

Site Plan Preliminary SP.

633 WEST GREEN LANE DEVELOPMENT

(Inclusive of parcel 04900012900)

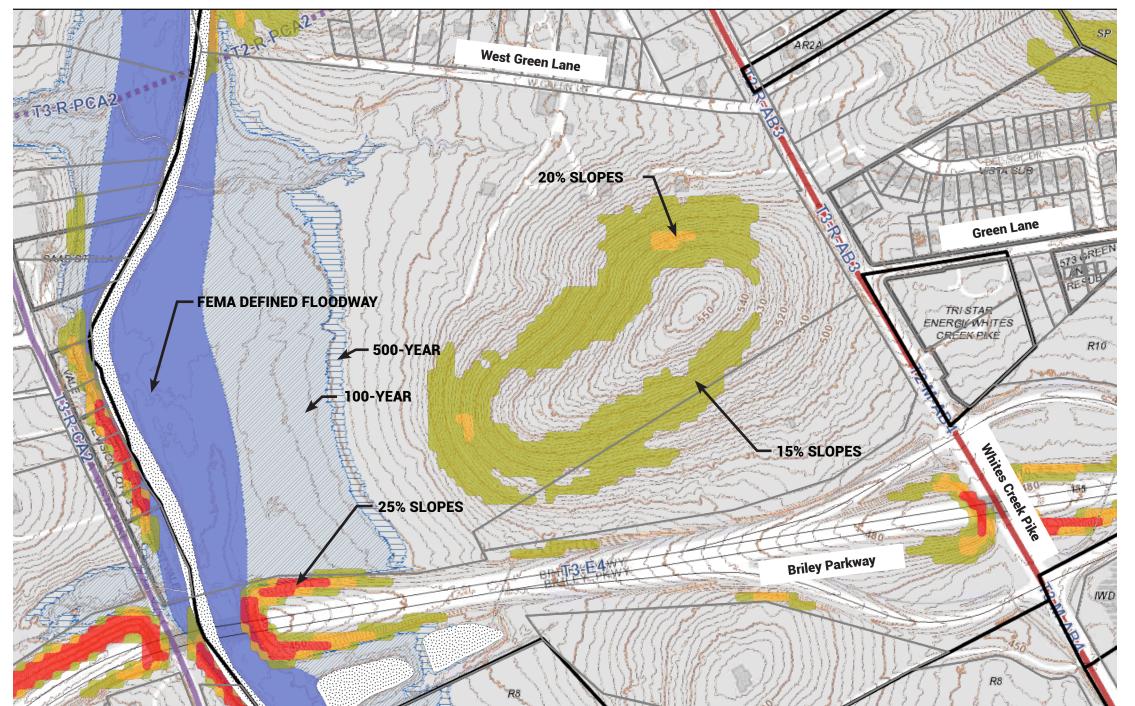
Case No. 2022SP-043-001



The Fifth Generation Property Company 1033 Demonbreun Street, #300 Nashville, TN 37203 Phone: 615.852.5060 Contact: Henry Menge henrym@fifthgenprop.com

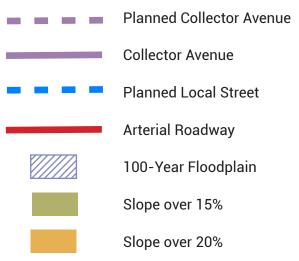


5100 Tennessee Avenue Nashville, TN 37209 Phone: 615.622.7200 Contact: Jeffrey Heinze jheinze@catalyst-dg.com Revised: December 01, 2022 Revised: November 28, 2022 Revised: October 17, 2022 Revised: October 04, 2022 Revised: August 19, 2022 Submitted: May 11, 2022



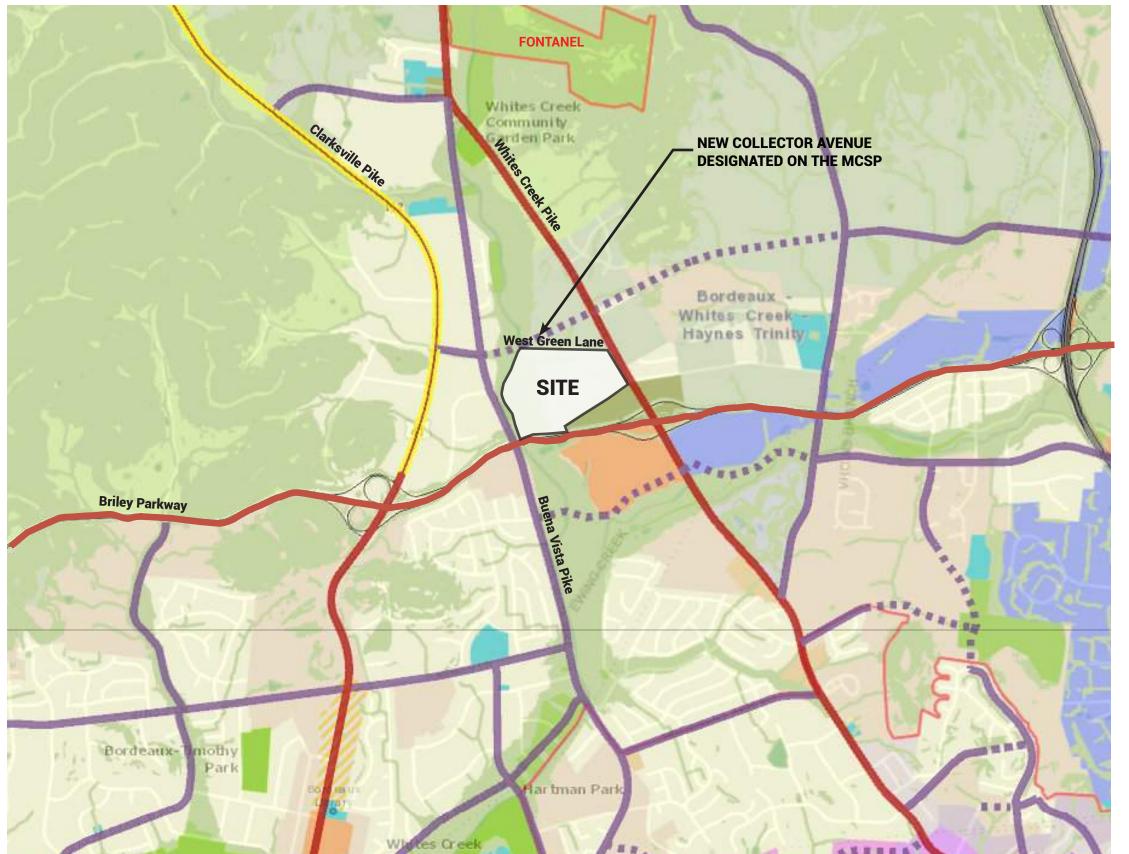
EXISTING CONDITIONS

LEGEND



VICINITY MAP





LAND USE CONTEXT

LEGEND

Major and Collector Street Plan

- Arterial-Boulevard Scenic
- Arterial-Boulevard
- Collector-Avenue
- ■ Planned Collector Avenue
- Planned Local Street

T2 Rural and T3 Suburban Policy Areas

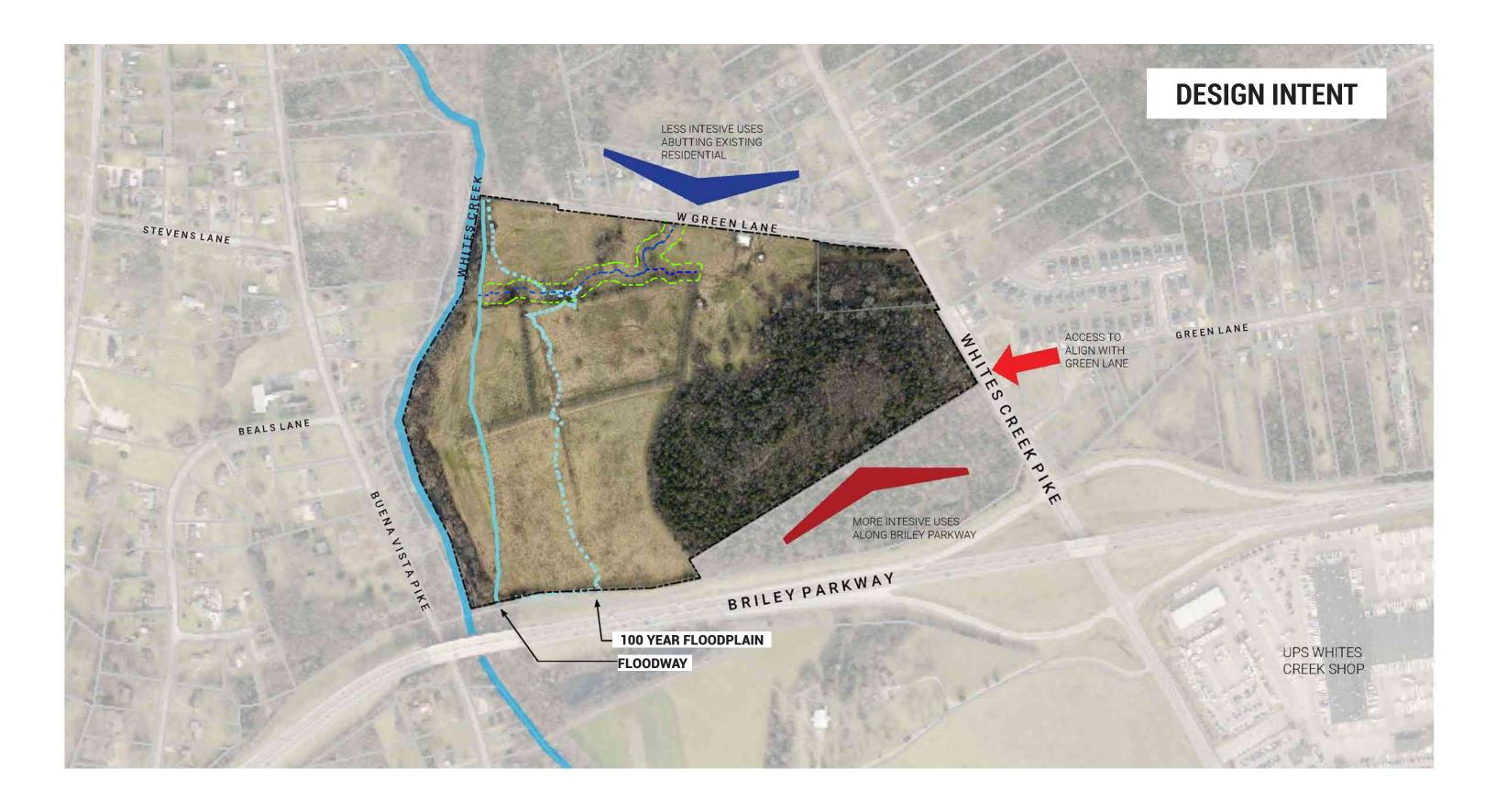
- T1 OS Natural Open Space
- T2 RA Rural Agriculture
- T2 RCS Rural Countryside
- T2 RM Rural Maintenance
- T2 NM Rural Neighborhood Maintenance
- T2 NC Rural Neighborhood Center
 - T3 NM Suburban Neighborhood Maintenance
 - T3 NE Suburban Neighborhood Evolving
- T3 RC Suburban Residential Corridor
 - T3 NC Suburban Neighborhood Center
- T3 CM Suburban Mixed Use Corridor
- T3 CC Suburban Community Center

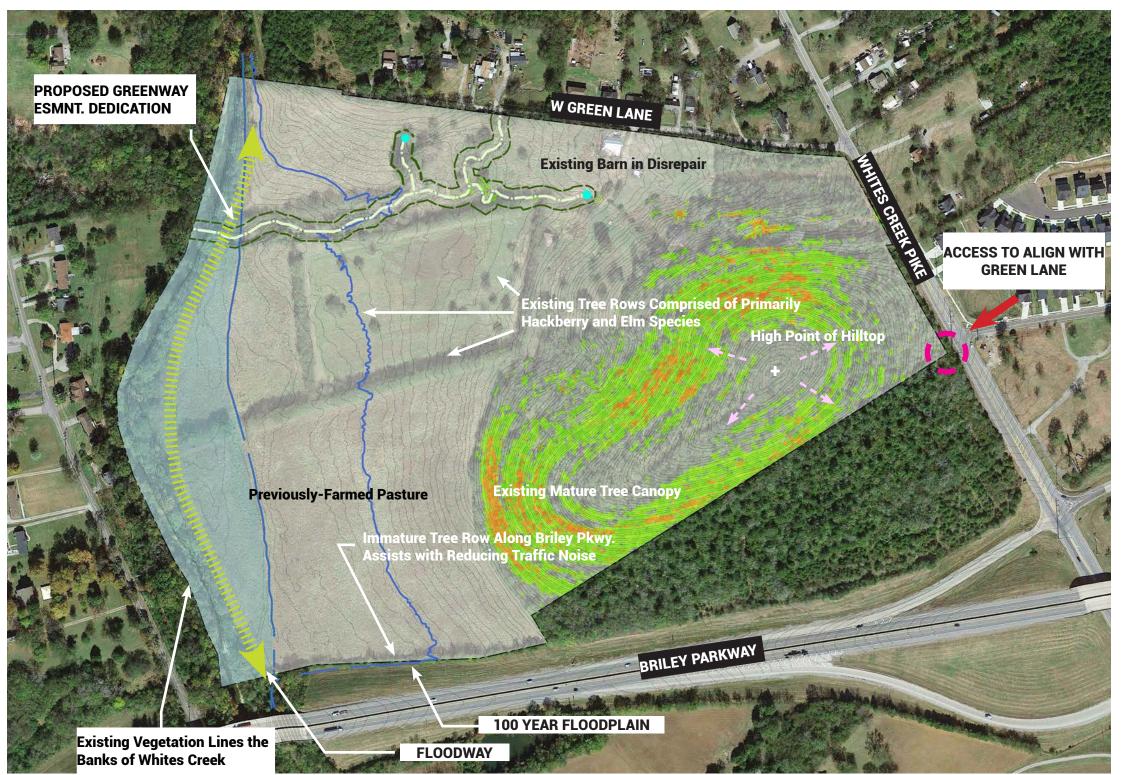
T2 NC - Rural Neighborhood Center

Generally located at the intersection of two prominent rural arterial boulevards where there is a small concentration of land used for low-intensity commercial and mixed uses, that is situated to serve rural neighborhoods and communities within a ten-minute drive, and its intensification is supported by surrounding residential development, adequate infrastructure and adequate access. A mix of building types is expected with preference to commercial buildings that accommodate consumer services and serve as gathering places for rural communities. Office, mixed-use, institutional and residential buildings are appropriate. Massing design principles include: strongly articulating the facade of large buildings and including elements such as doors and windows; arranging buildings into pedestrian-friendly groupings; and avoiding large unbroken expanses of pavement in parking areas.

Primary access is generally provided from an arterial-boulevard and may be provided from a collector avenue street. Secondary access may be provided by a local sidestreet. Shared access is used to avoid multiple curb cuts.

Landscaping is generally formal. Street trees and planting strips are appropriate. In surface parking lots, landscaping in the form of trees, bushes, and other plantings is provided.





SITE VEGETATION AND SPECIAL FEATURES

LEGEND

- - Stream Corridor (as delineated by CEC Study)
- – Limit of 30' Stream Buffer
- - 100-Year Floodplain Limit
- ← − Planned Metro Greenway Route
- Key Site Access Location (Aligns with Existing Green Lane) Potential for Signalized Intersection







View from Briley Parkway with Tree Screen to Remain





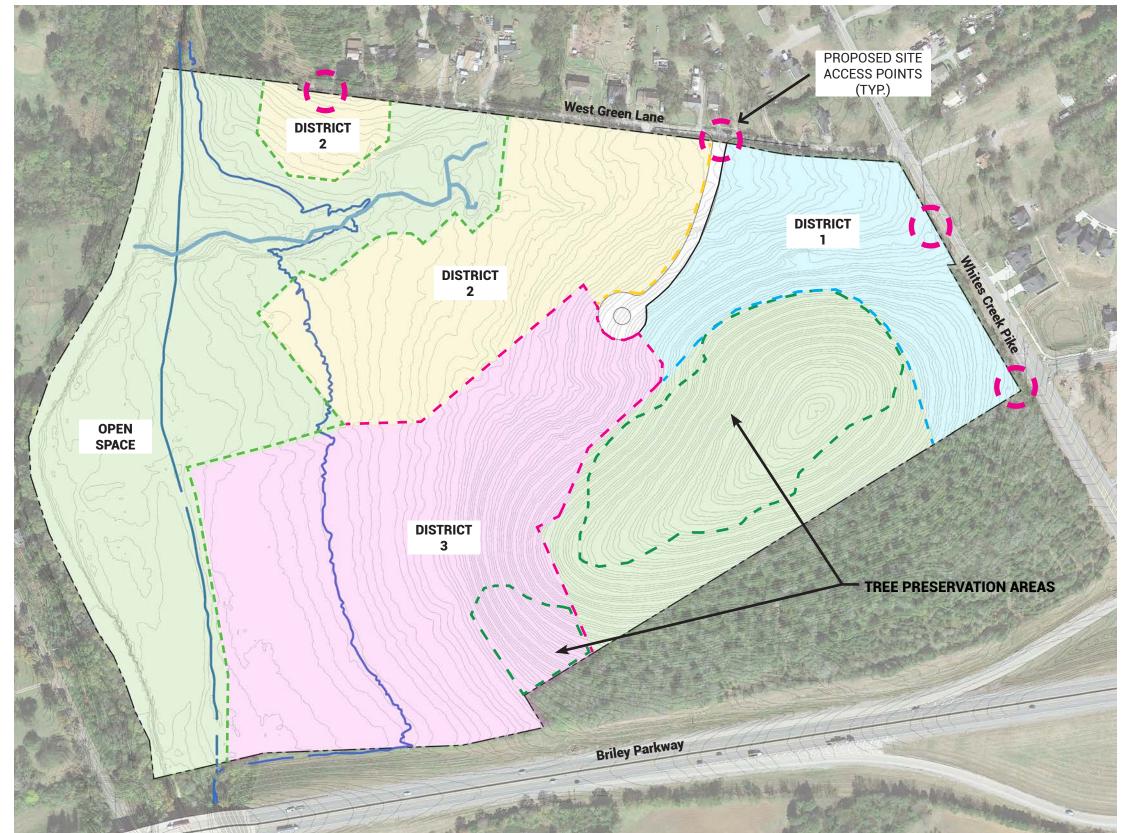








Catalyst Design Group
Project Name | City, State



SITE SPECIFIC PLAN

The purpose of this Site Specific Plan application is to permit a mixed-use development that is in keeping with the character standards and guidelines identified in the T2-NC policy.

The property lies within the Bordeaux/Whites Creek Community Plan and the regulatory plan supports the design principles identified in that document. The proposed SP Zoning will remain consistent with the T2 Rural Neighborhood Center (T2-NC) policy on the property (as described in the Nashville Next Community Character Manual) while achieving a varied mix of housing offerings as a stated policy goal. The Site Specific Plan document adopts the bulk regulations and associated development standards outlined within the MUN-NS (District 1 and 3) and R6-NS (District 2) base zoning districts while promoting an upgrade of development standards in terms of development form and retainage of existing vegetation patterns throughout the property.

- DISTRICT 1 | Mixed-Use Commercial, Retail, Office, Live/ Work Residences and Townhomes
- DISTRICT 2 | Single-Family and Cottage Residences
- DISTRICT 3 | Multifamily Low-Rise Flats and Apartments
- DISTRICT 4 | Open Space District



Catalyst Design Group | Case No. 2022SP-043-001 633 West Green Lane Development | Nashville, TN

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30' Front Setback W. GREEN LANE (ST 252) CREENING OF Masonry Signage Wall Street Tree Plantings A-1 Evergreen Hedge To Be Located Behind Horse Fence (Typ.) Signage Board That A-2 A-4 Tells The Story Of A-3 Whites Creek Located Here Masonry Columns At Entrances (Typ.) **Village Commons** Horse Fence (Typ.) Patio Courtyard **Pocket Park** Plaza A-5 A-6 B-5 B-6 15% SLOPES Graded Slope (Typ.) Low Masonry Knee Wall With Evergreen Screen Plantings In Front of Parking* Masonry Signage Wall Tree Preservation Area (7.8 Ac.) Mix Of Flowering Trees and Evergreen Backdrop Plantings B-8 ANDSCAPING STND. TO BE PROVIDED ALONG WHITES CREEK PK. STREET TREE PLANTINGS AT AN AVG. OF 50' o.c. (13 TOTAL, MIN. 3" CAL.) SECONDARY ACCENT AND CANOPY TREE PLANTINGS ALONG HORSE FENCING AND LOCATED WITHIN PARKING ISLANDS ALONG FRONTAGE (15 TOTAL, MIN. 3" CAL.) EVERGREEN TREE PLANTINGS TO SCREEN VIEW TO PARKING WHERE INDICATED (15 TOTAL, MIN. 6' HT. AT PLANTING) - EVERGREEN SHRUB PLANTINGS LOCATED BEHIND HORSE FENCING

DISTRICT 1: STREETSCAPE IMPROVEMENTS ALONG WHITES CREEK PIKE



Horse Fence



Masonry Signage Wall



Masonry Columns

* The design treatments (landscape screening/masonry walls) may shift within the final SP design based upon the final locations of the commercial building pads.

DISTRICT 3: PEDESTRIAN CONNECTIVITY EXHIBIT



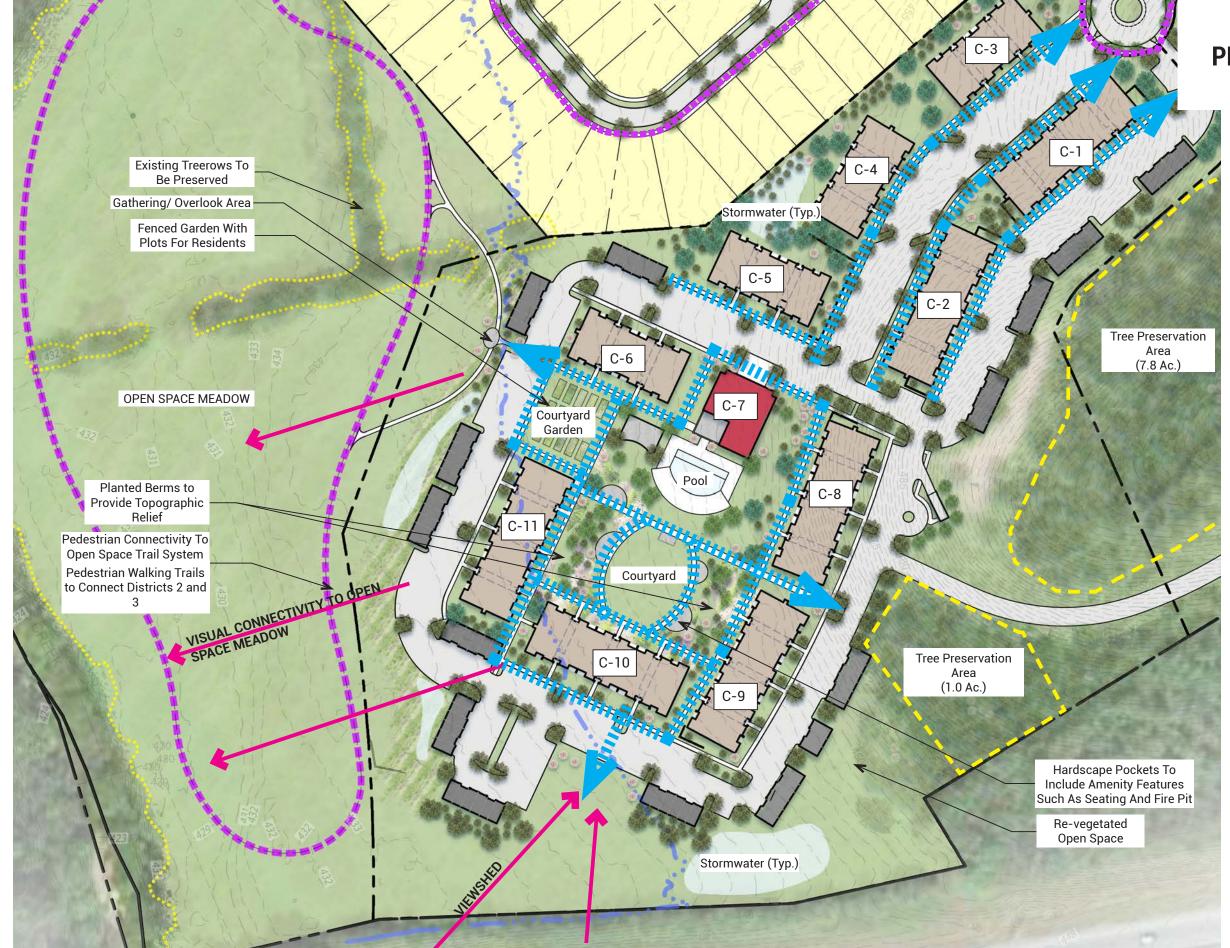
Crushed Stone Walking Trail



Parking Lot Pedestrian Crosswalk



Gathering/ Overlook Area to Open Space Meadow



Street Trees to Line Private Drive Courtyard Garden Courtyard Tree Preservation Area (1.0 Ac.)

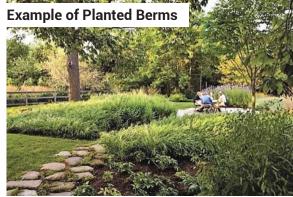
DISTRICT 3: PLANTING EXHIBIT









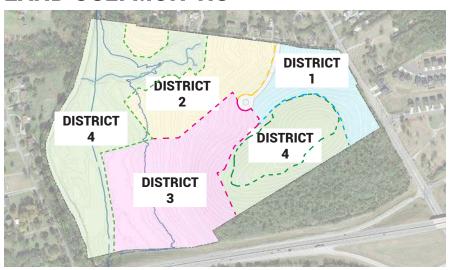








LAND USE: MUN-NS



"District 1" Uses	Uses allowed within MUN-NS zoning with the exception of those listed in the prohibited use section		
FAR/Density ¹	45,000 SF of Commercial uses and 41 Residential Units (inclusive of Live/Work Units)		
ISR	0.70		
General Maximum Building Height	2 stories to a maximum of 35 feet Townhomes and Live/Work Units shall be allowed to have a 2/3 story split condition wherein a floor level of grade is being transitioned across the unit.		
Side/Rear Setback	Min. of 20-foot rear setback and 5-foot required minimum side setback for the Overall District 1 Parcel; Min. of 10-foot separation between buildings		
Parking Requirements	Per Metro Code		
Glazing ²	Residential	20% Min. on the first floor level and 15% min. glazing on all upper stories of facades facing a public street	
Raised Foundations ³	Townhomes Only	18 in. Min to 36 in. Max	

- 1 No Maximum FAR applies to residential uses.
- 2 Minimum glazing requirements shall be required on building facades facing public streets. Glazing calculations shall be measured from floor to floor.
- 3 With the exception of accessible units, Live/Work units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36" along public streets and open spaces. Maintaining ADA access to units in order to meet fair housing requirements shall preempt this regulation.

COMMON AREA DESIGN STANDARDS

18. Designated "Village" and "Park" Common areas shall include pedestrian hardscape zones incorporating outdoor seating and/ or tables. Sidewalks shall be provided to encourage pedestrian circulation to these areas. The common areas shall introduce landscape plantings (i.e. trees, shrubs, and groundcover beds) in key visual areas. Tree plantings shall be provided around a minimum of 40% of the area perimeter with tree plantings provided on a maximum of 35' centers. The areas shall include lighting elements to provide the opportunity for use after dusk.

ARCHITECTURAL STANDARDS

- 1. Residential structures shall be oriented to a public street where possible. When not possible, residential structures may be oriented towards internal open space common areas.
- 2. Buildings shall provide a functional entry onto the street/sidewalk network or other public space to promote activity at the street level. Where feasible, given the constraints of the site grades, residential units fronting a public street or green space shall provide a connection/ entrance to the public sidewalk.
- 3. Vinyl siding, EIFS and untreated wood shall not be permitted with the exception of accent banding above 20' height from grade (or 2 residential floors).
- 4. Refuse collection and recycling facilities shall be screened from views from the public way through the combined use of fences, walls and landscaping in accordance with Metro Code standards.
- 5. Windows shall be vertically oriented at a ratio of 1.5:1 or greater. Planning staff may allow modifications to this standard for dormers, decorative windows, clerestory windows, egress windows, storefronts curtain walls and other special conditions.
- 6. If provided, porches shall have a minimum depth of 5 feet.
- 7. The maximum allowable building footprint size within the District shall be 10,000 s.f. Building articulation shall be provided for every 80 feet of horizontal building wall facing toward a public or private roadway frontage in one of the following forms:
 - Min. 12" depth offsets in the perimeter wall;
 - Facade material change;
 - Roof line offset with a min. height change of 24";
 - Color change coupled with a change in the installation pattern of the facade material (e.g. cementitious siding pattern change).
- 8. Buildings shall have strongly articulated facades, including such elements as windows and doors;
- 9. Buildings shall be arranged into pedestrian-friendly groupings
- 10. The placement of specific buildings, drives locations and building floor plate sizes shown within District 1 shall be granted flexibility to be adjusted at the Final SP Submission in order to address the needs of specific retailers.

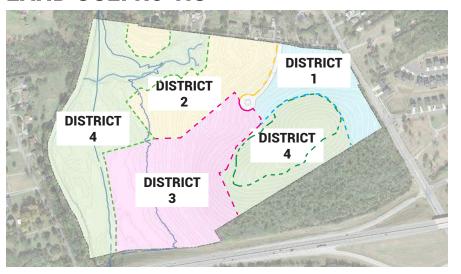
ACCESS & PARKING

- 11. Surface parking shall be located towards the interior of the site or be screened by landscaping.
- 12. All parking regulations to meet Metro Code (100% of on-street parking shall be allowed to count toward Code requirements).
- 13. Pedestrian connectivity within the center is high in order to allow pedestrians to park and walk from business to business. Sidewalks are present within the center. Crosswalks are provided at intersections, through parking lots, and at vehicular access points and are clearly marked to distinguish the pedestrian zone from the vehicular zone.
- 14. Site plan shall avoid large unbroken expanses of pavement in associated parking areas.

LANDSCAPE STANDARDS

- 15. Landscaping and tree density requirements per Metro Zoning Ordinance. A complete Landscape Plan will be submitted with the Final SP application.
- 16. A portion of the formal common areas indicated within the Concept Plan will be furnished with amenities for use by the community residents (examples: seating areas, walking pathways, fire pits, grills, play areas, public art, etc.).
- 17. Parking areas that occur along the edge of a Public or Private Street shall be screened by evergreen shrub plantings that shall have a minimum height of 30" at the time of installation. The spacing of the plantings shall be no greater than 48" on center. Supplemental tree plantings shall be provided with a 2" cal. tree (either canopy or understory) being provided per every 50 feet of parking lot frontage. The use of a low knee wall shall also be allowed, where selected as an option, to assist with screening parking areas.

LAND USE: R6-NS



"District 2" Uses	Uses allowed within R6-NS		
FAR/Density ¹	59 Single Family Residences ¹		
ISR	0.70		
General Maximum Building Height	2 stories in 35 feet		
Side/Rear Setback	Min. of 20 foot rear setback and 5 foot required minimum side setback on each lot		
Parking Requirements	Per Metro Code		
Raised Foundations ²	Residential	18 in. Min to 36 in. Max	
	·		

¹ No Maximum FAR applies to residential uses.

ARCHITECTURAL STANDARDS

- Residential structures shall be oriented to a public street where possible. When not possible, residential structures may be oriented towards internal open space common areas.
- 2. Buildings shall provide a functional entry onto the street/ sidewalk network or other public space to promote activity at the street level. Where feasible, given the constraints of the site grades, residential units fronting a public street or green space shall provide a connection/ entrance to the public sidewalk.
- 3. Vinyl siding, EIFS and untreated wood shall not be permitted with the exception of accent banding above 20' height from grade (or 2 residential floors).
- 4. Refuse collection and recycling facilities shall be screened from views from the public way through the combined use of fences, walls and landscaping in accordance with Metro Code standards.
- 5. Windows shall be vertically oriented at a ratio of 1.5:1 or greater. Planning staff may allow modifications to this standard for dormers, decorative windows, clerestory windows, egress windows, storefronts curtain walls and other special conditions.
- 6. If provided, porches shall have a minimum depth of 5 feet.
- 7. Lots fronting toward W. Green Ln. shall have a min. front setback of 30 feet from the street right-of-way.
- 8. Single family residential roofs shall have a min. pitch of 4/12.

ACCESS & PARKING

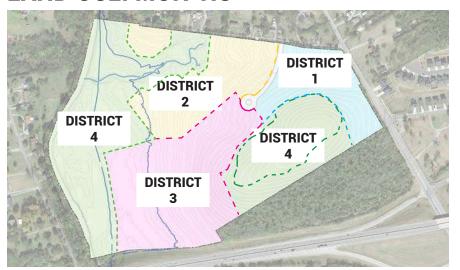
- 9. All parking regulations to meet Metro Code. (100% of On-Street parking shall be allowed to count toward Code requirements)
- 10. Garages shall be set back from the front wall of the primary residential structure by a minimum of 24".
- 11. No parking pads shall be in front of units fronting W. Green Lane.

LANDSCAPE STANDARDS

- 12. Landscaping and tree density requirements per Metro Zoning Ordinance. A complete Landscape Plan will be submitted with the Final SP application.
- 13. The existing treelines shall be preserved to the fullest extent possible with the exception of damaged or unhealthy trees and any requirement to install infrastructure utilities, and/or adjust grades within an area to meet stormwater requirements or perform mass grading.
- 14. Street tree plantings shall be provided along the internal public and private roadways with 1 minimum 2" cal. canopy tree being provided for every 50 feet of lot frontage along the street.
- 15.5' right-of-way dedication to be provided to accomodate a distance of 25' from the center line of W. Green Lane to provide a 5' sidewalk and a 4' planting strip.

² With the exception of accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36" along public streets and open spaces. Maintaining ADA access to units in order to meet fair housing requirements shall preempt this regulation.

LAND USE: MUN-NS



Residential allowed as the only Primary Use; 325 residential units		
No FAR provision shall apply to residential uses		
0.70		
3 stories in 52 feet; a Basement floor level is allowed when meeting Code definition for a Basement		
Min. of 20-foot rear setback and 5-foot required minimum side setback for the Overall District 3 parcel; min. of 10-foot separation between buildings		
Per Metro Code		
Residential	18 in. Min to 36 in. Max	
	32 No FAR 3 stories in 52 is allowed wh Min. of 20-forequired mir Overall Districts separate	

¹ No Maximum FAR applies to residential uses.

LANDSCAPE STANDARDS

- 16. Landscaping and tree density requirements per Metro Zoning Ordinance. A complete Landscape Plan will be submitted with the Final SP application.
- 17. A portion of the formal common areas indicated within the Concept Plan will be furnished with amenities for use by the community residents (examples: seating areas, walking pathways, fire pits, grills, play areas, public art, etc.).

ARCHITECTURAL STANDARDS

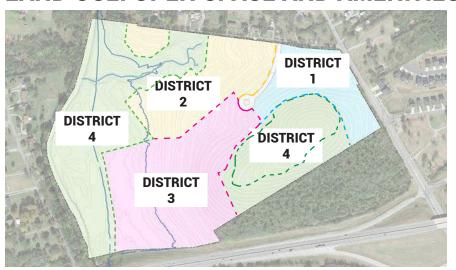
- 1. Residential structures shall be oriented to a public or private street where possible. When not possible due to the lack of frontage on a public or private street, residential structures may be oriented towards internal open space common areas or private drives with surface parking.
- 2. Buildings shall provide a functional entry onto the street/ sidewalk network or other public space to promote activity at the street level. Where feasible, given the constraints of the site grades, residential units fronting a public or private street or green space shall provide a connection/ entrance to the public sidewalk.
- The overall architectural style for the apartments and associated buildings shall be understood
 to reflect a farmstead appearance. The primary design material utilized with the architectural
 design shall be cementitious siding with accent features of masonry or stone materials.
- 4. Buildings shall have articulated facades, including elements such as windows and/ or doors. The perimeter façade of buildings shall be articulated for every 80 feet of horizontal wall on the longest side of the structure in one of the following forms:
 - * Min. 12" depth offset in the perimeter wall.
 - * Façade material change.
 - * Roof line offset with a min. height change of 24".
 - * Color change coupled with a change in the installation pattern of the façade material (e.g., cementitious siding pattern change).
- 5. Where provided, exterior balconies shall have a min. depth of 5 feet. The balcony feature shall be reinforced architecturally on the building elevation with a roof accent element such as a gable, façade projection, or other design element.
- 6. The roofline of buildings shall be broken up with gable elements or roof offsets to reduce the massing of apartment buildings.
- 7. Breezeway entrance shall be detailed in a manner that accentuates the entrance feature. The design elements shall include, but not be limited to,: A roof feature over the entry portal, a change in material or pattern of material installation, lighting elements, etc.
- Parking areas shall be screened from view from any public street and from Briley Parkway with landscaping.
- 9. Communal common areas as illustrated on the SP Plan shall be outfitted with supplemental landscaping to enhance these areas for use by the residents. The areas shall include some extent of specialty amenity features (example items being outdoor seating, outdoor dining tables, open plan areas, outdoor game elements, trellises, grilling stations, firepits, perennial color displays, community garden area, etc.).
- 10. Vinyl siding, EIFS and untreated wood shall not be permitted with the exception of accent banding above 20' height from grade (or 2 residential floors).
- 11. Refuse collection and recycling facilities shall be screened from views from the public way through the combined use of fences, walls and landscaping in accordance with Metro Code standards.
- 12. Windows shall be vertically oriented at a ratio of 1.5:1 or greater. Planning staff may allow modifications to this standard for dormers, decorative windows, clerestory windows, egress windows, storefronts curtain walls and other special conditions.
- 13. The garage buildings will have architecture similar to and complementing of the architecture of the multi-family buildings.

ACCESS & PARKING

- 14. Surface parking shall be located towards the interior of the site or be screened by landscaping from the view from a public street(s).
- 15. All parking regulations to meet Metro Code. (100% of On-Street parking shall be allowed to count toward Code requirements)

² With the exception of accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36" along public streets and open spaces. Maintaining ADA access to units in order to meet fair housing requirements shall preempt this regulation.

LAND USE: OPEN SPACE AND AMENITIES



DEVELOPMENT NOTES

- 1. A portion of the preserved open space shall be revegetated with tree plantings in order to reduce development runoff from the property. The acreage to be planted shall equate to 50% of the area from which trees are removed during grading operations. Plantings shall be a min. of 1" caliper at the time of planting and shall be spaced at an average of 30 feet o.c. (i.e. 48 tree plantings per acre of restored open space). Tree plantings may be a mix of canopy and understory trees.
- 2. A 30' width Greenway Easement shall be dedicated along Whites Creek in accordance with the routing anticipated on the Metro Greenways Master Plan.
- 3. The open space shall be maintained by the master development. Natural areas shall be mowed at least once a year.
- 4. The area on the Site Plan designated for tree preservation shall be protected during construction with tree protection fencing. No grading or installation of site utilities shall occur within the preservation area. The area shall be placed into site common area or tree preservation easement when the development is platted.
- 5. A portion of the open spaces indicated within the Concept Plan may be furnished with amenities for use by the community residents (examples: walking pathways, community garden, etc.).
- 6. Roadways, Drives, Utilities and Pedestrian Pathways shall be allowed to extend through the Open Space areas to accommodate and serve the needs of the other development districts.
- 7. Landscaping (inclusive of existing tree cover or new plantings) shall be utilized to screen the view of the private driveway through District 4 along the southern property boundary. This standard shall not apply should the private drive be aligned to allow for the adjoining parcel to the south to front units toward the drive in a comprehensive development plan or if the properties are held in a common ownership interest.
- 8. A 75' width Conservation Easement shall be dedicated from the limit of the FEMA mapped floodway for Whites Creek.

ADDITIONAL REGULATIONS + NOTES

Regulatory SP Notes:

- 1. The purpose of this SP is to receive preliminary SP approval to permit the development of 425 Total Residential Units and 45,000 SF of Commercial uses, and to accommodate the standards and uses allowable in the referenced base zoning districts.
- 2. The development shall be exempt from Chapter 4 of the Metro Subdivision Regulations (Rural Development Standards).
- 3. For any development standards, regulations and 8. requirements not specifically shown on the SP plan and/or included as a condition of Council approval, the property shall be subject to the standards, regulations, and requirements of the base zoning 9. The developer's final construction drawings shall designations referenced for each development district as of the date of the application request or application. STRP uses shall be prohibited.
- 4. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or Planning Department staff based upon final architectural, engineering, or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Major modifications shall not be permitted, except through an ordinance approved by the Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance.

FEMA Note:

5. A portion of this property does lie in a flood zone area designated as Zone A in the Federal Emergency Management Agency Flood Insurance Rate Map Panel Number 47037C0227H, effective April 5, 2017.

NDOT Notes:

6. The final site plan/building permit shall depict the required public sidewalks, any required grass strip or frontage zone, and the location of all existing and proposed vertical obstructions within the required

- to the issuance of Use and Occupancy Permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Where feasible, vertical obstructions are only permitted within the required grass strip or frontage zone.
- 7. Roadway Improvements that are a direct result 19. All dead-end roads over 150 ft. in length require a of this specific project or as determined by an approved Traffic Impact Study and the Department 20. A fire hydrant shall be provided within 100 ft. of any of Public Works shall be constructed.
- The project shall dedicate any required right-ofway within the project site that is identified as necessary to meet the adopted Major and Collector Street Plan (MCSP).
- comply with the design regulations established by the Nashville Department of Transportation (NDOT), in effect at the time of the approval of the preliminary development plan or final development plan or building permit, as applicable. Final design may vary based on field conditions.
- 10. All construction within the right of way shall comply with ADA and NDOT Standards and Specifications.
- 11. There shall be no vertical obstructions (signs, power poles, fire hydrants, etc.) within the proposed sidewalks. Where feasible, vertical obstructions shall be relocated out of the proposed sidewalks, where applicable.
- 12. If sidewalks are required then they should be shown on the plans per MCSP and NDOT standards and specs.
- 13. Submit copy of ROW dedications prior to bldg. permit sign off.
- 14. Development is to have a dumpster and recycling container(s), serviced by a private hauler. Single-Family Residences and Townhomes may arrange for individual trash pick-up by a private hauler service.
- 15. New public or private roads, as shown on the Regulatory Plan, shall be constructed in compliance with NDOT standards. A temporary terminus may be required during final SP.
- 16. Parking to be provided per Metro Code.

Fire Marshal Notes:

sidewalk and grass strip or frontage zone. Prior 17. No part of any building shall be more than 500ft.

- from a fire hydrant via a hard surface road. Metro Ordinance 095-1541 Sec. 1568.020 B
- 18. All fire department access roads shall be 20 feet minimum width and shall have an unobstructed vertical clearance of 13.5 feet.
- turnaround, this includes temporary turnarounds.
- fire department connection, if applicable.
- 21. Fire hydrants shall be in-service before any combustible material is brought on site.

NES Notes:

- 22. Where feasible, this development will be served with underground power and pad-mounted transformers.
- 23. New facilities will not be allowed to sit in or to pass through retention areas, including rain gardens, bioretention areas, bioswales, and the like. This 32. Limitation of the following uses within MUN zoning includes primary duct between pad-mounted transformers equipment, as well as service duct to a meter.

Stormwater Notes:

- ground elevation must be done in accordance with Stormwater Management Ordinance No. 78-840 Water Services.
- 25. Metro Water Services shall be provided sufficient and unencumbered ingress and egress at all times in order to maintain, repair, replace, and inspect any stormwater facilities within the property.
- 26. Size driveway culverts per the design criteria set forth by the Metro Stormwater Management is 15' RCP.)
- 27. Project intent is to be redeveloped per the 36. The Final SP documents may be submitted in requirements of Volume 5 (LID) of the Stormwater Manual. Detention will be provided or post developed runoff will be less than predeveloped runoff due to LID implementation.
- 28. Permeable pavement may be one of the measures incorporated to address stormwater runoff and water quality treatment along with rain gardens,

- bioswales, etc. The extent of each of the treatment measures shall be identified on the Final SP plans.
- 29. Drawing is for illustration purposes to indicate the basic premise of the development, as it pertains to Stormwater approval/ comments only. The final lot count and details of the plan shall be governed by the appropriate stormwater regulatoins at the time of final application.

Federal Compliance:

30. All development within the boundaries of this plan will meet the requirements of the Americans with Disabilities Act and the Fair Housing Act.

Development Notes:

- 31. Overall building height shall be measured per Metro Code. A basement floor level shall be defined in accordance with Metro Zoning Code.
- stnd. allowable land uses: Boarding House, Short Term Rental, Orphanage, Alternative Financing Services (i.e. Payday Loan Service), Hospice, Auction House, Waste Processing, Driving Range, Outdoor Music Venue with Amplified Sound.
- 24. Any excavation, fill, or disturbance of the existing 33. The development shall promote the use of downcast street and area lighting fixtures to minimize light spread onto adjacent property.
 - and approved by the Metropolitan Department of 34. Ground mounted development signage conveying the names of businesses shall be limited to a maximum height of 6 feet above adjacent grade. This standard shall not apply to gateway or monument signage features located to identify the entrances into the property or convey the overall development name at prominent visual nodes along the surrounding roadway corridors.
 - Manual. (Minimum driveway culvert in Metro ROW 35. Residential units may be transferred between any of the three districts up to 25 total units.
 - phases relating to the various development districts shown in the plan.









Example of Village Commercial Building Articulation Along The Street Edge with Material, Color and Roof Line Variations



Example of On-Street Parking on the Main Commercial Drive Through the Village



Example of Arcade Structure Extending Out Over a Portion of the Pedestrian Sidewalk Zone with Activated Uses



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VISIONING IMAGERY - COMMERCIAL/RETAIL/MIXED-USE





Street Edge with Material, Color and Facade Depth Variations



Example of Live/Work Townhome with First Floor Commerical Use





Example of Live/Work Townhome with First Floor Commerical Use

VISIONING IMAGERY - COMMERCIAL/RETAIL

Example of Outdoor Dining Venue Associated with Commerical Tenants





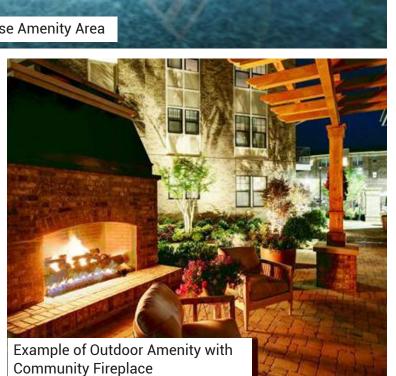


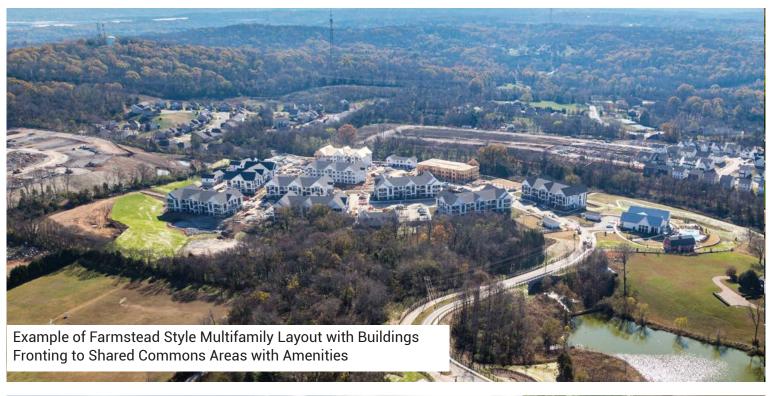
Apartment residences in District 3 are designed to blend seamlessly into the rural character of Whites Creek. The 3-story buildings with pitched roofs and gables are thoughtfully spaced throughout the community with generous landscaping designed to transition gently to the adjacent residential neighborhoods. View sheds remain open in key areas to allow residents to more fully appreciate the local setting. Consistent with a classic residential design, the roof pitch is 4 in 12 and the buildings have gables, breaking the roof line along each long side of the building. The "Cottage Farmstead" inspired façade is composed of materials such as lap siding and board and batten, providing a clean updated take on the traditional fabric of the surrounding community. Building entries feature accents such as stone and warm wood. Residents will enjoy outdoor living with large patios and balconies overlooking the surrounding tree buffered landscape, park-like picnic and gathering spaces, outdoor cooking stations, and a resort style pool amenity area.

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VISIONING IMAGERY - RESIDENTIAL (MULTIFAMILY)









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VISIONING IMAGERY - RESIDENTIAL (MULTIFAMILY)

Example of Cottage Farmstead Style Architecture

with Accent Materials at Building Entrances







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Structure





VISIONING IMAGERY - RESIDENTIAL (MULTIFAMILY)





Example of Single Family Residence with Appropriate Facade Materials and a Porch Feature

Example of Single Family Residence with Appropriate Facade Materials and a Porch Feature. Garage/ Porte-Cochere is Recessed Back from Primary Residence Front.



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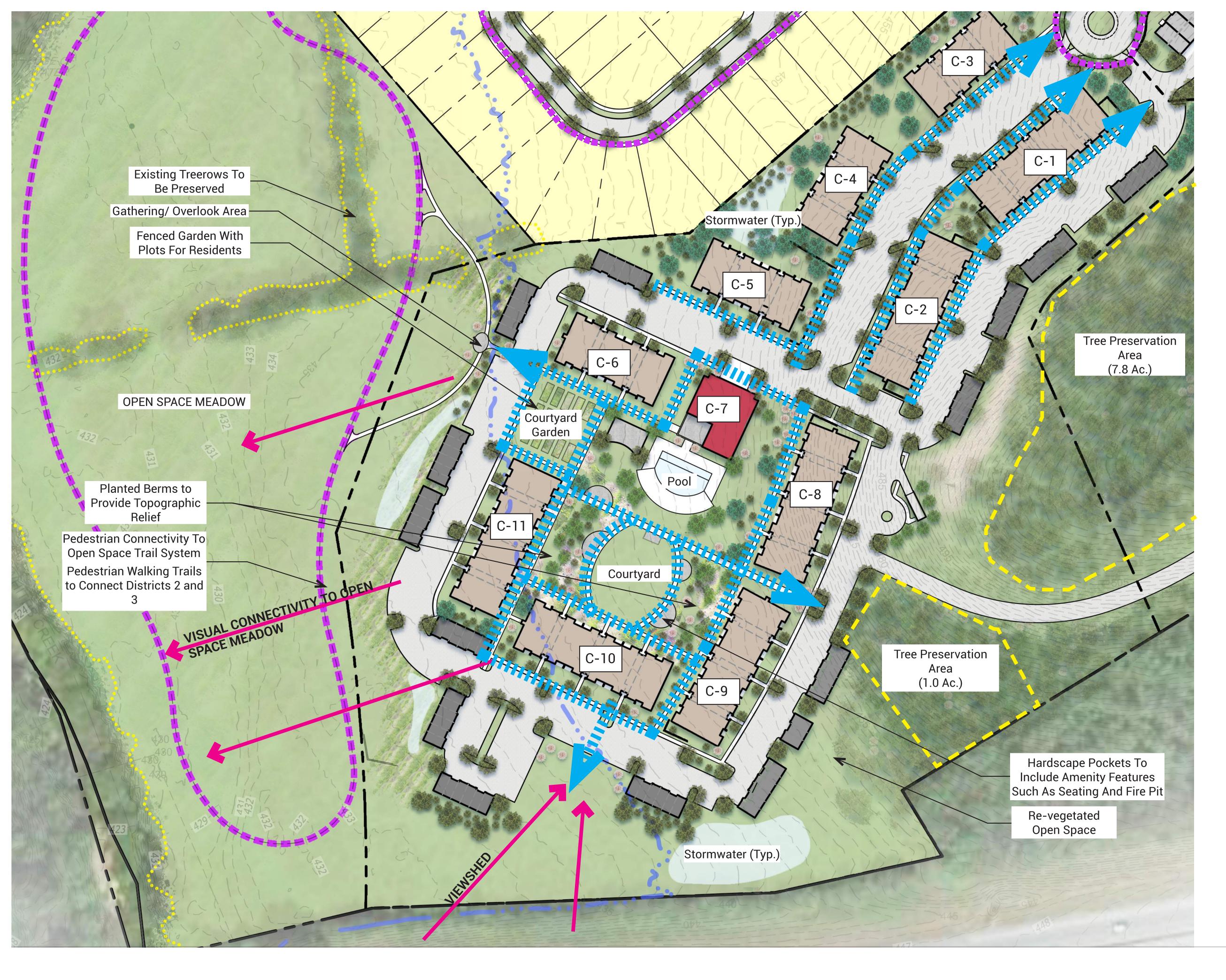


VISIONING IMAGERY - RESIDENTIAL (SINGLE-FAMILY & TOWNHOMES)











Crushed Stone Walking Trail



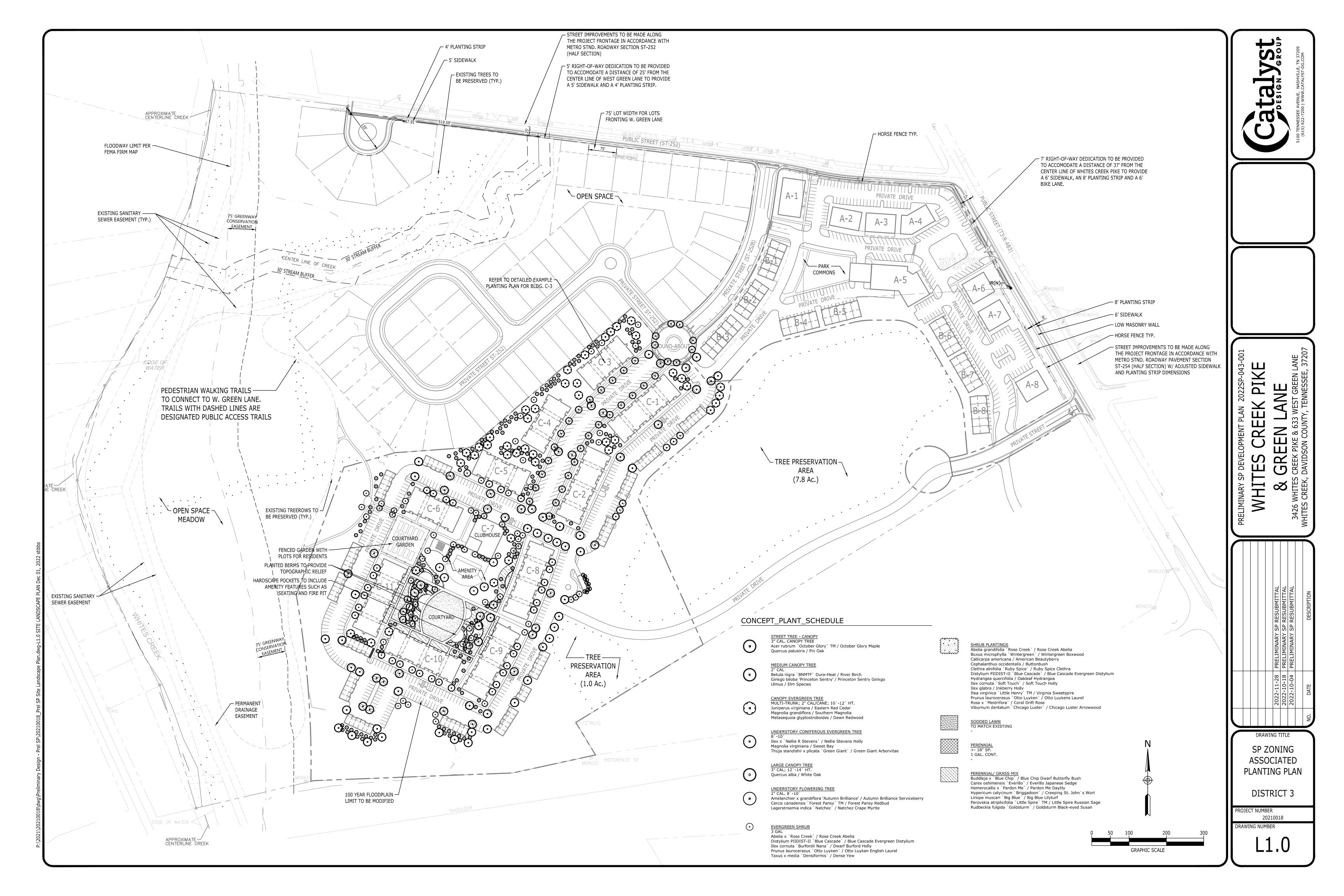
Parking Lot Pedestrian Crosswalk

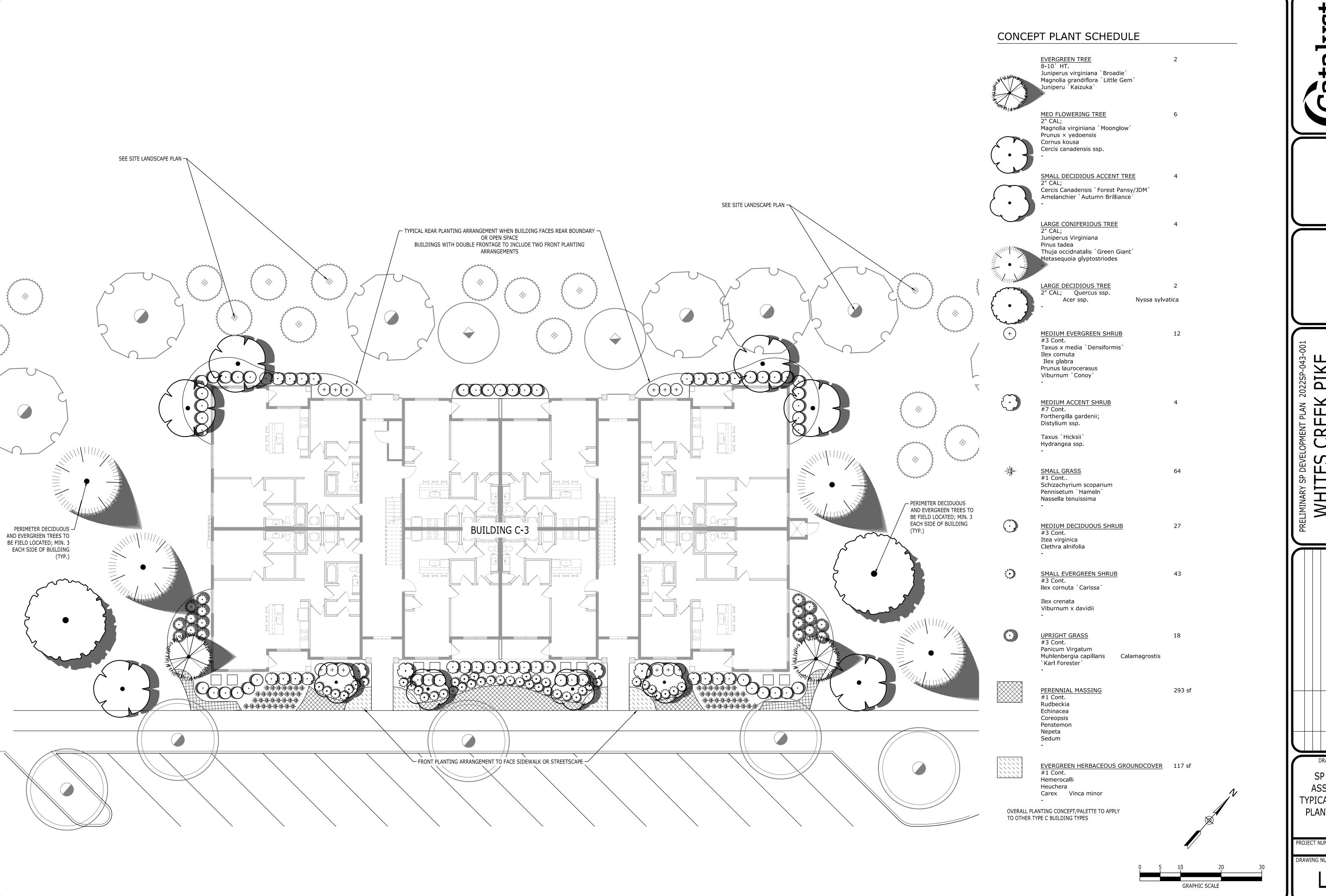


Gathering/ Overlook Area to Open Space Meadow









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DRAWING TITLE SP ZONING ASSOCIATED TYPICAL BUILDING PLANTING PLAN

20210018

DRAWING NUMBER