

## Erosion Control and Grading Notes

- Expose as small an area of soil as possible on the site for no more than 15 days. Keep dust within tolerable limits by sprinkling or other acceptable means.
- All cut/fill areas to have a minimum of 6" of topsoil cover. Areas dressed with topsoil shall receive 12 lbs. per 1000 sq. ft. of 10-10-10 fertilizer (unless otherwise specified in written specifications), 5 lbs. or more of Kentucky 31 rescue seed per 1000 sq. ft., and a straw mulch cover of 70%-80% coverage (approximately 125 lbs. per 1000 sq. ft.), unless otherwise noted within written specifications.
- Erosion control barrier is called out on plans and is to comply with the Metropolitan stormwater management manual, volume four, section TCP-14.
- Disturbed areas are to be graded to drain as indicated in the plan to sediment barriers during and upon the completion of construction.
- The contractor shall be responsible for the verification and the location of any existing utilities. It shall be the responsibility of the contractor to avoid damage to all existing utilities during construction. If damage does occur to any such installation, full repair will be accomplished as per the current specification governing such work.
- Any access routes to the site shall be based with crushed stone, ASTM #1 stone, 100' long and at least 6" thick.
- The placing and spreading of any fill material is to be started at the lowest point and brought up in horizontal layers of 8" thickness (or as directed by the soils investigative report). Said fill material is to be free of sod, roots, frozen soils, or any other decomposable material. Said fill is to be compacted to a minimum of 95% standard proctor, or as otherwise specified by the soils report or written specifications.
- The contractor shall notify the Metro Davidson County department of Public Works construction compliance division, three days prior to beginning the work.
- The contractor shall locate and stake the layout of the site in the field for inspection by the engineer. The contractor shall check the grades and final dimensions on the ground, and report any discrepancies to the engineer immediately for a decision.
- Surplus excavation of topsoil shall be placed on the site as approved by the owner for the purpose of future landscape use.
- The contractor shall furnish and install all necessary temporary works for the protection of the public and employees, including warning signs and lights.
- The contractor shall be responsible for any damage done to the premises or adjacent premises or injuries to the public during the construction caused by himself, his sub-contractors, or the carelessness of any of his employees.
- All work is to be completed with compliance to the rules and regulations set forth by Metro Water Services. The contractor shall give all necessary notice, obtain all permits, and pay fees required for the completion of his portion of the work. He shall also comply with all city, county and state laws and ordinance or regulations relating to portions of work which he is to perform.
- All erosion control measures shall remain in place until site is stabilized & construction is complete.
- Contractor to provide an area for concrete wash down and equipment fueling in accordance with metro CP-10 and CP-13 respectively. Contractor to coordinate exact location with NPDES department during the pre-construction meeting. Grading perimeters to include berms designed to control site wastes such as discarded building materials, chemicals, litter and sanitary wastes that may cause adverse impacts to water quality. The location of and/or notes referring to said bmp's shall be included on the EPSC plan.
- The buffer along waterways will be an area where the surface is left in a natural state, and is not disturbed by construction activity. This is in accordance with the Stormwater Management Manual Volume 1 - Regulations.
- Drawing is for illustration purposes to indicate the basic premise of the development, as it pertains to Storm Water approval /comments only. The final lot count and details of the plan shall be governed by the appropriate Storm Water regulations at the time of final application.

## Landscape Notes

- The landscape contractor shall coordinate all construction with the appropriate utility company and shall be responsible for damage to utilities. The landscape contractor shall verify the exact location of all utilities and take precautions to prevent damage to the utilities.
- All planting and mulch beds shall be sprayed with round-up (contractor's option) prior to the installation of mulch.
- Plant materials and stumps indicated for removal shall be removed and disposed off-site by the contractor. Backfill holes with topsoil free of roots and rocks.
- The landscape contractor shall be responsible for the fine grading of all planting areas.
- All planting areas shall be fertilized with 12# /1000 s.f. of 10-10-10 fertilizer.
- All planting beds shall have a minimum of 3" depth of shredded hardwood bark mulch.
- The landscape contractor shall verify all material quantities. In the event of a discrepancy, the quantities shown on the plan will take precedence.
- The landscape contractor shall provide the owner with written instructions on the proper care of all specified plant materials prior to final payment.
- Existing trees to remain shall be protected from construction damage. Selectively prune dead wood.
- All disturbed areas shall be planted with turf as indicated on the materials schedule.
- All deciduous trees, existing and proposed shall be pruned to provide 4' minimum clear trunk unless otherwise noted.
- The landscape contractor shall provide a one year warranty on all plant materials and replace any dead or dying material within that time period.
- No plant materials should be substituted without authorization by Dale & Associates. Plant sizes shown are minimums required by the local municipality and materials shown have been selected specifically for this project.
- All wire baskets shall be completely removed and disposed of, burlap should be removed or punctured in at least 5 places. Remove all twine from burlapped materials.
- Guying is not allowed unless required by municipality or site conditions. The landscape contractor shall remove wires after a one year period.
- No canopy tree shall be located within 15' of an overhead utility. No tree shall be located within a public utility easement. Locating plant materials within a drainage easement is acceptable, but only if installed as not to disturb existing drainage flow. In such instances, the materials shall be located no closer than 5' from the centerline of drainage.
- Lighting plan to be coordinated with proposed planting plan. no light poles to be located in tree islands. See lighting plan for proposed light locations.

## NDOT Notes

- All work within the public right of way requires an excavation permit from NDOT.
- Proof-rolling of all street subgrades is required in the presence of the NDOT inspector. Inspection of the binder course is required prior to final paving in the presence of the NDOT. These requests are to be made 24 hours in advance.
- Stop signs are to be 30 inch by 30 inch.
- Street signs to have six inch white letters on a nine inch green aluminum blade, high intensity reflective.
- All pavement marking are to be thermoplastic.
- Final construction plans and road grades shall comply with the design regulations established by NDOT. Slopes along roadways shall not exceed 3:1.

## Architectural Notes

- Building elevations for all street and open space-facing facades shall be provided with the final site plan. The following standards shall be met:
- Building facades fronting a street or open space shall provide a minimum of one principal entrance (doorway) and a minimum of 15% glazing.
  - Windows shall be oriented at a ratio of 1:5:1 or greater.
  - Building facades shall be constructed of brick, brick veneer, stone, cast stone, cementitious siding, glass, or material substantially similar in form and function, unless otherwise approved on detail building elevations.
  - A raised foundation of 18"-36" is required.
  - All mechanical units shall be screened from the right-of-way by landscaping or an enclosure, or a combination of both. Screening details are to be submitted with the final site plan.

## MWS Standard Private Utility Plan Notes

- All water and sewer construction shall be in accordance with specifications and standard details of the metro Water Services.
- All connections to existing manholes shall be by coring and resilient connector method.
- Vertical Double Check Valve Assemblies, that are located in interior rooms, can only be used for fire services.
- All water meters shall be a minimum of 24" not to exceed a maximum of 28" below finished grade.
- Irrigation line shall be copper from the meter to the backflow preventer.
- The minimum fees outlined in the capacity letter must be paid before commercial construction plans can be reviewed.
- All sewer services shall be minimum 6 inches in diameter, from connection at the main until the fires clean out assembly.
- Backflow device to remain accessible at all times.
- Plan size shall be 24" x 36" and shall show contours around meter boxes.
- Any unused existing water meters must be cut and capped at the public main.
- All lead or galvanized water service lines encountered with the project shall be reinstated with copper of like size from the water main to the meter box.
- Domestic and irrigation water meters and associated appurtenances shall not be placed in or under a paved or improved surface other than the portion of the service located within the right of way.
- Sanitary sewer taps shall be placed at the lowest adjacent sewer main elevation for each premises and shall not be located in or under a paved or improved surface other than the portion within the right of way.

## Water and Sewer Notes

- All water and sewer construction shall be in accordance with specifications and standard details of the Metro Water Services.
- The contractor is responsible for reimbursing the Metro Water Services the cost of inspection.
- The contractor is to provide and maintain the construction identification sign for private development approved.
- After completion of the sanitary sewer, the developer is responsible for the televising of the lines prior to final acceptance. The videotaping must be coordinated with the Metro Water Services Inspection Section. All costs will be borne by the developer.
- All connections to existing manholes shall be by coring and resilient connector method.
- Reduced Pressure Backflow Prevention Devices (RPBP) or dual check valve will be required on all test and fill lines (jumper) needed for water main construction and must be approved by the Metro Water Services.
- All water meters shall be a minimum of 24" not to exceed a maximum of 28" below finished grade.
- Upon completion of construction of water and/or sewer, the engineer shall provide the department with a complete set of as-built plans in digital (DWG & PDF) format. All drawings must be completed and submitted prior to acceptance of the sewers or water mains into the public system and any connections being made. (A) Sewer plans shall be issued by a licensed professional engineer and/or a registered land surveyor and shall include actual field angles between lines, all actual service lines and tee locations, the distance of the end of the service to property corners and lines and/or stonewalling and offset from sewer centerline to end of service line, the depth to the top of the end of the service line, and shall reflect all alignment and grade changes. (B) Water line plans shall be sealed by a Licensed Professional Engineer and/or a Registered Land Surveyor and shall include offset distance from the roadway centerline, or property line right of way, line depth, location of hydrants, valves, reducers, tees and pressure reducing devices where applicable.
- Pressure regulating devices will be required on the customer side of the meter when pressures exceed 100 psi.
- Pressure regulating devices will be required on the street side of the meter when pressures exceed 150 psi.
- All water mains must be located within the paved areas including all blow-off assemblies.
- All lead or galvanized water service lines encountered with this project shall be reinstated with copper of like size from the water main to the meter box.
- Domestic and irrigation water meters and associated appurtenance shall not be placed in or under a paved or improved surface other than the portion of the service located within the right of way.
- Sanitary sewer taps shall be placed at the lowest adjacent sewer main elevation for each premises and shall not be located in or under a paved or improved surface other than the portion within the right of way.

## Standard SP Notes

- The purpose of this SP is to obtain Preliminary approval for 237 residential units shown herein.
- Any excavation, fill or disturbance of the existing ground elevation must be done in accordance with Storm Water Management Ordinance No. 78-840 & Approved by the Metropolitan Department of Water Services.
- A portion of this property does lie within a flood hazard area as identified by FEMA on Map 47037C0393J, Dated: April 5, 2017
- All public sidewalks are to be constructed in conformance with metro public works sidewalk design standards.
- Wheel chair accessible curb ramps, complying with applicable metro public works standards, shall be constructed at street crossings.
- The required fire flow shall be determined by the metropolitan fire marshal's office, prior to the issuance of a building permit.
- Size driveway culverts per the design criteria set forth by the Metro Stormwater Manual (minimum driveway culvert in Metro right of way is 18" RCP).
- Metro Water Services shall be provided sufficient & unencumbered ingress & egress at all times in order to maintain, repair, replace & inspect any stormwater facilities within the property.
- Individual water and/or sanitary sewer service lines are required for each unit.
- Solid waste pickup to be provided by individual roll-away carts.
- Landscape and tree density requirements per Metro Zoning Ordinance.
- Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
- For any development standards, regulations and requirements not specifically shown on the SP plan and/or included as a condition of commission or council approval, the property shall be subject to the standards, regulations and requirements of the RMG-NS zoning district as of the date of the applicable request or application.

- The final Site Plan/building permit site plan shall depict any required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within any required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of any required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.

# Preliminary Specific Plan

# 13905 Old Hickory Boulevard

Metro Case Number 2025SP-007-001

Being Parcels 41 & 42 on Tax Map 182  
Antioch, Davidson County, Tennessee

## SPECIFIC PLAN DEVELOPMENT SUMMARY

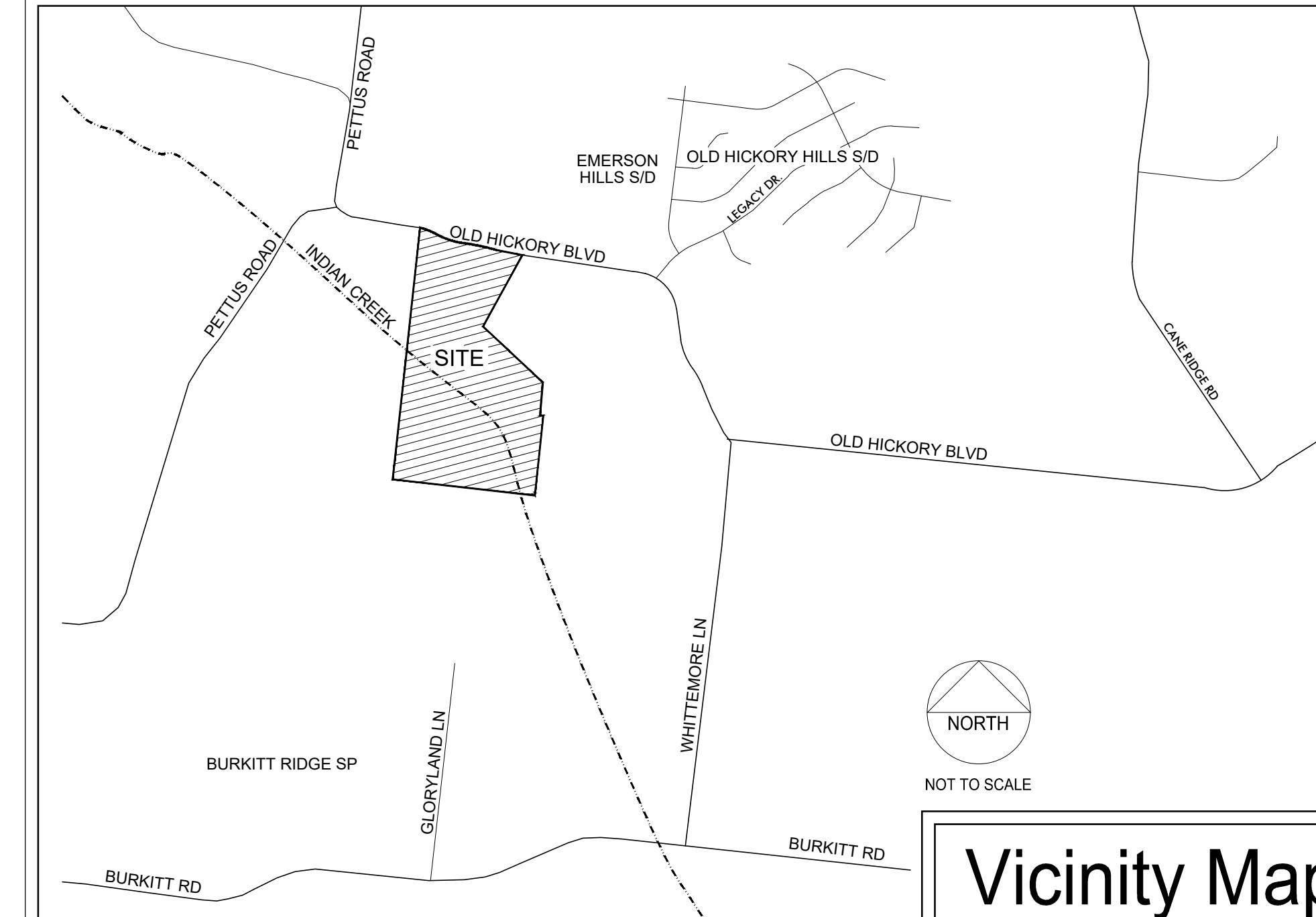
LAND USE	237 SINGLE & MULTI-FAMILY RESIDENTIAL UNITS (3.6 UNITS/AC)
TOTAL SITE AREA	66.72 ACRES
CURRENT ZONING AR2A	SURROUNDING ZONING AR2a & SP
MINIMUM LOT SIZES	1,500 Sq Ft TOWNEHOME LOT 4,000 Sq Ft SINGLE FAMILY (REAR LOAD) 5,000 Sq Ft SINGLE FAMILY (FRONT LOAD)
FLOOR AREA RATIO (FAR)	0.50 MAXIMUM / 0.30 PROPOSED
IMPERVIOUS SURFACE RATIO (ISR)	0.60 MAXIMUM / 0.35 PROPOSED
STREET YARD SETBACKS	15' FROM OLD HICKORY BOULEVARD 10' FROM INTERIOR RIGHT OF WAY (ALL ALLEY LOADED PRODUCT) 20' FROM INTERIOR RIGHT OF WAY (ALL STREET LOADED PRODUCT)
SIDE YARD SETBACKS	5' STREET LOAD / 3' ALLEY LOAD / 0' TOWNHOMES 5' FROM FRONT PROPERTY LINE FOR OPEN SPACE FRONTING UNITS
REAR YARD	5' FROM ALLEY RIGHT OF WAY (ALL ALLEY LOADED PRODUCT) 20' FROM REAR PROPERTY LINE (ALL STREET LOADED PRODUCT)
HEIGHT STANDARDS	3 STORIES MAX. IN 35 FEET. HEIGHT SHALL BE MEASURED FROM THE AVERAGE ELEVATION (4 MOST EXTERIOR CORNERS) TO THE MIDPOINT OF THE PRIMARY ROOF PITCH OR TO THE TOP OF THE PARAPET FOR A FLAT ROOF.
PARKING AND ACCESS	
ACCESS	ONE PUBLIC ROAD ACCESS & ONE PRIVATE ALLEY
DISTANCE TO NEAREST INTERSECTIONS	+/-1,000' WEST FROM PRIVATE DRIVE TO PETTUS ROAD +/-965' EAST FROM PUBLIC ROAD ACCESS TO EMERSON HILLS BLVD
DISTANCE TO INTERSECTION	+/-650' EAST FROM PUBLIC ROAD TO DRIVEWAY @13899 OLD HICKORY
REQUIRED PARKING	2 STALLS PER UNIT (SINGLE FAM) +2.5 STALLS PER UNIT (TOWNHOME)
PARKING PROVIDED	2 COVERED STALLS PER LOT/UNIT + SURFACE PARKING FOR GUESTS

Property Information  
Owner: Mike & Patsy Pence  
1064 West Bryan Road  
Elizabethtown, Kentucky 42702  
66.72 Total Acres (39.46 Acres Disturbed)  
Council District 31: John Rutherford

Developer  
Patterson Company  
1645 Westgate Circle  
Brentwood, Tennessee 37027  
Contact: Wes Patterson  
Phone: 615.467.8565

Civil Engineer  
Dale & Associates  
516 Heather Place  
Nashville, Tennessee 37204  
Contact: Michael Garrigan, PE  
Phone: 615.297.5166  
Email: michael@daleandassociates.net

Flood Note  
A Portion of this property is located within a Flood Hazard Area (Zone X) as depicted on the Flood Insurance Rate Map (FIRM)  
Number 47037C0393J dated 4/5/2017



Vicinity Map

## SHEET SCHEDULE

- C0.0 Cover Sheet
- C1.0 Overall Existing & Proposed Master Plan
- C1.1 Lot Table & Typical Layouts
- C2.0 Site Layout (1 of 2)
- C2.1 Site Layout (2 of 2)
- C3.0 Site Utility & Infrastructure (1 of 2)
- C3.1 Site Utility & Infrastructure (2 of 2)

Permits  
Case No 2025SP-007-001

D&A Associates  
Civil Engineering  
Land Planning & Zoning  
516 Heather Place  
Nashville TN 37204  
(615) 297-5166  
C0.0

D&A Project #24105  
13905 Old Hickory Boulevard

13905 Old Hickory Boulevard



### STREET LOADED LOTS

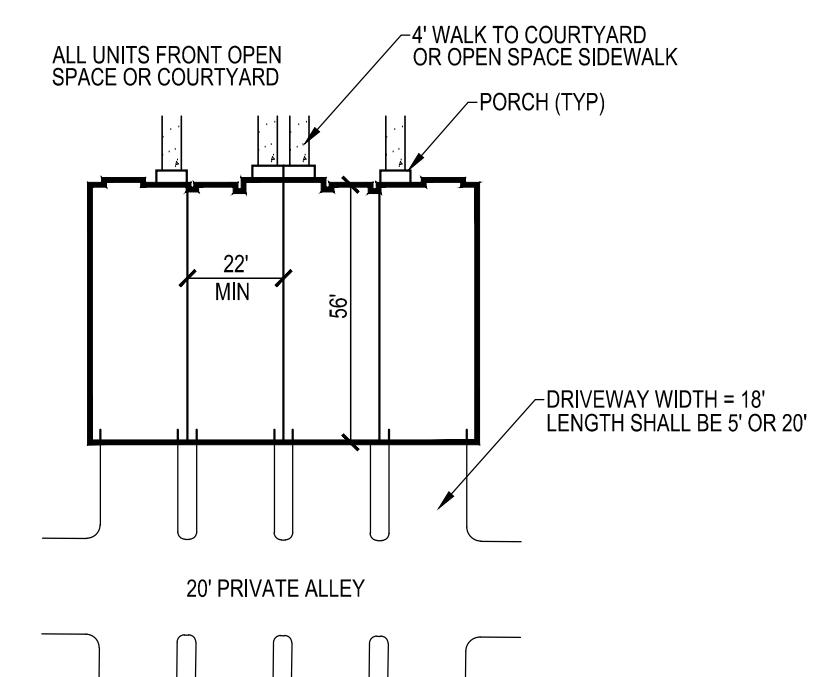
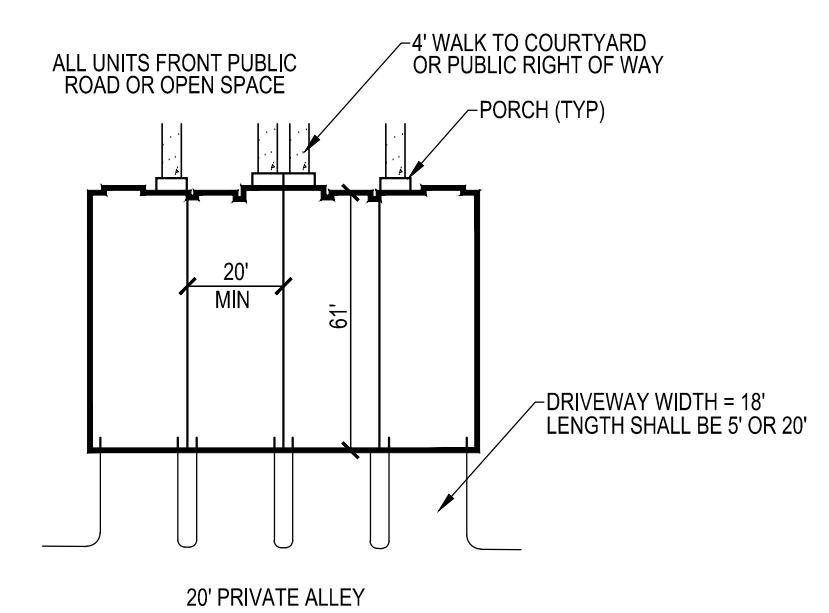
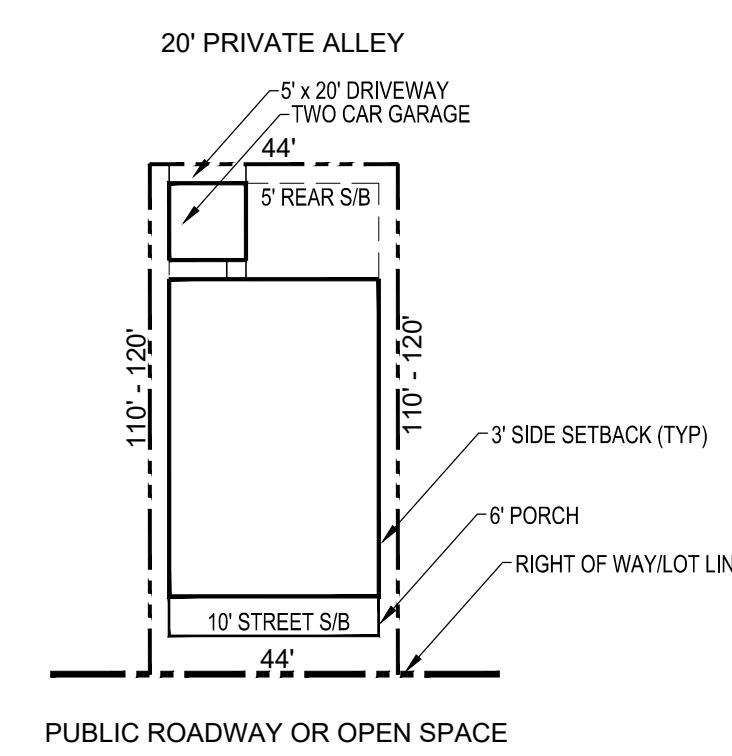
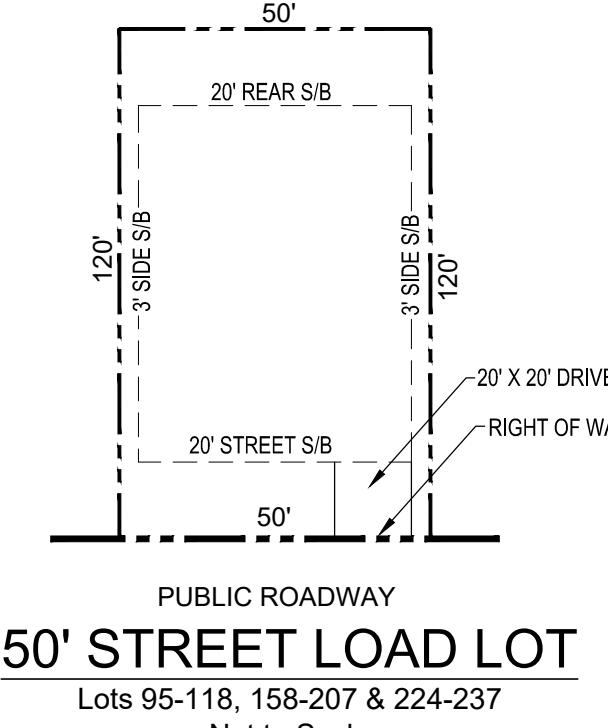
LOT NUMBER	SQ FOOTAGE
95	7,076
96	6,228
97	6,011
98	6,000
99	7,214
100	6,142
101	6,140
102	6,138
103	6,136
104	6,135
105	6,133
106	6,131
107	6,028
108	6,565
109	6,580
110	6,596
111	6,620
112	6,640
113	6,661
114	6,683
115	6,300
116	6,250
117	6,250
118	7,500
158	6,401
159	6,088
160	6,000
161	6,000
162	6,000
163	6,000
164	6,694
165	6,741
166	6,504
167	6,000
168	6,250
169	7,256
170	7,111
171	6,250
172	6,250
173	6,342
174	8,016
175	5,785
176	6,250
177	6,250

### ALLEY LOADED LOTS

LOT NUMBER	SQ FOOTAGE
178	6,250
179	6,250
180	7,033
181	6,250
182	6,048
183	6,735
184	8,310
185	8,201
186	6,793
187	6,120
188	6,120
189	6,120
190	6,120
191	6,120
192	6,120
193	9,233
194	7,048
195	7,106
196	7,579
197	7,909
198	6,250
199	6,250
200	6,250
201	6,250
202	6,250
203	6,250
204	6,250
205	5,927
206	6,367
207	8,213
224	6,000
225	6,000
226	6,000
227	6,000
228	6,000
229	6,000
230	6,000
231	6,000
232	6,000
233	6,000
234	6,000
235	6,709
236	6,767
237	6,407

### TOWNHOME UNITS

LOT NUMBER	SQ FOOTAGE
24	2,547
25	1,505
26	1,500
27	1,500
28	1,804
29	1,920
30	1,500
31	1,501
32	1,802
33	2,056
34	1,500
35	1,500
36	1,501
37	1,809
38	3,444
39	1,500
40	1,500
41	1,800
42	2,031
43	1,500
44	1,500
45	1,500
46	1,802
47	2,048
48	1,500
49	1,500
50	2,243
51	1,992
52	1,500
53	1,501
54	1,504
55	1,961
56	1,934
57	1,501
58	1,500
59	2,102
60	1,725
61	1,500
62	1,500
63	1,500
64	3,007
65	2,396
66	1,500
67	1,500
68	1,500
69	1,800
70	1,799
71	1,500
72	1,501



Lot Table &  
Typical Layouts

Dale & Associates  
Civil Engineering,  
Surveying,  
Land Planning & Zoning  
516 Heather Place  
Nashville, TN 37204  
(615) 297-5166

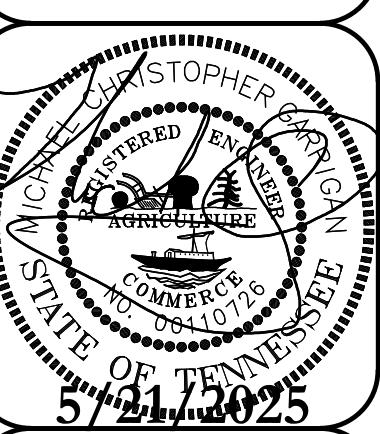
D&A Project #24105  
13905 Old Hickory Boulevard

Permits	Case No	2025SP-007-001
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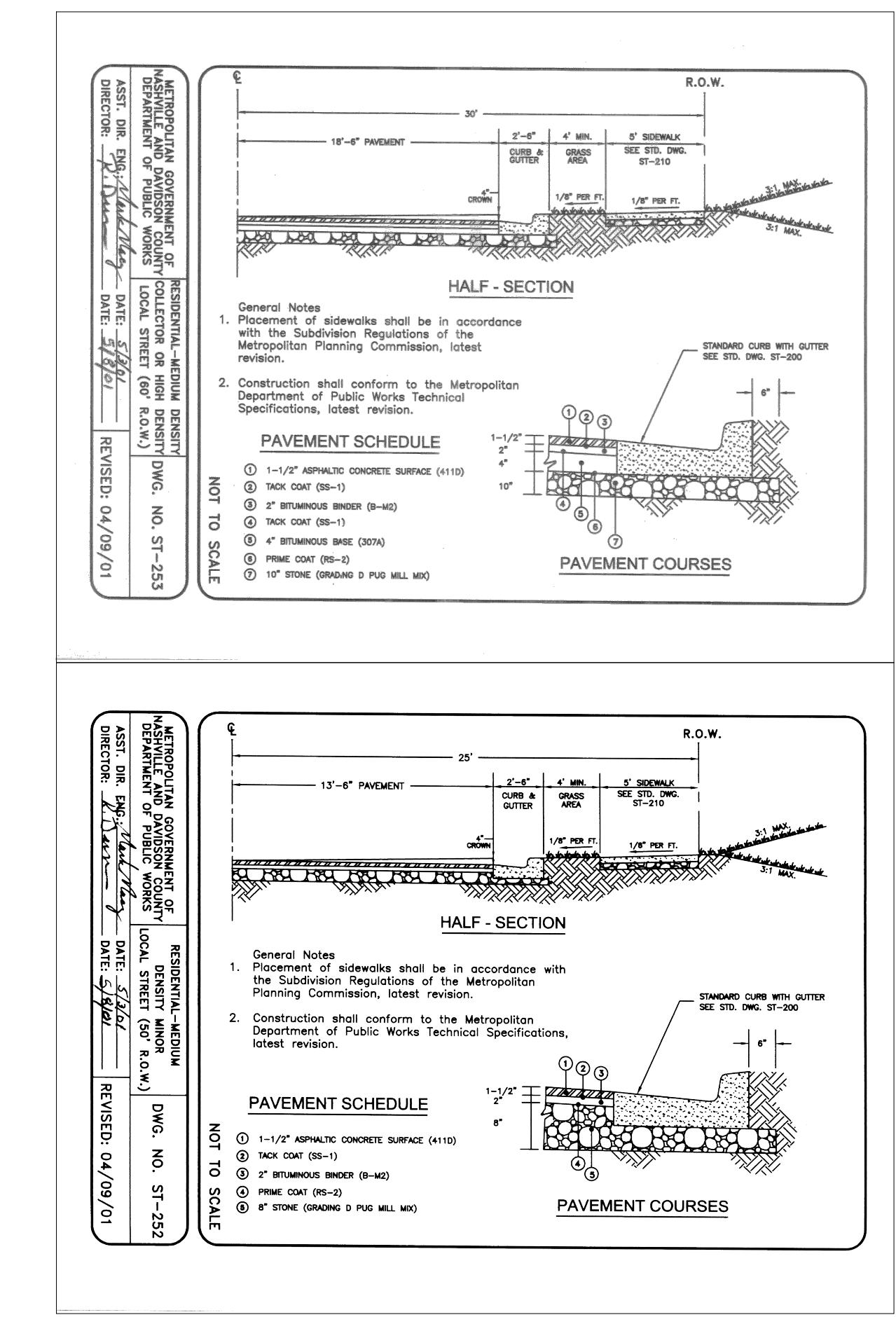
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# 13905 Old Hickory Boulevard

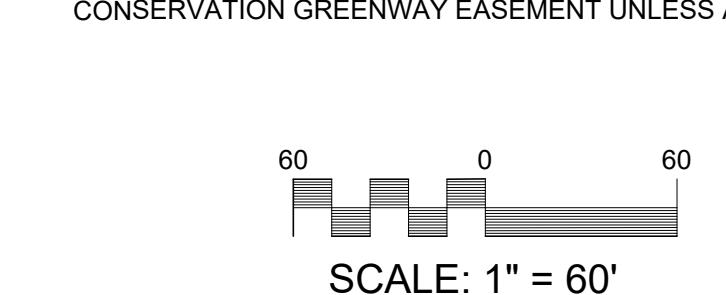
Being Parcels 41 & 42 on Metro Tax Map 182  
Antioch, Davidson County, Tennessee



Site Layout  
(1 of 2)



## METRO PARKS & GREENWAY NOTES



Permits  
Case No. 2025SP-007-001

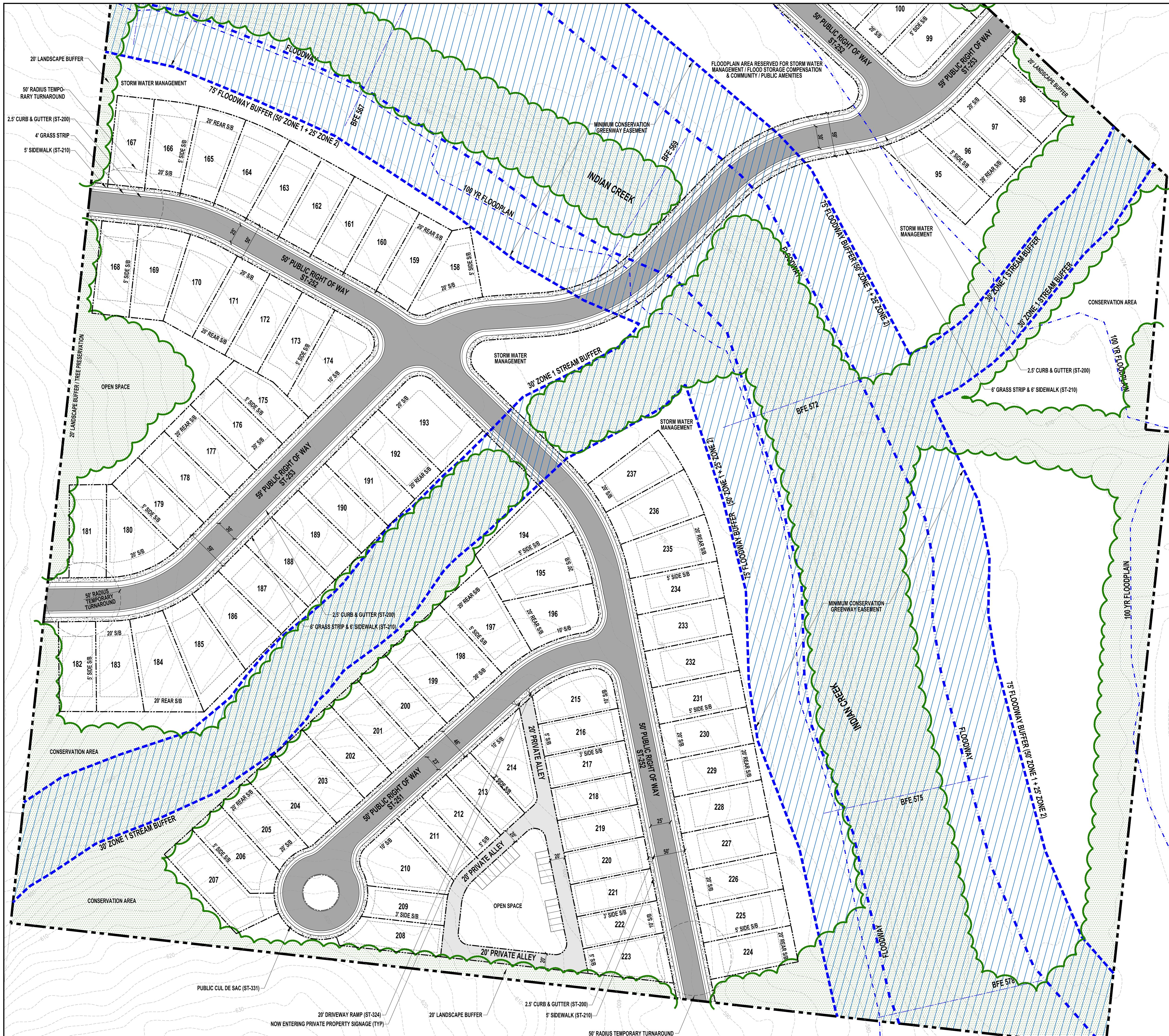
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(615) 277-5166

D&A Project #24105  
13905 Old Hickory Boulevard

Civil Engineering,  
Land Planning &  
Surveying



C2.0



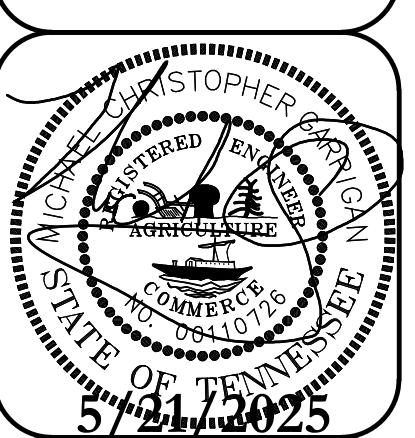
- GENERAL PLAN NOTES**

  - 1) SOLID WASTE & RECYCLING TO BE PROVIDED BY METRO WATER SERVICES VIA ROLL-AWAY CANS FOR THE SINGLE FAMILY & A PRIVATE HAULER FOR THE TOWNHOMES.
  - 2) DRAWING IS FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT, AS IT PERTAINS TO STORM WATER APPROVAL / COMMENTS ONLY. THE FINAL UNITS COUNT AND DETAILS OF THE PLAN SHALL BE GOVERNED BY THE FINAL SP & THE APPROPRIATE STORM WATER REGULATIONS AT THE TIME OF THE FINAL APPLICATION.
  - 3) THE FINAL SP SHALL DEPICT LANDSCAPE COMPLIANCE WITH THE ZONING CODE. THIS INCLUDES BUT MAY NOT BE LIMITED TO PERIMETER BUFFERYARDS, TREE DENSITY, STREET TREES AND SCREENING OF PARKING AREAS.
  - 4) PRIVATE ALLEYS TO BE CONSTRUCTED TO NDOT ST-263.
  - 5) A MINIMUM OF 2.5 CODE COMPLIANT PARKING SPACES PER UNIT SHALL BE PROVIDED FOR THE TOWNHOMES.

Drawing Date:  
November 2024

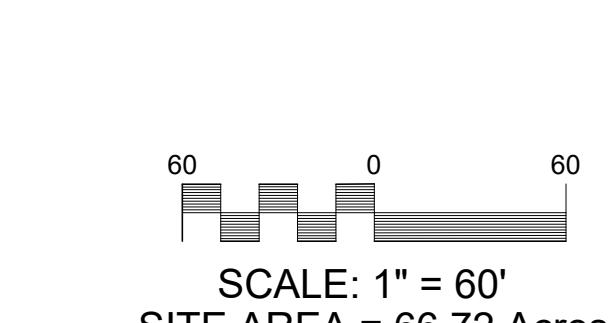
13905 Old Hickory Boulevard Preliminary SP

Being Parcels 41 & 42 on Metro Tax Map 182  
Antioch, Davidson County, Tennessee



## METRO PARKS & GREENWAY NOTES

- 1) A CONSERVATION EASEMENT, TO INCLUDE A MINIMUM OF THE INDIAN CREEK FLOODWAY PLUS 75' ZONE 1 + ZONE 2 BUFFERS, SHALL BE PROVIDED PRIOR TO THE FINAL SITE PLAN APPROVAL. EASEMENT SHALL BE CLEARLY DEPICTED BY BOUNDARY SURVEY, METES AND BOUNDS ON A SEPERATE SHEET WITH THE FINAL SITE PLAN.
  - 2) PROVIDE A LANDSCAPE/FENCING PLAN TO PROVIDE PARK-LIKE GREENWAY EXPERIENCE AND SCREEN SURFACE PARKING/DRIVEWAYS TO THE GREATEST EXTENT REASONABLY POSSIBLE WITH THE FINAL SITE
  - 3) GARBAGE CONTAINERS, DUMPSTERS, HVAC UNITS, OR LIKE SHALL NOT FRONT THE GREENWAY AND MUST BE SCREENED FROM GREENWAY VIEW.
  - 4) ANY ACCESS FROM THE SP DEVELOPMENT ONTO THE GREENWAY SHALL BE APPROVED BY METRO PARKS WITH THE FINAL SITE PLAN APPROVAL.
  - 5) THE CONSERVATION GREENWAY EASEMENT MUST BE DEDICATED AND MEMORIALIZED IN A METRO PARKS' CONSERVATION GREENWAY EASEMENT AGREEMENT, INCLUDING TWO EXHIBITS: A LEGAL DESCRIPTION AND A BOUNDARY SURVEY OF THE EASEMENT, ALL OF WHICH MUST BE EXECUTED BY THE PROPERTY OWNER, NOTARIZED, SUBMITTED TO, AND APPROVED BY GREENWAYS PRIOR TO FINAL SITE PLAN APPROVAL.
  - 6) PRIOR TO FINAL SP APPROVAL, APPLICANT SHALL COORDINATE WITH GREENWAYS STAFF TO DETERMINE THE FINAL EXTENT OF THE CONSERVATION GREENWAY EASEMENT NECESSARY TO ENSURE THAN AN AT GRADE GREENWAY TRAIL CROSSING OF THE PUBLIC RIGHT OF WAY AT AN APPROVED LOCATION NEAR THE BRIDGE CROSSING INDIAN CREEK CAN BE MADE WITH MINIMAL EARTWORK REQUIRED FOR THE CONSTRUCTION OF A GREENWAY TRAIL WITH CONNECTIONS TO THE PUBLIC SIDEWALKS THAT WILL BE FLUSH AND OTHERWISE ADA COMPLIANT. FINAL SP SHALL PROVIDE SUFFICIENT TRAIL LOCATION AND GRADING INFORMATION ON BOTH SIDES OF THE PUBLIC RIGHT OF WAY FOR ADA REVIEW.
  - 7) COORDINATE WITH GREENWAYS STAFF TTO PROCESS THE CONSERVATION GREENWAY EASEMENT AGREEMENT FOR PARK BOARD AND METRO COUNCIL APPROVAL.
  - 8) THE CONSERVATION GREENWAY EASEMENT AGREEMENT MUST BE RECORDED WITH THE DAVIDSON COUNTY REGISTER OF DEED'S OFFICE PRIOR TO ANY USE AND OCCUPANCY OR ISSUANCE OF THE FIRST USE AND OCCUPANCY LETTER FOR ANY NEW SINGLE FAMILY OR MULTIFAMILY RESIDENTIAL BUILDING.
  - 9) MAINTENANCE WITHIN GREENWAY CONSERVATION EASEMENT TO BE PERFORMED BY OWNER UNTIL SUCH TIME AS THE GREENWAY IS BUILT OR UPON OTHER APPROVED BY PARKS.
  - 10) GREENWAY TRAIL TO BE OPEN TO THE PUBLIC 365 DAYS/YEAR PER METRO PARKS APPROVED OPERATING HOURS, EXCEPT FOR TIMES OF ROUTINE MAINTENANCE WHERE TEMPORARY CLOSURE IS REQUIRED FOR PUBLIC SAFETY.
  - 11) NO OBSTRUCTIONS SUCH AS SITE FURNITURE, SIGNAGE, AND LIGHTING ARE ALLOWED IN A CONSERVATION GREENWAY EASEMENT UNLESS APPROVED BY GREENWAYS STAFF.



Permits  
Case No. 2025SP-007-001

Class 178      100-1001-007-002

Q&A Project #24105  
5 Old Hickory Boulevard

## STORM WATER MANAGEMENT NOTES

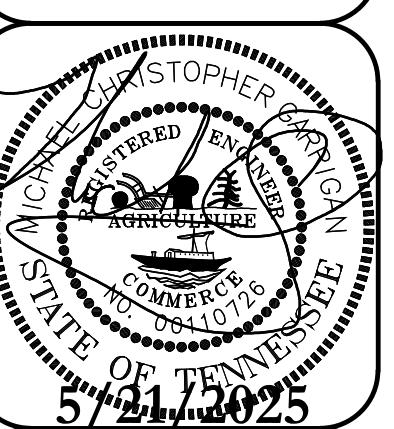
Drawing Date:  
November 2024

Revisions

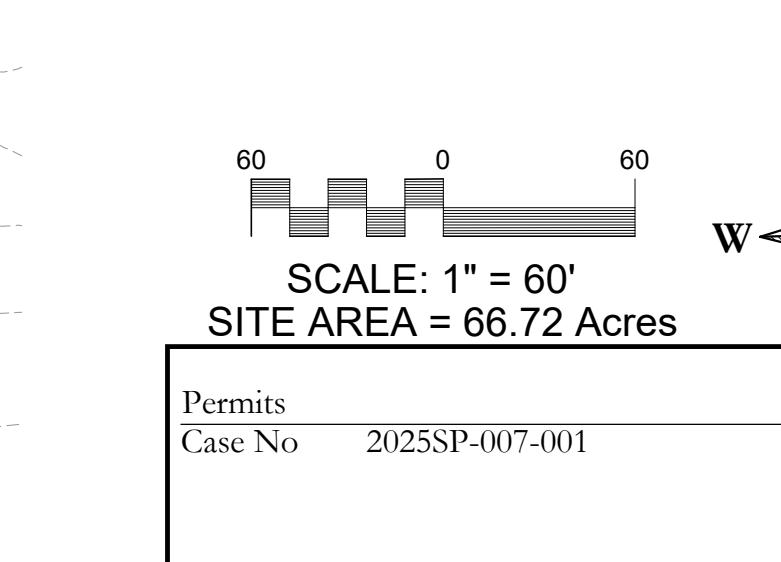
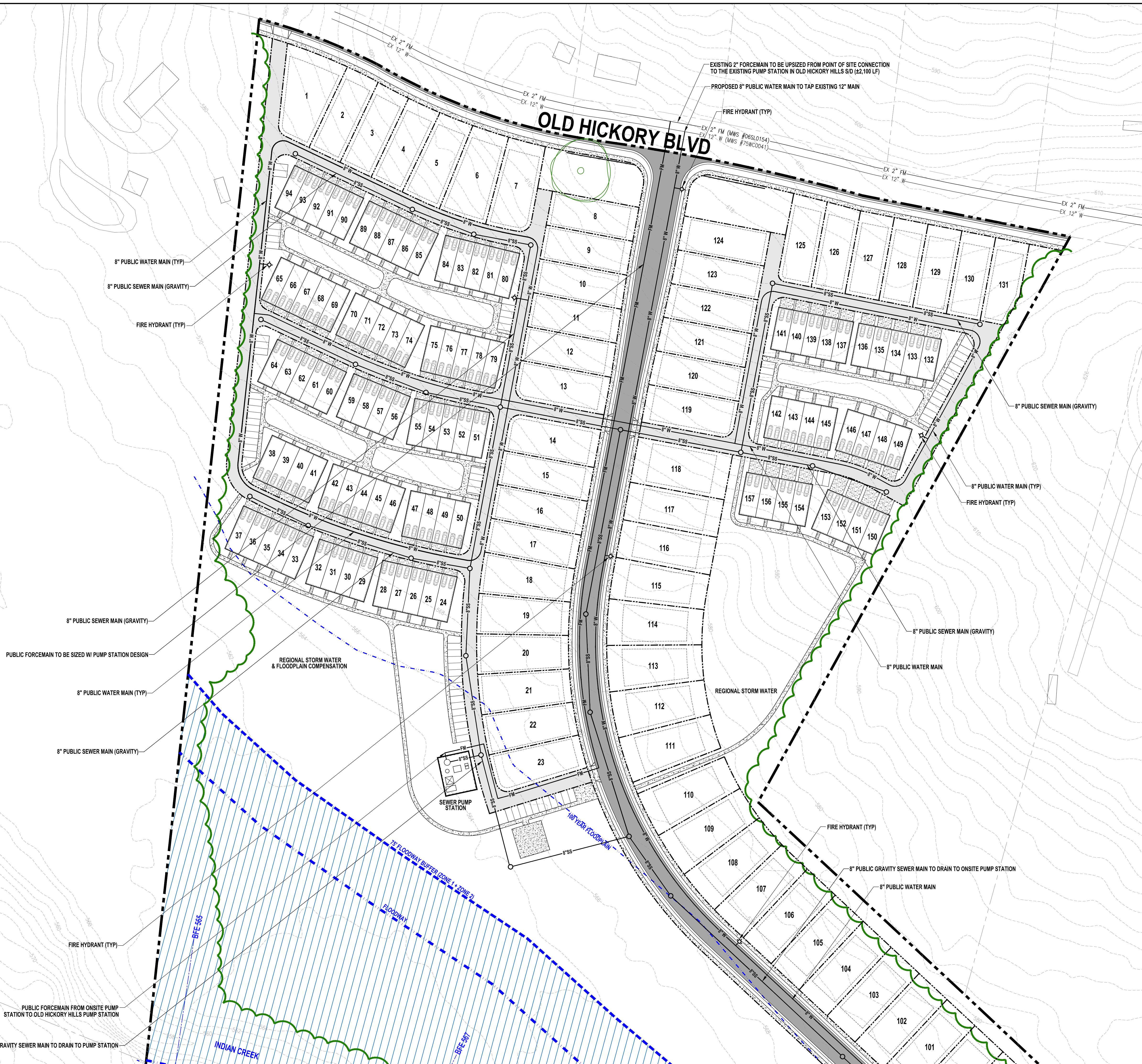
- 1) THE SOIL TYPES FOR THIS SITE ARE MIX OF TYPE 'B' SOILS (CONSISTING OF ARMOUR, ARRINGTON AND LINDELL), TYPE 'C' SOILS (CONSISTING OF BRADYVILLE & TALBOTT) AND TYPE 'D' SOILS (BARFIELD). THE 'B' SOILS MAKE UP APPROXIMATELY 55% OF THE SITE & ARE PRIMARILY LOCATED WITHIN THE FLOOD PRONE AREAS. THE 'C' SOILS MAKE UP APPROXIMATELY 31% OF THE SITE AND ARE LOCATED PRIMARILY WITHIN THE PROPOSED AREAS OF DISTURBANCE OR THE DEVELOPMENT FOOTPRINT. THE 'D' SOILS MAKE UP THE FINAL 14% OF THE PROPERTY AND ARE LOCATED NEAR THE NORTHEAST CORNER OF THE SITE.
- 2) THIS SITE IS RESPONSIBLE FOR BOTH WATER QUALITY & QUANTITY. PRELIMINARY DESIGN PROPOSES TO MEET THE REQUIREMENTS PRIMARILY THROUGH THE SIX REGIONAL STORM WATER AREAS LOCATED ON THE DRAWINGS. ADDITIONAL AREAS OF MORE LOCAL TREATMENT & DETENTION ARE ANTICIPATED TO BE LOCATED THROUGHOUT THE PROPERTY IN ORDER TO COMPLETE THE SITE'S STORM WATER MANAGEMENT DESIGN. THE REGIONAL AREAS ARE ANTICIPATED TO CONSIST OF BIORETENTION COUPLED WITH DRY PONDS FOR ADDITIONAL QUANTITY STORAGE & THE LOCAL STORM WATER MANAGEMENT AREAS ARE ANTICIPATED TO CONSIST OF PERMEABLE PAVERS WITHIN THE VISITOR PARKING SPACES AND/OR DRIVEWAYS. ALONG WITH POCKET BIORETENTION / INFILTRATION TRENCHES DESIGNED IN CONJUNCTION WITH AMENITIES LOCATED WITHIN THE COURTYARDS & OTHER OPEN SPACES. THE SITE'S DESIGN PROVIDES FOR AMPLE GREEN SPACE SUCH THAT STORM WATER MANAGEMENT CAN BE PROVIDED TO MEET OR EXCEED REQUIREMENTS WITHOUT BEING OBVIOUS OR UNAPPEALING.
- 3) THE BRIDGE CROSSING OF INDIAN CREEK SHALL BE A SPAN BRIDGE OR BOTTOMLESS (3 SIDED) BOX OR ARCHED CULVERT. A FLOOD STUDY SHALL BE PROVIDED WITH THE FINAL SP SHOWING A NO-RISE CONDITION.
- 4) THE PRELIMINARY DESIGN OF THIS SITE DEPICTS AREAS THAT WILL NECESSITATE FILL WITHIN THE FLOOD PLAIN. SAID AREAS INCLUDE BUT MAY NOT BE LIMITED TO LOTS 22, 23 & 37 AS WELL AS THE SEWER PUMP STATION. THESE AREAS OF FILL ARE PROPOSED TO BE COMPENSATED BY CUT WITHIN THE THREE AREAS NOTED AS FLOODPLAIN STORAGE COMPENSATION AREAS. THESE THREE AREAS HAVE BEEN SPECIFICALLY CHOSEN TO BE IN LOCATIONS WHERE TREE CANOPY DOES NOT CURRENTLY EXIST SO THAT THE FLOOD STORAGE REMOVED CAN BE REPLACED WITHOUT ADVERSE IMPACT.
- 5) STORM SEWER SYSTEM ON THIS PLAN IS SHOWN SCHEMATICALLY. FINAL DESIGN WILL BE PROVIDED DURING THE FINAL SP PROCESS AND MUST SHOW COMPLIANCE WITH THE REQUIREMENTS OF THE STORMWATER MANAGEMENT MANUAL.

## WATER & SEWER NOTES

Preliminary SP  
13905 Old Hickory Boulevard  
Being Parcels 41 & 42 on Metro Tax Map 182  
Antioch, Davidson County, Tennessee



Site Utility &  
Infrastructure  
(1 of 2)



D&A Project #24054  
Nashville Event Center

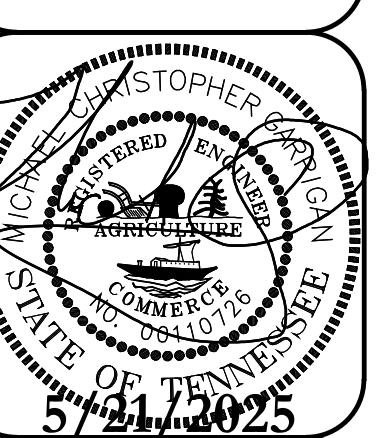
516 Heather Place  
(615) 297-5166

Civil Engineering  
Land Planning & Zoning

**Dale & Associates**  
C3.0

# 13905 Old Hickory Boulevard

Being Parcels 41 & 42 on Metro Tax Map 182  
Antioch, Davidson County, Tennessee



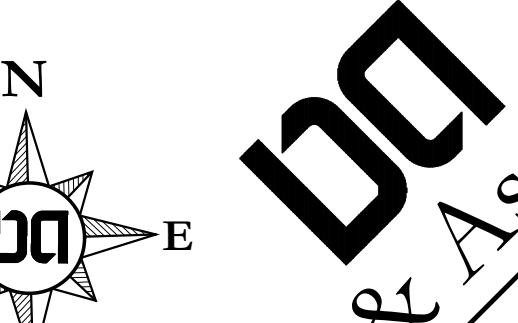
Site Utility &  
Infrastructure  
(2 of 2)

## STORM WATER MANAGEMENT NOTES

- THE SOIL TYPES FOR THIS SITE ARE MIX OF TYPE 'B' SOILS (CONSISTING OF ARMOUR, ARRINGTON AND LINDELL), TYPE 'C' SOILS (CONSISTING OF BRADYVILLE & TALBOT) AND TYPE 'D' SOILS (BARFIELD). THE 'B' SOILS MAKE UP APPROXIMATELY 55% OF THE SITE & ARE PRIMARILY LOCATED WITHIN THE FLOOD PRONE AREAS. THE 'C' SOILS MAKE UP APPROXIMATELY 31% OF THE SITE AND ARE LOCATED PRIMARILY WITHIN THE PROPOSED AREAS OF DISTURBANCE OR THE DEVELOPMENT FOOTPRINT. THE 'D' SOILS MAKE UP THE FINAL 14% OF THE PROPERTY AND ARE LOCATED NEAR THE NORTHEAST CORNER OF THE SITE.
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- STORM SEWER SYSTEM ON THIS PLAN IS SHOWN SCHEMATICALLY. FINAL DESIGN WILL BE PROVIDED DURING THE FINAL SP PROCESS AND MUST SHOW COMPLIANCE WITH THE REQUIREMENTS OF THE STORMWATER MANAGEMENT MANUAL.

## WATER & SEWER NOTES

- SEWER SERVICE TO BE PROVIDED BY METRO WATER SERVICES. PRELIMINARY DESIGN IS TO PROVIDE PUBLIC GRAVITY SEWER THROUGHOUT THE SITE THAT DRAINS TO THE PUMP STATION SHOWN ON THIS DRAWING. FROM THE PUMP STATION, THE FORECEMAIN WILL CARRY SEWERAGE TO THE EXISTING PUMP STATION IN OLD HICKORY HILLS SUBDIVISION, APPROXIMATELY 2,000 LF EAST OF THE SUBJECT SITE.
- AS OUTLINED IN THE AVAILABILITY LETTER DATED 11/7/2024 THERE ARE OFFSITE IMPROVEMENTS TO THE EXISTING SEWERAGE SYSTEM DOWNSTREAM OF THE SUBJECT SITE THAT MUST BE COMPLETED IN ASSOCIATION WITH THIS DEVELOPMENT.
- WATER SERVICE TO BE PROVIDED BY METRO WATER SERVICES. PRELIMINARY DESIGN IS TO EXTEND NEW PUBLIC 8" WATER MAINS THROUGHOUT THE SITE UTILIZING INDIVIDUAL METERS AND GANG BOX METERS AS SHOWN. FIRE HYDRANTS ARE SHOWN TO BE PROVIDED IN ACCORDANCE WITH REQUIREMENTS OF THE FIRE MARSHAL'S OFFICE FOR PROPER COVERAGE.
- WATER & SEWER DESIGNS SHOWN HEREIN ARE SCHEMATIC FOR PRELIMINARY PURPOSES ONLY. FINAL WATER & SEWER DESIGNS WILL BE PROVIDED IN DETAIL WITH THE FINAL SP.



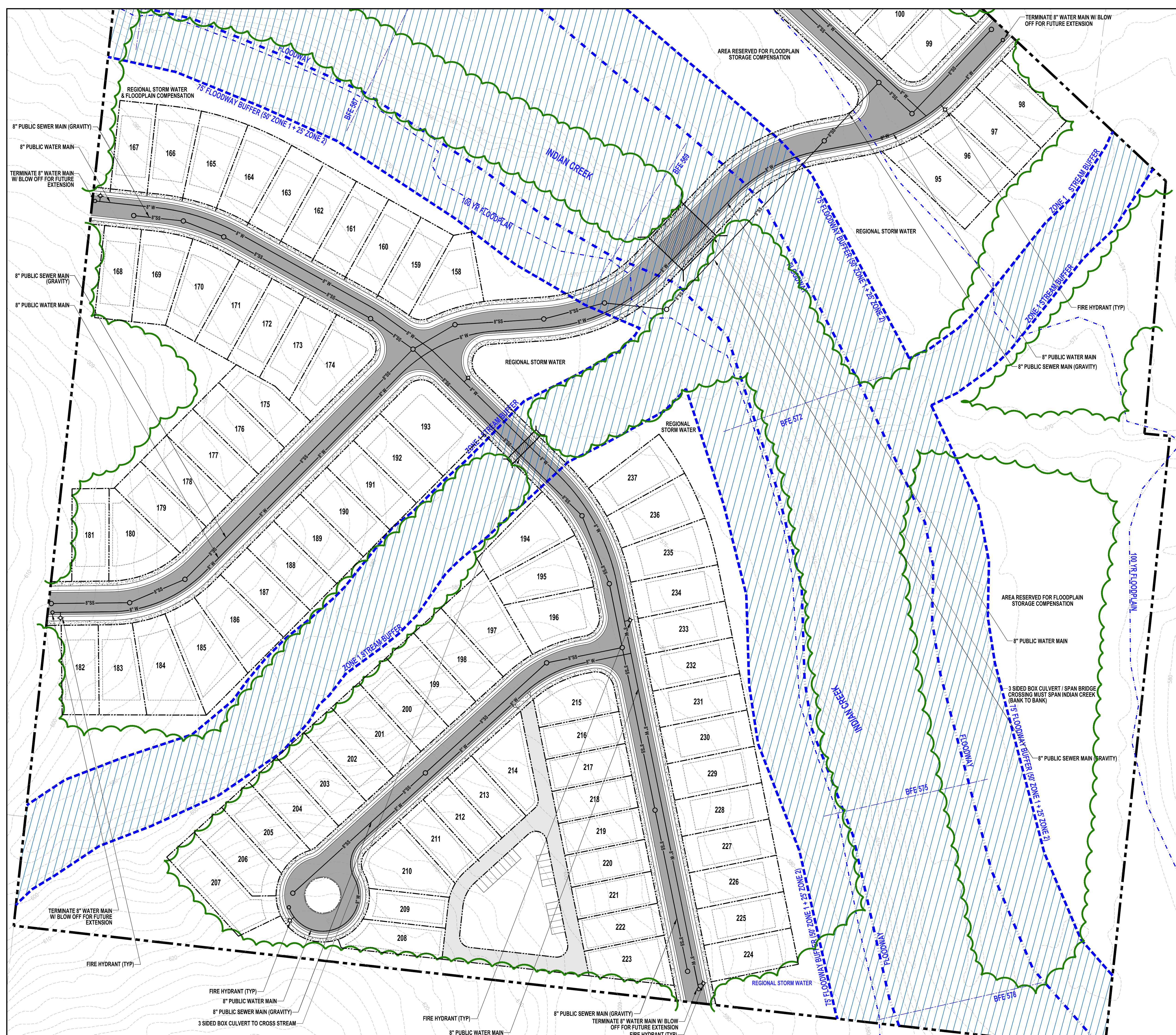
D&A Project #24054  
Nashville Event Center

C3.1

N  
W  
E  
S

SCALE: 1" = 60'  
SITE AREA = 66.72 Acres

Permits	Case No.	2025SP-007-001
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PATTERSON  
COMPANY

# Elmcrest

**Kevin Richter**

615-416-5113



# Community Amenities

## Gathering Places

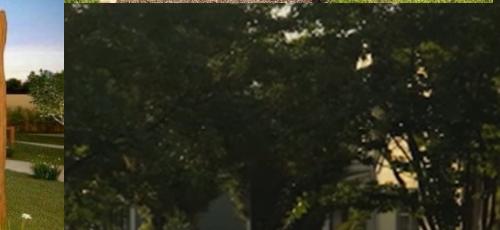
- Community Pavilion
- Landscaped Pocket Parks
- Covered Mail Box Kiosk Center
- Firepit



# Community Amenities

## Recreation

- Side Walks Throughout
- Playgrounds
- Exercise Equipment
- Hammock Garden
- Disk Golf



*Our*  
**HOMES**

**Single Family**

- 2-3 car garage
- 1600-3400 sq. ft.





# *Our* Townhomes

- 1 car garage
- 2 car garages