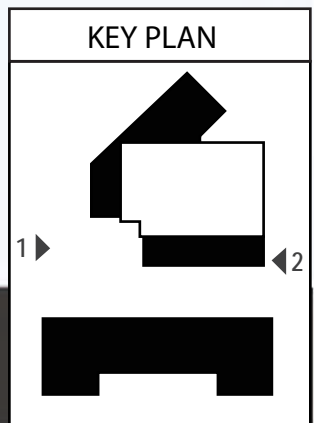
VIEW 2

RAILROAD ELEVATION



VIEW 1

2nd AVENUE NORTH ELEVATION





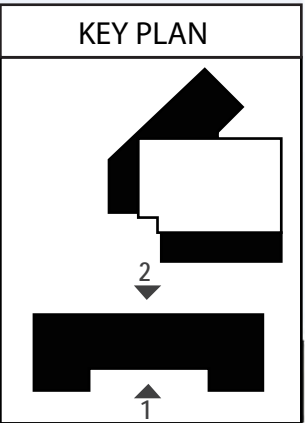
VIEW 2

GREENWAY ELEVATION



VIEW 1

VAN BUREN STREET ELEVATION



2nd & VAN BUREN

PRELIMINARY SPECIFIC PLAN



Toll Brothers
APARTMENT LIVING