

# MODERA

---

# CHESTNUT HILL



COUNCIL DISTRICT 17:

TERRY VO  
SP NAME: MODERA CHESTNUT HILL  
SP NUMBER: 2025SP-AAA-AAA  
PLAN PREPARATION DATE: JUNE 11, 2025

OWNERS OF RECORD:

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COOPER CARRY

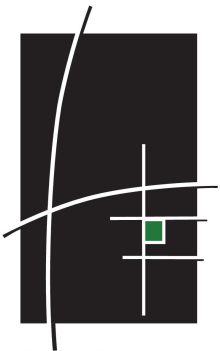
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GREGMILLER@COOPERCARRY.COM  
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Hawkins Partners, Inc.  
landscape architects

HAWKINS PARTNERS, INC  
110 SOUTH 10TH STREET SECOND FLOOR  
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CONTACT: BRIAN PHELPS  
B.PHELPS@HAWKINSPARTNERS.COM  
615.255.5218



An aerial photograph of a city grid, showing streets, buildings, and green spaces. The word "CONTEXT" is overlaid in large, white, sans-serif capital letters, centered horizontally and partially obscured by a semi-transparent dark band.

CONTEXT



## PURPOSE NOTE

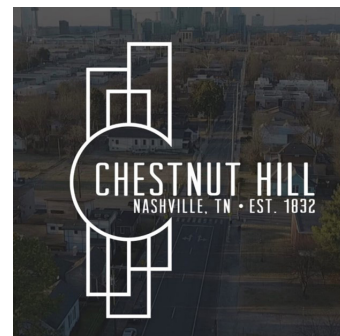
Through the Specific Plan (SP) District Ordinance, Mill Creek Residential (“Client”) intends to transform and repurpose the existing industrial and vacant parcels into a mixed-use community with apartments, live/work units, community space, and community amenities including a dog park, open space, and sports courts including pickleball, basketball, and bocce. The project is expected to be developed in one phase and will include a maximum of 400 multifamily units, inclusive of approximately 8 live/work loft units totaling ~9,000 square feet. The project’s live/work units and dog park are planned to be located in the center of the property on Andrew Whitmore Street with newly built sidewalks and landscaping. The pickleball, basketball, and bocce courts will be located within a repurposed Worthy of Conservation Building with the existing façade and murals to be preserved. Each of 2nd Avenue South, Old Radnor Car line Road, and Andrew T Whitmore Street will become vibrant streets with stoops, sidewalks, benches, parallel parking, landscaping, and street lighting. The mixed-use development is planned to actively engage with the neighborhood through prominent public art installations, seating/furniture, stoops, pedestrian walkability, a dog park, pickleball courts, and clubhouse frontage.

The site is approximately 5.79 gross acres and is bound on the north by two neighborhood churches on Hart Street, industrial warehouses on its east, over 750 linear feet of frontage facing 2nd Ave South on its west, and a shuttle/car lot to its south. The site is comprised of over 30 parcels and is currently zoned Industrial Warehousing/ Distribution (“IWD”), Mixed-Use Limited (“MUL-A-NS”), and R6. The existing Community Character Policies are T4 MU and Conservation

## CHESTNUT HILL

The Modera Chestnut Hill property is located in Nashville, Tennessee in the Chestnut Hill neighborhood, approximately 1.5 miles south of downtown Nashville. The Chestnut Hill neighborhood is bordered by Lafayette Street/Murfreesboro Pike to the northeast, Interstate 40 to the north, and the CSX railroad lines to the southeast and east. Chestnut Hill was established in the mid-to-late 19th century, making it one of Nashville’s oldest neighborhoods. Originally known as Trimble Bottoms, Chestnut Hill was once entirely owned by the Trimble family of Nashville. Meharry Medical College, now located west of Charlotte Avenue, was first located in Chestnut Hill under the name Central Tennessee College. Tom Wilson Park,

an 8,000-seat baseball stadium built in 1929 and demolished in 1946, was home to the Nashville Elite Giants of the Southern Negro League. Tom Wilson Park also hosted spring training for the Nashville Vols, a minor league team, where Babe Ruth, Lou Gehrig and Roy Campanella played exhibition games against The Nashville Vols.





# VICINITY MAP





VICINITY MAP

Parcel ID	Address	Current Zoning	CCM Policy	# of Acres
10508004800	1302 1st Ave S	R6		0.1
10508004700	1304 1st Ave S	R6		0.1
10508004600	1306 1st Ave S	R6		0.1
10508004500	1308 1st Ave S	R6		0.1
10508007700	1300 2nd Ave S	MUL-A-NS	T4 MU	0.06
10508007800	1302 2nd Ave S	IWD	T4 MU	0.06
10508007900	1304 2nd Ave S	IWD	T4 MU	0.06
10508008000	1306 2nd Ave S	IWD	T4 MU	0.07
10508008100	1310 2nd Ave S	IWD	T4 MU	0.12
10508008200	1312 2nd Ave S	IWD	T4 MU	0.09
10508008300	1314 2nd Ave S	IWD	T4 MU	0.13
10508008400	1316 2nd Ave S	IWD	T4 MU, CO	0.48
10508008700	1328 2nd Ave S	IWD	T4 MU, CO	0.12
10508008800	1330 2nd Ave S	IWD	T4 MU, CO	0.14
10508008900	1400 2nd Ave S	IWD	T4 MU, CO	0.16
10508009000	0 2nd Ave S	IWD	T4 MU, CO	0.06
10508009100	0 2nd Ave S	IWD	T4 MU	0.06
10508009200	0 2nd Ave S	IWD	T4 MU	0.09
10508009300	1410 2nd Ave S	IWD	T4 MU	0.25
10508007000	8 Andrew T Whitmore Street	IWD	T4 MU, CO	0.81
10508009500	13 Andrew T Whitmore Street	IWD	T4 MU	0.05
10508009600	15 Andrew T Whitmore Street	IWD	T4 MU	0.07
10508009700	17 Andrew T Whitmore Street	IWD	T4 MU	0.09
10508009800	19 Andrew T Whitmore Street	IWD	T4 MU	0.29
10508009900	21 Andrew T Whitmore Street	IWD	T4 MU, CO	0.09
10508028800	22 Andrew T Whitmore Street	IWD	CO	0.46
10508006600	24 Andrew T Whitmore Street	IWD	CO	0.4
10508010000	27 Andrew T Whitmore Street	IWD	T4 MU, CO	0.18
10508010100	29 Andrew T Whitmore Street	IWD	T4 MU, CO	0.09
10508010200	31 Andrew T Whitmore Street	IWD	T4 MU, CO	0.09
10508010300	33 Andrew T Whitmore Street	IWD	T4 MU, CO	0.09
10508010400	37 Andrew T Whitmore Street	IWD	CO	0.39
10508010500	39 Andrew T Whitmore Street	IWD	CO	0.09
Total				5.54

Figure 1: Study Area for Wedgewood-Houstonand Chestnut Hill Planning Study





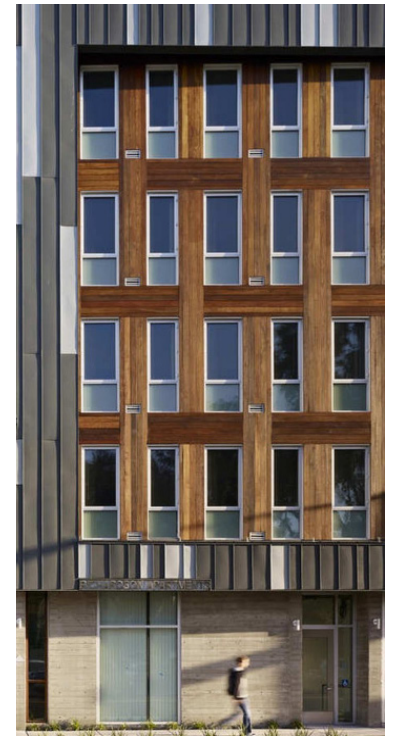
## VISION

The Vision for the Modera Chestnut Hill site is to transform a largely vacant, industrial corridor into a vibrant, mixed-use environment that reflects the existing community in both architecture and urban design. The master plan repairs broken and abandoned streets and connects the site to the overall Chestnut Hill community by improved walkability and public amenities.

## INSPIRATION

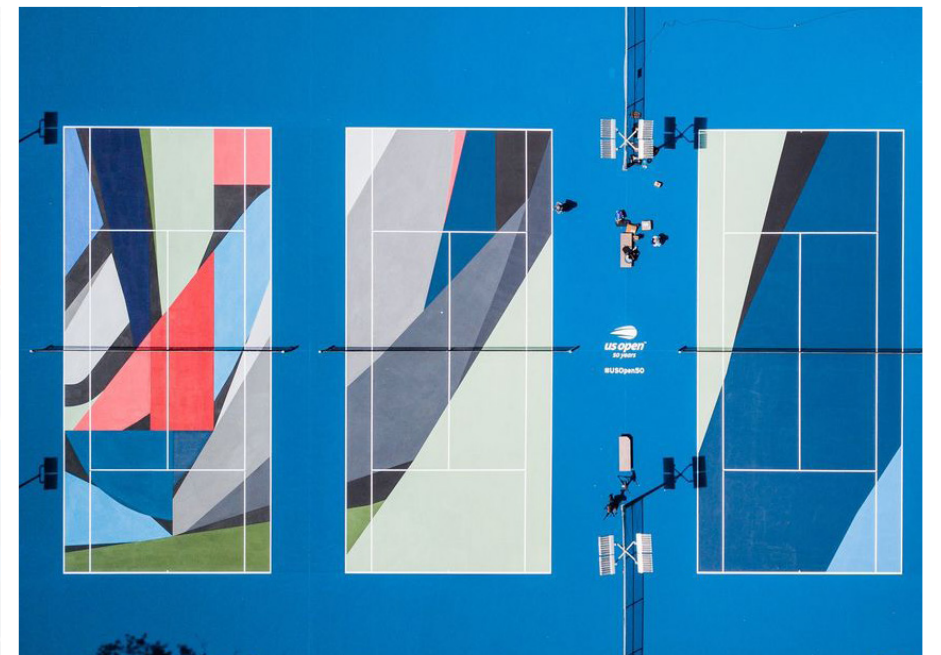
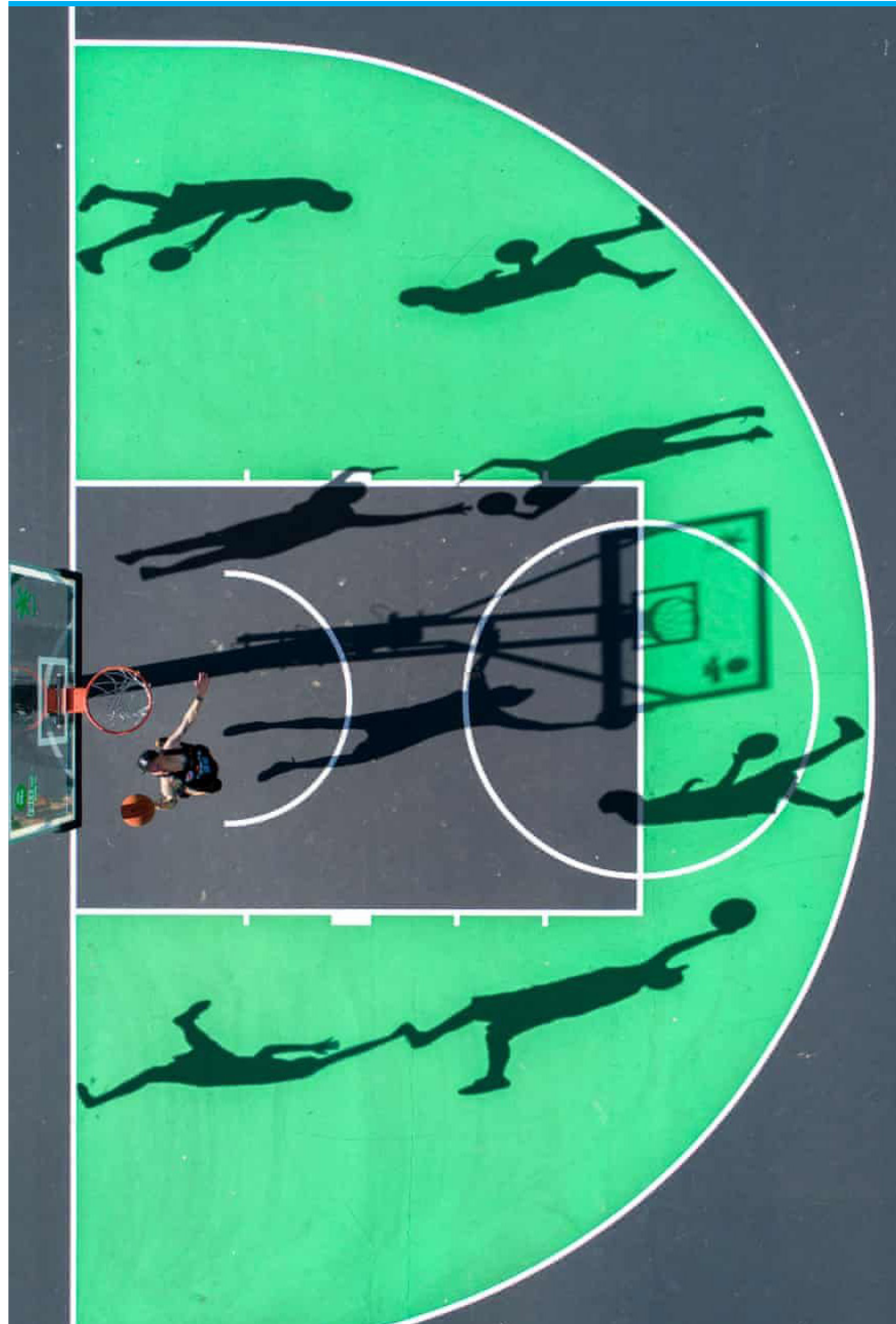
Chestnut Hill itself is steeped in history and character. The collection of historic and modern homes intermingled with retail and industrial uses creates a unique and diverse neighborhood. The architectural inspiration draws from the context of the neighboring buildings and creative community that is Chestnut Hill.

CONTEMPORARY URBANISM ELEVATED





# COMMUNITY AMENITY INSPIRATIONS





# COMMUNITY AMENITY INSPIRATIONS





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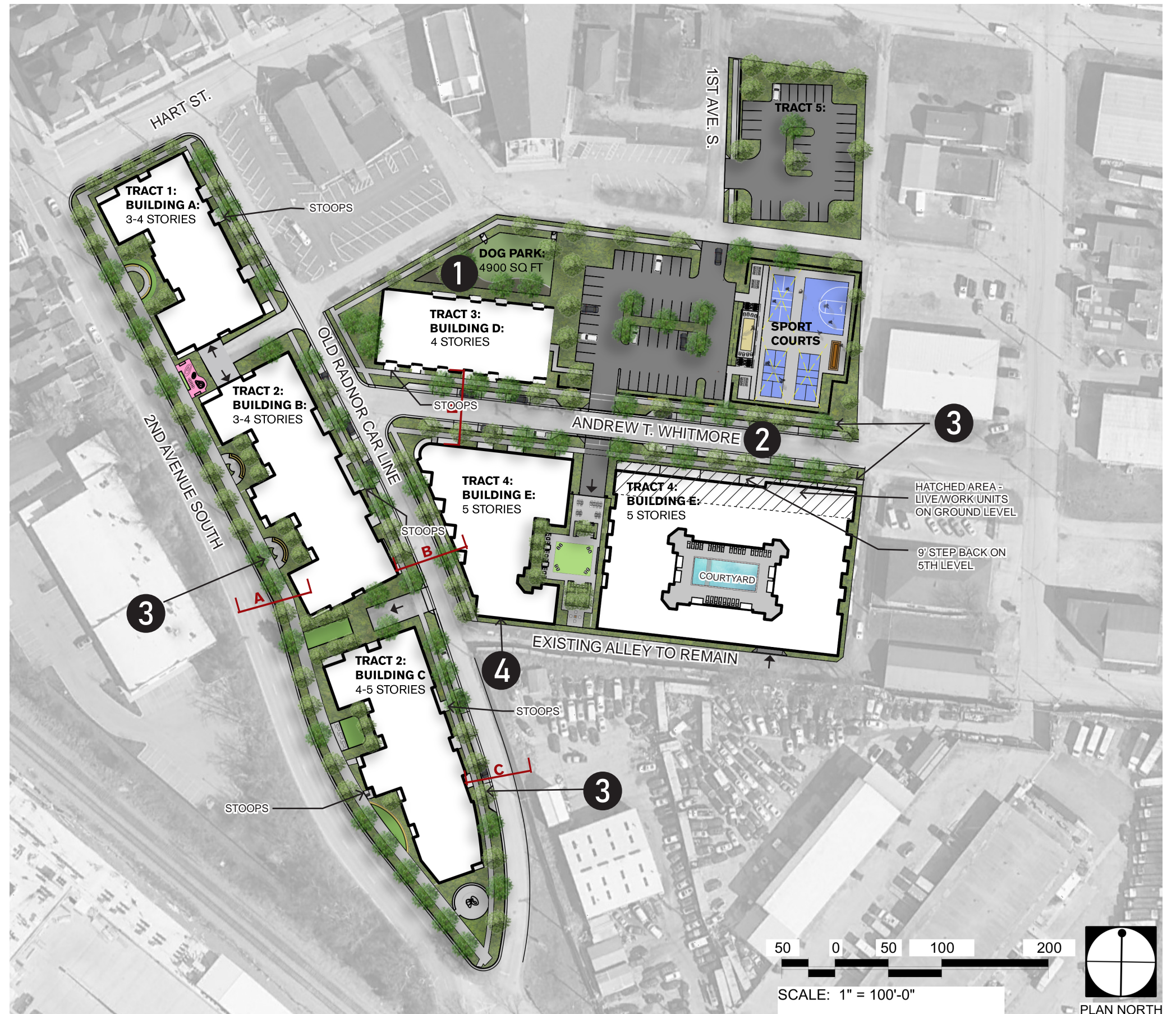
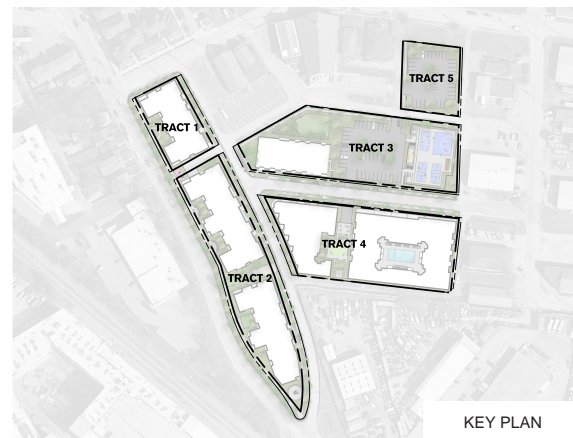




# SITE PLAN

We have adjusted the length of Tract 2A and 2B to be a maximum of 225' long. We have shortened the Tract 2B to create a pocket park at the intersection of 2nd Avenue and Old Radnor Car Line.

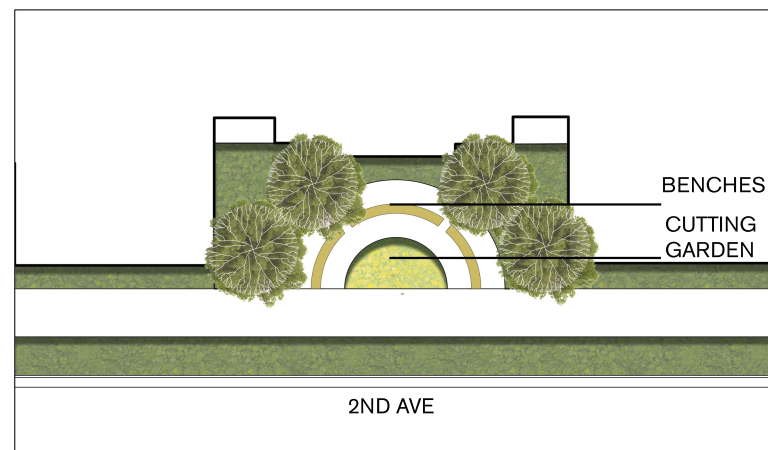
- 1 PRIVATELY MANAGED PUBLIC DOG PARK
- 2 PUBLIC NEIGHBORHOOD AMENITIES
- 3 NEW/EXPANDED SIDEWALKS AND LANDSCAPING
- 4 EXTERIOR MURAL ARTWORK
- ← PARKING ENTRY



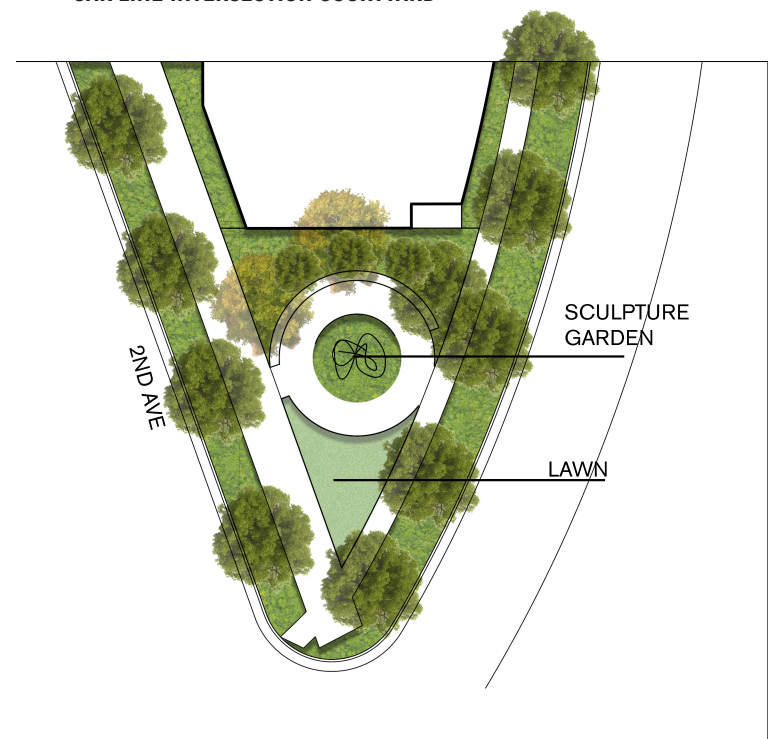


# SITE PLAN VIGNETTES

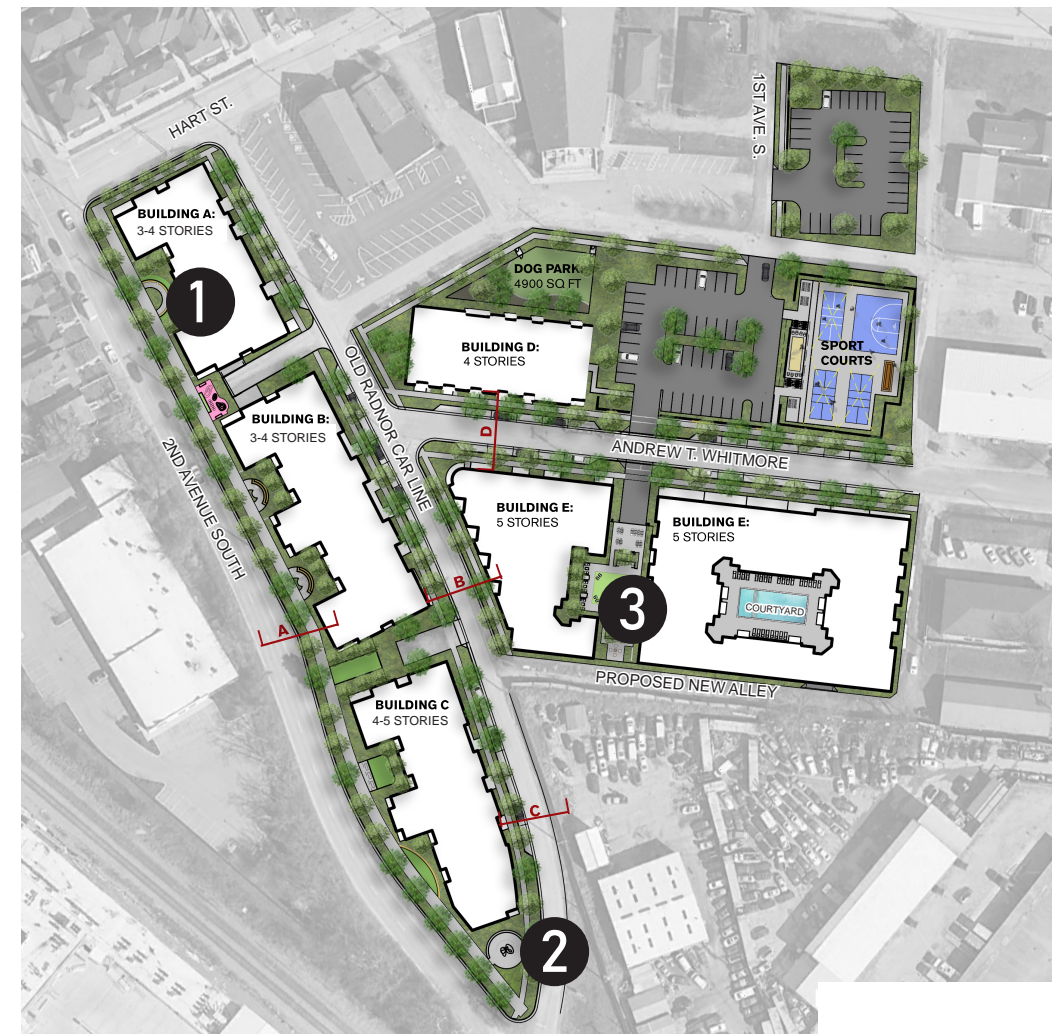
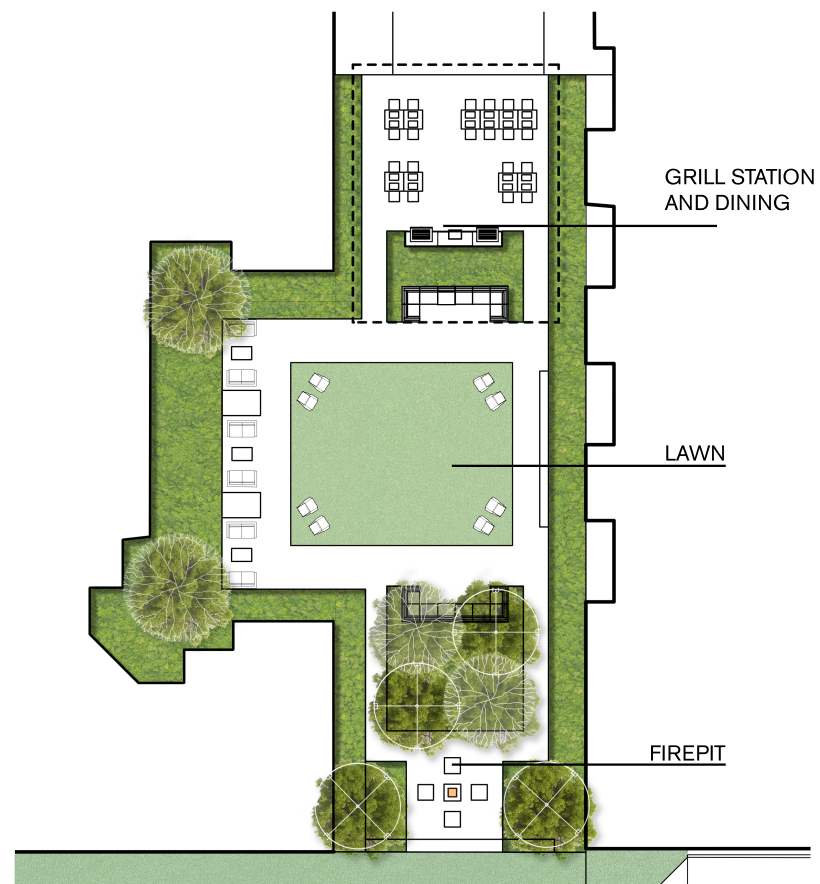
**1** TRACTS 1, 2A, 2B COURTYARDS ALONG 2ND AVENUE SOUTH



**2** 2ND AVENUE SOUTH AND OLD RADNOR CAR LINE INTERSECTION COURTYARD



**3** TRACT 4 ELEVATED COURTYARD





# 2ND AVENUE STREET SCAPE VIGNETTES

## IMAGINATION GARDEN

Colorful sculptural play mounds transform childhood imagination into physical adventure. Parent seating provides comfortable supervision while children explore dynamic landforms that encourage creative play and active learning.



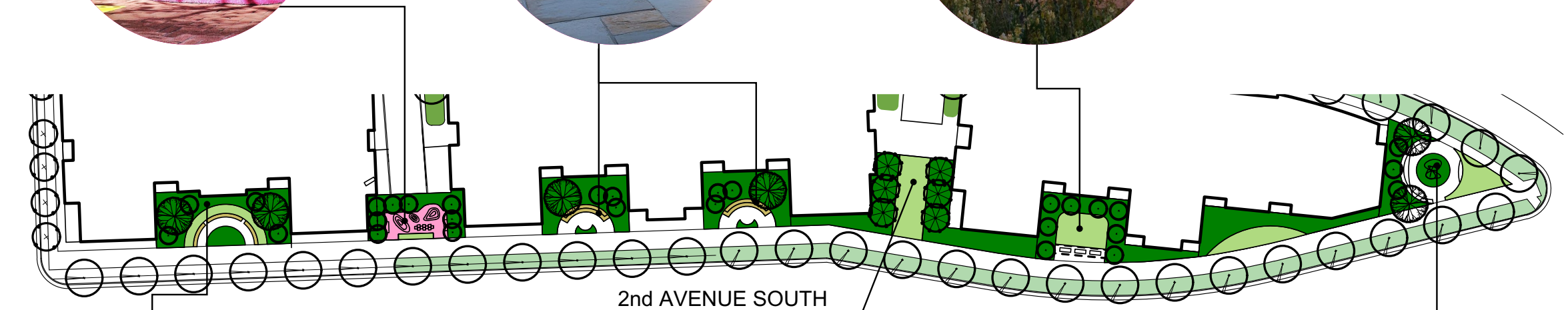
## STAGE GARDENS

Twin terraced wooden amphitheaters offer intimate, flexible front porch along the street. These dual-purpose structures accommodate impromptu small community gatherings, outdoor meetings, or quiet contemplation within the urban landscape.



## SWING GARDENS

Three suspended swings beneath a modern shade structure build intergenerational relationships through shared casual play. This social catalyst encourages neighbors to interact naturally while providing an engaging vantage point for community watching along the street.



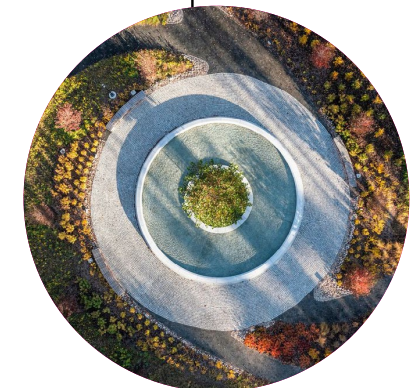
## CUTTING GARDEN

A fragrant cutting garden with circular benches strengthens neighborhood bonds as residents share gardening knowledge and enjoy seasonal blooms. The garden provides an opportunity for the community to bring a little bit of joy home with them through modest bouquets of blooming perennials.



## VISTA GARDEN

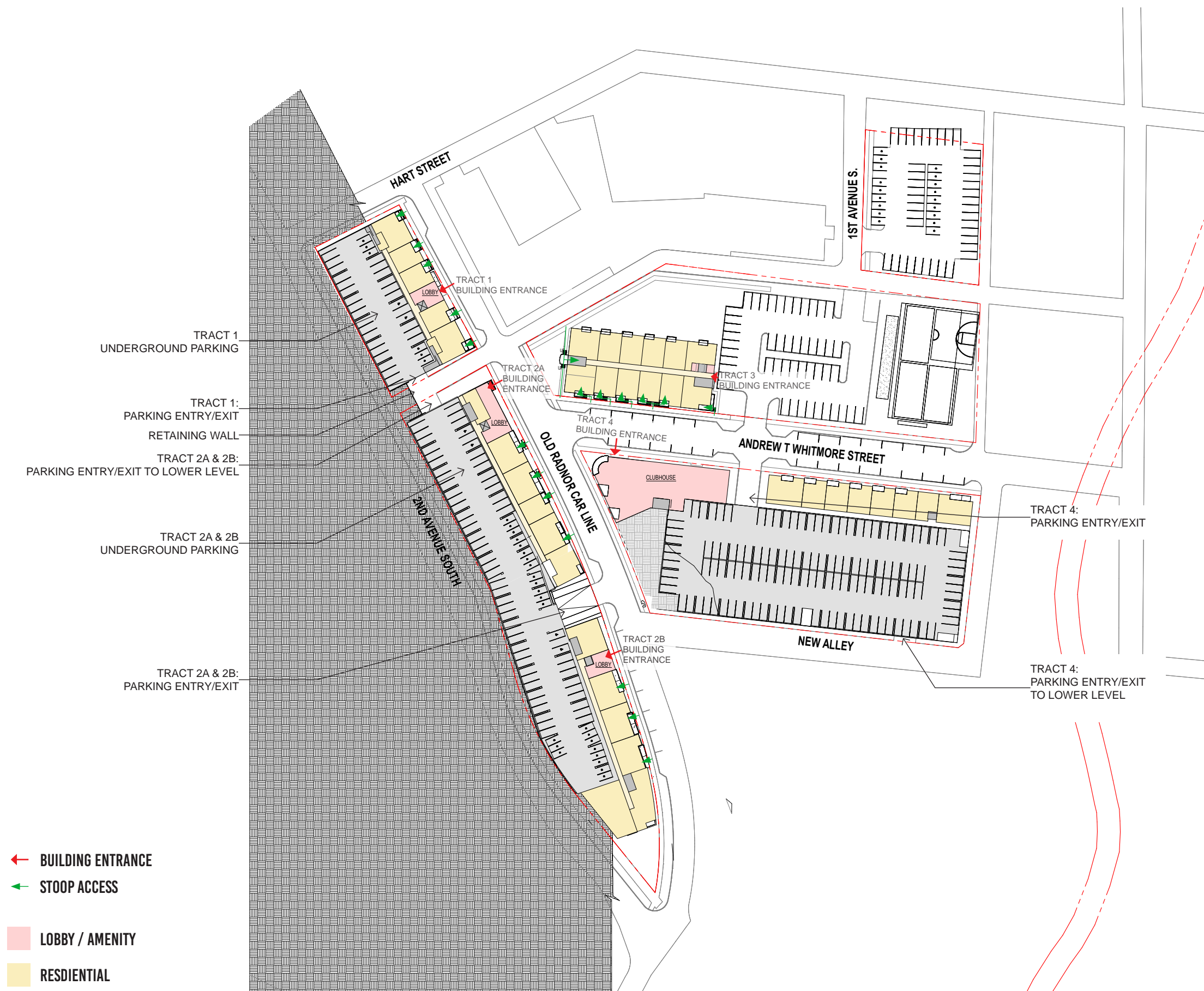
A manicured lawn flanked by flowering trees creates a dramatic visual corridor between buildings.



## PUBLIC SCULPTURE GARDEN

A circular exhibition space showcases youth-created artwork while managing stormwater through integrated bioretention features. This educational landscape celebrates local creativity while demonstrating sustainable design principles to the community.

# GROUND LEVEL PLAN





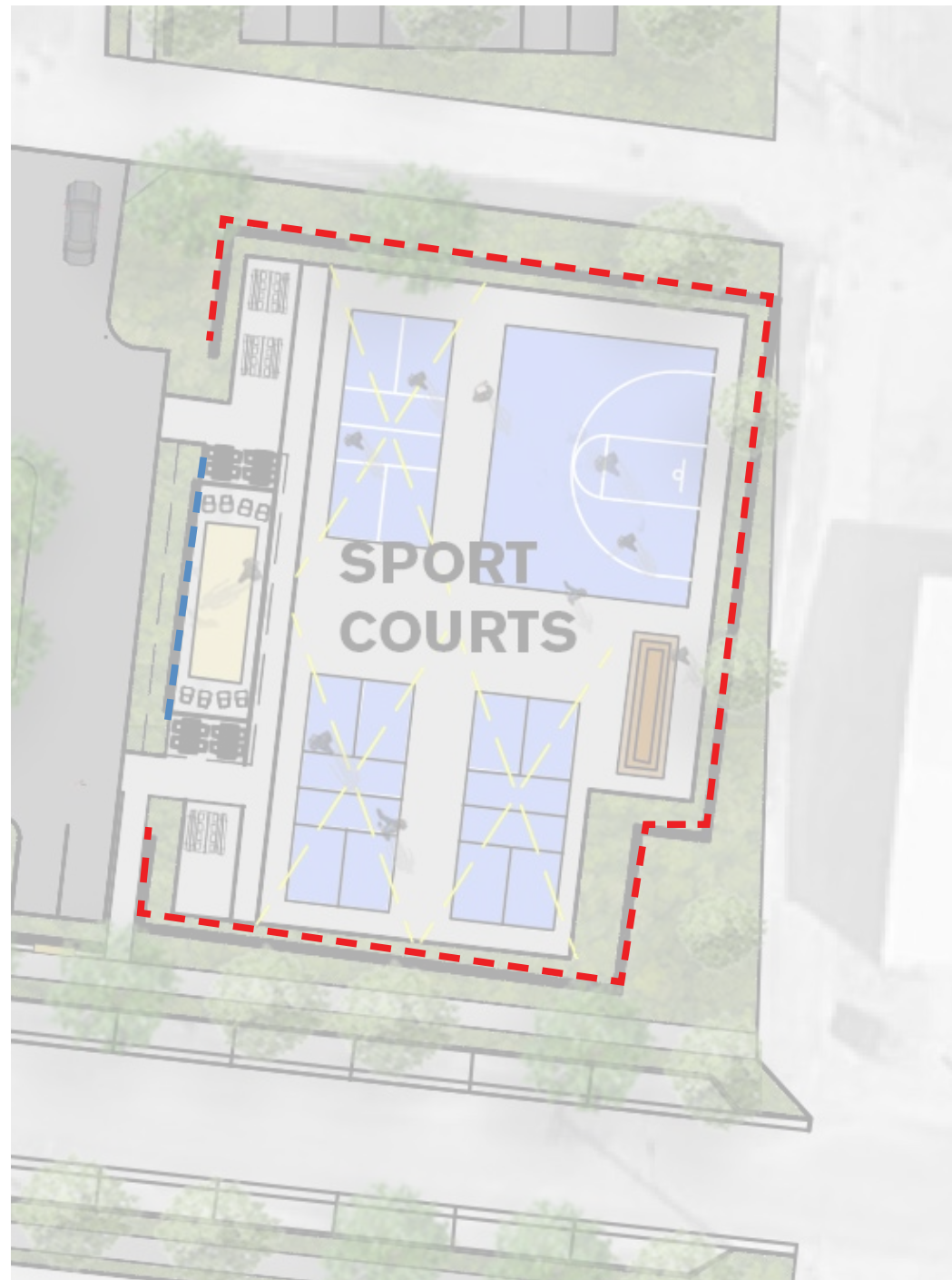
# TYPICAL LEVEL PLAN

 RESIDENTIAL





# EXISTING WOC BUILDING

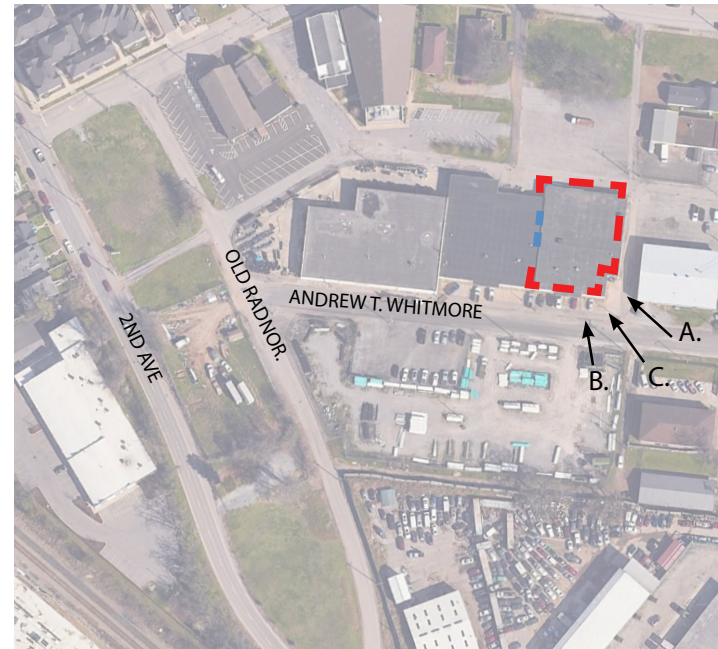


- = EXISTING BUILDING OUTLINE. TO BE PRESERVED AS LANDSCAPE WALL.
- = NEW LANDSCAPE WALL FOR SCREENING

## NARRATIVE

The existing building walls are being preserved and thoughtfully integrated into the design of the new landscape as backdrop walls for the sports courts. Rather than demolish these structural elements, the design team recognized their potential to serve a new purpose—anchoring the space both physically and historically. The preserved walls offer a sense of continuity and identity, grounding the new recreational area in its rich, existing context.

NOTE: All renovations and structural openings shall be based on the assessment and recommendations of a structural engineer, subject to review by Metro Planning staff.





# REPURPOSED WOC BUILDING





The exterior walls of the existing Worthy of Conservation Building will be preserved, and the building will be repurposed to include pickle ball, basketball, and bocce courts that are open to the public.





# MASTER PLAN OVERVIEW



Aerial View from the West



Street View from West



Street View from East



# 2ND AVENUE STREETSCAPE

We plan to revitalize the east side of 2nd Avenue by increasing the public sidewalks, adding landscaping, and creating a welcoming and walkable experience for the community. Stoops were added where feasible to create connections to the sidewalk activating the streets.





# 2ND AVENUE STREETSCAPE

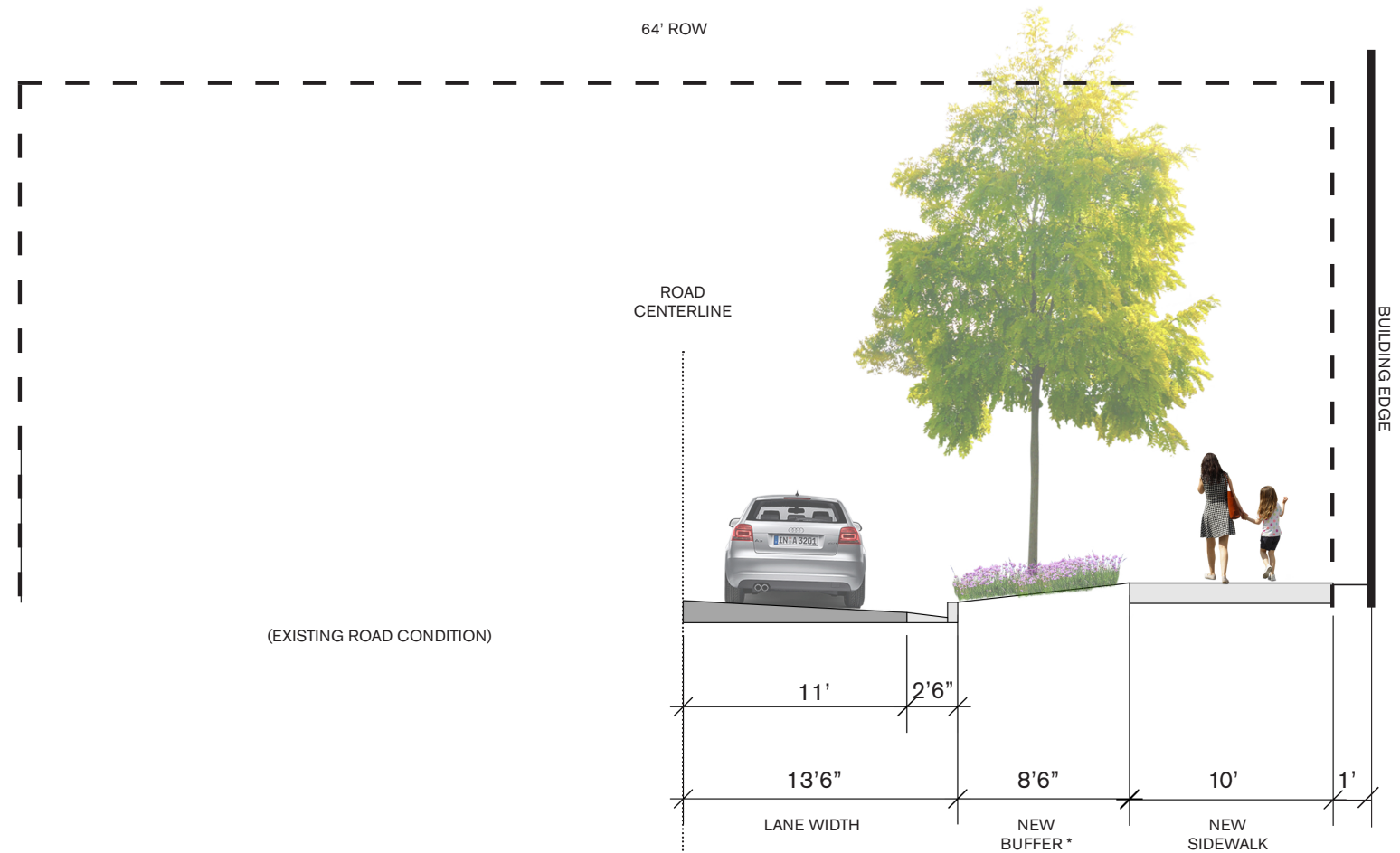
The courtyards along 2nd avenue are no longer fenced in and are open to the public.





# 2ND AVENUE STREETSCAPE OPTION 1

NOTE : The cross-section shall accommodate the final design concept with High-Capacity Transit in the MCSP Modification document without requiring any further modification to the curb line. Final location to be agreed upon by NDOT, WeGo, and the development.

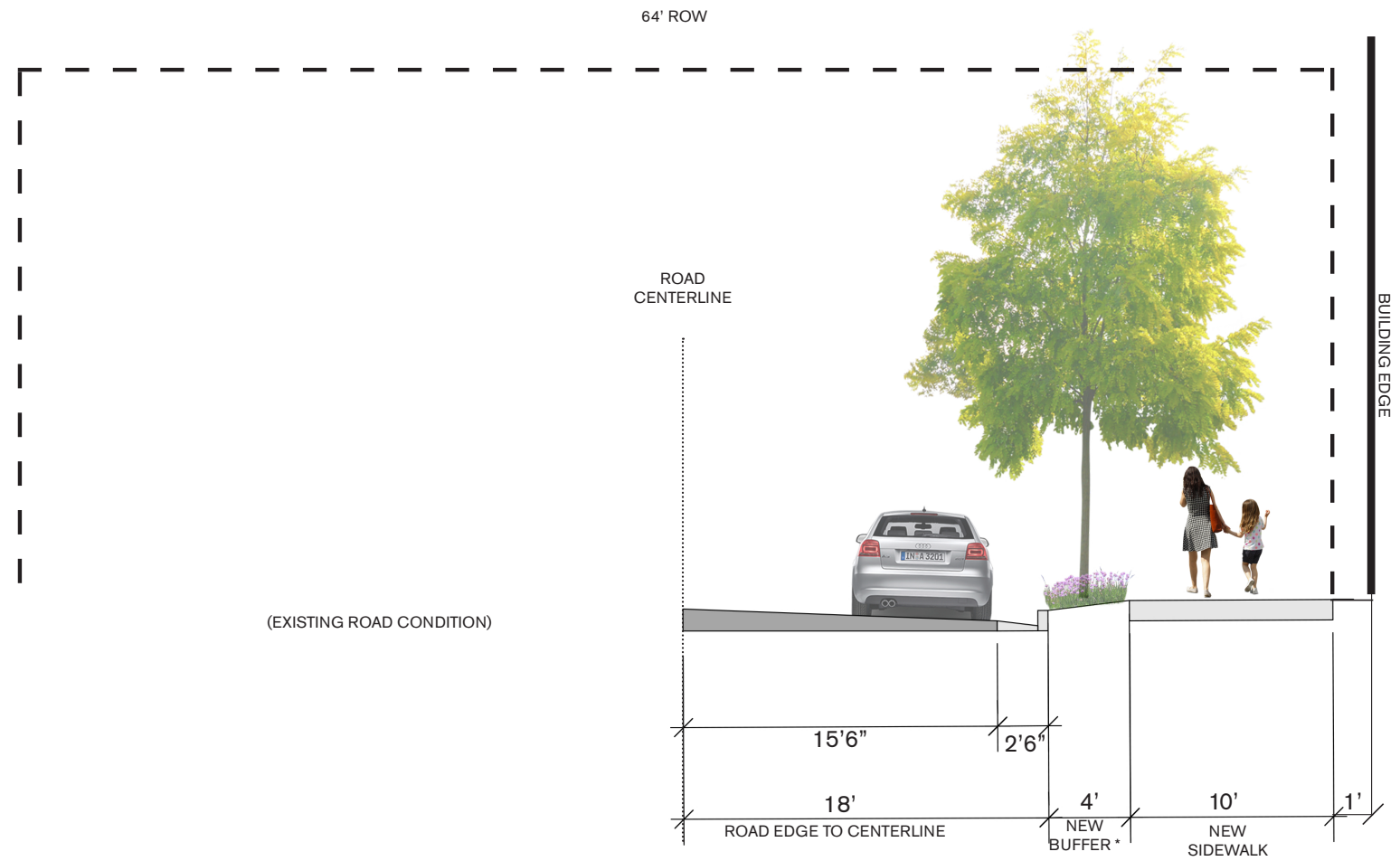


**SECTION A:**  
**2ND AVENUE**  
**OPTION 1**



# 2ND AVENUE STREETSCAPE OPTION 2

NOTE : The cross-section shall accommodate the final design concept with High-Capacity Transit in the MCSP Modification document without requiring any further modification to the curb line. Final location to be agreed upon by NDOT, WeGo, and the development.



**SECTION A:**  
**2ND AVENUE**  
**OPTION 2**



# TRACT 4 PERSPECTIVE ANDREW T WHITMORE

Building E building was split into two blocks with a maximum length of 225'. The separation between buildings is 40' and becomes an amenity space. A 9' stepback was added to the 5th floor on the left side reducing the building to a pedestrian friendly scale. Live/Work units will be located on Andrew T Whitmore, across from the repurposed Worthy of Conservation building.





# ANDREW T WHITMORE STREETSCAPE



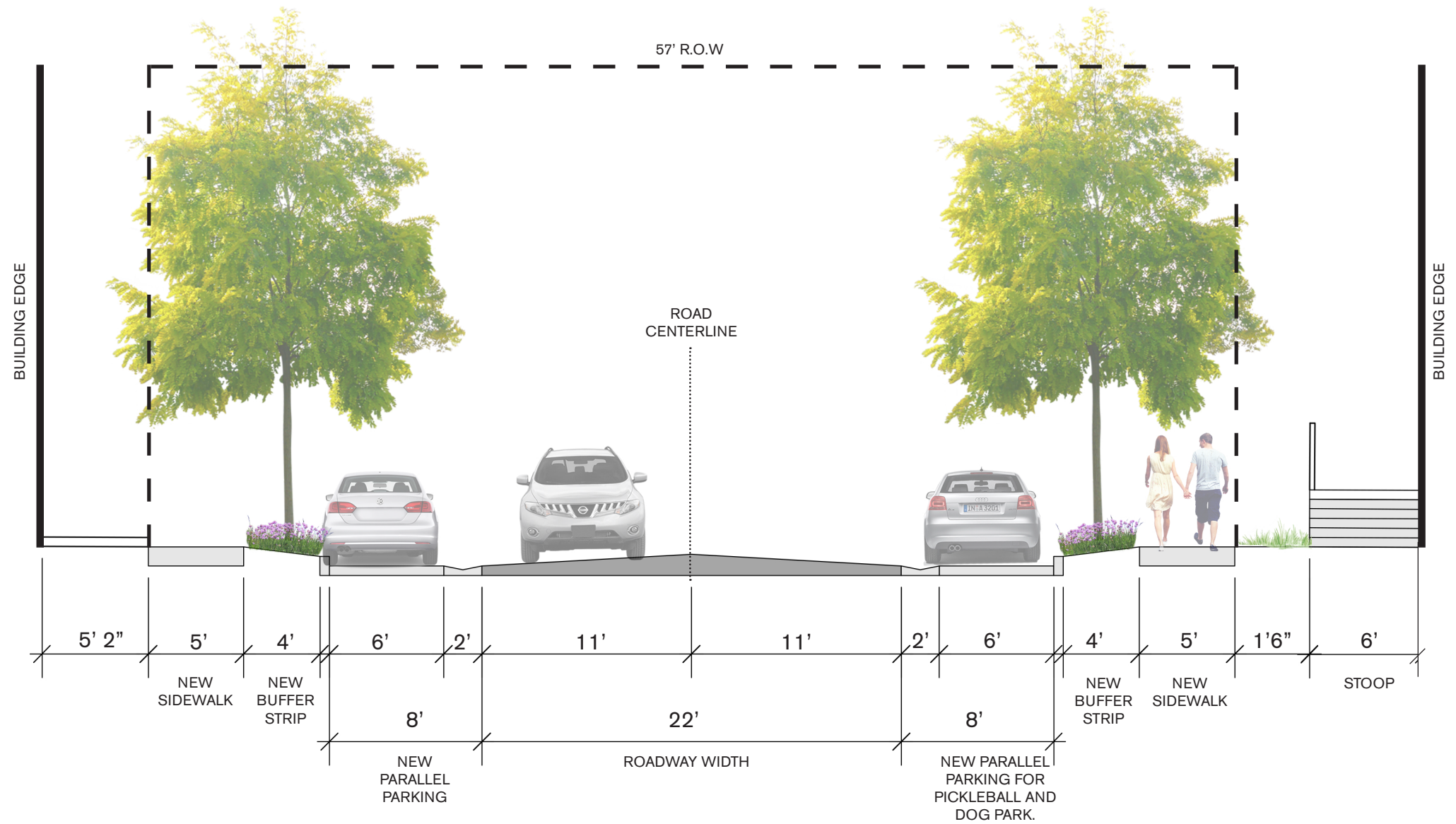


# ANDREW T WHITMORE STREETSCAPE





# ANDREW T WHITMORE STREETSCAPE



**SECTION D:**  
**ANDREW T WHITMORE ST**



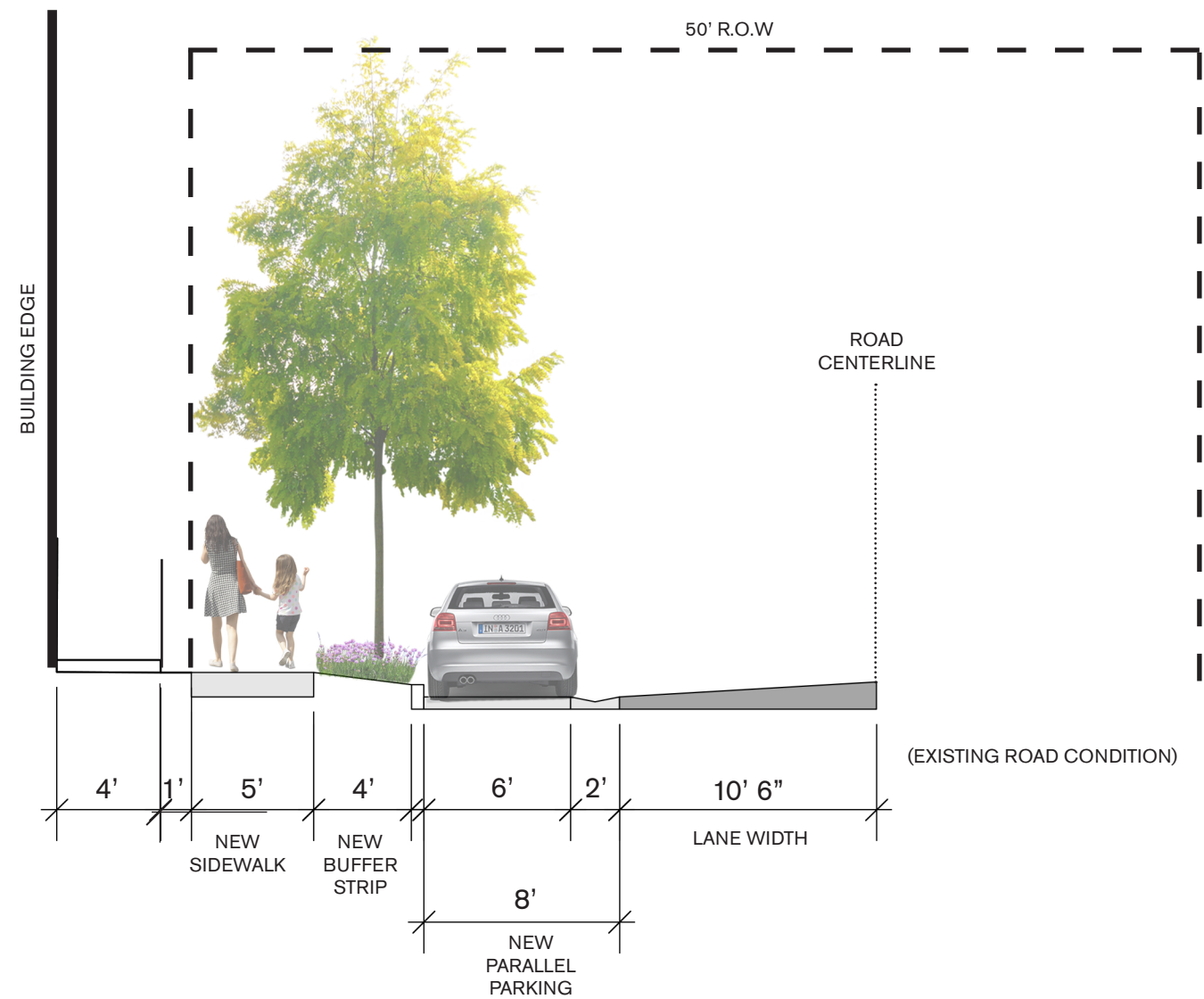
# OLD RADNOR CAR LINE STREETSCAPE

Old Radnor Car Line Road will be enhanced from solely a car-dependent street to a walkable, pedestrian-friendly street with new sidewalks, landscaping, and parallel parking on the west side of the street.





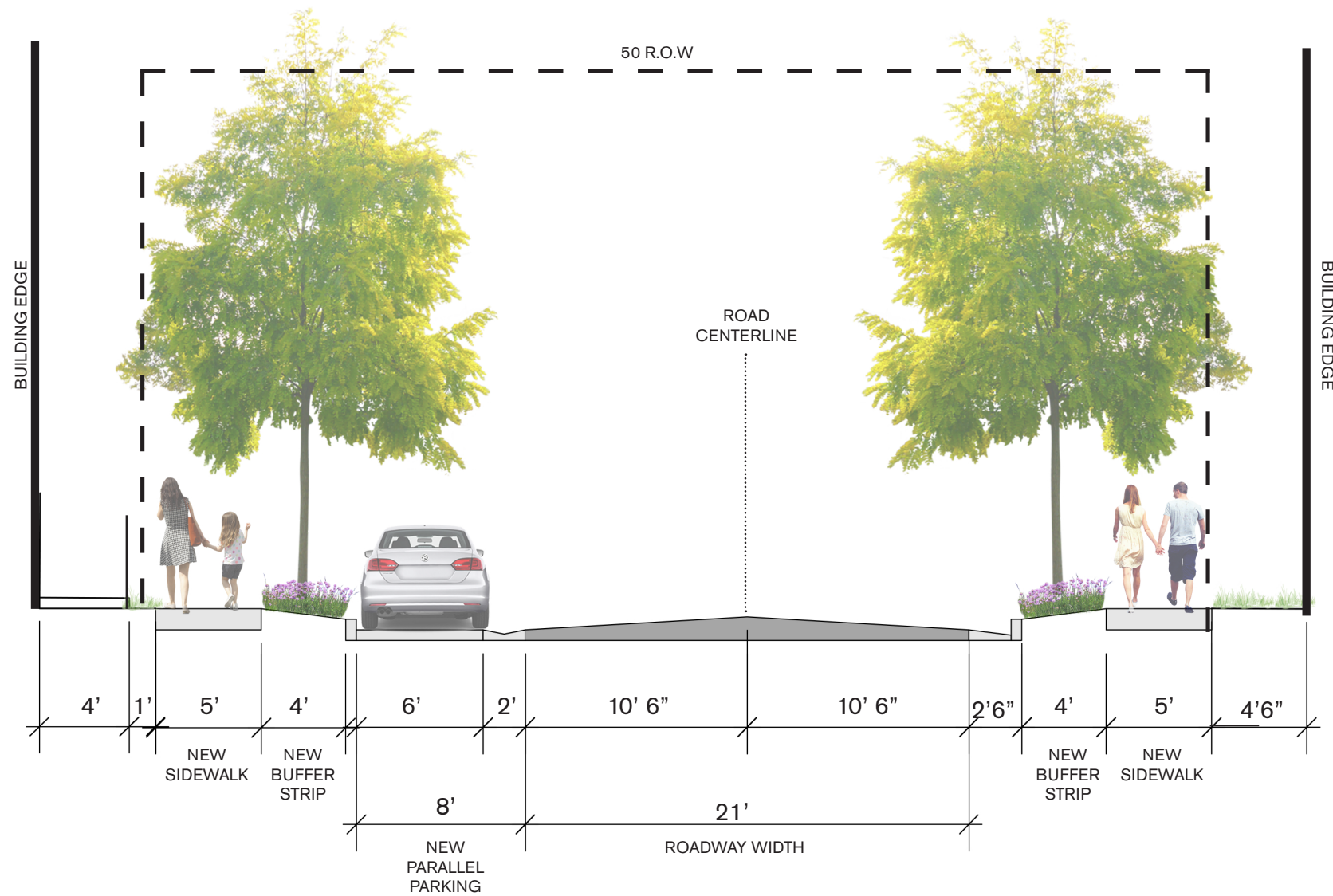
# OLD RADNOR CAR LINE STREETSCAPE



**SECTION C:**  
**OLD RADNOR CAR LANE CONDITION 1**



# OLD RADNOR CAR LINE STREETSCAPE



**SECTION B**  
**OLD RADNOR CAR LANE CONDITION 2**



# TRACT 1 BUILDING ELEVATIONS



OLD RADNOR CAR LINE ELEVATION



2ND AVENUE ELEVATION



HART STREET ELEVATION



# TRACT 2A BUILDING ELEVATIONS



OLD RADNOR CAR LINE ELEVATION



2ND AVENUE ELEVATION



# TRACT 2B BUILDING ELEVATIONS



2ND AVENUE ELEVATION

- EL 506' - 0" BLDG C - ROOF PLATE
- EL 495' - 6" BLDG C - LEVEL 5
- EL 484' - 10" BLDG C - LEVEL 4
- EL 474' - 2" BLDG C - LEVEL 3
- EL 463' - 6" BLDG C - LEVEL 2



OLD RADNOR CAR LINE ELEVATION

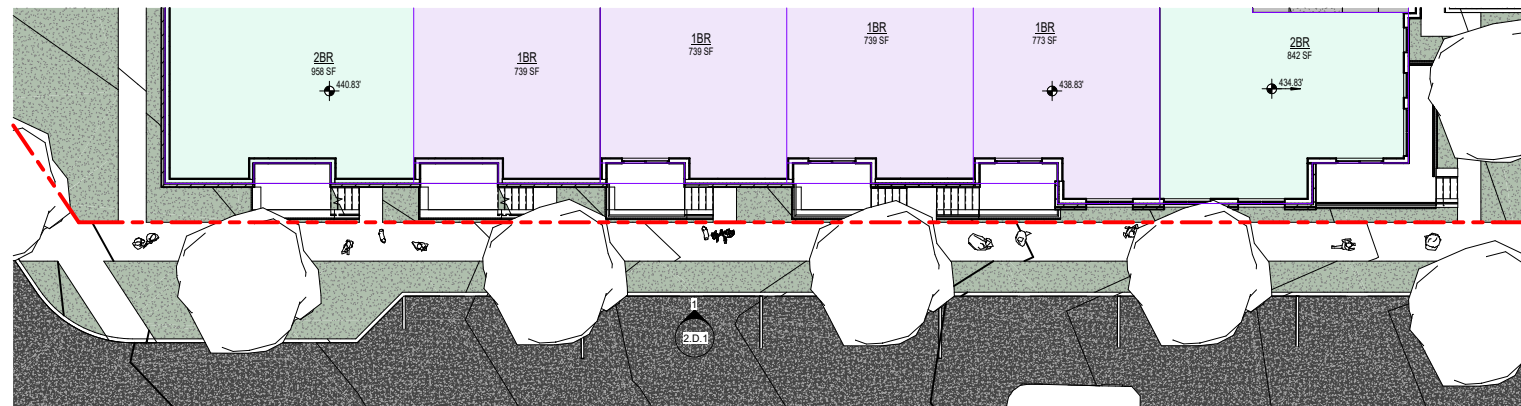
Typical Stoops

- EL 506' - 0" BLDG C - ROOF PLATE
- EL 495' - 6" BLDG C - LEVEL 5
- EL 484' - 10" BLDG C - LEVEL 4
- EL 474' - 2" BLDG C - LEVEL 3
- EL 463' - 6" BLDG C - LEVEL 2
- EL 447' - 6" BLDG B&C - LEVEL 1 & P1
- EL 437' - 10" BLDG B&C - LEVEL P2

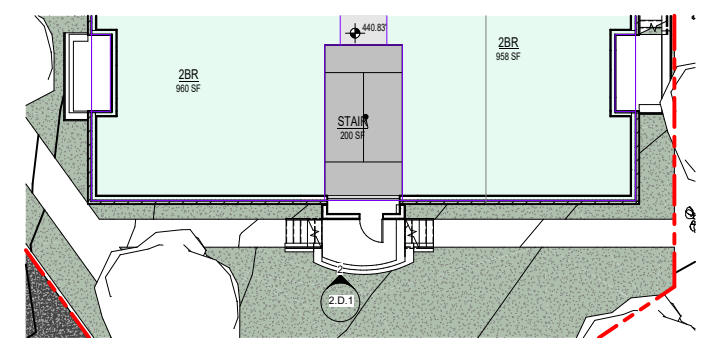


# TRACT 3 BUILDING ELEVATIONS

A 4-story building was added to Tract 3 with stoops to help activate Andrew T Whitmore.



ANDREW T WHITMORE ELEVATION



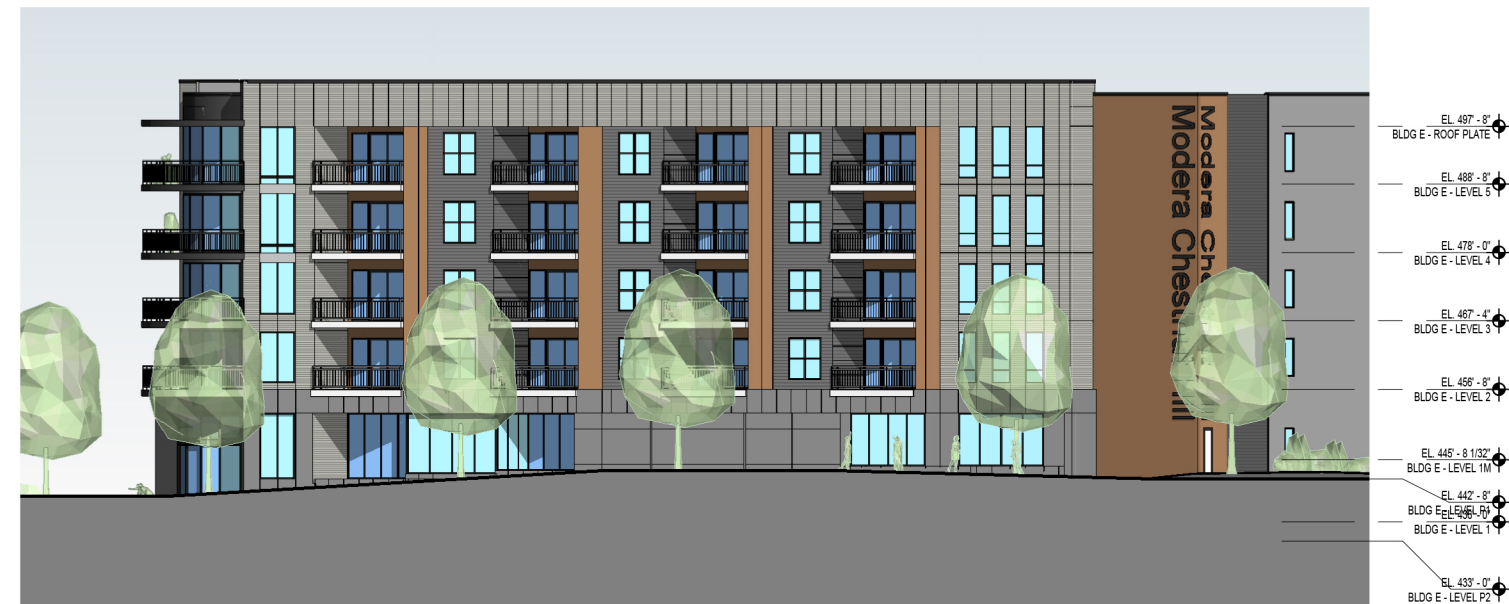
OLD RADNOR CAR LINE ELEVATION



# TRACT 4 BUILDING ELEVATIONS



ANDREW T WHITMORE STREET ELEVATION



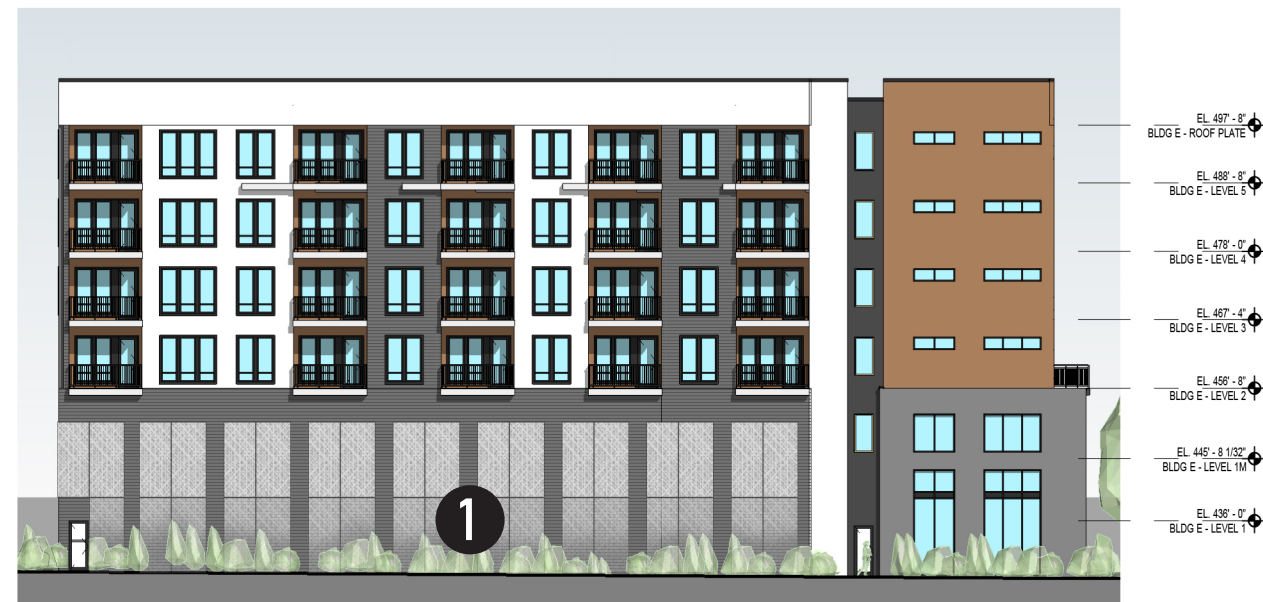
OLD RADNOR CAR LINE ELEVATION



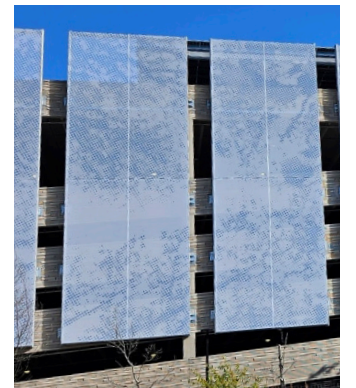
# TRACT 4 BUILDING ELEVATIONS



SOUTH ALLEY ELEVATION



EAST ALLEY ELEVATION



1 Perforated metal screen inspiration





Connectivity: Bus, & Pedestrian Access

## ACCESS

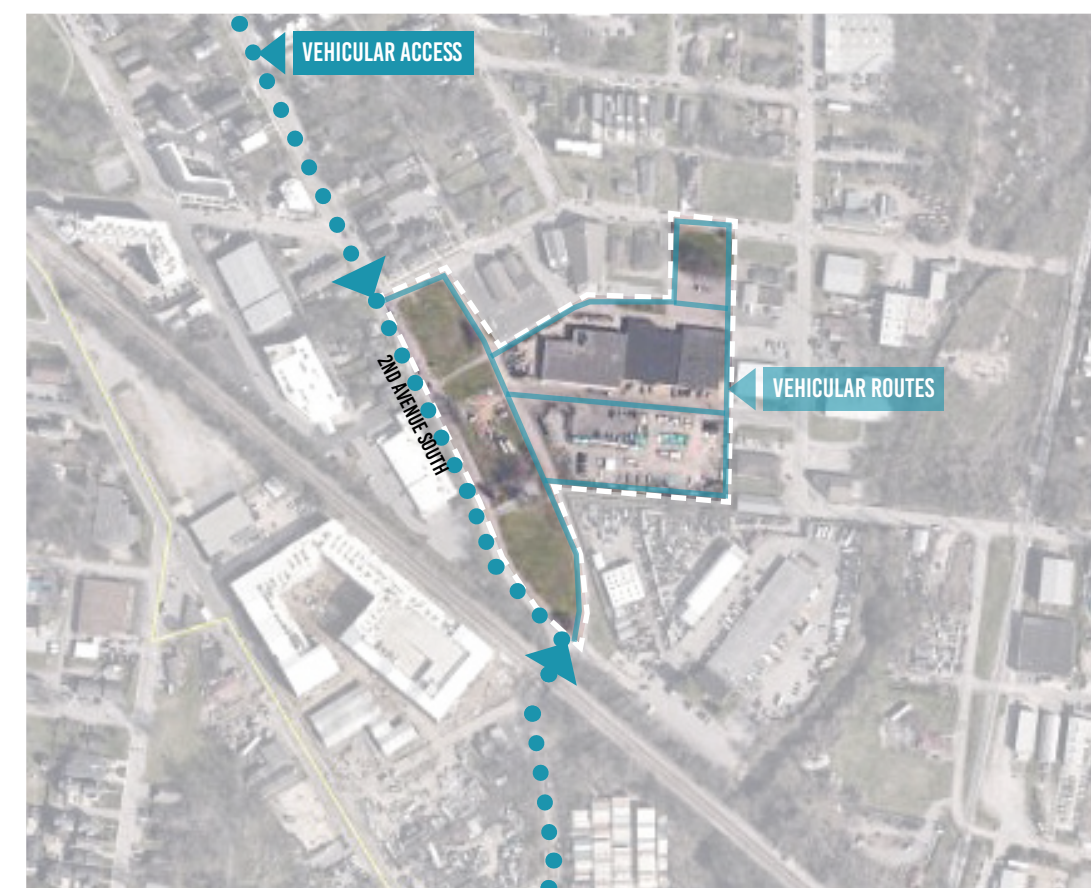
The site's primary access point is planned by way of 2nd Avenue South, which connects to the rest of Chestnut Hill and downtown Nashville to the north and Wedgewood-Houston to the south. The east side of 2nd Avenue South is planned to increase the existing ~5' sidewalks to 10' and provide 8.5' landscape buffers which will be lined with trees every ~30'.

The property's central boundary on Andrew T Whitmore would lay the foundation for a vibrant pedestrian experience with the dog park and pickleball courts on the street's north side and ~8 live/work units on the street's south side. While Andrew T Whitmore is currently a local street without any sidewalks, the street is planned to have new ~5' sidewalks, landscaping, and parallel parking on both north and south sides of the street. Old Radnor Car Line Road will also be improved with new ~5' sidewalks, landscaping, and parallel parking on the west side of the street. None of the existing roads or alleys within the site will be closed as part of this development.

## CONNECTIVITY

Located east of 2nd Avenue South and on the southern end of the Chestnut Hill neighborhood, The Modera Chestnut Hill site is within 5 minutes of Downtown Nashville. The neighborhood is known for its mix of historic and modern homes and unique retail shops and restaurants. Chestnut Hill can be accessed from Downtown Nashville via 2nd Avenue South by vehicular access and by Route 52 WeGo Bus. Nearby No. 52 Nolensville bus route stops are located on 2nd Ave S at Hart St, and on 4th Ave N at Rains Ave. WeGo would support off-site sidewalk and crosswalk improvements to improve transit accessibility. Developer shall liaise and reach agreement with WeGo Robert.Johnson@Nashville.gov about transit accessibility improvements prior to Final SP.

The intent of the Modera Chestnut Hill project is to improve the walkability and streetscape of the site and create public amenities that can be shared by both Modera Chestnut Hill residents and neighbors. The privately-owned, public dog park and pickleball courts along with the ~8 planned live/work units will provide much needed services and benefits to the immediate area. As 2nd Avenue South continues to improve into a vibrant pedestrian experience, Modera Chestnut Hill will further the neighborhood's cohesive blend of historic and modern development.



Accessibility: Entrances & Parking



# REGULATIONS





Development Standards	
	(Full Build Out)
Total Acreage	5.79 Acres
FAR	3 Max.
Building/ Structure Type	Type V; Type III; Podium Type I
Max. Building Height	5 Stories
Impervious Surface Ratio	Max. 0.9
Building Coverage	21%
Building Stepback	N/A
Front Setbacks	0' - 15' Built-To-Line MUL-A-NS
Side Setbacks	0
Rear Setback	0' - 15'
Perimeter Landscaping Buffer	Per Landscape Plan
Parking	1.50x/unit, per SP
Permitted Uses:	All uses permitted per MUL-A-NS, Community Garden, Mobile Vendor, Dog Park, Sports Court, Community Space, Live/Work Units defined as rental units that can be utilized as a residential unit and a workspace or solely a residential unit
Prohibited Uses:	Short-Term Rental Property (STRP), Owner-Occupied and Short-Term Rental Property (STRP) not Owner-Occupied uses shall be prohibited
Residential Uses	400 dwelling units Max.
Non-Residential uses	12,000 GSF Max.
Default Zoning District	MUL-A-NS

DEVELOPMENT STANDARDS

Council District 17: Terry Vo  
SP Name: Modera Chestnut Hill  
SP Number: 2025SP-AAA-AAA  
Plan Preparation Date: June 11, 2025

Owners of Record:

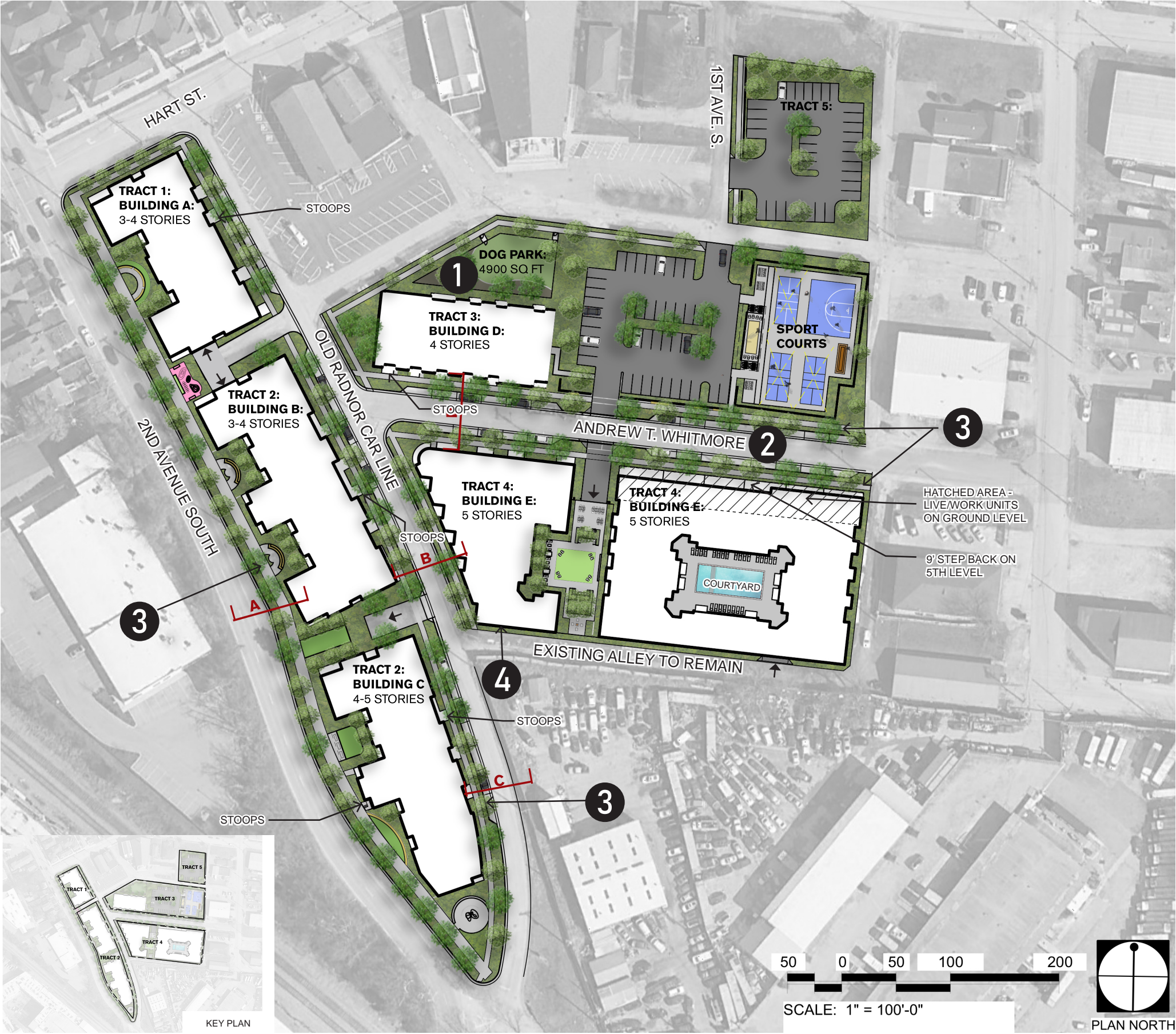
Clarence L. Steinhouse III  
John T. Steinhouse  
James W. Steinhouse

Design Professional:

Greg Miller, Principal  
Cooper Carry  
191 Peachtree Street, Suite 2400  
Atlanta, Georgia 30303  
404.237.2000

- 1 PRIVATELY MANAGED PUBLIC DOG PARK
- 2 PUBLIC NEIGHBORHOOD AMENITIES
- 3 NEW/EXPANDED SIDEWALKS AND LANDSCAPING
- 4 EXTERIOR MURAL ARTWORK
- ← PARKING ENTRY

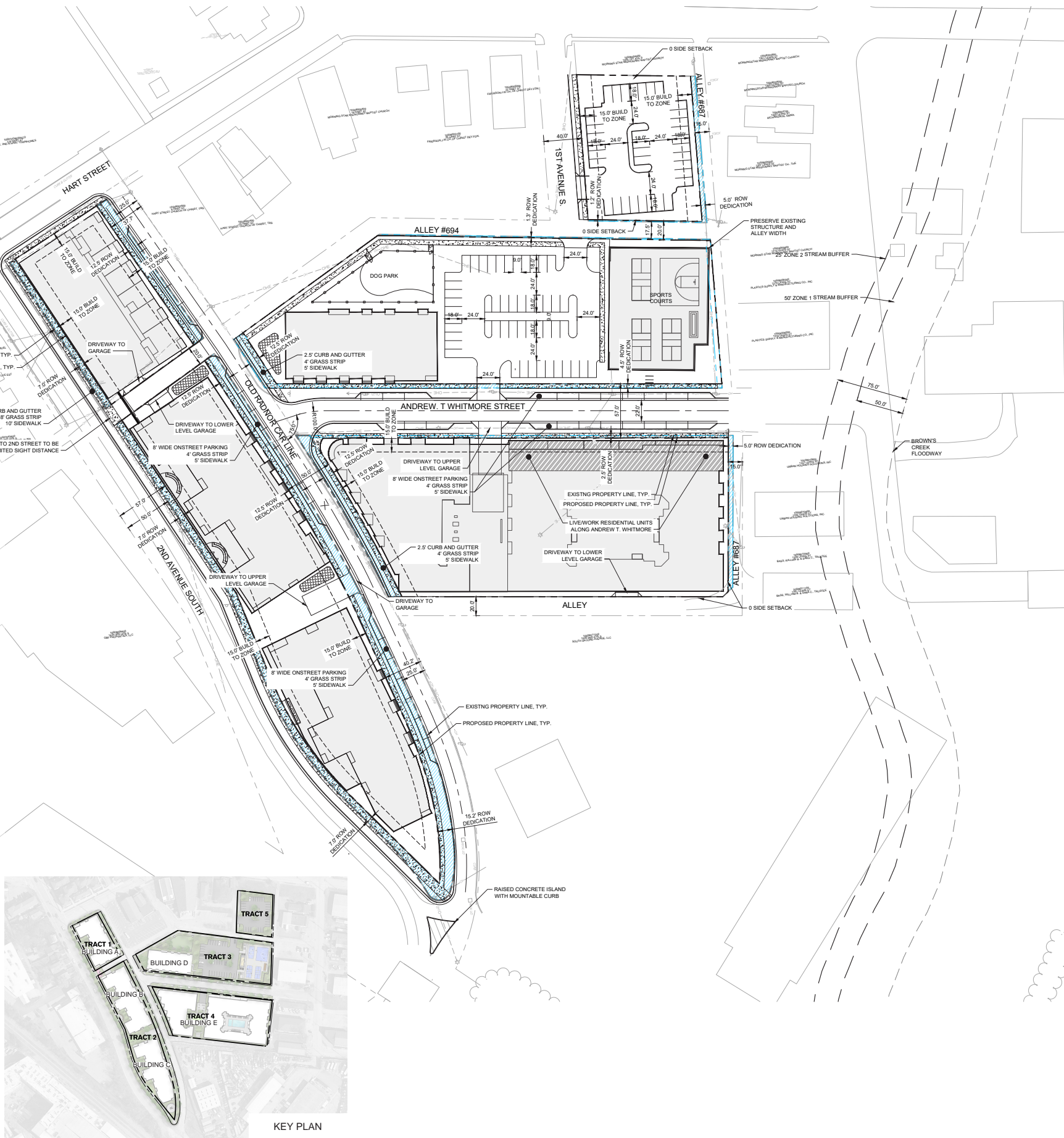
Note: The Final Site Plan/ Building Permit Site Plan shall depict the required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.





# SITE PLAN

Site Plan Data Table	
Site Address:	See Page 6
Existing Zoning:	IWD; MUL-A-NS; R6
Existing Policy:	14 MU; CO
Proposed Zoning:	SP
Allowed Uses:	MUL-A-NS
Fallback Zoning:	Tracts One, Two, Three, Four; MUL-A-NS
Acreage:	Tract Five: RM20-A-NS +/- 5.79 AC (Per Survey)
Density:	Maximum of 400 Multi-Family Units, including a maximum of 15 Live/Work Units
Floor Area Ratio:	Total: +/- 1.75, Max 3.00 per MUL-A-NS
	Tract One: 2.08
	Tract Two: 2.06
	Tract Three: 0.64
	Tract Four: 2.71
	Tract Five: 0.00
Impervious Surface Ratio:	Max 0.90
Parking Required:	Maximum 1.0/1-bed, 1.5/2-bed or more, per U20 Standards
Parking Provided:	1.50x/unit, per SP
	Tract One: 35
	Tract Two: 187
	Tract Three: 42
	Tract Four: 260
	Tract Five: 32
Square Footages:	Total: +/- 426,000 GSF
	Tract I: +/- 49,000 GSF
	Tract II: +/- 142,000 GSF
	Tract III: +/- 46,000 GSF
	Tract IV: +/- 189,000 GSF
	Tract V: 0.00 GSF
Building Height:	Tract One (Building A): Average grade = 444.33 Height from average Grade=39.65' 4 stories on Old Radnor Car Line 3 stories on 2nd Ave Tract Two (Building B): Average Grade=450.25 Height from Average Grade= 41.5' 4 stories on Old Radnor Car Line 3 stories on 2nd Ave Tract Two (Building C): Average Grade=451.25 Height from Average Grade= 51.25' 5 stories on Old Radnor Car Line 4 stories on 2nd Ave Tract Three (Building D): Average Grade=434.15' Height from Average Grade= 52' 4 stories on Andrew T Whitmore Tract Four (Building E): Average Grade=434' Height from Average Grade= 66.34' 5 stories on Andrew T. Whitmore St
Permitted Uses:	All uses permitted per MUL-A-NS, Community Garden, Mobile Vendor, Dog Park, Sports Court, Community Space, Live/Work Units defined as rental units that can be utilized as a residential unit and a workspace or solely a residential unit
Prohibited Uses:	Short-term Rental Property (STRP), Owner-Occupied and Short-Term Rental Property (STRP); no Owner Occupied uses shall be permitted
Non-residential uses:	Only allowed within Tract Three and Four
Residential Uses:	400 dwelling units Max.



- ### SITE LAYOUT NOTES
1. INSTALL CONCRETE JOINTS WHERE SHOWN ON PLANS AND DETAILS. ALIGN ON WALLS, BUILDINGS, RADA, ETC. EVENLY SPACE BETWEEN ELEMENTS AS SHOWN. PROVIDE EXPANSION JOINTS BETWEEN CONCRETE PAVEMENT AND ALL VERTICAL ELEMENTS (WALLS, CURBS, ETC.).
  2. LAYOUT ALL CURVES SMOOTHLY WITH NO ABRUPT CHANGES AT TANGENT POINTS.
  3. ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
  4. REFER TO IRRIGATION NOTES L1-03.
  5. LAYOUT ALL ELEMENTS IN FIELD AND CONTACT OWNER'S REPRESENTATIVE FOR APPROVAL BEFORE BEGINNING ANY CONSTRUCTION.
  6. CONTRACTOR TO TAKE ALL PRECAUTIONS TO FIND AND AVOID SITE UTILITIES. ALL UTILITIES ARE NOT SHOWN ON DRAWING. VERIFY LOCATIONS AND CONSIDER SUCH WHEN ESTIMATING.
  7. ALL LANDSCAPE ISLANDS SHALL BE MOUNDED WITH TOPSOIL 4" ABOVE THE CURB LINE TO PROMOTE POSITIVE DRAINAGE.
  8. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
  9. ALL PAVEMENT MARKINGS AND SIGNAGE SHALL BE INSTALLED PER THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
  10. ALL SIDEWALK WITHIN THE RIGHT-OF-WAY SHALL BE CONSTRUCTED PER MPW STANDARD DWG NO. ST-210.
  11. SAW CUT LINES SHALL BE DONE IN A STRAIGHT NEAT LINE A MINIMUM OF 18" FROM THE EXISTING EDGE OF PAVEMENT.
  12. REFER TO ARCHITECTURAL PLAN FOR STRIPING WITHIN THE BUILDING.
  13. ALL TREE GRATES SHALL BE ADA COMPLIANT AND 400 TRAFFIC RATED.
  14. REFER TO SITE LANDSCAPE PLANS BY LANDSCAPE ARCHITECT FOR STREETSCAPE DETAILS.

- ### NDOT CONSTRUCTION NOTES
1. ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY REQUIRES AN EXCAVATION PERMIT FROM NDOT.
  2. PROOF-ROLLING OF ALL STREET SUB-GRADES IS REQUIRED IN THE PRESENCE OF THE NDOT INSPECTOR. REQUESTS TO BE MADE 24 HRS. IN ADVANCE.
  3. STOP SIGNS TO BE 30 INCH BY 30 INCH.
  4. ALL ROW DEDICATIONS ARE TO HAVE 6 INCH TALL WHITE LETTERS ON 9 INCH GREEN ALUMINUM BLADE, AND BE VERTICALLY STAGGERED.
  5. STREET NAME SIGNS SHALL BE ASSEMBLED USING EXTRUDED SIGN BLADES.
  6. ALL SIGNS SHALL HAVE 3M REFLECTIVE COATING.

- ### PUBLIC WORKS NOTES
1. THE DEVELOPER'S FINAL CONSTRUCTION DRAWINGS SHALL COMPLY WITH THE DESIGN REGULATIONS ESTABLISHED BY THE DEPARTMENT OF PUBLIC WORKS. FINAL DESIGN MAY VARY BASED ON FIELD CONDITIONS.
  2. ALL ROW DEDICATIONS ARE TO BE RECORDED PRIOR TO ISSUANCE OF THE BUILDING PERMITS.
  3. ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY REQUIRES AN EXCAVATION PERMIT FROM THE DEPARTMENT OF PUBLIC WORKS.
  4. PROOF-ROLLING OF ALL STREET SUB-GRADES IS REQUIRED IN THE PRESENCE OF THE PUBLIC WORKS INSPECTOR. REQUEST TO BE MADE 24 HOURS IN ADVANCE.
  5. STOP SIGNS TO BE 30 INCH BY 30 INCH.
  6. STREET SIGNS TO HAVE SIX INCH WHITE LETTERS ON A NINE INCH GREEN ALUMINUM BLADE AND BE MOUNTED VERTICALLY STAGGERED.
  7. ALL SIGNS TO HAVE 3M REFLECTIVE COATING.
  8. REFUSE COLLECTION, RECYCLING AND MECHANICAL EQUIPMENT SHALL BE FULLY SCREENED FROM PUBLIC VIEW BY THE COMBINATION OF FENCES, WALLS AND LANDSCAPING.
  9. HVAC UNITS SHALL BE LOCATED AT THE REAR HALF OF THE SIDE UNIT, BEHIND THE UNIT, OR ON THE ROOF OF EACH BUILDING. HVAC UNITS ON ROOF MUST BE SCREENED FROM VIEW ALONG PRIMARY AND SECONDARY FRONTAGES.
  10. THE FINAL SITE PLAN/BUILDING PERMIT SITE PLAN SHALL DEPICT ANY REQUIRED PUBLIC SIDEWALKS, ANY REQUIRED GRASS STRIP OR FRONTAGE ZONE AND THE LOCATION OF ALL EXISTING AND PROPOSED VERTICAL OBSTRUCTIONS WITHIN ANY REQUIRED SIDEWALK AND GRASS STRIP OR FRONTAGE ZONE. PRIOR TO THE ISSUANCE OF USE AND OCCUPANCY PERMITS, EXISTING VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUTSIDE OF ANY REQUIRED SIDEWALK. VERTICAL OBSTRUCTIONS ARE ONLY PERMITTED WITHIN ANY REQUIRED GRASS STRIP OR FRONTAGE ZONE.

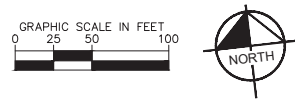
### METRO PERMITTING REFERENCE

BUILDING PERMIT:	<b>TBD</b>
GRADING PERMIT:	<b>TBD</b>
WATER PERMIT:	<b>TBD</b>
SANITARY SEWER PERMIT:	<b>TBD</b>

### PAVEMENT LEGEND

	LIGHT DUTY CONCRETE
	HEAVY DUTY CONCRETE
	GRASS
	RIGHT OF WAY DEDICATION

\* REFERENCE C8-00 FOR TYPICAL PAVEMENT SECTIONS



COORDINATES ARE  
NAD83.  
ALL ELEVATIONS ARE  
REFERENCED TO THE  
NAVD 1988.

KEY PLAN



STANDARD NOTES

- 1. The purpose of this SP is to receive preliminary approval to permit a Multifamily development with a maximum of 400 multi-family residential units. Short term rental property (STRP), owner-occupied, and short term rental property (STRP), non owner-occupied, uses are prohibited.
- 2. For any development standard, regulations and requirements not specifically shown on the SP plan and/or included as a condition of Council approval, the property shall be subject to the standards, regulations, and requirements of MUL-A-NS base zoning as of the date of the application request or application.
- 3. Modifications to the preliminary SP plan may be approved by the Planning Commission or its design based upon final architectural, engineering, or site design and actual site conditions. All modifications shall be consistent with the principles and further objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by the Metro Council that increase the permitted density and floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance.
- 4. The development is currently planned to be constructed in a single phase and planning and design for the Final SP will occur after approval of the preliminary SP by Metro Planning Commission.
- 5. Short term rental property (STRP), Owner-Occupied and Short-Term Rental Property (STRP) uses shall not be permitted.

FEMA NOTES

- 6. The project will meet the requirements of the development standards.
- 7. Some of the property does lie in a flood hazard zone, but will be mitigated during the design and construction phase of the project.

ARCHITECTURAL STANDARDS

- 8. New buildings shall avoid continuous uninterrupted blank facades. At a minimum, the facade plane shall be interrupted by one of the following for thirty (30) linear feet of street frontage:
  - a. A change in building material or building openings
  - b. A horizontal undulation in the building facade of two (2) feet or greater
  - c. A porch, stoop, window or balcony; porches shall be a min. six (6) feet in depth
  - d. A mural or other form of art installation
- 9. Building facades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 20% glazing for residential uses and 30% glazing for non-residential uses.

- 10. Refuse collection, recycling and mechanical equipment shall be screened from public view by the combination of fences, wall, or landscaping
- 11. Building facades shall be constructed of brick, brick veneer, stone, cast stone, cementitious siding, and glass, or materials substantially similar in form and function, unless otherwise approved on detailed building elevations included with the preliminary SP.
- 12. HVAC units shall be located on the roof of each building.
- 13. Bicycle parking will be provided per the Metro Zoning code. Bicycle parking locations to be identified in the Final Site Plan.
- 13-1. Building Height shall be measured per Metro Zoning Code.

NDOT NOTES

- 14. The final site plans and building permits shall depict the required public sidewalks, any required grass strip or frontage zone, and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Where feasible, vertical obstructions are only permitted within the required grass strip or frontage zone.
- 15. The development totals represented in the Traffic Impact Study represent the current target development scenarios for the maximum residential units and non-residential SF. The final allocation, density and intensity of the proposed uses may adjust based on market conditions prior to FINAL SP application. If the development program allocation changes, a revised traffic study may be required for Metro review and approval prior to the filling of the FINAL SP applications. All Metro recommended improvements must be provided within the FINAL SP applications per any revised and approved traffic study.
- 16. All parking regulations to meet 1.5x/unit.
- 17. Roadway improvements that are direct result of this specific project as determined by the approved Traffic Improvement Study and the Nashville DOT shall be constructed.
- 18. Any new improvements within existing public right-of-way within the project site that is identified as necessary to meet the adopted roadway plans shall be dedicated.
- 19. The developer’s final construction drawings shall comply with the design regulations established by NDOT, in effect at the time of the approval of the preliminary development plan or final development plan or building permit, as applicable. Final designs may vary based on field conditions.
- 20. The design of the public infrastructure is to be coordinated with the Final SP. The roads, pedestrian infrastructure, bicycle routes, etc are to be designed and constructed per NDOT standards and specifications.
- 21. Back of house, loading zones, vehicular and pedestrian access

- points, and code required bicycle parking will be indicated in the Final SP.
- 22. All construction within the right of way shall comply with ADA and NDOT Standards and Specifications.
- 23. Where feasible, vertical obstructions shall be relocated out of the proposed sidewalks where applicable.

LANDSCAPE STANDARDS

- 24. The developer of this project shall comply with the requirements of the SP and adopted tree ordinance.
- 25. Street trees shall be provided, irrigated, and maintained by Owner along all street frontages at a minimum spacing average of 40 linear feet. All street trees placed within ROW shall count toward tree density unit credit outlined in Metro Zoning Code 17.24.

FIRE MARSHAL NOTES

- 27. New commercial developments shall be protected by a fire hydrant that complies with the 2006 edition of NFPA 1 Table H. To see Table H go to: (<http://www.nashfire.org/prev/tableH51.htm>)
- 28. No part of any building shall be more than 500 feet from a fire hydrant via a hard surface road. Metro Ordinance 095-1541 Sec. 1568.020 B297. The development will have fire department access roads that shall be 18 feet minimum width and shall have an unobstructed vertical clearance of 13.5 feet.
- 29. A fire hydrant shall be provided within 100 feet of the fire department connection.
- 30. Fire hydrants shall be in-service before any combustible material is brought on site.
- 31. Fire apparatus access roads shall be provided and maintained in accordance with the adopted fire code and standards.
- 32. Except as approved by the fire code official; fire apparatus access roads shall extend to within 150 feet of all portions of the facility and all portions of the exterior walls of the first story of the building as measured by an approved route around the exterior of the building or facility.
- 33. Buildings over 30 feet in height shall meet fire department aerial apparatus access requirements.
- 34. Dead end fire apparatus access roads in excess of 150 feet shall be provided with an approved fire apparatus turnaround.
- 35. All points of the building shall be within 500 feet of a fire hydrant when measured via approved fire apparatus access route.
- 36. All buildings and/or developments are required to meet the fire-flow requirements listed in the adopted code prior to construction.
- 37. Fire department connections for standpipe/sprinkler system shall



be within 100 feet of the fire hydrant via approved access route.

38. Developments of one- or two-family dwelling units where the number of dwelling units exceeds 30 shall be

provided with two separate and approved fire apparatus access roads.

39. Buildings exceeding 30 feet in height or 62,000 square feet in area (124,000 fully sprinklered) shall be provided with two separate and approved fire apparatus access roads.

40. Where two separate and approved fire apparatus access roads are required, they shall be placed a distance apart equal to not less than one-half of the length of the maximum overall diagonal dimension of the property or area to be served, measured in a straight line between accesses. The AHI may approve variations to this requirement in the event remoteness cannot be accomplished.

41. The maximum grade for fire apparatus access roads shall not exceed 10% without approval from the fire code official.

42. Gates across fire apparatus access roads shall comply with adopted code and standards.

43. Approval of a preliminary or final site plan is not an approval for building construction. Full and complete review of building plans is required prior to approval for construction and may require changes to the site.

#### NES NOTES

44. Where feasible, this development will be served with underground power and pad-mounted transformers.

45. New facilities will not be allowed to sit in or to pass through retention areas, including rain gardens, bio-retention areas, bioswales, and the like. This includes primary duct between pad-mounted transformer equipment as well as service duct to a meter.

#### STORMWATER NOTES

46. Any excavation, fill, or disturbance of the existing ground elevation must be done in accordance with Stormwater Management Ordinance No. 78-840 and approved by the Metropolitan Department of Water Services.

47. Metro Water Services shall be provided sufficient and unencumbered ingress and egress at all times in order to maintain, repair, replace, and inspect any stormwater facilities within the property.

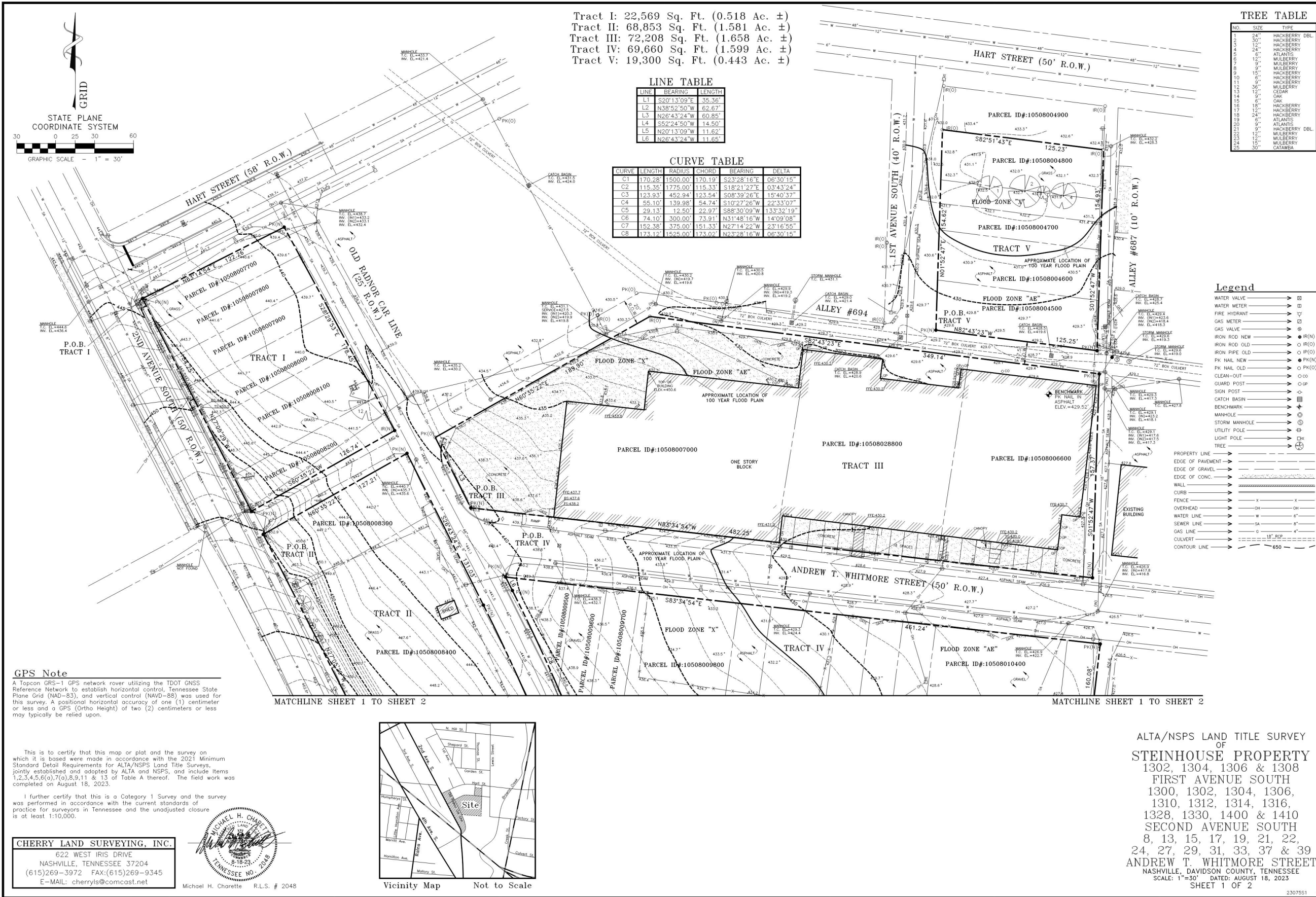
48. Size driveway culverts per the design criteria set forth by the Metro Stormwater Management Manual. (Minimum driveway culvert in Metro ROW is 15" RCP).

49. The project intent is for the site to be redeveloped per the requirements of the current Stormwater Management Manual.

#### FEDERAL COMPLIANCE

50. All development within the boundaries of this plan will meet the requirements of the Americans with Disabilities Act and the Fair Housing Act



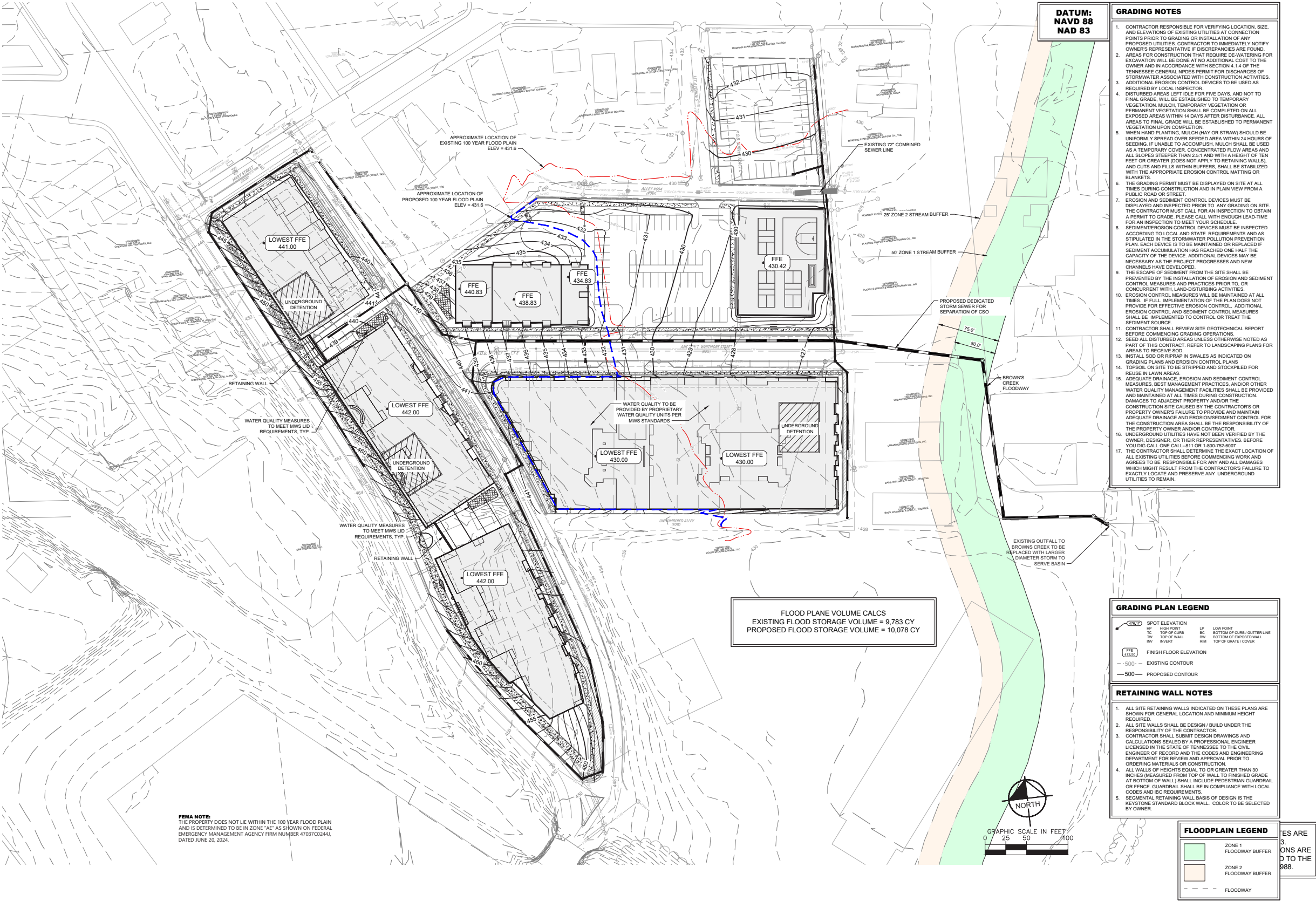




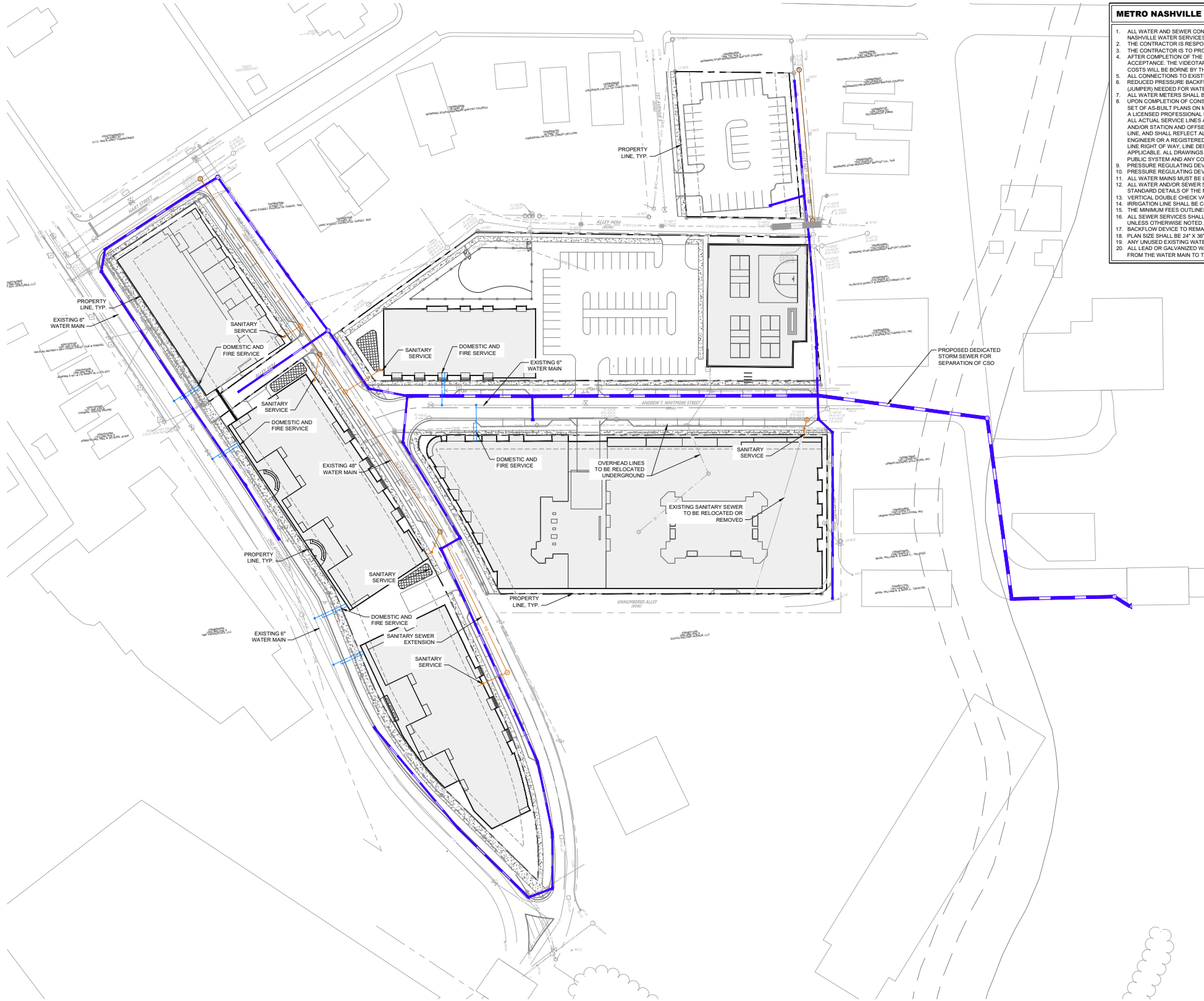




# GRADING PLAN & DRAINAGE







**METRO NASHVILLE WATER SERVICES STANDARD NOTES**

1. ALL WATER AND SEWER CONSTRUCTION SHALL BE IN ACCORDANCE WITH SPECIFICATIONS AND STANDARD DETAILS OF THE METRO NASHVILLE WATER SERVICES.
2. THE CONTRACTOR IS RESPONSIBLE FOR REIMBURSING THE METRO NASHVILLE WATER SERVICES THE COST OF INSPECTION.
3. THE CONTRACTOR IS TO PROVIDE AND MAINTAIN THE CONSTRUCTION IDENTIFICATION SIGN FOR PRIVATE DEVELOPMENT APPROVED.
4. AFTER COMPLETION OF THE SANITARY SEWER, THE DEVELOPER IS RESPONSIBLE FOR THE TELEVISIONING OF THE LINES PRIOR TO FINAL ACCEPTANCE. THE VIDEO TAPING MUST BE COORDINATED WITH THE METRO NASHVILLE WATER SERVICES INSPECTION SECTION. ALL COSTS WILL BE BORNE BY THE DEVELOPER.
5. ALL CONNECTIONS TO EXISTING MANHOLES SHALL BE BY CORING AND RESILIENT CONNECTOR METHOD.
6. REDUCED PRESSURE BACKFLOW PREVENTION DEVICES (RPBP) OR DUAL CHECK VALVE WILL BE REQUIRED ON ALL TEST AND FILL LINES (JUMPER) NEEDED FOR WATER MAIN CONSTRUCTION AND MUST BE APPROVED BY THE METRO NASHVILLE WATER SERVICES.
7. ALL WATER METERS SHALL BE A MINIMUM OF 24" NOT TO EXCEED A MAXIMUM OF 28" BELOW FINISHED GRADE.
8. UPON COMPLETION OF CONSTRUCTION OF WATER AND/OR SEWER, THE ENGINEER SHALL PROVIDE THE DEPARTMENT WITH A COMPLETE SET OF AS-BUILT PLANS ON MOIST ERASABLE MYLARS IN REVERSE AND IN DIGITAL (.DWG) FORMAT. SEWER PLANS SHALL BE SEALED BY A LICENSED PROFESSIONAL ENGINEER OR A REGISTERED LAND SURVEYOR AND SHALL INCLUDE ACTUAL FIELD ANGLES BETWEEN LINES, ALL ACTUAL SERVICE LINES AND TEE LOCATIONS, THE DISTANCE OF THE END OF THE SERVICE LINE TO PROPERTY CORNERS AND LINES AND/OR STATION AND OFFSET FROM SEWER CENTERLINE TO END OF SERVICE LINE, THE DEPTH TO THE TOP OF THE END OF THE SERVICE LINE, AND SHALL REFLECT ALL ALIGNMENT AND GRADE CHANGES. WATER LINE PLANS SHALL BE SEALED BY A LICENSED PROFESSIONAL ENGINEER OR A REGISTERED LAND SURVEYOR AND SHALL INCLUDE OFFSET DISTANCE FROM THE ROADWAY CENTERLINE, OR PROPERTY LINE RIGHT OF WAY, LINE DEPTH, LOCATIONS OF HYDRANTS, VALVES, REDUCERS, TEES AND PRESSURE REDUCING DEVICES WHERE APPLICABLE. ALL DRAWINGS MUST BE COMPLETED AND SUBMITTED PRIOR TO ACCEPTANCE OF THE SEWERS OR WATER MAINS INTO THE PUBLIC SYSTEM AND ANY CONNECTIONS BEING MADE.
9. PRESSURE REGULATING DEVICES WILL BE REQUIRED ON THE CUSTOMER SIDE OF THE METER WHEN PRESSURES EXCEED 100 PSI.
10. PRESSURE REGULATING DEVICES WILL BE REQUIRED ON THE STREET SIDE OF THE METER WHEN PRESSURES EXCEED 150 PSI.
11. ALL WATER MAINS MUST BE LOCATED WITHIN THE PAVED AREA INCLUDING ALL BLOW-OFF ASSEMBLIES.
12. ALL WATER AND/OR SEWER SERVICES, ALONG WITH APPURTENANCES, SHALL BE INSTALLED IN ACCORDANCE WITH SPECIFICATIONS AND STANDARD DETAILS OF THE METRO NASHVILLE WATER SERVICES.
13. VERTICAL DOUBLE CHECK VALVE ASSEMBLIES, THAT ARE LOCATED IN INTERIOR ROOMS, CAN ONLY BE USED FOR FIRE SERVICES.
14. IRRIGATION LINE SHALL BE COPPER FROM THE METER TO THE BACKFLOW PREVENTER.
15. THE MINIMUM FEES OUTLINED IN THE CAPACITY LETTER MUST BE PAID BEFORE COMMERCIAL CONSTRUCTION PLANS CAN BE REVIEWED.
16. ALL SEWER SERVICES SHALL BE 6 INCHES IN DIAMETER, FROM CONNECTION AT THE MAIN UNTIL THE FIRST CLEAN OUT ASSEMBLY, UNLESS OTHERWISE NOTED.
17. BACKFLOW DEVICE TO REMAIN ACCESSIBLE AT ALL TIMES.
18. PLAN SIZE SHALL BE 24" X 36", AND SHALL SHOW CONTOURS AROUND METER BOXES.
19. ANY UNUSED EXISTING WATER METERS MUST BE CUT AND CAPPED AT PUBLIC MAIN.
20. ALL LEAD OR GALVANIZED WATER SERVICE LINES ENCOUNTERED WITH THIS PROJECT SHALL BE REINSTATED WITH COPPER OF LIKE SIZE FROM THE WATER MAIN TO THE METER BOX.

**UTILITY NOTES**

1. CONTRACTOR RESPONSIBLE FOR VERIFYING EXISTING ELEVATIONS COMPARED TO THOSE SHOWN ON PLAN PRIOR TO GRADING. NOTIFY OWNER'S REPRESENTATIVE IF DISCREPANCIES ARE FOUND.
2. AREAS FOR CONSTRUCTION THAT REQUIRE DE-WATERING FOR EXCAVATION WILL BE DONE AT NO ADDITIONAL COST TO THE OWNER.
3. ALL WATER AND SEWER MATERIALS AND CONSTRUCTION SHALL BE IN COMPLIANCE WITH METRO NASHVILLE WATER SERVICES STANDARD SPECIFICATIONS.
4. MAINTAIN MINIMUM 10 FEET HORIZONTAL SEPARATION BETWEEN WATER & SANITARY SEWER OR 18" VERTICAL SEPARATION AT CROSSING LOCATIONS.
5. REFER TO METRO NASHVILLE WATER SERVICES STANDARD SPECIFICATIONS FOR PIPE BEDDING REQUIREMENTS.
6. ALL WATER VALVES TO BE LOCATED OUTSIDE OF PAVED AREAS WHERE POSSIBLE.
7. THE CONTRACTOR SHALL PROVIDE A COMPLETE SET OF AS-BUILT DRAWINGS OF WATER & SANITARY SEWER CONSTRUCTION PRIOR TO THE ACCEPTANCE OF THE PUBLIC UTILITIES.
8. CONTRACTOR RESPONSIBLE FOR DISCONNECTING EXISTING WATER SERVICE LINES AT THE PUBLIC MAIN PER METRO WATER SERVICES STANDARDS. PAVEMENT REPAIR SHALL BE DONE IN ACCORDANCE TO METRO PUBLIC WORKS STANDARDS.

**MWS STANDARD PRIVATE UTILITY PLAN NOTES**

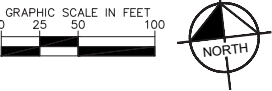
1. ALL WATER AND/OR SEWER SERVICES, ALONG WITH APPURTENANCES, SHALL BE INSTALLED IN ACCORDANCE WITH SPECIFICATIONS AND STANDARD DETAILS OF THE METRO WATER SERVICES.
2. ALL CONNECTIONS TO EXISTING MANHOLES SHALL BE BY CORING AND RESILIENT CONNECTOR METHOD.
3. VERTICAL DOUBLE CHECK VALVE ASSEMBLIES, THAT ARE LOCATED IN INTERIOR ROOMS, CAN ONLY BE USED FOR FIRE SERVICES.
4. ALL WATER METERS SHALL BE A MINIMUM OF 24" NOT TO EXCEED A MAXIMUM OF 28" BELOW FINISHED GRADE.
5. IRRIGATION LINE SHALL BE COPPER FROM THE METER TO THE BACKFLOW PREVENTER.
6. THE MINIMUM FEES OUTLINED IN THE CAPACITY LETTER MUST BE PAID BEFORE COMMERCIAL CONSTRUCTION PLANS CAN BE REVIEWED.
7. ALL SEWER SERVICES SHALL BE MINIMUM 6 INCHES IN DIAMETER, FROM CONNECTION AT THE MAIN UNTIL THE FIRST CLEAN OUT ASSEMBLY.
8. BACKFLOW DEVICE TO REMAIN ACCESSIBLE AT ALL TIMES.
9. PLAN SIZE SHALL BE 24" X 36", AND SHALL SHOW CONTOURS AROUND METER BOXES.
10. ANY UNUSED EXISTING WATER METERS MUST BE CUT AND CAPPED AT THE PUBLIC MAIN.
11. ALL LEAD OR GALVANIZED WATER SERVICE LINES ENCOUNTERED WITH THIS PROJECT SHALL BE REINSTATED WITH COPPER OF LIKE SIZE FROM THE WATER MAIN TO THE METER BOX.
12. DOMESTIC AND IRRIGATION WATER METERS AND ASSOCIATED APPURTENANCES SHALL NOT BE PLACED IN OR UNDER A PAVED OR IMPROVED SURFACE OTHER THAN THE PORTION OF THE SERVICE LOCATED WITHIN THE RIGHT OF WAY.
13. SANITARY SEWER TAPS SHALL BE PLACED AT THE LOWEST ADJACENT SEWER MAIN ELEVATION FOR EACH PREMISES AND SHALL NOT BE LOCATED IN OR UNDER A PAVED OR IMPROVED SURFACE OTHER THAN THE PORTION WITHIN THE RIGHT OF WAY.

**EXISTING UTILITIES NOTE**

CONTRACTOR SHALL VERIFY LOCATION OF ALL EXISTING UTILITIES AND NOTIFY ENGINEER IMMEDIATELY OF ANY DISCREPANCIES AND/OR CONFLICTS WITH EXISTING OR PROPOSED UTILITIES PRIOR TO PROCEEDING.

**VALVE NOTE**

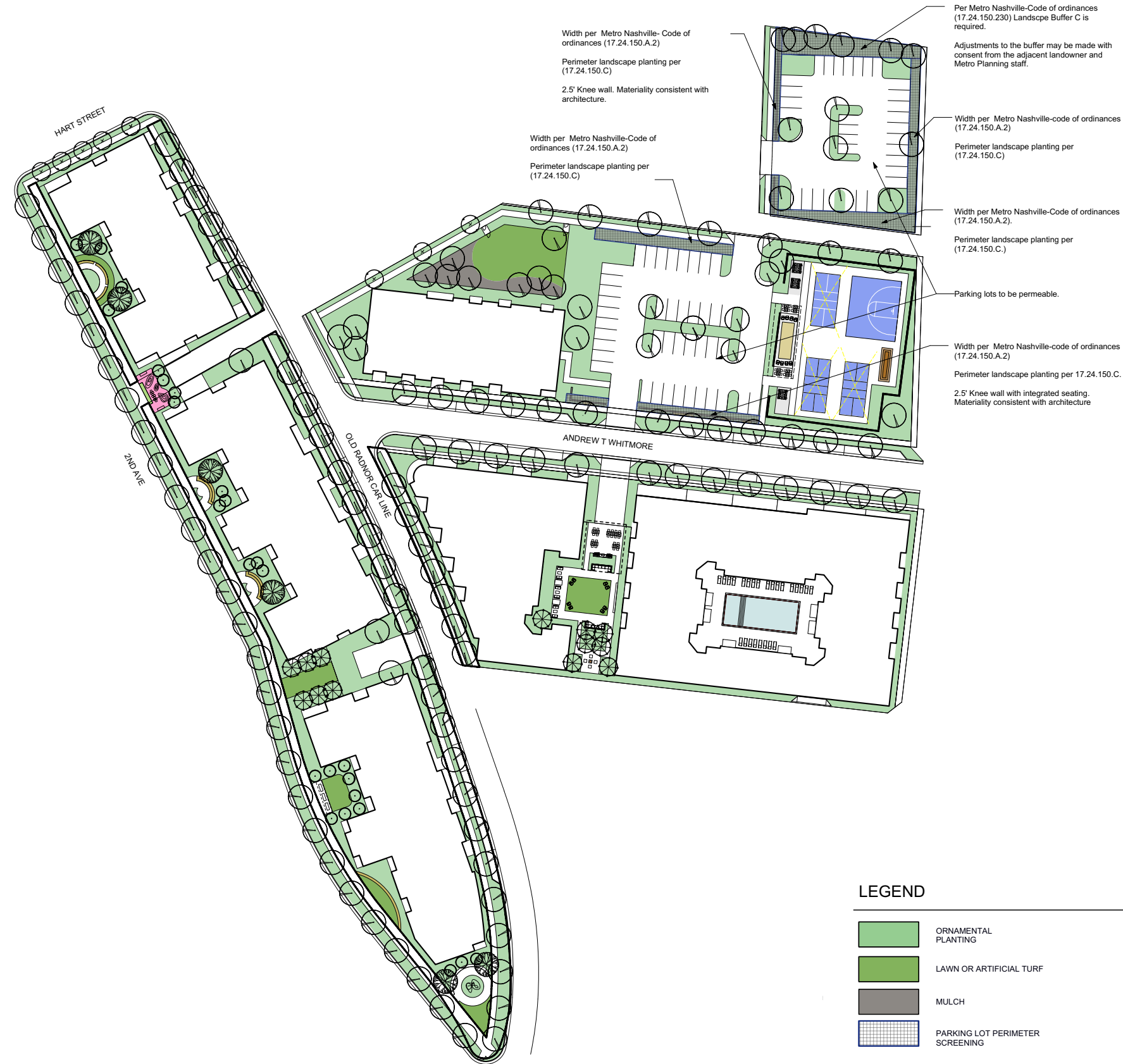
ALL VALVES SHALL BE INSTALLED IN METRO NASHVILLE WATER SERVICES APPROVED VALVE BOX AND COVER. COVER TO BE MARKED WITH "WATER".









COORDINATES ARE NAD83. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988.



# LANDSCAPE PLAN



## LEGEND

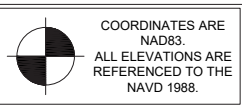
	ORNAMENTAL PLANTING		3" SHADE TREE
	LAWN OR ARTIFICIAL TURF		2" UNDERSTORY TREE
	MULCH		
	PARKING LOT PERIMETER SCREENING		

## NOTES:

1. Landscape requirements shall be per Metro Code.
2. Landscape Plan to be approved with Final Site Plan.



MODERA CHESTNUT HILL  
#2025SP-AAA-AAA







 **MILL CREEK**  
RESIDENTIAL