

## GRANT APPLICATION SUMMARY SHEET

**Grant Name:** Greenways TAP Opry Mills 2024  
**Department:** PARKS & RECREATION  
**Grantor:** TENNESSEE DEPARTMENT OF TRANSPORTATION  
**Pass-Through Grantor (If applicable):**  
**Total Applied For** \$12,821,417.78  
**Metro Cash Match:** \$5,494,893.33  
**Department Contact:** Alan Enzo  
 862-8400  
**Status:** CONTINUATION

**Program Description:**

Greenways TAP Opry Mills 2024 Grant. Metro Parks is applying for a Transportation Alternatives Program grant from the State of TN with estimated award of \$12,821,417.78, with a match of \$5,494,893.33 provided by Metro Parks, for the construction of the Opry Mills Connector. This multi-use path links the Stones River Greenway and Cumberland River Greenway to the Opry Mills entertainment, retail, and hospitality campus. Parks Match will come from Fund 40119, BU 40402119. Note: Grant period may change based on when award is received.

**Plan for continuation of services upon grant expiration:**

Project will be funded into the future through multiple year capital allocations.

**APPROVED AS TO AVAILABILITY OF FUNDS:**

**APPROVED AS TO FORM AND LEGALITY:**

Kevin Crumbaker 9/20/2024 | 8:12 AM PDT  
**Director of Finance** **Date**  
*AP RW*

Courtney Mohan 9/20/2024 | 10:41 AM CDT  
**Metropolitan Attorney** **Date**

**APPROVED AS TO RISK AND INSURANCE:**

Balagun Cobb 9/20/2024 | 10:18 AM CDT  
**Director of Risk Management** **Date**  
**Services**

Freddie O'Connell 9/20/2024 | 8:53 AM PDT  
 Metropolitan Mayor **Date**  
 (This application is contingent upon approval of the application by the Metropolitan Council.)

### Grants Tracking Form

Part One

<b>Pre-Application</b> <input type="radio"/>		<b>Application</b> <input checked="" type="radio"/>		<b>Award Acceptance</b> <input type="radio"/>		<b>Contract Amendment</b> <input type="radio"/>	
<b>Department</b>	<b>Dept. No.</b>	<b>Contact</b>				<b>Phone</b>	<b>Fax</b>
PARKS & RECREATION	040	Alan Enzo				862-8400	862-8414
<b>Grant Name:</b>	Greenways TAP Opry Mills 2024						
<b>Grantor:</b>	TENNESSEE DEPARTMENT OF TRANSPORTATION	<b>Other:</b>					
<b>Grant Period From:</b>	03/01/25	<b>(applications only) Anticipated Application Date:</b>		09/20/24			
<b>Grant Period To:</b>	02/28/30	<b>(applications only) Application Deadline:</b>		10/04/24			
<b>Funding Type:</b>	STATE	<b>Multi-Department Grant</b>		<input type="checkbox"/> <b>If yes, list below.</b>			
<b>Pass-Thru:</b>		<b>Outside Consultant Project:</b>		<input type="checkbox"/>			
<b>Award Type:</b>	COMPETITIVE	<b>Total Award:</b>		\$12,821,417.78			
<b>Status:</b>	CONTINUATION	<b>Metro Cash Match:</b>		\$5,494,893.33			
<b>Metro Category:</b>	Est. Prior.	<b>Metro In-Kind Match:</b>		\$0.00			
<b>CFDA #</b>	N/A	<b>Is Council approval required?</b>		<input checked="" type="checkbox"/>			
<b>Project Description:</b>	<b>Applic. Submitted Electronically?</b> <input checked="" type="checkbox"/>						
<p>Greenways TAP Opry Mills 2024 Grant. Metro Parks is applying for a Transportation Alternatives Program grant from the State of TN with estimated award of \$12,821,417.78, with a match of \$5,494,893.33 provided by Metro Parks, for the construction of the Opry Mills Connector. This multi-use path links the Stones River Greenway and Cumberland River Greenway to the Opry Mills entertainment, retail, and hospitality campus. Parks Match will come from Fund 40119, BU 40402119. Note: Grant period may change based on when award is received.</p>							
<b>Plan for continuation of service after expiration of grant/Budgetary Impact:</b>							
Project will be funded into the future through multiple year capital allocations.							
<b>How is Match Determined?</b>							
<b>Fixed Amount of \$</b>		or		30.0%		<b>% of Grant</b>	
						<b>Other:</b> <input type="checkbox"/>	
<b>Explanation for "Other" means of determining match:</b>							
<b>For this Metro FY, how much of the required local Metro cash match:</b>							
<b>Is already in department budget?</b>		Y		<b>Fund</b>		40119 <b>Business Unit</b>	
<b>Is not budgeted?</b>				<b>Proposed Source of Match:</b>		40402119	
<b>(Indicate Match Amount &amp; Source for Remaining Grant Years in Budget Below)</b>							
<b>Other:</b>							
<b>Number of FTEs the grant will fund:</b>		0.00		<b>Actual number of positions added:</b>		0.00	
<b>Departmental Indirect Cost Rate</b>		17.91%		<b>Indirect Cost of Grant to Metro:</b>		\$3,280,451.32	
<b>*Indirect Costs allowed?</b> <input type="radio"/> Yes <input checked="" type="radio"/> No		<b>% Allow.</b>		<b>Ind. Cost Requested from Grantor:</b>		\$0.00 <b>in budget</b>	
		0.00%					
<b>*(If "No", please attach documentation from the grantor that indirect costs are not allowable. See Instructions)</b>							
<b>Draw down allowable?</b> <input type="checkbox"/>							
<b>Metro or Community-based Partners:</b>							

Part Two

Grant Budget										
Budget Year	Metro Fiscal Year	Federal Grantor	State Grantor	Other Grantor	Local Match Cash	Match Source (Fund, BU)	Local Match In-Kind	Total Grant Each Year	Indirect Cost to Metro	Ind. Cost Neg. from Grantor
Yr 1	FY25	\$0.00	\$12,821,417.78	\$0.00	\$5,494,893.33	40119, 40402119	\$0.00	\$18,316,311.11	\$3,280,451.32	\$0.00
Yr 2	FY__									
Yr 3	FY__									
Yr 4	FY__									
Yr 5	FY__									
<b>Total</b>		<b>\$0.00</b>	<b>\$12,821,417.78</b>	<b>\$0.00</b>	<b>\$5,494,893.33</b>		<b>\$0.00</b>	<b>\$18,316,311.11</b>	<b>\$3,280,451.32</b>	<b>\$0.00</b>
<b>Date Awarded:</b>					<b>Tot. Awarded:</b>		<b>Contract#:</b>			
(or) <b>Date Denied:</b>					<b>Reason:</b>					
(or) <b>Date Withdrawn:</b>					<b>Reason:</b>					

Contact: [vaughn.wilson@nashville.gov](mailto:vaughn.wilson@nashville.gov)  
[juanita.paulsen@nashville.gov](mailto:juanita.paulsen@nashville.gov)

Rev. 5/13/13  
5908

GCP Received  
09/12/2024



GCP Approved 09/13/2024

**Transportation Alternatives Program Application 2025**

Organization: Nashville

**Application Requirements**

- 1. For detailed instructions, access the [Transportation Alternatives Program Instruction Booklet](#).
- 2. Project applications MUST be complete and contain all supporting materials as outlined in the application.
- 3. The Transportation Alternatives Program (TAP) is a reimbursable grant program. Local governments are required to pay 100% of all project invoices up-front prior to seeking reimbursement of the 80% federal share.

**Application Submittal Information**

- Check box if re-submittal of a previously non-awarded application.  
Provide the year of last submission:
- Check box if application was also submitted to the Chattanooga, Knoxville, Nashville, or Memphis MPO for TAP funding consideration.

**Project Name and Physical Location**

Project Title: Opry Mills Greenway Connector

Project Ranking (if multiple applications submitted by applicant):

Physical Address for Project (including 9-digit zip code):

Street Address:	0 Two Rivers Ct., Nashville, TN 37214-1207 to 433 Opry Mills Dr., Nashville, TN 37214-2440		
City:	Nashville	State: Tennessee	Zip Code: 37214-2440
County:	Davidson County		
Current Applicant Population:	687788	Applicant Unique Entity Identifier (UEI):	LGZLHP6ZHM55

**Local Agency Point of Accountability**

Agency Name:	Metropolitan Government of Nashville & Davidson Cty., Dept. of Parks and Recreation, Greenways & Open Space Div.		
Contact Name:	Cindy Harrison	Title: Asst. Director Metro Nashville Parks & Recreation	
Phone:	(615) 862-8400	Email: cindy.harrison@nashville.gov	

**Design Consultant Contact (if applicable)**

Company Name:	Collier Engineering		
Contact Name:	Mike Weisz	Title: Project Manager	
Phone:	615-331-1441	Email: mike.weisz@collierengineering.com	

**Transportation Alternatives Program Application 2025**

Organization: Nashville

Select only those that apply: (Descriptions of each activity can be located in the [Transportation Alternatives Program Instruction Booklet](#).)

- Pedestrian and Bicycle Facilities** – All facilities must be hard-surfaced, ADA compliant and provide adequate linear connectivity. Sidewalks must be a minimum of 5-feet wide and shared-use facilities must be a minimum of 10-feet wide. TAP funds can be used for sidewalks, walkways or curb ramps; bike lane striping, wide paved shoulders, bike parking and bus racks; pedestrian safety/traffic calming; off-road trails; bike and pedestrian bridges and underpasses and ADA compliance. Funding may not be used for spot improvements, repairs or maintenance
- Safe Routes for Non-Drivers** – Access and accommodation for children, older adults and individuals with disabilities
- Conversion of Abandoned Railway Corridors to Trails** – Construction of pedestrian or shared-use hard-surfaced, ADA accessible trails and rail-with-trail projects
- Scenic Turnouts and Overlooks** – Construction of scenic turnouts, overlooks and viewing areas
- Outdoor Advertising Management** – Billboard inventories and removal of illegal and nonconforming billboards. Inventory control may include, but not be limited to, data collection, acquisition and maintenance of digital aerial photography, video logging, scanning and imaging of data, developing and maintain an inventory and control database and hiring of outside legal counsel
- Historic Preservation & Rehab of Historic Transportation Facilities** – Preservation of buildings and facades in historical districts and restoration of historic buildings for transportation-related purposes; access improvements to historic sites. Restoration of railroad depots, bus stations and lighthouses; rehabilitation of rail trestles, tunnels, bridges and canals. All applications are subject to State Historic Preservation Office review and approval.
- Vegetation Management** – Improvement of roadway safety; prevention of invasive species; providing erosion control
- Archaeological Activities** – Projects related to impacts from implementation of current highway construction projects
- Storm Water Mitigation** – Pollution prevention and abatement activities to address storm water management; water pollution prevention related to highway construction or due to highway runoff
- Wildlife Management** – Reduction of vehicle-caused wildlife mortality; restoration and maintenance of connectivity among terrestrial and aquatic habitats

**Transportation Alternatives Program Application 2025**

Organization: Nashville

**Project Description**

Include project name, project location, and scope of work. The project location should run from west to east or south to north and must include local/state/federal road names. You must include a color-coded map that clearly delineates the project (with road names, a north arrow and the project location highlighted) and any photographs, plans, drawings and applicable preliminary sketches. You must also provide a photograph of the beginning and ending termini if you are submitting for a pedestrian/bicycle facility. Please do not submit any documents larger than 11" x 17".

**If this is a pedestrian/bicycle facility, you must provide distinct and connecting termini (logical destination points such as federal/county/city offices, historic sites, schools, libraries, vehicular trailheads, etc.). Future unbuilt phases do not qualify as eligible destination points.** Note that inadequate connectivity may affect the projected costs for construction .

Sidewalks must connect to existing sidewalks, bikeways must connect to existing bikeways, greenways must connect to existing greenways or each facility must connect to an approved trailhead ( with adequate ADA parking) or an eligible destination point. All bicycle and pedestrian facilities must be ADA compliant and provide adequate linear connectivity to existing accessible facilities. Pedestrians cannot utilize bike lanes, and all pedestrian facilities must provide adequate separation from vehicular traffic , typically accomplished with the installation of a 6-inch non-mountable curb or adequate greenspace separation dependent on vehicular volume and speed.

Funding may not be used for spot improvements, repairs or maintenance, and all new construction for shared-use facilities must be a minimum of 10-feet wide. Address any questions concerning connectivity to the TAP Office at [TDOT.Enhancements@tn.gov](mailto:TDOT.Enhancements@tn.gov):

The Opry Mills Greenway Connector will be located in a bend of the Cumberland River, known as Pennington Bend, in Nashville. The scope of work is to construct 1-mile of paved ADA compliant greenway from the Cumberland River Pedestrian Bridge to Opry Mills Mall. The Opry Mills greenway connector will provide a regionally significant link to the Opry Mills complex, popular for its entertainment, retail, and hospitality offerings. As part of Nashville's 104-mile network of greenways, the Opry Mills connector will be accessible from existing greenways which users can now follow for 27-miles from the Hermitage community in eastern Nashville to Tennessee State University in north Nashville. Greenways in Nashville often parallel waterways and are built within a 75' wooded riparian conservation buffer. Greenways are popular resources for residents who want safe accessible alternative modes of transportation, places to enjoy nature, and recreation opportunities. The new greenway connector will meet the existing Stones River Greenway after the Cumberland River Pedestrian Bridge and follow the scenic wooded river's edge reaching Opry Mills about one mile to the north . The 12' wide greenway surface will be concrete for the first quarter mile, transitioning to a 12' wide bituminous surface for .65 miles. The greenway route widens at Cumberland River viewsheds to see the beauty and wildlife of the Cumberland River. The terrain in the first .50 miles past the pedestrian bridge poses construction challenges including steep slopes, woods, and wide drainage outlets. To safely navigate this stretch, construction plans include 2 truss bridges, 2 box culvert bridges, a wall supported ramp and a concrete boardwalk. These elements provide ADA compliance and traverse the slopes in an environmentally sensitive way . At .9 miles, travelers will cross Opry Mills Drive on a crosswalk and follow a 12' wide concrete walk on a curbed island, separated from vehicular traffic, reaching the Opry Mills Mall in .16 miles. A local bus transportation drop-off at Opry Mills Mall is located less than 100-feet from the greenway's mall access location. Greenway amenities will

**Transportation Alternatives Program Application 2025**

Organization: Nashville

include native landscaping, interpretive signs, ADA compliant parking spaces, bicycle racks, benches, and trailhead signs at both termini. The Opry Mills connector, as all Metro's greenways, has been designed to meet Public Right of Way Accessibility Guidelines, developed by the Access Board under ADA. Nashville's planning documents support greenways and recognize the benefits greenways provide. Nashville's Connect Downtown Action Plan (2024) prioritizes adding safety upgrades in Nashville's downtown core and links to greenways. Greenways are part of Big Move 4 which seeks to create safe transit networks to access downtown by offering separated multi-modal facilities. The Imagine East Bank (2022) is a riverbank development plan proposing to re-connect people to the river and increase environmental resiliency. The plan shows improvements to the Cumberland River Greenway system which is connected to Opry Mills via the proposed project. Nashville Next (2015) focused on Nashville's future. Implementation policies encourage investing in walkability and resiliency. Access Nashville 2040 is an appendix to Nashville Next focused on safe transportation, noting the environmental, fiscal, and health benefits of greenways. Plan to Play (2017) Nashville's Master Plan for Parks, suggests Triple Bottom Line metrics to measure the impact of projects in providing social, environmental, and financial benefits. A Community Plan for the area nearest Opry Mills calls out the Opry Mills Connector as a high priority project. The city is invested in seeing this important connection completed and is engaged in the planning, development, and construction of this project. Preliminary engineering (NEPA and Design) and right-of-way costs have been completed. Plan links, maps and public comment are found in attachments.

- Attach map(s) here:  
[https://tdot.intelligrants.com/\\_Upload/17797\\_1175726-MapsandLayoutPlans.pdf](https://tdot.intelligrants.com/_Upload/17797_1175726-MapsandLayoutPlans.pdf)  
(Maximum size for all map attachments = 20 megabytes)
- Attach photo(s) and/or artist rendering(s) here:  
[https://tdot.intelligrants.com/\\_Upload/17797\\_1175727-TerminiPhotosandRenderings.pdf](https://tdot.intelligrants.com/_Upload/17797_1175727-TerminiPhotosandRenderings.pdf)  
(Maximum size for all photo and/or artist rendering attachments = 20 megabytes)

**Safety**

Provide a brief description of how the proposed project will improve safety for pedestrians, bicyclists, and other transportation users.

All elements of this project are designed to Public Right-Of-Way Accessible Guidelines. The two proposed trailheads have been designed with ample area to view greenway maps without impeding travelers who are not stopping. Ramps and bridges will be furnished with 42" heavy duty metal railings at elevated locations and viewing points. Visitors stopping at vistas will have space so as not to impede through travelers. Surface transitions will be flush to prevent tripping hazards and DWS tiles will be installed at street crossings. The greenway crossing on Opry Mills Drive will feature warning elements. Pedestrian crossing warning signs will be installed 100' before the crossing, with rumble strips on the road surface and yellow advance warning striping per MUTCD leading up to the 10' wide longitudinal crosswalk in both directions. The greenway will be separated from vehicular traffic after the crossing with 6" non-mountable curbs. A 12' wide concrete walk on a greenway island will guide visitors to the mall where DWS tiles, ramps and a broadly striped crosswalk continue the accessible route. There will be two accessible parking stalls located at the Opry Mills Drive greenway crossing, connected to the greenway with a DWS tile and ramp. Eight existing accessible parking spaces are located at the mall entrance to provide greenway access from the Opry Mills terminus. Public transportation bus drop-off and pick-up is located 100' to the north of the greenway entrance to the mall, accessible via mall sidewalk and ramps. This project makes use of all safety protocols meeting local, state and federal regulations. Layout plans illustrating these safety measures are attached to this submission. The two proposed truss bridges will be outfitted with conduit to provide for future lighting of the greenway, increasing

**Transportation Alternatives Program Application 2025**

Organization: Nashville

safety after dusk. Greenways planners have selected a lighting fixtures and plan to begin illumination of the Greenways to extend useable hours and increase user safety. The car dependency culture imposes serious risks to non-drivers. Crash data is kept on both Walk-Bike Nashville and Vision Zero's websites. Data from these sites reveals that in Nashville, an average of 75 pedestrians and 8 cyclists are killed each year in on-road crashes. Greenways form an off-road infrastructure that reduces the potential for these fatalities by offering safe inclusive access to jobs, entertainment, community, and cultural venues. The greenway network currently provides 104 miles of safe, accessible, off-road travel throughout Nashville. Opry Mills is one of the largest commercial centers in Davidson County employing thousands of workers and providing services to millions of residents and visitors each year. The venue has limited connections to the rest of the metro area due to its location in a bend in the Cumberland River. Vehicular travel options from the west are Briley Parkway, S.R. 155, a controlled access highway, prohibiting cyclists and pedestrians. S.R. 155's Opry Mills exit had an annual average daily traffic flow of over 101,813 trips (2023 TDOT data) generated by employees and visitors heading to and from the Opry Complex. McGavock Pike, a two-lane minor arterial road with an AADT of 6681 (2023 TDOT data) bisects the Opry Mills area and intersects with S.R. 155 at the Opry Mills exit, widening to 8 lanes near Opry Mills Drive. These routes are not safe for cyclists or pedestrians. The bus route from East Nashville to the mall averages 45-minutes. With the Opry Mills Connector in place, a cyclist riding on the greenway from East Nashville at 10-mph would reach the mall in 35-minutes. This regionally significant greenway connector will provide safe access for all users from three council districts: a population of about 60,300 residents living east, west, and south of Opry Mills. And meeting the need expressed at the public input meeting who support safe walking and biking connections in their communities.

**Connectivity**

Provide a brief description of how the proposed project will increase connectivity among a mixture of landuses and activity centers such as transit, residential neighborhoods, low-income housing, medical facilities, schools, retail, parks, employment centers, etc.

Nashville's Greenways include 104-miles of paved publicly accessible greenways throughout Metro Nashville. Greenways are linear parks that provide health and wellness, equality and environmental justice benefits, while preserving and protecting wildlife habitat. The EPA Environmental Justice screening and mapping tool shows the Opry Mills and Donelson area as having a 50-80% transportation access burden, reinforcing the need for the Opry Mills Greenway connection. This is significant given that East Nashville across the river from Opry Mills shows no access burden. Low-income areas are mapped on the EPA Environmental Justice site. Opry Mills will be connected to low-income neighborhoods via the proposed greenway. Nashville is compliant with Title VI. The Opry Mills Connector will be part of the Stones River Greenway, a 15-mile continuous paved greenway. The greenway begins in Shelby Park, as the Shelby Bottoms Greenway, heading east through Shelby Bottoms Park and then north to Cornelia Fort Airpark, following the Cumberland River for 7-miles. There are over a dozen access points to the greenway from East Nashville. The Shelby Bottoms Greenway ends, and the Stones River Greenway begins at the Cumberland River Pedestrian Bridge. The bridge crosses the Cumberland River, and the Stones River Greenway continues to the east through Two Rivers, Heartland, and Ravenwood Parks. The greenway follows the Stones River south past Stone Hall Mansion Park to the southern greenway terminus at Percy Priest Lake for 8-miles. The Donelson neighborhood has over a dozen greenway access points to the existing trail. Heading west from Shelby Park, the Shelby Bottoms Greenway connects to downtown Nashville via the Davidson Street Multi-Use Path. The Davidson Street path connects to the John Seigenthaler Pedestrian Bridge, a nationally listed historic landmark, at Cumberland River Park on the east bank of the Cumberland River in the shadow of the Titans Stadium. The John Seigenthaler Bridge is an integral piece of the Cumberland River Greenway, located on both east and west banks of the Cumberland River. The

**Transportation Alternatives Program Application 2025**

Organization: Nashville

east bank is the site of a 338-acre planned development which will extend the Cumberland River greenway and add more green infrastructure along the river with affordable housing, retail offerings, business offices and a new stadium. The Cumberland River Greenway on the west bank can be followed in two directions; one leading directly into Nashville's downtown core and all the attractions it offers including the Schermerhorn Symphony Center, a cultural landmark, Bridgestone Arena, the Country Music Hall of Fame, and the Walk of Fame Park, to name a few. Following the route to the north, the greenway connects to the Ascend Amphitheatre at Riverfront Park, Fort Nashborough, an historic site, and Public Square Park. The greenway intersects with the State Park Connector, a spur that leads west past the Sounds Stadium and the Bicentennial Capital Mall State Park, ending at the Nashville Farmers Market, a popular venue for food, drinks, and artisanal wares. The Cumberland River greenway continues north past the State Park Connector along the Cumberland River to the historic Germantown neighborhood and a connector leading to Morgan Park. The Cumberland River greenway continues north from Germantown passing under Interstate 65 where the greenway is routed atop a levee, ending at the Ted Rhoades Golf Course in North Nashville, a distance of about 7-miles from Shelby Bottoms Greenway and Shelby Park. The greenway network described above passes through six Nashville Council Districts, representing about 17% of Nashville's population, connecting low-income neighborhoods, several business centers, tourist attractions and potentially the Opry Mills Entertainment complex. Greenway and EJ maps are attached to this application in the Plans and Maps attachments.

**Economic Impact**

How will this project aid economic development in the community or serve economically disadvantaged populations?

The Metropolitan Government of Nashville and Davidson County will construct and maintain the Opry Mills Greenway connector providing public access for all in perpetuity. The proposed greenway will build on past Federal, State, and local pedestrian and bicycle infrastructure investments to provide an alternative transportation link to a regional destination. The Cumberland River Pedestrian Bridge, a picturesque iconic structure on the Stone's River Greenway, is a former Transportation Enhancements Grant Award project. Built in 2007, this bridge connects East Nashville to the Donelson and Hermitage neighborhoods. Hundreds of jobs are created in Nashville to design, build, and maintain the greenway network. Tourism supports economies and the Opry Mills greenway connector will enrich tourism options and revenues. The Nashville Convention and Visitors Corp. reports that Nashville's visitors spend over 10-billion dollars annually at Nashville's shops, restaurants, hotels, music venues and tourist attractions. The Opry Mills complex employs thousands in hospitality, retail, maintenance, management, and marketing operations. The Gaylord resort hotel has more than 2800 rooms, with 200 retail shops in the Opry Mills Mall and the Grand Old Opry seats over 2300 guests. Opry Mills Gaylord Resort and Grand Ole Opry have been and remain top Nashville destinations, evidenced by TDOT highway traffic counts, which returned to pre-covid levels in 2023. Opry Mills owners and management have provided easements and a re-alignment of their parking area to accommodate the greenway in a configuration that will carry people safely to the mall. They see the economic advantages in making this connection. Outdoor tourism contributes significantly to Nashville's economy and the city is on several US top places to visit lists. Southern Living calls out Nashville as the 3rd South Best City 2024 as a domestic travel destination. Nashville's greenways are specifically mentioned in "Best Times to Visit for Outdoor Fun" for travelers seeking "biking, walking, and sightseeing right in the city." Global Traveler ranked Nashville in the top 5 for Best Domestic Adventure Destinations for recreation amenities. Trail research shows that trails benefit economy of place across several sectors of public life. 1. Community health and wellness is improved which reduces the public health burden with decreased rates of obesity, high blood pressure, and heart disease. A 2011 study by the American Heart Association found that for every \$1 spent to build trails, \$3 in medical costs



**Transportation Alternatives Program Application 2025**

Organization: Nashville

were saved. A 2014 study by the American Journal of Public Health found a positive correlation between proximity to trail infrastructure and the amount of exercise a community engaged in. 2. Single family home property values have been shown to enjoy a positive premium of 3-5% when located close to a trail. A survey by the National Associations Homebuilders found that access to trails ranked 3-4 out of 19 amenities home buyers seek. 3. Tourism revenues are increased with the development of trail networks. Bicycle trail tourism revenues capture millions of dollars and the return on investment exceeds 2:1. 4. The Opry Mills greenway will be built in a riparian buffer, which provides additional economic benefits. Riparian buffers improve water quality by removing sediment, nitrogen, and phosphorus from water. At water treatment plants, the cleaner water saves on treatment chemicals and electricity costs. A 1-acre wooded riparian buffer sequesters 4 metric tons of carbon annually. Protecting buffers saves the U.S. economy billions of dollars in flood damage to homes. This regionally significant greenway will return this investment thru its location in Nashville, connection to a regional destination, recreation access for 63,000 residents, and continued protection of a wooded riparian buffer. Links to the research articles referenced above are in the public involvement section of the attachments.

**Right of Way Impact**

- Is all land necessary for the project publicly owned or leased? Yes (  ) No ( )
- Is any part of the project to be constructed inside State or Federal highway right-of-way? Yes (  ) No ( )
- Is the project along or adjacent to a State or Federal Highway? Yes (  ) No ( )
- Will the project impact an existing or eligible National Register Historic Site or District? Yes ( ) No (  )
- Does the project include a pedestrian/bike bridge or tunnel or impact an existing TDOT structure? Yes (  ) No ( )
- Will any part of your project (including parallel pedestrian facilities) impact a rail or trolley line? Yes ( ) No (  )

**Support and Public Involvement**

This application must include the local, statewide, and legislative support for the proposed project, and any federal, state or local government mandates or resolutions for the project must be attached. Prior to submitting an application, the applicant must hold a design public involvement meeting to notify the local community of the intended project scope. In addition, correspondence must be sent to the appropriate MPO/MTPO/TPO or Rural Planning Organization (RPO) describing the proposed activity and inviting input regarding the project. The date(s) and time(s) of public meetings that were held as well as the sign-in sheet, the record of all public comments received and the disposition of said comments must be included with the completed application

Upload the following supporting documentation:

Attach public involvement meeting supporting documentation here:  
[https://tdot.intelligrants.com/\\_Upload/17797\\_1175707-PlanReferences,Links,PublicComment.pdf](https://tdot.intelligrants.com/_Upload/17797_1175707-PlanReferences,Links,PublicComment.pdf)

[ ] Will submit to TDOT at a later date

Attach any federal, state, or local government mandates or resolutions for the project or other letters of support here:  
[https://tdot.intelligrants.com/\\_Upload/17797\\_1175709-2b.MPOSupport.pdf](https://tdot.intelligrants.com/_Upload/17797_1175709-2b.MPOSupport.pdf)

[ ] Will submit to TDOT at a later date

**Transportation Alternatives Program Application 2025**

Organization: Nashville

**Legislative Districts**

Please provide the legislative districts that correspond to the proposed project location, NOT the address of the applicant:

- 1. Senate District Number: 20
- 2. House District Number: 51
- 3. U.S. Congressional Representative District Number: 6

**Tennessee Metropolitan Planning Organization (MPO) or Rural Planning Organization (RPO)**

Select the applicable organization from one of the following dropdown lists:

A map of Tennessee's planning organizations can be found at:

[https://www.tn.gov/content/dam/tn/tdot/long-range-planning/maps/MPO\\_RPO\\_Map\\_November\\_2018v4.pdf](https://www.tn.gov/content/dam/tn/tdot/long-range-planning/maps/MPO_RPO_Map_November_2018v4.pdf)

(Do not select from both lists.) [Map provided on next page.](#)

MPO: Nashville MPO

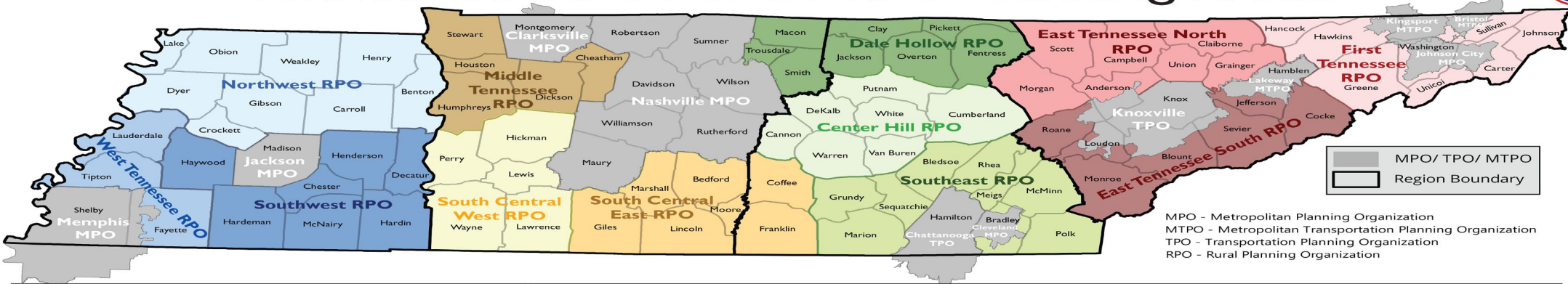
OR

RPO:

Are you within the Urban Boundary of the MPO? Yes ( ✓ ) No ( )



# Tennessee MPO / TPO / RPO Planning Areas



**GREENWAY MAP LEGEND**

- Existing Greenway and Trail
- Park
- Water Corridor: Framework
- Water Corridor: Conservation
- Other Greenway Corridors
- Greenway Bridge

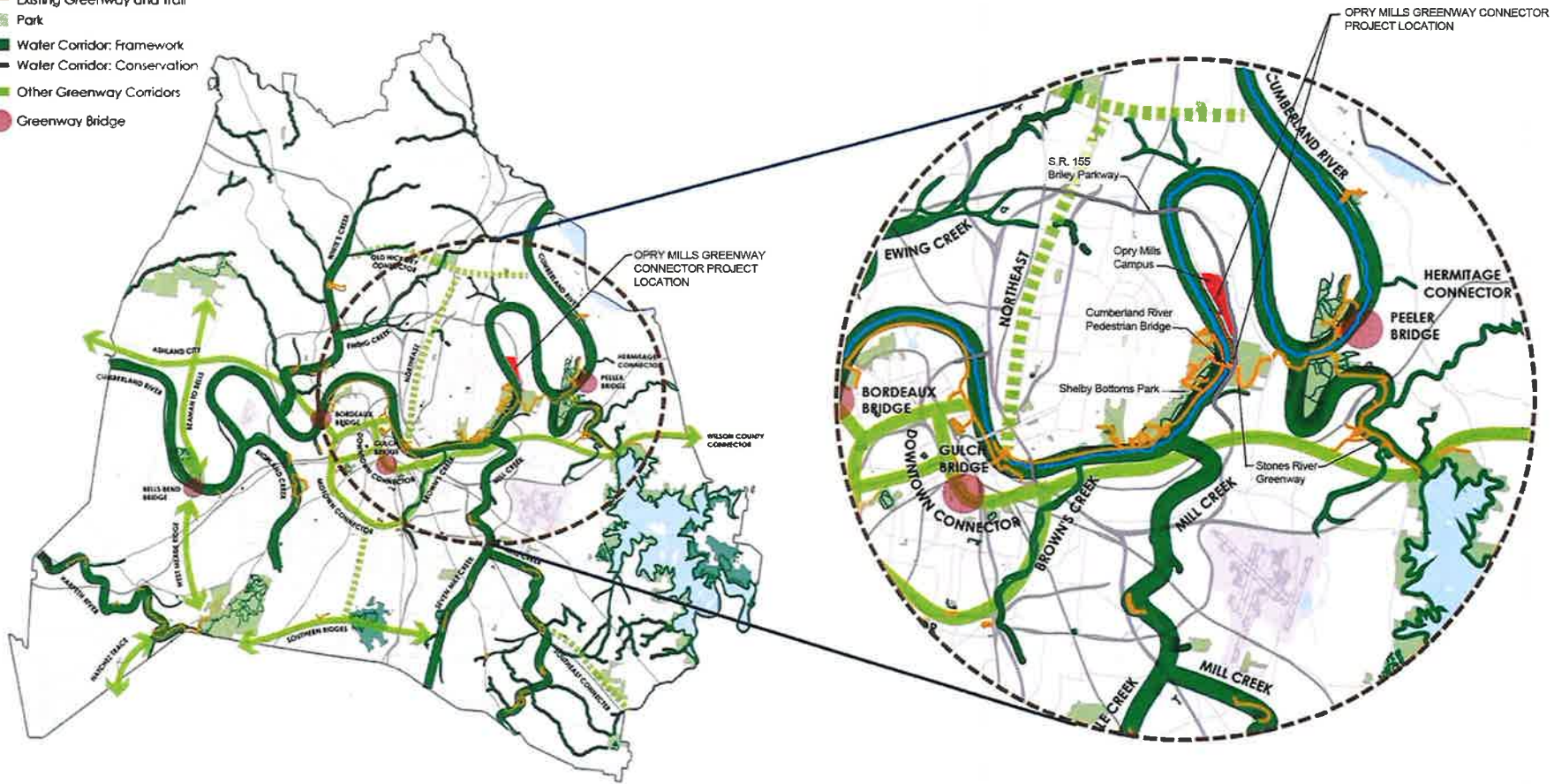
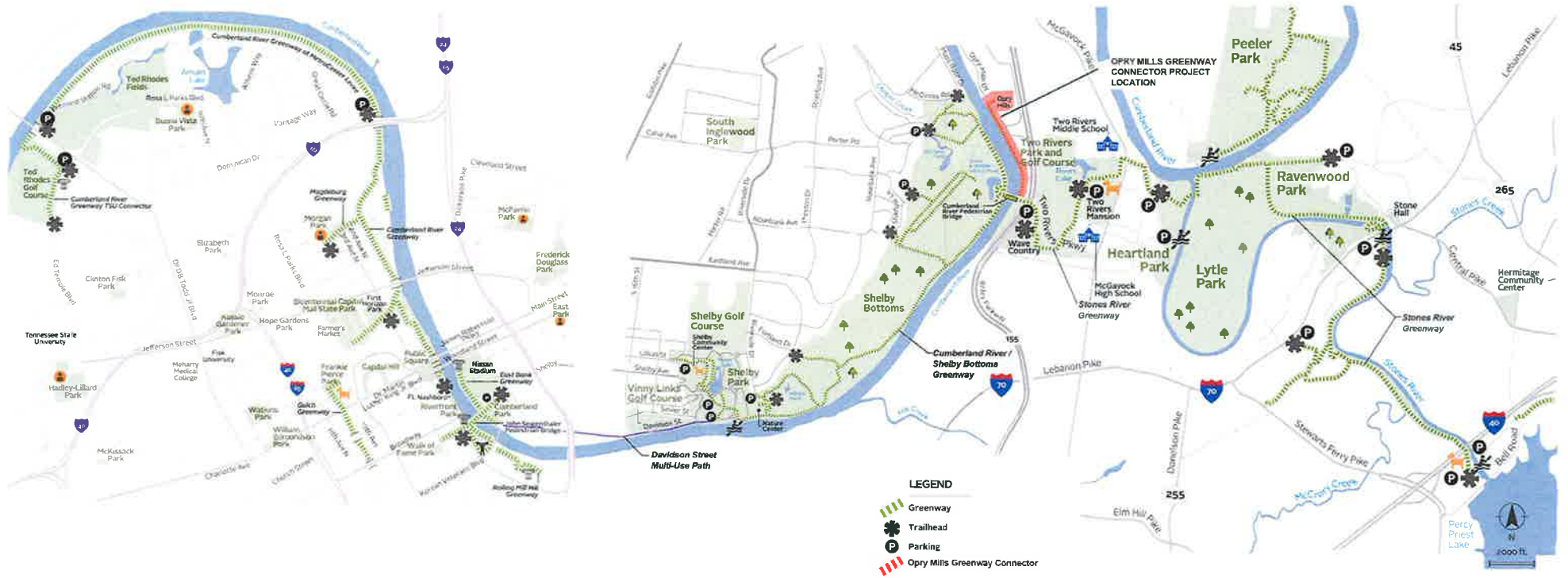


Figure 7-2 Greenways Vision Plan

# NASHVILLE GREENWAY CONNECTIONS TO OPRY MILLS GREENWAY CONNECTOR PROJECT LOCATION



DocuSign Envelope ID: E9853ED3-2F0B-43C9-942E-C53243B0AAD2

Please note: Territory data (except Puerto Rico) is not available as comparable to the US. It is only comparable to the territory itself by using the 'Compare to State' functionality. Likewise, some of the indicators may not be available for territories.

Compare to US  Compare to State

**Environmental Burden Indicators**

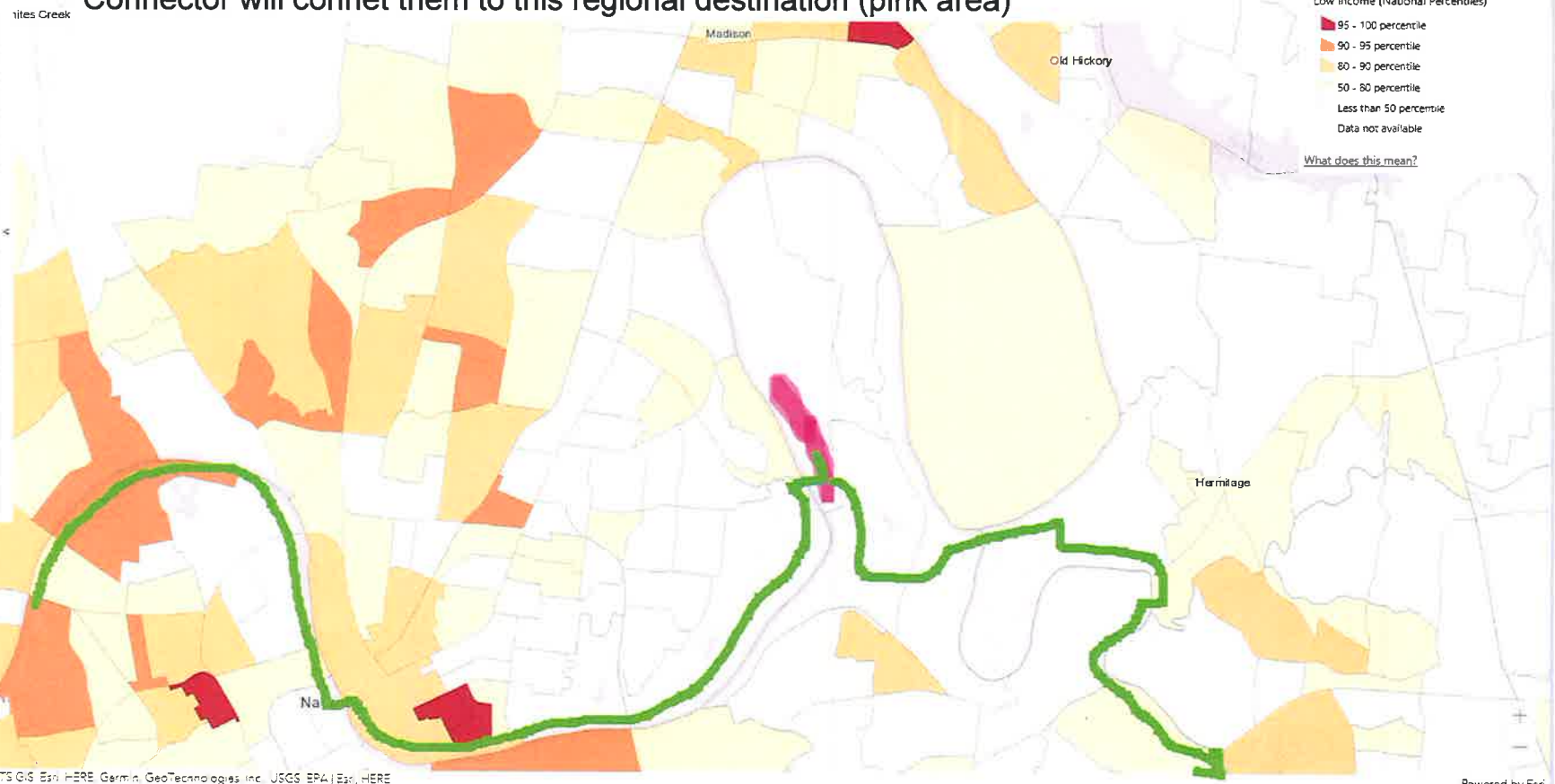
**Socioeconomic Indicators**

- Demographic Index
- Supplemental Demographic Index
- People of Color
- Low Income**
- Unemployment Rate
- Limited English Speaking
- Less Than High School Education
- Under Age 5
- Over Age 64

**Environmental Justice Indexes**

- Supplemental Indexes**
- Climate Change
- Health Disparities
- Critical Service Gaps

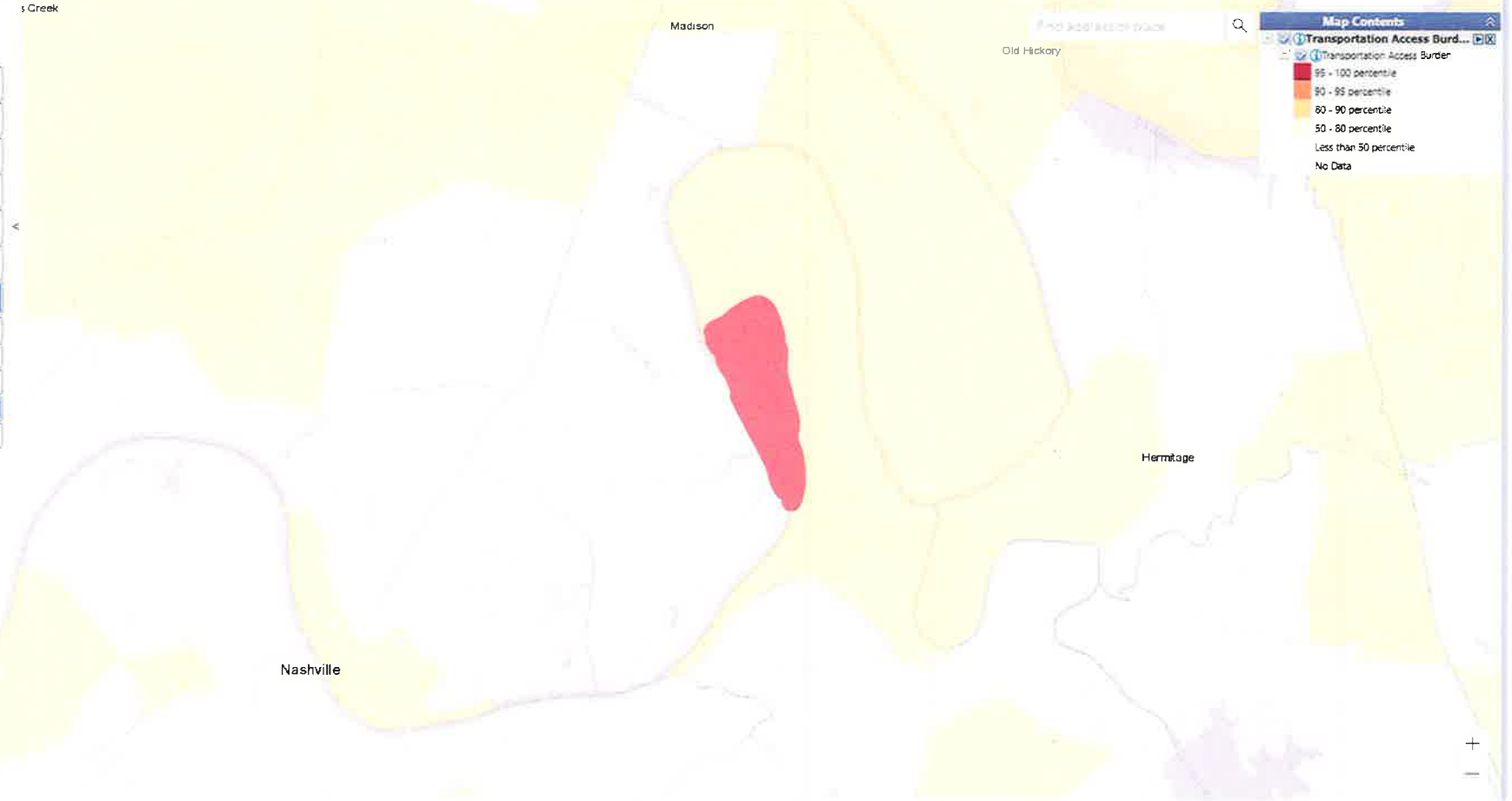
## Existing Greenway (green line) connects low income neighborhoods, Opry Mills Connector will connect them to this regional destination (pink area)



DocuSign Envelope ID: E9853ED3-2F0B-43C9-942E-C53243B0AAD2

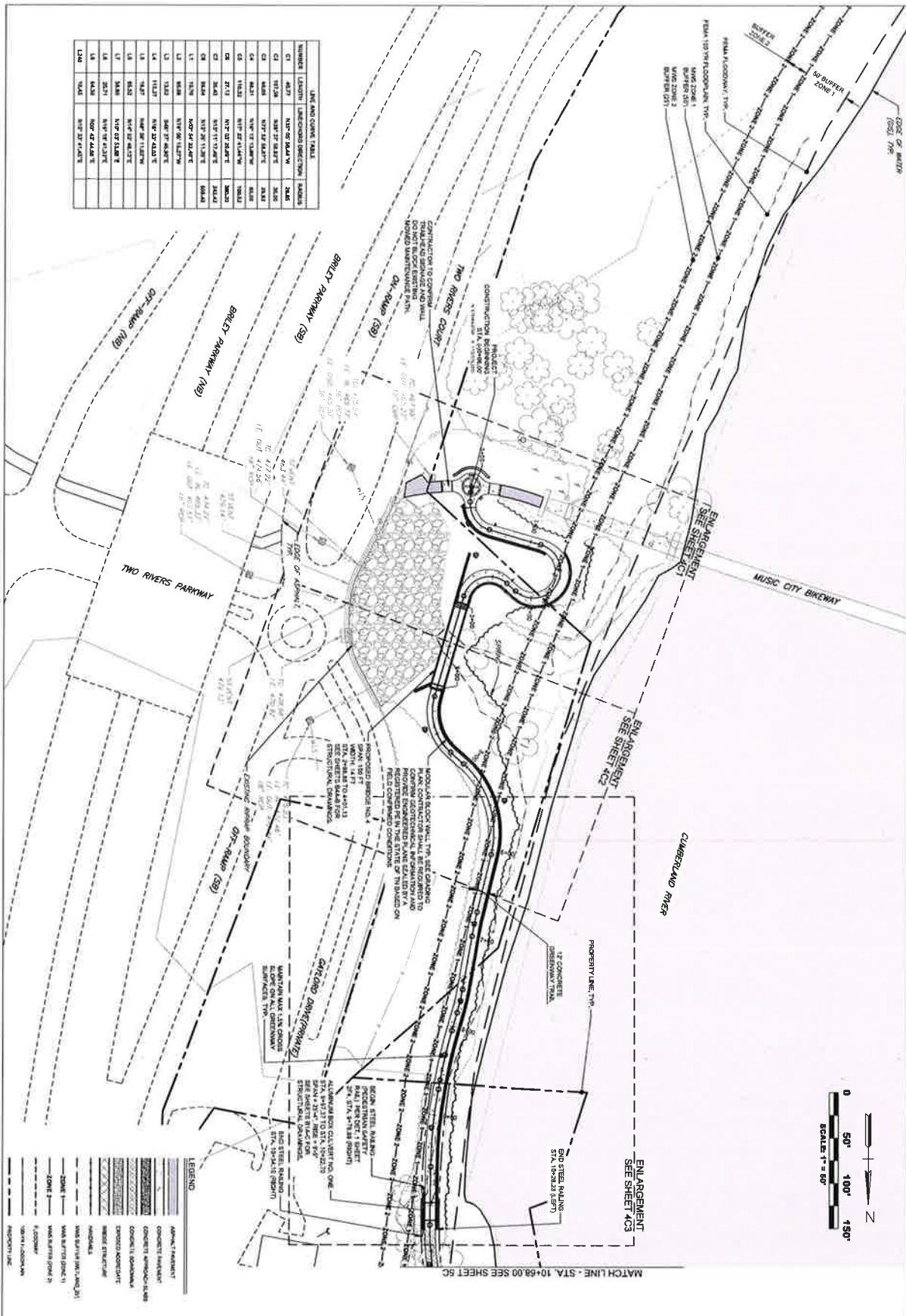
Compare to US Compare to State

- Environmental Burden Indicators
- Socioeconomic Indicators
- Environmental Justice Indexes
- Supplemental Indexes
- Climate Change
- Health Disparities
- Critical Service Gaps**
  - Broadband Gaps
  - Lack of Health Insurance
  - Housing Burden
  - Transportation Access Burden**
  - Food Desert



Project area shown in pink. Map shows transportation access burden. Donelson, Hermitage and Pennington Bend have less access than East, South and Downtown do.





NUMBER	LENGTH	LABORER DIRECTION	BARREL
01	10.00	WEST TO EAST	24.00
02	10.00	WEST TO EAST	24.00
03	10.00	WEST TO EAST	24.00
04	10.00	WEST TO EAST	24.00
05	10.00	WEST TO EAST	24.00
06	10.00	WEST TO EAST	24.00
07	10.00	WEST TO EAST	24.00
08	10.00	WEST TO EAST	24.00
09	10.00	WEST TO EAST	24.00
10	10.00	WEST TO EAST	24.00
11	10.00	WEST TO EAST	24.00
12	10.00	WEST TO EAST	24.00
13	10.00	WEST TO EAST	24.00
14	10.00	WEST TO EAST	24.00
15	10.00	WEST TO EAST	24.00
16	10.00	WEST TO EAST	24.00
17	10.00	WEST TO EAST	24.00
18	10.00	WEST TO EAST	24.00
19	10.00	WEST TO EAST	24.00
20	10.00	WEST TO EAST	24.00
21	10.00	WEST TO EAST	24.00
22	10.00	WEST TO EAST	24.00
23	10.00	WEST TO EAST	24.00
24	10.00	WEST TO EAST	24.00
25	10.00	WEST TO EAST	24.00
26	10.00	WEST TO EAST	24.00
27	10.00	WEST TO EAST	24.00
28	10.00	WEST TO EAST	24.00
29	10.00	WEST TO EAST	24.00
30	10.00	WEST TO EAST	24.00
31	10.00	WEST TO EAST	24.00
32	10.00	WEST TO EAST	24.00
33	10.00	WEST TO EAST	24.00
34	10.00	WEST TO EAST	24.00
35	10.00	WEST TO EAST	24.00
36	10.00	WEST TO EAST	24.00
37	10.00	WEST TO EAST	24.00
38	10.00	WEST TO EAST	24.00
39	10.00	WEST TO EAST	24.00
40	10.00	WEST TO EAST	24.00
41	10.00	WEST TO EAST	24.00
42	10.00	WEST TO EAST	24.00
43	10.00	WEST TO EAST	24.00
44	10.00	WEST TO EAST	24.00
45	10.00	WEST TO EAST	24.00
46	10.00	WEST TO EAST	24.00
47	10.00	WEST TO EAST	24.00
48	10.00	WEST TO EAST	24.00
49	10.00	WEST TO EAST	24.00
50	10.00	WEST TO EAST	24.00

**PROPOSED LAYOUT**

PROJECT NO. 46

DATE: 05/24/2014  
 DRAWN BY: J. B. BROWN  
 CHECKED BY: J. B. BROWN  
 SCALE: 1" = 50'

**CONSTRUCTION PLANS**

**STONES RIVER GREENWAY-  
OPRY MILLS GREENWAY CONNECTOR**

DEPARTMENT OF PARKS AND RECREATION  
METRO NASHVILLE & DAVIDSON COUNTY, TN

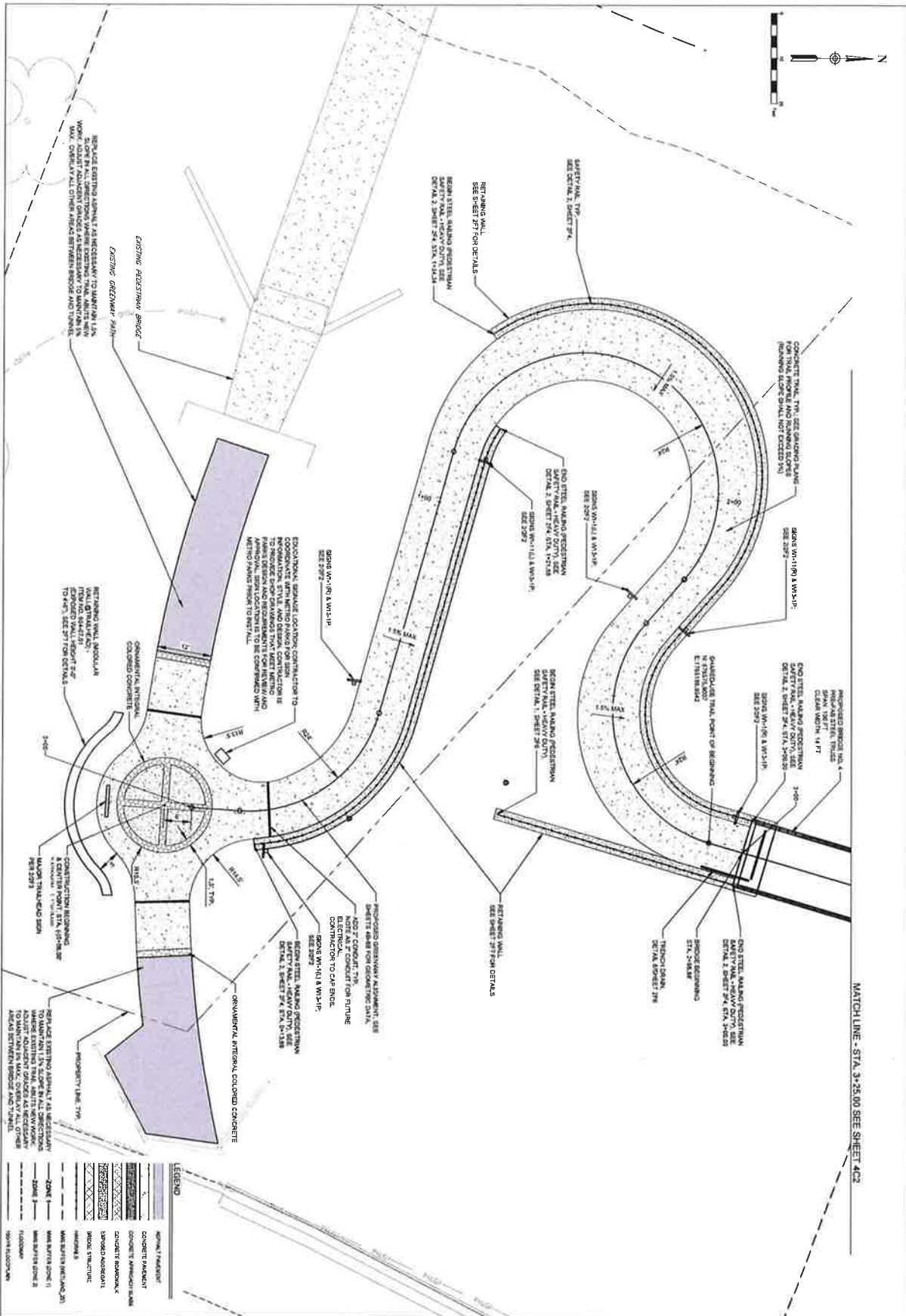
**COLLIER ENGINEERING CO., INC.**  
CONSULTING & DESIGN & CONSTRUCTION

2848 NOLENSVILLE PIKE  
PHONE: (615) 331-1441

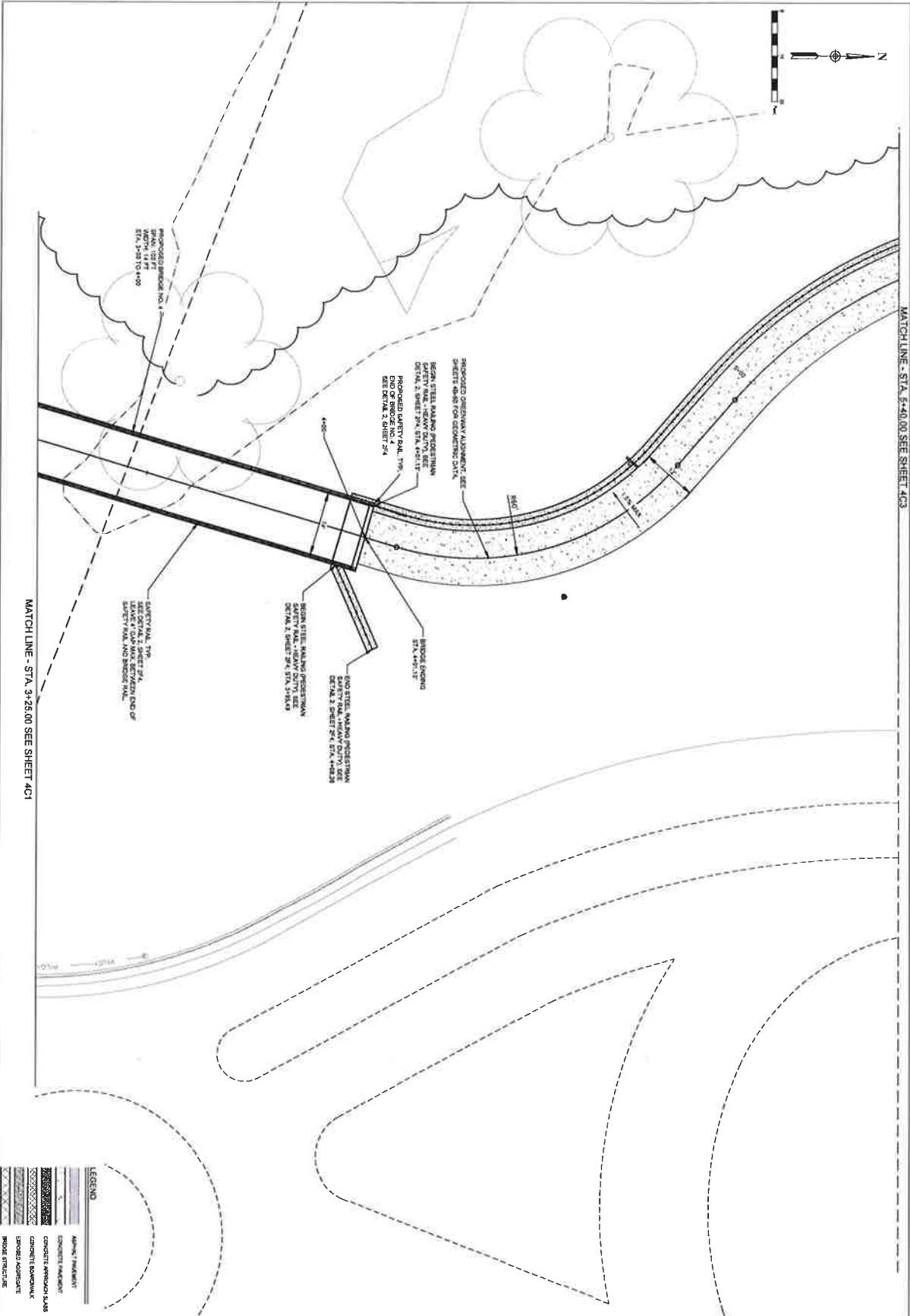
NASHVILLE, TN 37211  
FAX: (615) 331-1050

REVISIONS	
NO.	DESCRIPTION
1	ISSUED FOR PERMIT
2	ISSUED FOR PERMIT
3	ISSUED FOR PERMIT
4	ISSUED FOR PERMIT
5	ISSUED FOR PERMIT
6	ISSUED FOR PERMIT
7	ISSUED FOR PERMIT
8	ISSUED FOR PERMIT
9	ISSUED FOR PERMIT
10	ISSUED FOR PERMIT





<p><b>LEGEND</b></p> <ul style="list-style-type: none"> <li>--- NORMAL FINISH</li> <li>--- CONCRETE PARAPET</li> <li>--- CONCRETE PARAPET WITH</li> <li>--- CONCRETE ROUGHCAST</li> <li>--- BRICK FINISH</li> <li>--- BRICK FINISH WITH</li> <li>--- WALL WITH CONC. F.</li> <li>--- FLOORING</li> <li>--- OPEN FOOTING</li> </ul>	<p><b>CONSTRUCTION PLANS</b>  <b>STONES RIVER GREENWAY-                  OPRY MILLS GREENWAY CONNECTOR</b>                  DEPARTMENT OF PARKS AND RECREATION                  METRO NASHVILLE &amp; DAVIDSON COUNTY, TN</p>	<p><b>COLLIER</b>                  ENGINEERING CO., INC.                  CONSULTING • DESIGN • CONSTRUCTION</p>	<p><b>REVISIONS</b></p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>DATE</th> <th>BY</th> <th>DESCRIPTION</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	DATE	BY	DESCRIPTION												
DATE	BY	DESCRIPTION																
<p>DATE: 05/27/2024                  DRAWN BY: [Name]                  CHECKED BY: [Name]                  SCALE: 1" = 10'-0"</p>	<p>2949 MOLESVILLE PIKE NASHVILLE, TN 37211                  PHONE: (615) 331-1441 FAX: (615) 331-1050</p>																	



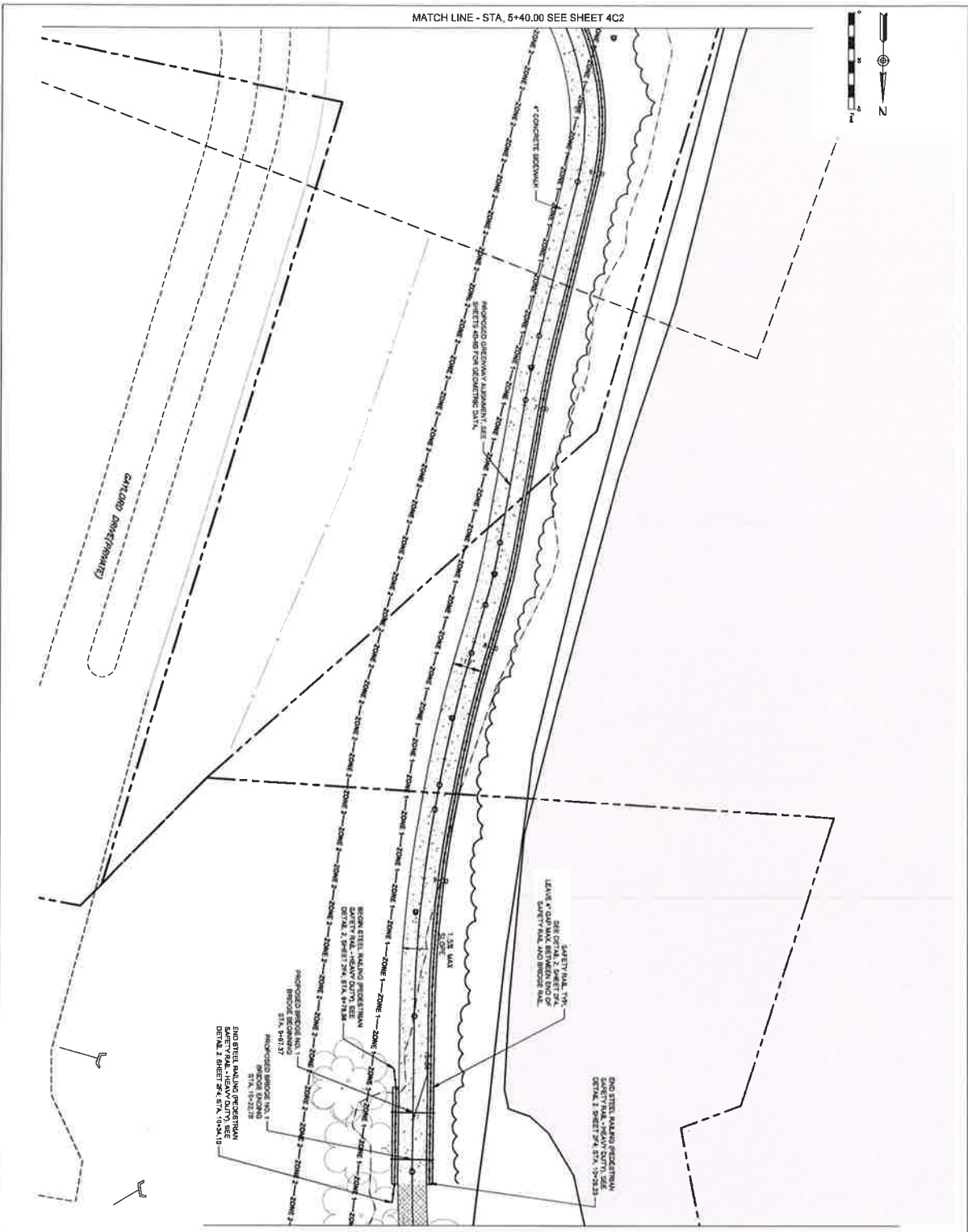
PROPOSED LAND



**CONSTRUCTION PLANS**  
**STONES RIVER GREENWAY- OPRY MILLS GREENWAY CONNECTOR**  
 DEPARTMENT OF PARKS AND RECREATION  
 METRO NASHVILLE & DAVIDSON COUNTY, TN

**COLLIER ENGINEERING CO., INC.**  
 CONSULTING & DESIGN & CONSTRUCTION  
 2948 NOLENSVILLE PIKE  
 NASHVILLE, TN 37214  
 PHONE: (615) 331-1441 FAX: (615) 331-1050

REVISIONS		
NO.	DATE	DESCRIPTION
1	05/24/2024	METRO ADA COMMENTS
2		TRUST MATERIAL SUBMITTALS
3		TRUST AGREEMENT



LEGEND

[Symbol]	APPROXIMATE PROPOSED
[Symbol]	CONCRETE PAVEMENT
[Symbol]	CONCRETE PAVEMENT BASE
[Symbol]	PROPOSED ASPHALT
[Symbol]	PROPOSED STRUCTURE

DATE: 03/24/2014  
 DRAWN BY: [Name]  
 CHECKED BY: [Name]  
 DESIGNED BY: [Name]  
 SCALE: 1" = 20'

PROPOSED LAYOUT

SHEET NO. 4C3



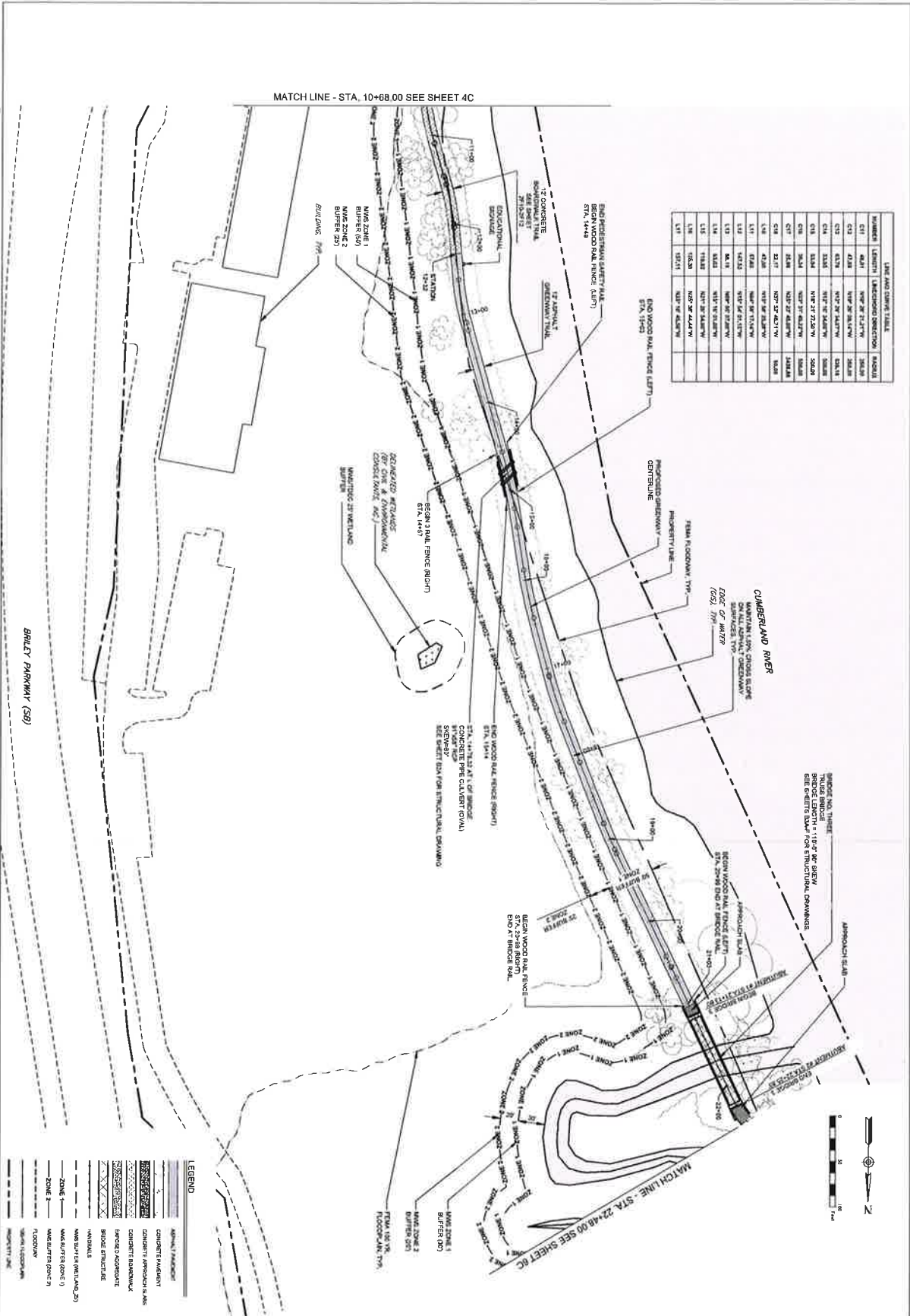
CONSTRUCTION PLANS  
 STONES RIVER GREENWAY-  
 OPRY MILLS GREENWAY CONNECTOR  
 DEPARTMENT OF PARKS AND RECREATION  
 METRO NASHVILLE & DAVIDSON COUNTY, TN

**COLLIER**  
 ENGINEERING CO., INC.  
 CONSULTING ENGINEERS & ARCHITECTS

2948 NOLENSVILLE PIKE  
 PHONE: (615) 331-1441

NASHVILLE, TN 37211  
 FAX: (615) 331-1010

REVISIONS			
NO.	DATE	BY	DESCRIPTION
1	10/20/09	AKJ	METRO ADA COMMENTS
2	10/20/09	AKJ	TRIP NOTES, CORRECTED
3	10/20/09	AKJ	TRIP REVISIONS



WORKS	LENGTH	LANE/CHORD DIRECTION	ANALYSIS
C17	4.51	NW 27 21.17W	SM24
C18	4.28	NW 27 20.14W	SM24
C19	6.19	NW 27 14.17W	SM24
C20	3.34	NW 27 14.17W	SM24
C21	3.24	NW 27 22.21W	SM24
C22	3.24	NW 27 14.17W	SM24
C23	3.24	NW 27 14.17W	SM24
C24	3.24	NW 27 14.17W	SM24
C25	3.24	NW 27 14.17W	SM24
C26	3.24	NW 27 14.17W	SM24
C27	3.24	NW 27 14.17W	SM24
C28	3.24	NW 27 14.17W	SM24
C29	3.24	NW 27 14.17W	SM24
C30	3.24	NW 27 14.17W	SM24
C31	3.24	NW 27 14.17W	SM24
C32	3.24	NW 27 14.17W	SM24
C33	3.24	NW 27 14.17W	SM24
C34	3.24	NW 27 14.17W	SM24
C35	3.24	NW 27 14.17W	SM24
C36	3.24	NW 27 14.17W	SM24
C37	3.24	NW 27 14.17W	SM24
C38	3.24	NW 27 14.17W	SM24
C39	3.24	NW 27 14.17W	SM24
C40	3.24	NW 27 14.17W	SM24
C41	3.24	NW 27 14.17W	SM24
C42	3.24	NW 27 14.17W	SM24
C43	3.24	NW 27 14.17W	SM24
C44	3.24	NW 27 14.17W	SM24
C45	3.24	NW 27 14.17W	SM24
C46	3.24	NW 27 14.17W	SM24
C47	3.24	NW 27 14.17W	SM24
C48	3.24	NW 27 14.17W	SM24
C49	3.24	NW 27 14.17W	SM24
C50	3.24	NW 27 14.17W	SM24

**LEGEND**

[Symbol]	APPROACH SLAB
[Symbol]	CONCRETE PAVEMENT
[Symbol]	CONCRETE APPROACH SLAB
[Symbol]	CONCRETE SUBGRADE
[Symbol]	BRIDGE APPROACH
[Symbol]	BRIDGE STRUCTURE
[Symbol]	MANHOLE
[Symbol]	MANHOLE (W/LAND)
[Symbol]	MANHOLE (ZONE 1)
[Symbol]	MANHOLE (ZONE 2)
[Symbol]	MANHOLE (ZONE 3)
[Symbol]	PROPERTY LINE

**PROPOSED LAYOUT**

DATE: 05/14/2014  
 DRAWN BY: [Name]  
 CHECKED BY: [Name]  
 SCALE: 1" = 30'

**CONSTRUCTION PLANS**  
**STONES RIVER GREENWAY-  
 OPTRY MILLS GREENWAY CONNECTOR**  
 DEPARTMENT OF PARKS AND RECREATION  
 METRO NASHVILLE & DAVIDSON COUNTY, TN

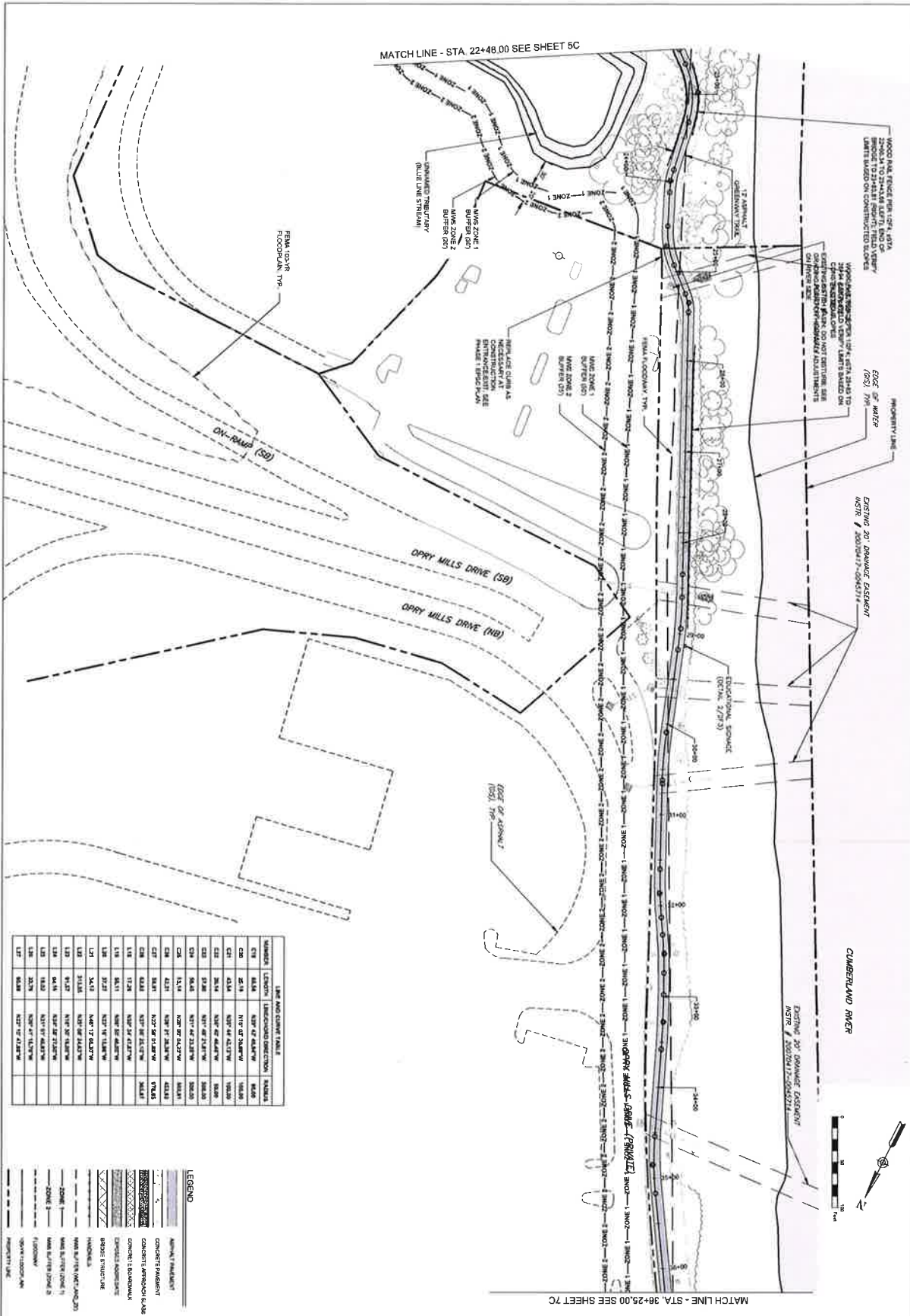
**COLLIER ENGINEERING CO., INC.**  
 CONSULTING & DESIGN • CONSTRUCTION

2949 NOLANSVILLE PIKE  
 PHONE: (615) 331-1441

NASHVILLE, TN 37211  
 FAX: (615) 331-1050

**REVISIONS**

NO.	DATE	BY	REASON
1	05/14/2014	AAJ	METRO ADA COMMENTS
2	05/14/2014	AAJ	THAT METRO SUBMITTAL
3	05/14/2014	AAJ	THAT APPROVING



MATCH LINE - STA. 22+48.00 SEE SHEET 5C

MATCH LINE - STA. 96+25.00 SEE SHEET 7C

NUMBER	LENGTH	LINE AND CURVE TABLE
178	10.00	SEPT OF 24.00' W
179	10.00	SEPT OF 24.00' W
180	10.00	SEPT OF 24.00' W
181	10.00	SEPT OF 24.00' W
182	10.00	SEPT OF 24.00' W
183	10.00	SEPT OF 24.00' W
184	10.00	SEPT OF 24.00' W
185	10.00	SEPT OF 24.00' W
186	10.00	SEPT OF 24.00' W
187	10.00	SEPT OF 24.00' W
188	10.00	SEPT OF 24.00' W
189	10.00	SEPT OF 24.00' W
190	10.00	SEPT OF 24.00' W
191	10.00	SEPT OF 24.00' W
192	10.00	SEPT OF 24.00' W
193	10.00	SEPT OF 24.00' W
194	10.00	SEPT OF 24.00' W
195	10.00	SEPT OF 24.00' W
196	10.00	SEPT OF 24.00' W
197	10.00	SEPT OF 24.00' W
198	10.00	SEPT OF 24.00' W
199	10.00	SEPT OF 24.00' W
200	10.00	SEPT OF 24.00' W

**LEGEND**

- CONCRETE PAVEMENT
- CONCRETE APPROACH SLAB
- CONCRETE SIDEWALK
- CONCRETE CURB
- CONCRETE UTILITY
- CONCRETE FLOORPLAN
- CONCRETE FOUNDATION
- CONCRETE WALL
- CONCRETE COLUMN
- CONCRETE BEAM
- CONCRETE SLAB
- CONCRETE ROOF
- CONCRETE FLOOR
- CONCRETE CEILING
- CONCRETE PARTITION
- CONCRETE DOOR
- CONCRETE WINDOW
- CONCRETE STAIR
- CONCRETE ELEVATOR
- CONCRETE ESCALATOR
- CONCRETE MECHANICAL
- CONCRETE ELECTRICAL
- CONCRETE TELEPHONE
- CONCRETE CABLE
- CONCRETE GAS
- CONCRETE WATER
- CONCRETE SEWER
- CONCRETE RAIN
- CONCRETE DRAIN
- CONCRETE VENT
- CONCRETE EXHAUST
- CONCRETE FURNACE
- CONCRETE BOILER
- CONCRETE CHILLER
- CONCRETE CONDENSER
- CONCRETE COIL
- CONCRETE PIPING
- CONCRETE FITTING
- CONCRETE VALVE
- CONCRETE FLANGE
- CONCRETE GASKET
- CONCRETE BRACKET
- CONCRETE HANGAR
- CONCRETE SUPPORT
- CONCRETE ANCHOR
- CONCRETE BOLT
- CONCRETE NUT
- CONCRETE WASHER
- CONCRETE PLATE
- CONCRETE RIVET
- CONCRETE WELD
- CONCRETE CUT
- CONCRETE PATCH
- CONCRETE REPAIR
- CONCRETE RESTORE
- CONCRETE PRESERVE
- CONCRETE MAINTAIN
- CONCRETE MONITOR
- CONCRETE INSPECT
- CONCRETE TEST
- CONCRETE EVALUATE
- CONCRETE REPORT
- CONCRETE DOCUMENT
- CONCRETE ARCHIVE
- CONCRETE RETRIEVE
- CONCRETE ACCESS
- CONCRETE UPDATE
- CONCRETE DELETE
- CONCRETE INSERT
- CONCRETE MOVE
- CONCRETE COPY
- CONCRETE PASTE
- CONCRETE PRINT
- CONCRETE EXPORT
- CONCRETE IMPORT
- CONCRETE LINK
- CONCRETE UNLINK
- CONCRETE HYPERLINK
- CONCRETE ANNOTATE
- CONCRETE DEANNOTATE
- CONCRETE COMMENT
- CONCRETE UNCOMMENT
- CONCRETE TRACK
- CONCRETE UNTRACK
- CONCRETE MARK
- CONCRETE UNMARK
- CONCRETE LOCK
- CONCRETE UNLOCK
- CONCRETE HIDE
- CONCRETE UNHIDE
- CONCRETE LOCKDOWN
- CONCRETE UNLOCKDOWN
- CONCRETE LOCKDOWN
- CONCRETE UNLOCKDOWN
- CONCRETE LOCKDOWN
- CONCRETE UNLOCKDOWN
- CONCRETE LOCKDOWN
- CONCRETE UNLOCKDOWN
- CONCRETE LOCKDOWN
- CONCRETE UNLOCKDOWN

**PROPOSED LAYOUT**

DATE: 05/24/2024  
 DRAWN BY: [Name]  
 CHECKED BY: [Name]  
 SCALE: 1"=20'

**CONSTRUCTION PLANS**  
**STONES RIVER GREENWAY-  
 OPRY MILLS GREENWAY CONNECTOR**  
 DEPARTMENT OF PARKS AND RECREATION  
 METRO NASHVILLE & DAVIDSON COUNTY, TN

**COLLIER ENGINEERING CO., INC.**  
 CONSULTING & DESIGN & CONSTRUCTION

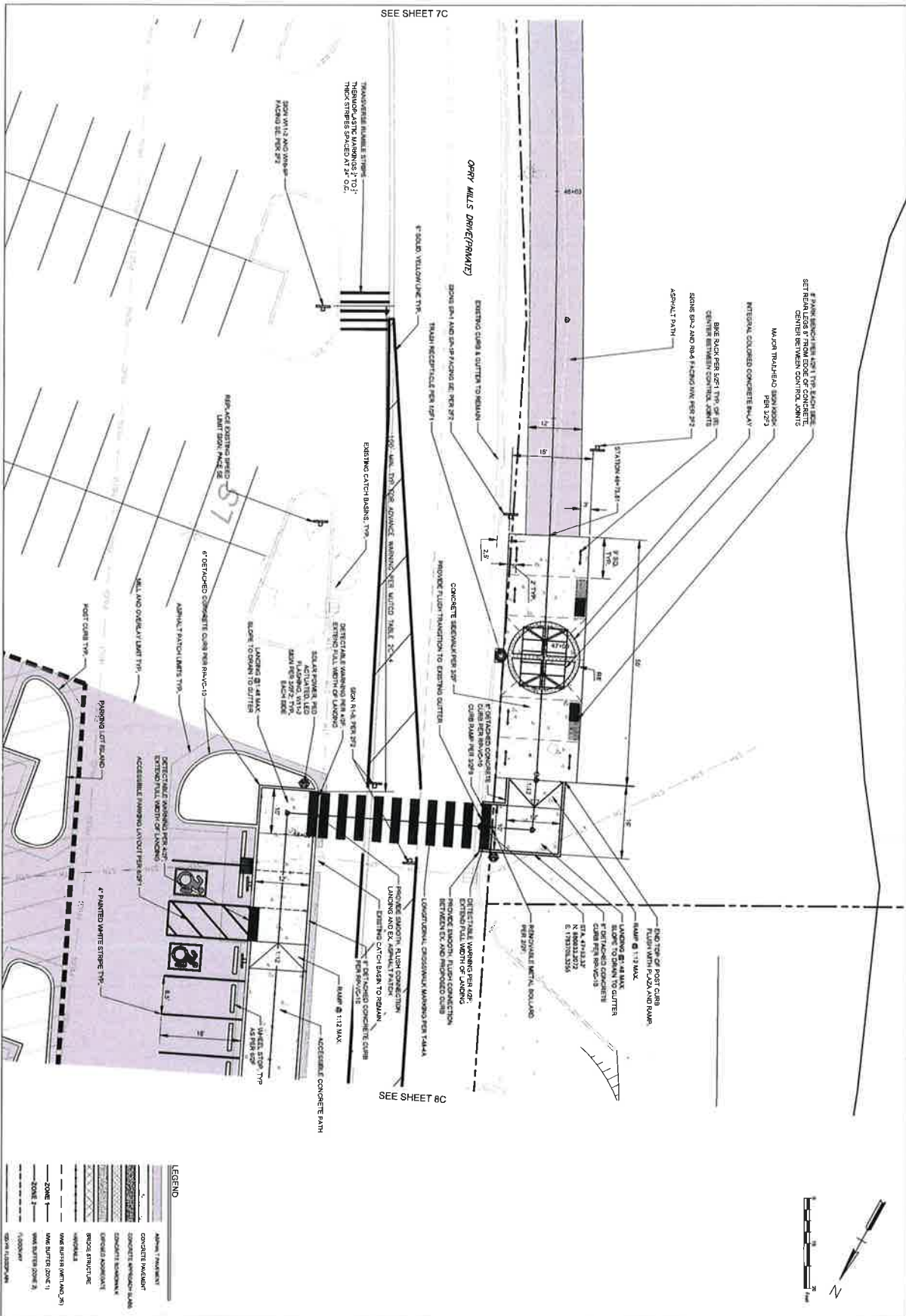
2849 NOLENSVILLE PIKE  
 PHONE: (615) 331-1441

NASHVILLE, TN 37211  
 FAX: (615) 331-1050

**REVISIONS**

NO.	DATE	BY	DESCRIPTION
1	05/24/2024	AAJ	ISSUE FOR CONSTRUCTION
2	05/24/2024	AAJ	REVISED PER COMMENTS
3	05/24/2024	AAJ	REVISED PER COMMENTS
4	05/24/2024	AAJ	REVISED PER COMMENTS





SEE SHEET 7C

SEE SHEET 8C

**LEGEND**

	ASPHALT FINISHMENT
	CONCRETE SLABWORK
	CONCRETE REINFORCEMENT
	CATCHMENT STRUCTURE
	GROOVED STRUCTURE
	UTILITY
	ZONE 1
	ZONE 2
	VMS BUFFER ZONE 1
	VMS BUFFER ZONE 2

**PROPOSED LAYOUT**

SHEET NOS. 7C1

**CONSTRUCTION PLANS**

**STONES RIVER GREENWAY- OPRY MILLS GREENWAY CONNECTOR**

**DEPARTMENT OF PARKS AND RECREATION METRO NASHVILLE & DAVIDSON COUNTY, TN**

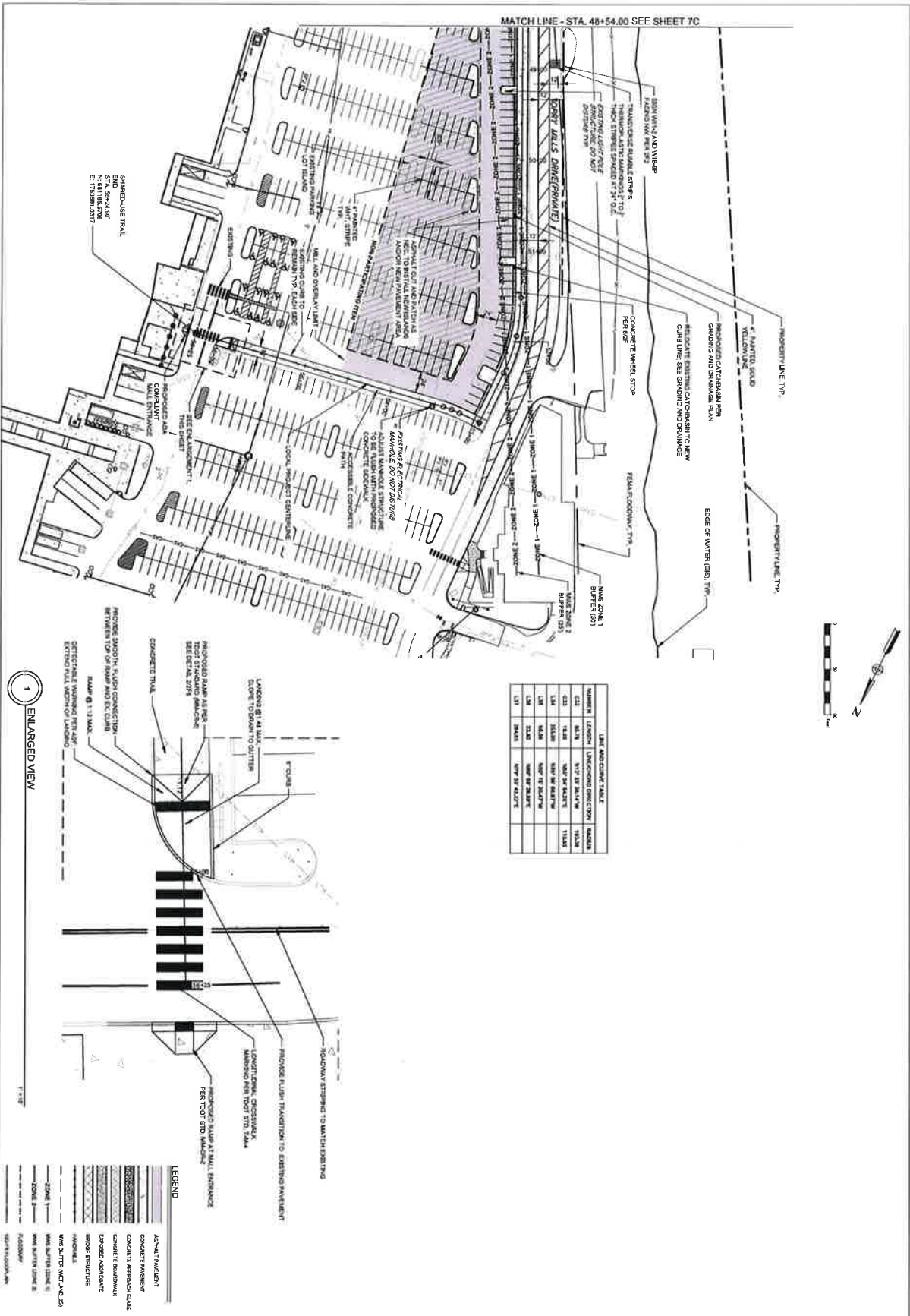
**COLLIER ENGINEERING CO., INC.**

2948 NOLENSVILLE PIKE  
PHONE: (615) 331-1441

NASHVILLE, TN 37211  
FAX: (615) 331-1050

**REVISIONS**

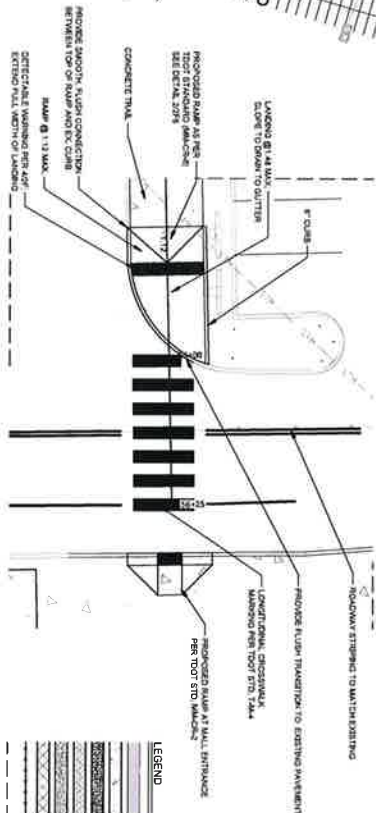
DATE	BY	DESCRIPTION
11/14/2014	RAJ	REVISION COMMENTS
12/10/2014	RAJ	POST SUBMITTAL REVISIONS
01/08/2015	RAJ	FINAL REVISIONS



MATCH LINE - STA. 48+54.00 SEE SHEET 7C

NUMBER	LENGTH	LINENOISE DIRECTION	REMARKS
123	14.82	N47° 32' 34.17"W	11.58' AS
124	1.84	S89° 36' 34.71"E	11.58' AS
125	33.28	S89° 36' 34.71"E	11.58' AS
126	33.28	N89° 36' 34.71"W	11.58' AS
127	33.28	N89° 36' 34.71"W	11.58' AS
128	33.28	S89° 36' 34.71"E	11.58' AS

1 ENLARGED VIEW



**LEGEND**

[Symbol]	ASPHALT PAVEMENT
[Symbol]	CONCRETE PAVEMENT
[Symbol]	CONCRETE APPROACH SLAB
[Symbol]	PROPOSED ASPHALT
[Symbol]	PROPOSED CONCRETE
[Symbol]	PROPOSED FLOORING
[Symbol]	WAVE BUFFER (MATERIAL 2)
[Symbol]	WAVE BUFFER (ZONE 1)
[Symbol]	WAVE BUFFER (ZONE 2)
[Symbol]	FLOORING
[Symbol]	PROPOSED FLOORING

**PROPOSED LAYOUT**

**Metro**

**DAVIDSON COUNTY**

**2012**

**DATE:** 12/12/12  
**DESIGNED BY:** [Name]  
**CHECKED BY:** [Name]  
**DATE:** 12/12/12

**CONSTRUCTION PLANS**

**STONES RIVER GREENWAY-OPDY MILLS GREENWAY CONNECTOR**

**DEPARTMENT OF PARKS AND RECREATION**

**METRO NASHVILLE & DAVIDSON COUNTY, TN**

**COLLIER ENGINEERING CO., INC.**

CONSULTING & DESIGN • CONSTRUCTION

2949 NOLANSVILLE PIKE  
 PHONE: (615) 331-1441

NASHVILLE, TN 37211  
 FAX: (615) 331-1050

NO.	DATE	BY	REVISION
1	12/12/12	AAJ	METRO ADA COMMENTS
2	12/12/12	BYM	THEY INITIAL SUBMITTAL
3	12/12/12	BYM	TRAC REQUESTING



**Trees, Shrubs and Groundcovers for Opry Mills Greenway Connector –5.24.24 Plans**

**NATIVE CANOPY TREES**

8	Acer rubrum 'PNI 0268'	October Glory Red Maple	14'-16' Ht., 3" Caliper
7	Juniperus Virginiana	Eastern Red Cedar	12'-15' Ht., 2" Caliper
9	Lirodendron tulipifera 'Little Volunteer'	Little Volunteer Tulip Poplar	14'-16' Ht., 2" Caliper
3	Quercus lyrata 'Howler'	Redstone Overcup Oak	14'-16' Ht., 3" Caliper
1	Quercus lyrata 'Hopeuliki'	Marquee Overcup Oak	14'-16' Ht., 3" Caliper
2	Quercus muehlenbergii	Chinkapin Oak	14'-16' Ht., 3" Caliper
<b><u>NON-NATIVE CANOPY TREES</u></b>			
3	Gleditsia triacanthos inermis 'Suncole'	Sunburst Honey Locust	14'-16' Ht., 3" Caliper

**33 TOTAL.**

**NATIVE ORNAMENTAL TREES**

5	Cercis canadensis	Eastern Redbud	8'-10' Ht., 2" Caliper
---	-------------------	----------------	------------------------

**NON – NATIVE ORNAMENTAL TREES**

9	Magnolia stellata 'Royal Star'	Royal Star Magnolia	9'-12', Multi-Stem
4	Pistacia chinensis	Chinese Pistache	9'-12', 2.5" Caliper

**18 TOTAL.**

**NATIVE SHRUBS**

10	Aronia arbutifolia	Red Chokeberry	30-36" Overall Ht./Spread
9	Vaccinium arboretum	Farkleberry	54-60" Overall Ht./Spread

**19 TOTAL.**

**NATIVE GROUNDCOVER**

39	Rhus aromatica 'Gro-Low'	Gro-Low Fragrant Sumac	24" Ht., 36" Spread
----	--------------------------	------------------------	---------------------

**NON-NATIVE GROUNDCOVER**

88	Juniperus virginiana Grey Owl	Grey Owl Juniper	24" Ht., 48" Spread
151	Panicum virgatum 'Northwind'	Northwind Switchgrass	24-30" Overall Ht./Spread

**278 TOTAL.**



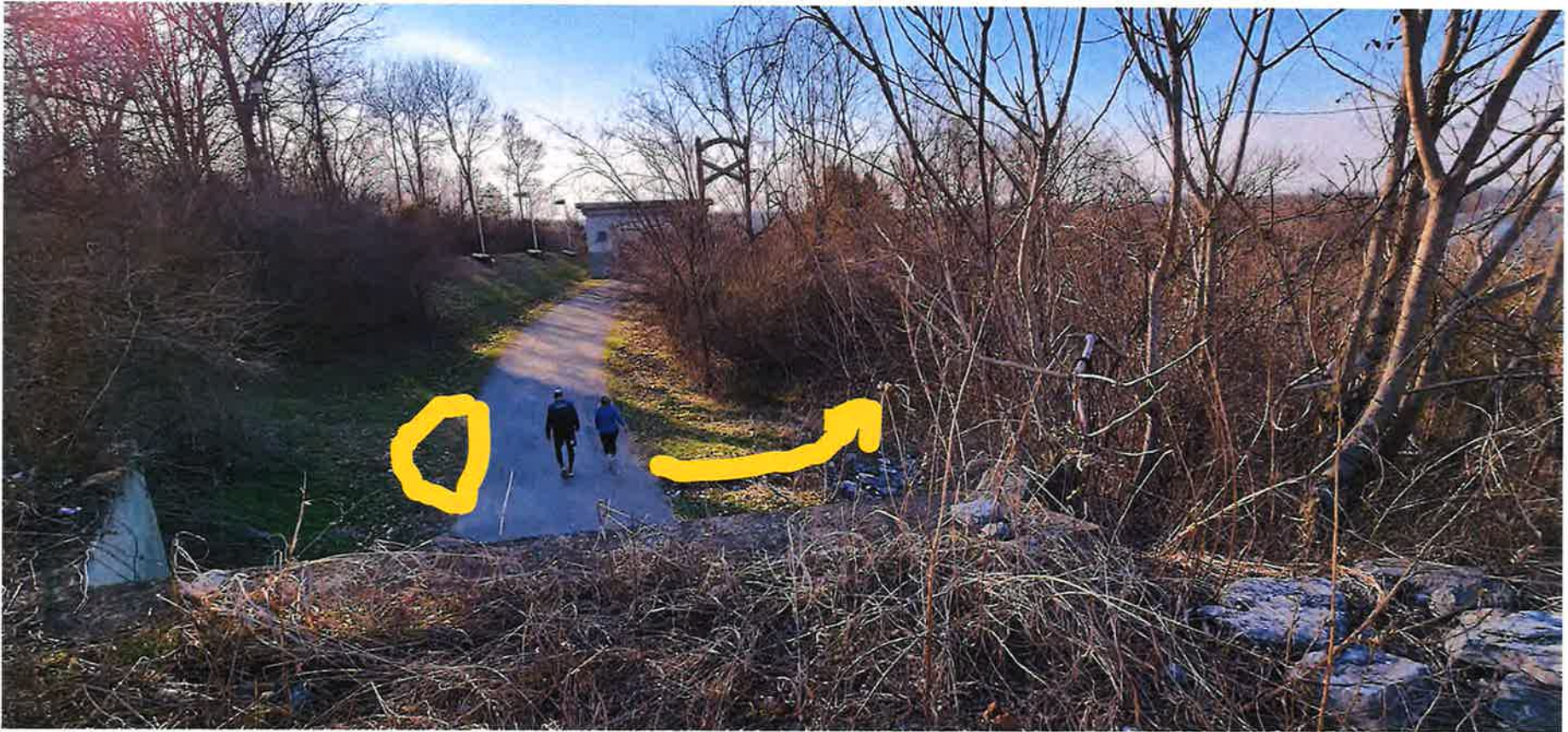
Looking East on Stones River Greenway to tunnel under S.R. 155 from beginning point of Opry Mills Greenway Connector. Pedestrian Bridge to west.



Looking west towards Cumberland River Pedestrian Bridge from proposed starting place for Opry Mills Connector.



Looking towards tunnel in winter season.



Stones River Greenway from atop the tunnel. Pedestrians are at approx location of proposed trailhead area.



Cumberland River View from Pedestrian Bridge. Opry Mills Riverboat down river in middle of photo



Opry Mills Mall Greenway route through parking area



Greenway Route through existing island





Crosswalk area at Opry Mills Mall with bus stop shown to left. ADA ramps will be installed at approximate locations shown



03.21.2024  
METRO PARKS & RECREATION  
DAVIDSON COUNTY, TN

**SWITCHBACK PLANTING**  
OPRY MILLS CONNECTOR TRAIL

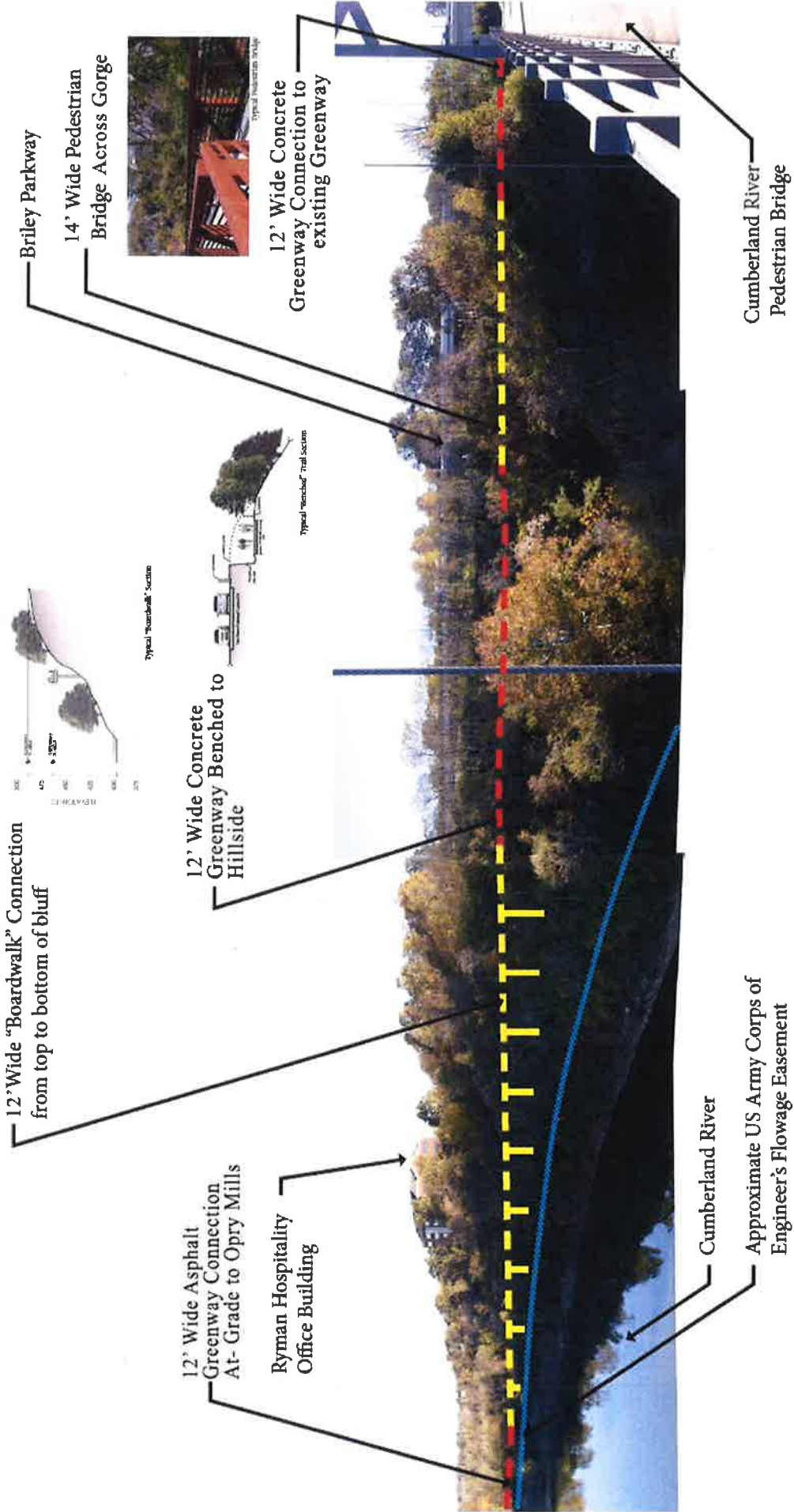




03.21.2024  
METRO PARKS & RECREATION  
DAVIDSON COUNTY, TN

**PEDESTRIAN BRIDGE #4**  
OPRY MILLS CONNECTOR TRAIL







# The Nashville Parks & Greenways Master Plan

March 2017

Megan Barry, Mayor  
Christy Smith, Chair,  
Metropolitan Board of Parks  
and Recreation  
Tommy Lynch, Director

**PARKS ADDRESS A BROAD RANGE OF PUBLIC NEEDS**

2.3 | RETURN ON INVESTMENT OF PARKS

**2.3 NASHVILLENEXT**

In 2015, after a three-year process of intense public engagement, the Metro Planning Department produced a new General Plan for Nashville and Davidson County. Consensus was built all across the community that provided a shared vision, goals, policies, and actions that will be a planning roadmap for Nashville’s next 35 years. Plan to Play is built on foundation of NashvilleNext. That plan adopted several principles that guided decision-making. They are:

- Ensure opportunity for all
- Expand accessibility
- Create economic prosperity
- Foster stronger neighborhoods
- Advance education
- Champion the environment
- Be Nashville

Throughout Plan to Play, it becomes clear that perhaps no other single area of investment is better positioned to contribute to the fulfillment of all of these goals than parks and greenways.



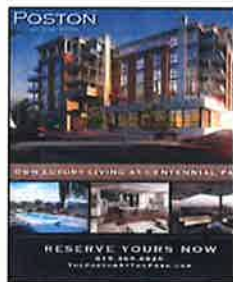
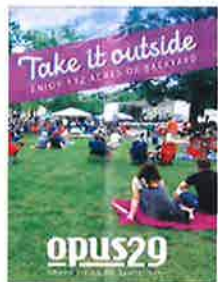
**INVESTING IN QUALITY-OF-LIFE**

In 2015, Nashville Public Radio aired a local story about people who move to Nashville first and find the job later. Every recent transplant interviewed was motivated by the quality of life Nashville offers. Reporter Blake Farmer opened the story with this: “Let’s start with a side note: Parks seem to seal the deal -- from the neighborhood playground to Centennial Park and its full size replica of the Parthenon.”

He goes on to talk with several newcomers, including Jacki Holland. “We were at Sevier Park and it was Easter,” Holland said. “It was

beautiful and my daughter was playing on the playground. We were just laying on the hill enjoying the day and we just sort of looked at each other and said, ‘This could be nice. This could be a nice life for us.’”

These findings support Mayor Barry’s positioning of parks and greenways as infrastructure. **It reflects an understanding of the many ways in which parks provide essential services at a great return on investment.**



Developments in Nashville often promote their proximity to parks and greenways.

Indeed, the economic benefits of Nashville’s park and greenways are substantial, are measurable, and offer an excellent return on investment. The intangible benefits, while harder to measure, were also confirmed during the first round of Plan to Play public meetings. Participants were asked to respond to a question with a single word. The question was, “How do you feel when you’re in a park or on a greenway?” The responses:

## 2.5 THE EMOTIONAL RESPONSE



Figure 2-1 Word cloud of Public Meeting #1 “feeling” responses

The Metro Parks and Greenways system has been funded and has grown steadily over the years because the community advocates and the community’s leadership have appreciated, to a large degree, the intrinsic values parks provide to the community and the surrounding region. But looking more closely beyond some of the intrinsic values, six measures of the economic benefits of the Metro park and greenway system provide clear evidence that investments in the infrastructure of land and facilities, programs, and operations of a healthy park system yield extremely valuable benefits to the community now and into the future. Continued sustainable investment in the park system’s growth and maintenance could be one of Metro Government’s best tools to advance the NashvilleNext community vision.

## 2.4 CONCLUSIONS

The economic return-on-investment to sustainably fund the park system as infrastructure is truly quantifiable – low costs compared to very high returns at multiple levels. The Triple Bottom Line metrics for choosing to invest in Metro’s park system rank extremely high for all three bottom line measures: social, environmental, and economic. Finally, the collective intangible benefits become more evident when park users express their emotional responses to how parks and greenways make them feel.

## 7.2 | CAPITAL IMPROVEMENTS - LAND

### GREENWAY CORRIDOR PRIORITY PLAN

The priority plan lists greenway priorities for the next ten years, while acknowledging the value in remaining flexible and opportunistic. The pace of project readiness is driven by funding and, most importantly, when right of way has been obtained. Piecing the greenway puzzle together largely depends on cooperation and participation of landowners, developers, and many local, state, and federal agencies. The priority plan should be revisited annually to update priorities based on new information or newly identified needs. See Figure 7-8.

#### New Corridors

- Downtown Loop
- North Gulch to Bicentennial Mall
- South Gulch to South Street
- Charlotte Rail Trail to 440 Spoke
- Mid-Town Loop
- 440 Greenway
- Brown's Creek, Fairgrounds to Cumberland River/Rolling Mill Hill/Riverfront Park

#### Tourism and Cultural Trails

- Music City Center to Adventure Science Center and Ft. Negley
- Stones River to The Hermitage

#### Regional Connections

- Mill Creek to Williamson County (Concord Road)
- Rail-with-Trail to Wilson County
- Rail-with-Trail to Cheatham County
- The Trace Connector

#### Under-served Areas

- East-Elmington Parkway
- Northeast Quadrant, East to West
- Southeast Anchor Park

#### Key Connections

##### Major bridges and boardwalks as connecting icons:

- Opry Mills Connector (Shelby Bottoms and Stones River)
- Brookmeade Park to Bells Bend Pedestrian Bridge and/or Ferry
- Neelys Bend to Stones River Pedestrian Bridge and/or Ferry

#### Links connecting destinations

- Schools
- Libraries
- Neighborhoods
- Businesses
- Shopping
- Restaurants
- Entertainment
- Transit, etc.



# MEETING COMMUNITY NEEDS

## 5.1 | PUBLIC ENGAGEMENT

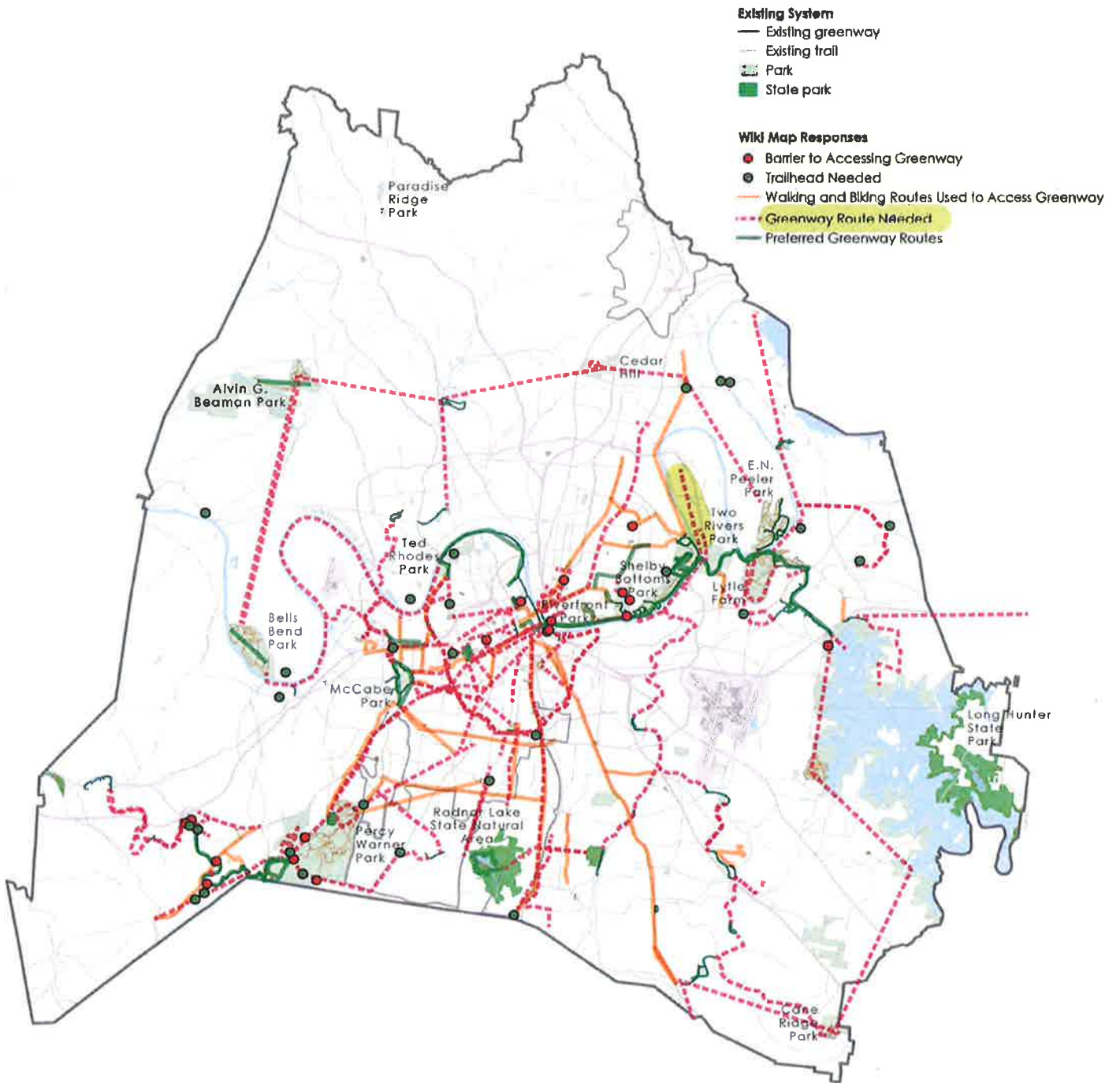


Figure 5-1 INTERACTIVE MAP SUMMARY



## Health, Livability & the Built Environment Goals and Policies

### BE Goal 1

All Nashville residents have a choice of vibrant, safe, and healthy neighborhoods across many different communities and contexts.

#### BE Policy 1.1

Preserve residential character in established neighborhoods, while accommodating housing options that meet Nashville's changing needs.

#### BE Policy 1.2

Create safe, walkable community, retail, and employment centers across the county that fit within each community's context and character and meet the needs for people of all ages and abilities.

#### BE Policy 1.3

Encourage the development, redevelopment, or improvement of property, buildings, and landscapes to promote safety and reduce opportunities for crime.

### BE Goal 2

Nashville promotes the safety and wellness of its residents, workers, and visitors.

#### BE Policy 2.1

Encourage physical activity and promote social and mental well-being by improving public spaces (such as public streets, sidewalks, and parks), reducing barriers to all pedestrians, and providing green space.

#### BE Policy 2.2

Improve the health quality of Nashville's air, water, light, and land, both outside and indoors.

#### BE Policy 2.3

Improve mental health and wellbeing of Nashville residents through advocacy, education, research, and service.

### Goals and Policies

- » **Goals** set broad direction for the plan by applying the Guiding Principles to NashvilleNext's seven plan elements. They identify, for each element, what NashvilleNext is trying to achieve.
- » **Policies** extend goals by providing more detail. They give more direct guidance on community decision making, without specifying which tools to use. (Identifying and adopting which tool is a job for actions and implementation.) As implementation occurs, if one particular tool is rejected by the public, the policy guidance remains.

**BE Policy 2.4**

Ensure all neighborhoods have healthy food options—including locally grown food—particularly neighborhoods with low levels of car ownership.

**BE Policy 2.5**

Increase connections to the network of accessible greenways, creeks, and rivers that connects Nashville residents to nature throughout Davidson County.

**BE Policy 2.6**

Host active and passive activities and amenities at community parks and open spaces for people of all ages.

**BE Goal 3**

Nashvillians have equitable access to goods, services, multiple modes of transportation, and public safety.

**BE Policy 3.1**

Support access throughout the county for all users (including walkers, cyclists, transit riders, and drivers) of roadways and greenways.

**BE Policy 3.2**

Match transportation infrastructure to the needs of police, fire, and emergency medical personnel to maintain response times and keep residents, workers, and visitors safe.

**BE Policy 3.3**

Ensure equitable access to high speed Internet service throughout the county and adapt online services to mobile Internet users.

**BE Policy 3.4**

Support communities by diversifying the use of public buildings and facilities in the services they offer (such as libraries, schools, community centers, and police and fire stations).

**BE Policy 3.5**

Facilitate the access of health and wellness services to meet changing health care needs.

**BE Policy 3.6**

Maintain infrastructure and programs to keep the public safe during emergencies.



# A General Plan for Nashville & Davidson County

Adopted June 22, 2015  
Amended August 24, 2017



## Volume III: Community Plans



**Donelson -  
Hermitage -  
Old Hickory**



**Stones River Greeway**

In some areas, a multi-use path may be a more appropriate solution than a sidewalk, bikeway or greenway. A multi-use path is a greenway, but instead of following a river or creek as a greenway does, a multi-use path follows a street. A multi-use path can be beneficial by being a more efficient provision of infrastructure (if it is built on one side of the corridor, unlike sidewalks and bikeways on both sides of a street) and the greenway-like design can be more in keeping with a rural or suburban setting.

### Recommended Greenway System Connections and Multi-Use Paths

The following greenway segments, ridge trails, and multi-use paths are recommended to complement the existing greenway system:



**Runner along greenway**

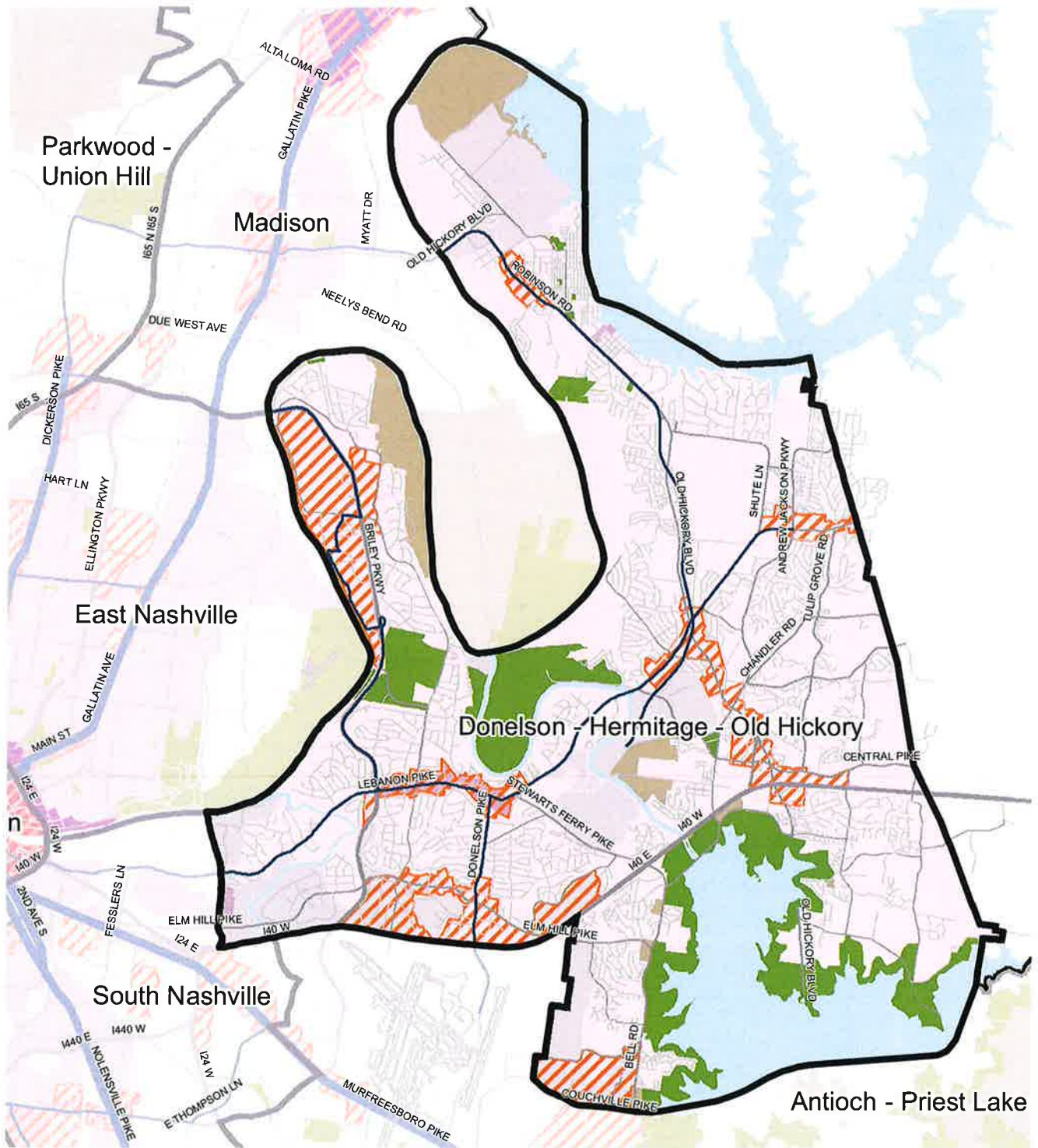
- » **Mill Creek Greenway:** Complete the Mill Creek Greenway from the Antioch – Hickory Hollow area to the Donelson area.
- » **McGavock Pike Multi-Use Path:** Develop a multi-use path adjacent to McGavock Pike from Lebanon Pike to the Stones River Greenway.
- » **Peeler Park Greenway Bridge:** Connect people walking and biking with a bridge over the Cumberland River from the Stones River Greenway to Peeler Park

- » **Opry Mills Connector:** Connect Two Rivers Greenway to Opry Mills.



**Greenway bridge across the Cumberland**

**Figure DHOH-2: Transect**  
 Donelson-Hermitage-Old Hickory detail



**Transects Legend**

- |  |  |   |  |
|--|--|---|--|
|  Centers      |  Subarea Boundaries |  T1 Natural  |  T5 Center   |
|  Anchor Parks |  Immediate Need     |  T2 Rural    |  T6 Downtown |
|  |  Long-Term Need     |  T3 Suburban |  D District  |
|  |  |  T4 Urban    |  Water       |



**Access Nashville Cycling Project #11**

McGavock Pike Multi-Use Path–Develop a multi-use path adjacent to McGavock Pike from Lebanon Pike to the Stones River Greenway.

A Multi-Use Path along McGavock Pike from Lebanon Pike to the Stones River Greenway is a Community Priority and will allow neighbors to safely and comfortably walk and bike to the Stones River Greenway, the Cumberland River Pedestrian Bridge and East Nashville, and the Opryland/Music Valley area via a planned connection. McGavock Pike is a crucial bikeway connection between existing bikeways and greenways in East Nashville and planned bikeways and greenways in Donelson, South Nashville, and Southeast Nashville. Refer to Figure DHOH-26.

**Access Nashville Cycling Project #12**

Peeler Park Greenway Bridge–Connect people walking and biking with a bridge over the Cumberland River from the Stones River Greenway to Peeler Park.

Peeler Park plays an important role in Nashville’s Green Network. A bicycle and pedestrian connection across the Cumberland River between the Stones River and Peeler Park Greenways will expand access to outdoor recreation opportunities and create additional access for cyclists who travel between Madison and Donelson or Hermitage. The Peeler Park Greenway Bridge is a Community Priority. Refer to Figure DHOH-27.

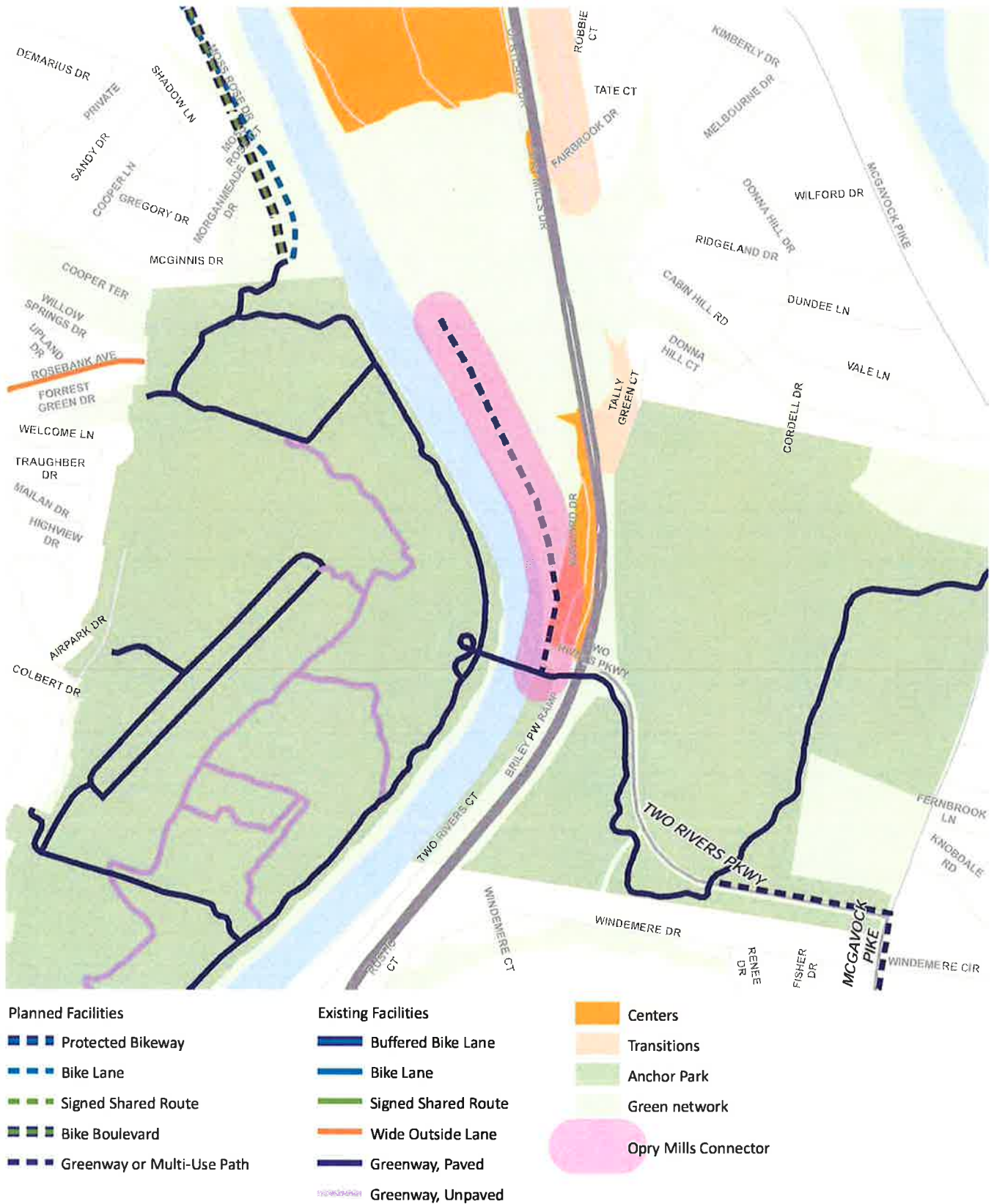
**Access Nashville Cycling Project #13**

Opry Mills Connector–Create a direct bicycle and pedestrian connection between the Stones River Greenway and Opry Mills.

The Opry Mills Connector provides access to shopping, employment, and cultural activities in the Opryland/Music Valley area from the Stones River and Shelby Bottoms Greenways. This short connection would dramatically expand access to Music Valley for people who walk and bike the greenways from East Nashville, Donelson, and Hermitage, while also creating a low-stress bicycle connection for tourists and residents alike between downtown and Opryland. The Opry Mills Connector is a Community Priority. Refer to Figure DHOH-28.

### Figure DHOH-28: Access Nashville Bicycling Project #13: Opry Mills Connector

Create a direct bicycle and pedestrian connection between the Stones River Greenway and Opry Mills.









# A General Plan for Nashville & Davidson County

Adopted June 22, 2015

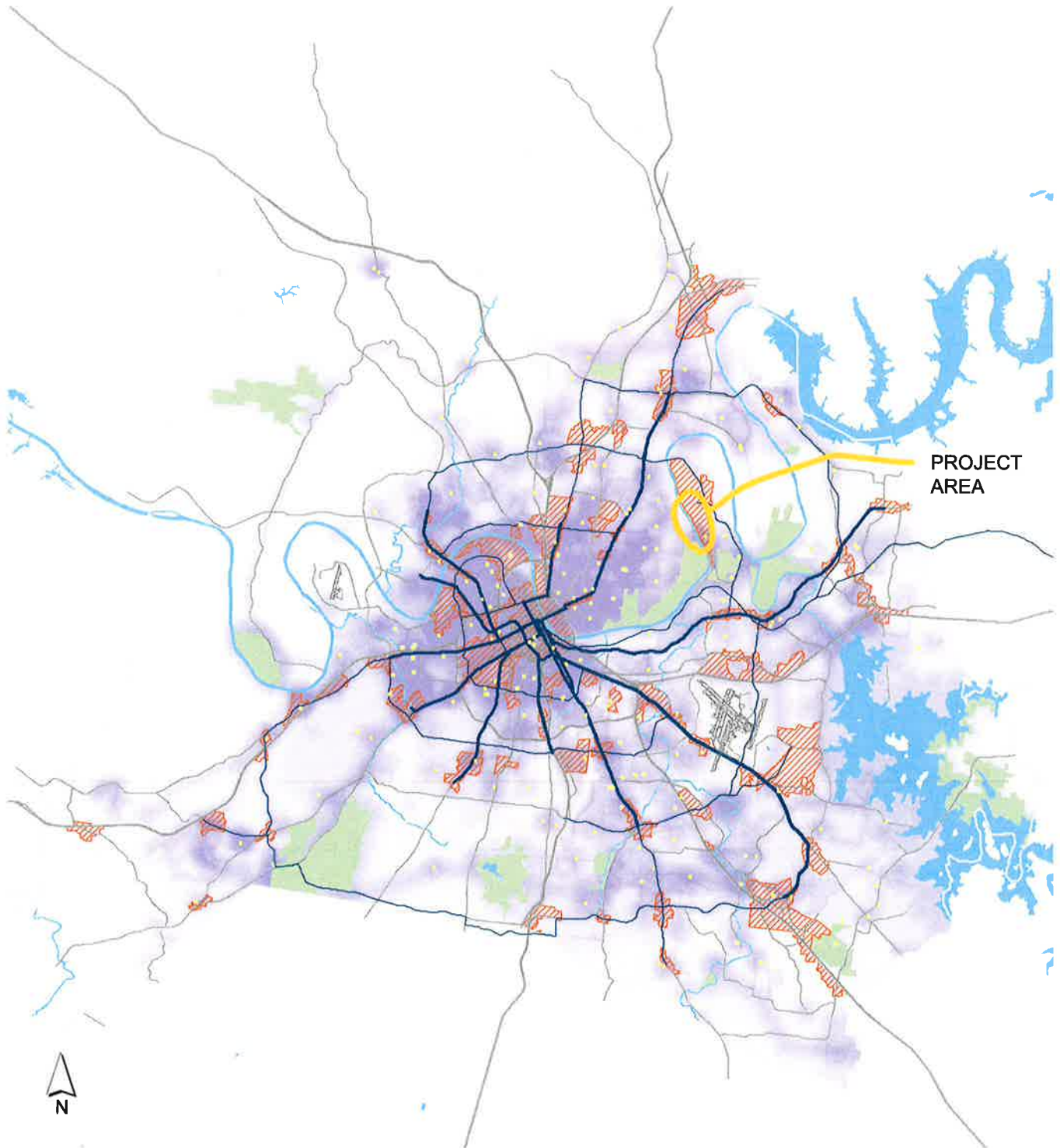
# Volume V: Access Nashville 2040




 Certified per TCA 13-4-202 as a part of the Nashville-Davidson County General Plan adopted by the Metropolitan Nashville-Davidson County Planning Commission and including all amendments to this part as of June 22, 2015.

  
 Executive Secretary

**Figure 7: Pedestrian generators and NashvilleNext centers**



**PROJECT AREA**

**Pedestrian Generator Index Legend**

- |              |                                      |   |
|--------------|--------------------------------------|---|
| Centers      | Priority Corridors<br>Immediate need | Pedestrian Generator Index<br>High : 65 |
| Water Bodies | Long-term need                       | Low : 0                                 |
| Anchor Parks | Schools                              |   |

## Access Nashville's Transportation Vision



Thinking through how our environment allows people with mobility needs access opportunities is critical for Nashville's future.

Access Nashville's transportation vision guides our city's decisions in creating a multimodal transportation network focused on providing access.

*By 2040, efficient land use policies and strategic investments in Nashville's transportation network will link all road users, regardless of their mode of transportation, with meaningful access to social and economic opportunities.*

To achieve this vision, eight Accessibility Principles guide transportation improvements, linking them with the rest of NashvilleNext.

### 1. Create a place with efficient community form and transportation choices.

One of Metro's largest publicly accessible assets and fiscal investments is our transportation network, including our streets, buses, sidewalks, multi-use paths, greenways, benches, bikeways, and many other components. The design of our streets should provide access to all users, be informed by local context, and improve our city's quality of life and aesthetics. To provide access for all road users, we must right-size our city streets to accommodate people who ride buses, bicycle, and walk. Increasing the number of different types of users on a street, improving walkability, creating neighborhoods with a mix of uses with higher densities at strategic locations, and designing streets for slower traffic speeds will result in better places, reduced traffic fatalities, improved health for our population, and a network that operates more efficiently than one dependent on a single transportation mode. Transportation choices across the lifespan that improve accessibility to needs will only improve the quality of life of all Nashvillians. As we age, we may need different travel choices.<sup>2</sup>



Building placement with wide sidewalks is crucial to link people walking.

<sup>2</sup> Todd Litman, 2010, *Evaluating Public Transportation Health Benefits for the American Public Transit Association* - [http://www.apta.com/resources/reportsandpublications/Documents/APTA\\_Health\\_Benefits\\_Litman.pdf](http://www.apta.com/resources/reportsandpublications/Documents/APTA_Health_Benefits_Litman.pdf); Dumbaugh, Eric and Rae, Robert(2009) 'Safe Urban Form: Revisiting the Relationship Between Community Design and Traffic Safety', *Journal of the American Planning Association*, 75: 3, 309 — 329 - <http://dx.doi.org/10.1080/01944360902950349>; US DOT, 2014, *Safer People Safer Streets: Summary of U.S. Department of Transportation Action Plan to Increase Walking and Biking and Reduce Pedestrian and Bicyclist Fatalities* - [http://www.dot.gov/sites/dot.gov/files/docs/safer\\_people\\_safer\\_streets\\_summary\\_doc\\_acc\\_v1-11-9.pdf](http://www.dot.gov/sites/dot.gov/files/docs/safer_people_safer_streets_summary_doc_acc_v1-11-9.pdf)

### What are the benefits of walking on health?

- » Reduce the risk of heart disease
- » Improve blood pressure and blood sugar levels
- » Maintain body weight and lower the risk of obesity
- » Enhance mental health
- » Reduce the risk of osteoporosis
- » Reduce the risk of breast and colon cancer
- » Reduce the risk of type 2 diabetes<sup>1</sup>

<sup>1</sup> *The Benefits of Walking, American Heart Association*  
- [http://www.startwalkingnow.org/whystart\\_benefits\\_walking.jsp](http://www.startwalkingnow.org/whystart_benefits_walking.jsp)

## 5. Improve human health and the environment.

Different transportation options present both positive and negative impacts on human health and the environment. When people can choose to walk, bike, or take transit, they are able to improve their health and reduce the impact of environmental pollution associated with driving. Public health professionals recognize that a city's urban fabric—its buildings and street layout—can positively contribute to increased physical activity. People who live and work in walkable neighborhoods spend more time being physically active, spend less time driving, are more physically fit, and live lives that emit less pollution.<sup>8</sup>



Greenways provide a buffer between development and streams, which improves water quality and decreases flooding risks. They are used for recreation but are also a link in today's transportation system.

## 6. Ensure financial responsibility.

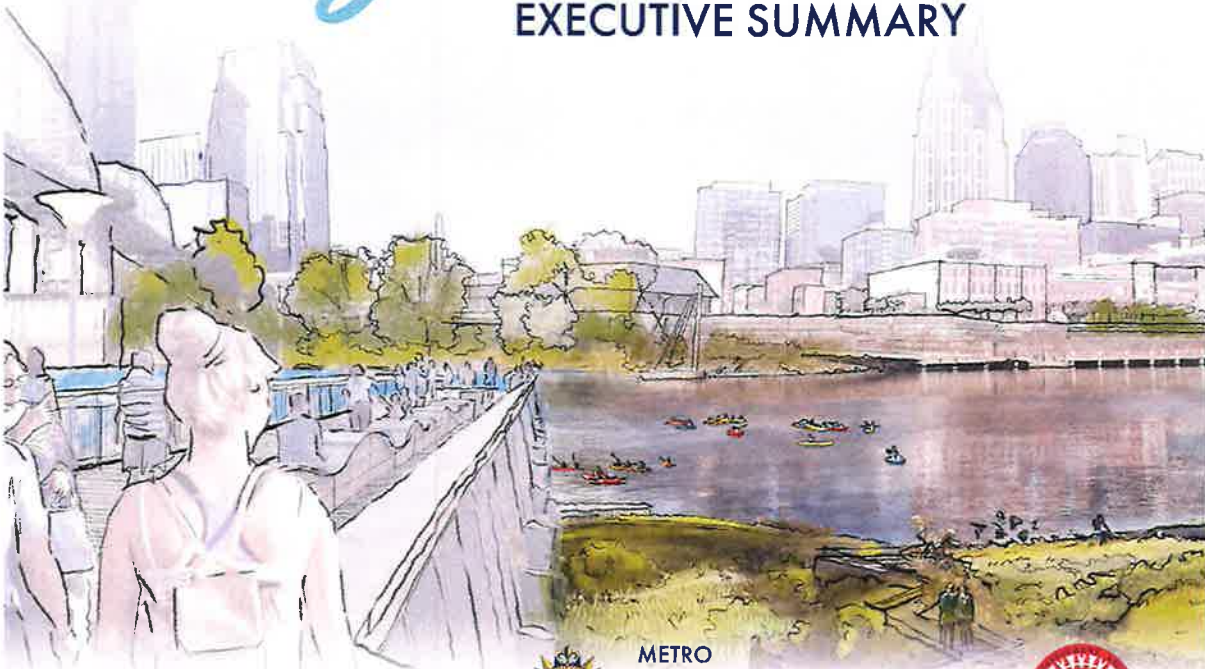
Installing and maintaining transportation infrastructure is costly, and opportunities to increase transportation budgets are limited. Moving forward, all levels of government must work within existing budgets to maintain transportation assets much more efficiently. Because our region's street and highway network is mostly built, our top investment priorities are maintaining existing facilities and increasing meaningful access to different transportation modes.

<sup>8</sup> *Landstreet, Julia, Dr. Bill Paul, and Food Policy Council, NashvilleNext Health, Livability, and the Built Environment Background Report (March 2013) <http://www.nashville.gov/Portals/0/SiteContent/Planning/docs/NashvilleNext/next-report-Health-Livability-BuiltEnvironment.pdf>*



# *imagine* eastbank

## EXECUTIVE SUMMARY



adopted October 6, 2022



**METRO  
NASHVILLE  
PLANNING  
DEPARTMENT**

**NDOT**



## key plan components and priority action items

Imagine East Bank proposes an ambitious vision that will take years, if not decades, to be fully realized. Many components of the vision are interdependent, requiring careful consideration of priorities, phasing, resources, and other factors like additional feasibility or subsequent planning studies.

The recommendations presented in this document cover a wide range of next steps from small-scale to ambitious capital improvement projects. Four priority recommendations — foundational to the implementation of the vision — are listed below.

### 1. Develop an East Bank implementation strategy

- » Set forth a clear process for the redevelopment of Metro-owned property on the East Bank, consistent with the vision.
- » Following the adoption of the vision plan by the Metro Planning Commission, new zoning change requests should be evaluated in accordance with the vision.
- » Create design guidelines for Metro-owned property on the East Bank.
- » Establish a design review and approval process — such as a design review committee — to ensure proposed developments meet the intent and standards of the vision.
- » Set aside land on Metro-owned properties for key public resources outlined in the vision, including park(s), greenways, rights-of-way, affordable housing, a mobility hub, and cultural assets.
- » Determine the needs for Metro services, such as fire safety, and plan for appropriate allocation in future development agreements.

- » Acquire, or negotiate through agreements, land for public infrastructure.
- » Explore feasibility of creating a Business Improvement District (BID), to support public services and manage assets on the East Bank.
- » Develop a parking operational plan — that incorporates the needs of the city, mixed-use developments, and the Titans Stadium — that also aligns with the mobility strategy.

### 2. Advance livability through implementation

- » Utilize the policies and procedures of land development — such as zoning reviews, lease negotiations, and development incentives — to address community needs like housing, fresh food, and childcare.
- » For each phase of development, establish clear housing benchmarks — consistent with the Imagine East Bank vision and the city's Unified Housing Strategy — for which Metro is a landowner or a participant in an agreement with private landowners. See pg. 56 for Phase I benchmarks.
- » Engage the Affordable Housing Task Force and the community to assist in establishing affordability goals for each phase of development to include a mix of income ranges, including goals for deep affordability.
- » Explore strategic funding or policy models and partnerships that link development with community objectives for equitable and affordable housing, workforce development, and educational attainment.
- » The city's new Office of Economic Growth and Small Business Development should

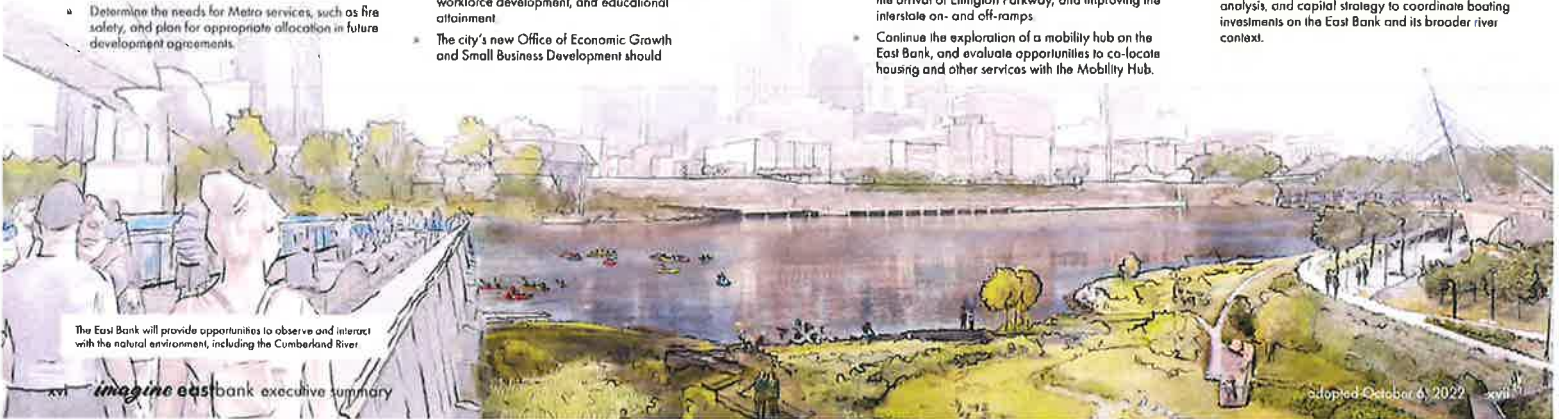
deploy strategies that lead to long-term, stable opportunities for small, local and minority-owned businesses on the East Bank.

### 3. Place multimodal transportation at the forefront

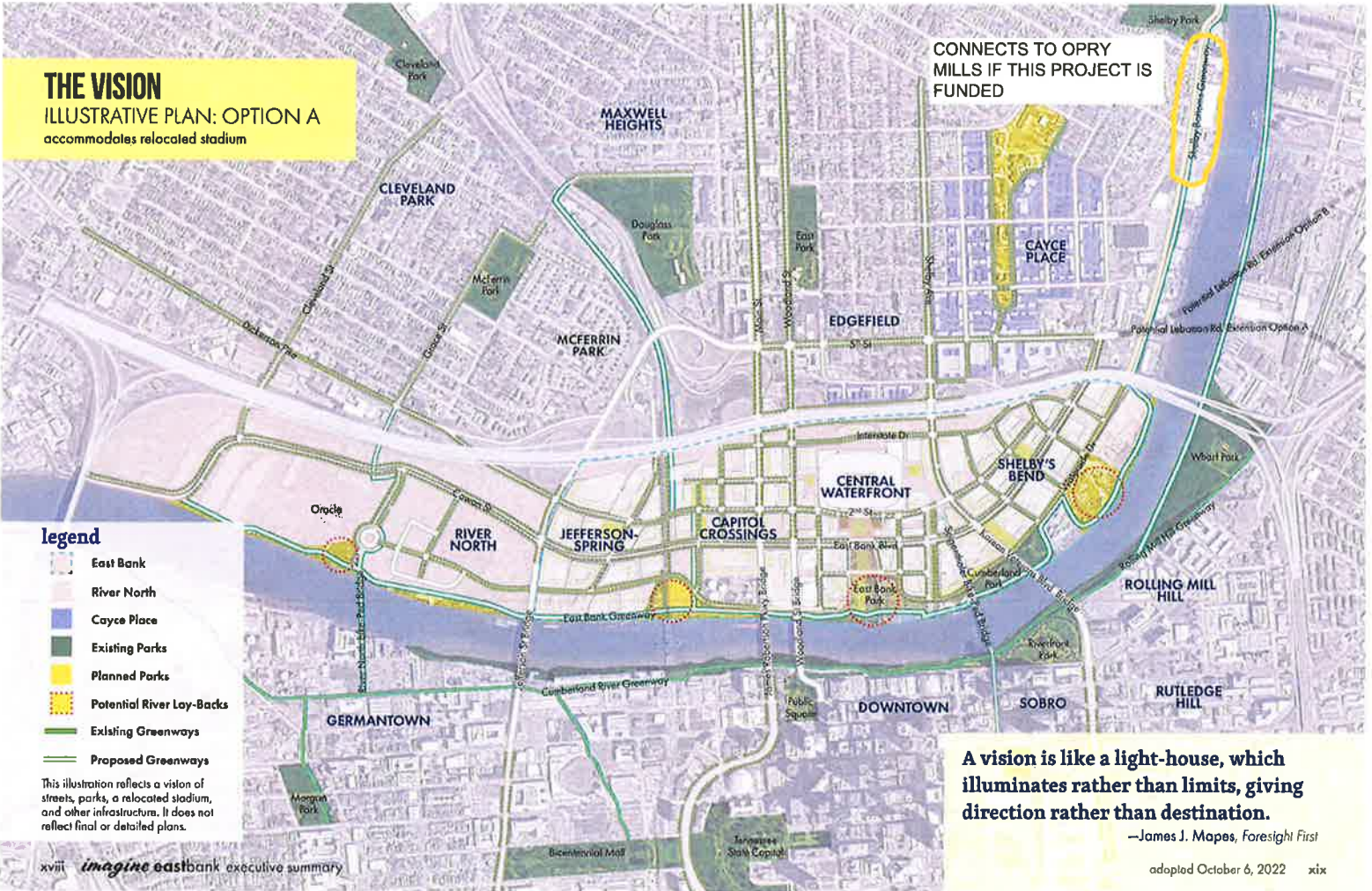
- » Continue feasibility exploration, planning, design, and construction of the East Bank Boulevard
- » Dedicate lanes for transit service, in accordance with NDOT and WeGo operational standards
- » Dedicate lanes for bikeways, sidewalks, and streetscape improvements that are consistent with the vision.
- » Prioritize a separated and safe bicycling experience through detailed design and operational standards
- » Coordinate with NDOT and WeGo to ensure the East Bank Boulevard creates safe, reliable connections to and through the East Bank, and between adjacent neighborhoods and corridors, such as extensions to Dickerson Pike and Murfreesboro Pike.
- » Negotiate and acquire rights-of-way needed to advance the mobility network, including for the East Bank Boulevard and other streets.
- » Develop and approve streetscape design standards to ensure the creation of complete streets on the East Bank.
- » Partner with TDOT to prioritize key improvements, such as renovation or replacement of state bridges, the arrival of Ellington Parkway, and improving the interstate on- and off-ramps.
- » Continue the exploration of a mobility hub on the East Bank, and evaluate opportunities to co-locate housing and other services with the Mobility Hub.

### 4. Invest in performative and resilient outdoor spaces

- » Lay-back the Cumberland River bank in various locations on the East Bank and in River North, and provide (on Metro-owned land) or encourage (on privately-owned land) various waterfront programming and amenities as desired by the community, such as open space and/or boating investments
- » Set aside land for a riverfront park on Metro property that expands access to outdoor recreational amenities, engages the river, and improves resiliency through riparian edge management
- » Establish a resiliency framework that sets goals and benchmarks for environmental design and performance — such as outdoor space requirements and minimum elevations for development.
- » Collaborate with Metro Parks and Greenways to explore the feasibility of developing an implementation plan for parks and greenways on the East Bank — including financing mechanisms for design, construction, and ongoing operations and maintenance.
- » Develop an implementation plan for district stormwater management for the Central Waterfront neighborhood, in coordination with Metro Water Services.
- » Develop an operational plan, feasibility/market analysis, and capital strategy to coordinate boating investments on the East Bank and its broader river context.



The East Bank will provide opportunities to observe and interact with the natural environment, including the Cumberland River



**THE VISION**  
 ILLUSTRATIVE PLAN: OPTION A  
 accommodates relocated stadium

CONNECTS TO OPRY MILLS IF THIS PROJECT IS FUNDED

- Legend**
- East Bank
  - River North
  - Cayce Place
  - Existing Parks
  - Planned Parks
  - Potential River Lay-Backs
  - Existing Greenways
  - Proposed Greenways

This illustration reflects a vision of streets, parks, a relocated stadium, and other infrastructure. It does not reflect final or detailed plans.

**A vision is like a light-house, which illuminates rather than limits, giving direction rather than destination.**

—James J. Mapes, *Foresight First*

adopted October 6, 2022 xix

# CONNECT DOWNTOWN

## Action Plan

APRIL 2024



**NDOT**

**WeGo**  
Public Transit

**TN** TDOT  
Tennessee Department of Transportation

**NASHVILLE  
DOWNTOWN  
PARTNERSHIP**



## What are Connect Downtown's five big moves?

Addressing the mobility challenges facing Downtown Nashville requires a combination of projects, programs, and policies that serve people using all forms of transportation.

Doing just one thing—or focusing on just one mode of travel—won't have the kind of impact that's needed. Therefore, Connect Downtown's recommendations work as a system of investments that are organized into five big moves.



### BIG MOVE 1

#### Manage Congestion

Upgrade signals, improve traffic operations, and better manage events to keep people moving and improve system resiliency.



### BIG MOVE 2

#### Improve Safety

Advance Vision Zero projects and programs to make Downtown's streets safer for people, especially Downtown's most vulnerable travelers.



### BIG MOVE 3

#### Move More People

Prioritize buses on key corridors and increase the amount of service to provide faster and more reliable trips throughout the region.



### BIG MOVE 4

#### Create Complete Networks

Develop safe, separated, and connected walking, rolling, biking, and scooting facilities to help people of all ages and abilities get into and around Downtown.



### BIG MOVE 5

#### Maximize the Curb

Flex the uses of the curb throughout the day for deliveries, service vehicles, and passenger pick-up and drop-off, including charter buses, to support local businesses, venues, museums, and residents.

Together, these projects and strategies will make travel more reliable, more comfortable, faster, and safer for thousands of Nashvillians. For example, the new Traffic Management Center will help NDOT respond to congestion in real time, clearing traffic more quickly during special events, emergencies, and peak travel periods. And the transit priority corridors will move nearly 50,000 people each day, saving hours a month for bus riders and car drivers!

**From new signals to safety projects to more frequent bus service to mobility lanes to flexible curb regulations, Connect Downtown includes projects and programs that support the entire Middle Tennessee region.**

This map gives an overview of the major capital projects included in Connect Downtown, and there are many complementary strategies described in the Action Plan. The Connect Downtown projects also amplify other NDOT, WeGo, and TDOT priorities. Building on Metro's implementation of Vision Zero and WalknBike and WeGo's investments in nMotion and the Better Bus Plan, Connect Downtown includes projects that make regional investments work better.

**TRANSIT PRIORITY CORRIDORS**

Connect Downtown recommends **three Transit Priority Corridors (TPCs)** to link WeGo Central with the new SoBro and East Bank Mobility Centers and provide bus service every 3 to 5 minutes:

- Westside TPC
- James Robertson TPC
- East Bank TPC

**PRIORITY LOADING AREAS**

To ensure that the curb provides the most value for the most people—and that the uses of the curb are prioritized for activities that can't be moved elsewhere—Connect Downtown recommends a focus on **flexible uses and pilot programs that support additional space for loading and deliveries.**

**MOBILITY LANES**

Downtown Nashville's future network of mobility lanes **adds new, safe facilities, upgrades select bike lanes, and links to the existing and planned Greenways system.** Projects include the 1st Ave, 3rd Ave, and 7th Ave mobility lanes, as well as a Church St / Union St / Woodland Bridge connector.



**FLEXIBLE CURB USES (4 AM - 4 PM)**



**TECHNOLOGY & DEMAND MANAGEMENT**

Connect Downtown recommends installing new **adaptive signal technology**, giving NDOT the ability to better manage traffic. Along with new **transportation demand management and event management strategies**, Downtown's streets will move more people.



## LINKS TO DOCUMENTS REFERENCED IN THE NARRATIVES:

### I. PLANNING:

1. [ConnectDowntown ActionPlanExecutiveSummary April2024.pdf \(nashville.gov\)](#)
2. [Imagine-East-Bank-Executive-Summary.pdf \(nashville.gov\)](#)
3. [next-volume1-GH-StrategyImplementation Amended2017.pdf \(nashville.gov\)](#)
4. [next-volume5-AccessNashville2040.pdf](#)
5. <https://filetransfer.nashville.gov/portals/0/sitecontent/Parks/docs/PlanToPlay/2017-04-12%20Parks%20MP%20Book.pdf>
6. <https://www.nashville.gov/departments/planning/long-range-planning/community-plans/donelson-hermitage-old-hickory>

### II. ECONOMY OF TRAILS:

1. Southern Living; <https://www.southernliving.com/cities-2024-8422755> calls out Nashville as the 3<sup>rd</sup> South Best City 2024 as a domestic travel destination. Nashville's greenways are specifically mentioned on the same site in the Best Times to Visit for Outdoor Fun linked article. 2.
2. <https://www.southernliving.com/best-time-to-visit-nashville-8600532> for travelers seeking "biking, walking, and sightseeing right in the city."
3. Global Traveler, <https://www.globaltravelerusa.com/global-traveler-announces-the-winners-of-the-2024-leisure-lifestyle-awards/> ranked Nashville in the top 5 for Best Domestic Adventure Destinations due to recreation amenities.
4. [Health Benefits of Trails - American Trails](#) includes summaries of several studies from 2011 to 2016 that speak to the health benefits of trails and health equity within urban communities.
5. [The Impact of Trails and Greenways on Property Values | Finance for the Field | Parks & Recreation magazine | NRPA](#) summarizes several studies that explore increased property values for homes close to trails.
6. [The Business of Trails: A Compilation of Economic Benefits - American Trails](#) summarizes trail studies that have focused on their economic benefits in areas after trail corridors were built.
7. <https://www.cambridge.org/core/journals/journal-of-agricultural-and-applied-economics/article/costs-of-reducing-nutrients-from-riparian-buffers-in-western-maryland/3E10CB44C6513AA1357B84B9FDAA5F47> speaks to the economic benefits of riparian buffers, which clean water and air.
8. [https://www.waterboards.ca.gov/sanfranciscobay/water\\_issues/programs/watershed/Riley\\_2009.pdf](https://www.waterboards.ca.gov/sanfranciscobay/water_issues/programs/watershed/Riley_2009.pdf) provides a cost and function comparison between a brick and mortar water treatment plant and a riparian buffer.
9. <https://www.cbo.gov/publication/59753#:~:text=Among%20those%20homes%2C%20the%20total,one%2Dquarter%20of%20EAD%20nationwide> provides a snapshot of current and future flood damage costs to mortgages backed by the federal government.



ESTABLISHED  
(1901)

## Public Input Meeting

**What:** Proposed Opry Mills Greenway Connector  
**Who:** Metro Parks Department  
**When:** Thursday, October 15, 5 to 7 p.m.  
**Where:** Two Rivers Mansion, 3130 McGavock Pike, Nashville, TN 37214

Metro Parks intends to apply for a grant from the Tennessee Department of Transportation and is seeking public input on the proposed project, a trail connection between the Stones River Greenway and Cumberland River Pedestrian Bridge to Opry Mills Mall and Gaylord Opryland Resort & Convention Center complex.

Interested individuals may see proposed preliminary concepts and speak with engineering consultants and Metro staff **between 5 and 7 pm**.

Comment cards will be available and comments may also be emailed to:

Councilman Jeff Syracuse, District 15, at [Jeff.Syracuse@nashville.gov](mailto:Jeff.Syracuse@nashville.gov)  
Councilman Brett Withers, District 6, at [Brett.Withers@nashville.gov](mailto:Brett.Withers@nashville.gov)  
Councilman Anthony Davis, District 7, at [Anthony.Davis@nashville.gov](mailto:Anthony.Davis@nashville.gov)

**Project Description:** If funded, the Opry Mills connector would be located in an easement Metro Parks has below the office building at Two Rivers Parkway and Briley Parkway granted by Gaylord Entertainment several years ago.

A bridge/boardwalk combination would run parallel to the river along the bluff between the Cumberland River Pedestrian Bridge and Opry Mills Mall. It would be on the Donelson side of the Cumberland River.

It will connect East Nashville, Donelson and Hermitage neighborhoods to Opry Mills and provide connection for tourists and Nashville visitors to connect to downtown via cycling.

For more information, contact:  
Mark Bradfield at Metro Parks, [mark.bradfield@nashville.gov](mailto:mark.bradfield@nashville.gov), [615-862-8400](tel:615-862-8400).



Community Meeting - October 15, 2015

**Comments:**

PLEASE CONTINUE  
w/ PROJECT

**Stones River Greenway - Opry Mills Connector**

Community Meeting - October 15, 2015

**Comments:**

Great Solution! Much Needed.  
Can't wait

SEAN ONKEL

**Stones River Greenway - Opry Mills Connector**

Community Meeting - October 15, 2015

**Comments:**

wants brochure/  
any maps

**Stones River Greenway - Opry Mills Connector**

Community Meeting - October 15, 2015

**Comments:**

I Think This IS A VERY GOOD  
PROJECT + I STRONGLY SUPPORT IT

JACK Kirby

Community Meeting - October 15, 2015

**Comments:**

- #1 - Employees of Opry Mills & Hotel could bike to work (and could save an hour potentially on their commute) and would open a parking spot for customers.
  - #2 - Donelson teenagers could get to Opry Mills for movies, food & entertainment without parents driving them & contributing to traffic during drop-off & pick-up.
- OVER -

**Stones River Greenway - Opry Mills Connector**

Community Meeting - October 15, 2015

**Comments:**

The Greenway can't come soon enough! Until then I don't have good access to shop at the mall.  
Covered bike parking at the mall side would be ideal.

**Stones River Greenway - Opry Mills Connector**

Community Meeting - October 15, 2015

**Comments:**

Sounds like a great idea - we have been hoping for this since we moved to Donelson 3 years ago! I do as much shopping on my bike as possible and it would be great not having to worry about trying to park at Opry Mills.

Vicki Beare

**Stones River Greenway - Opry Mills Connector**

Community Meeting - October 15, 2015

**Comments:**

My family in Inglewood wholeheartedly supports this project, and hopes for more like it. We are on the right track...  
Keep going!  
- David McDowell

downtown tourists.

#4 - Bicycle share station at Donelson  
train station could get folks from train  
to Opryland in 15-20 minutes!

Peter Beare

**Stones River Greenway - Opry Mills Connector**  
Community Meeting - October 15, 2015

**Comments:** I would really like to see this  
extension go through because it is  
really needed by everyone - walkers, bikers, &  
TOURISTS. I am very glad to see that  
there are still hopes for this to go through  
because I was on the early greenways  
program with Phil Ponder. All we need  
to do is get the word out & this  
extension will happen. Good luck to  
all of us.

MIKE FRANKIEH

**Stones River Greenway - Opry Mills Connector**  
Community Meeting - October 15, 2015

**Comments:**  
Love this idea - just wish it could be completed  
sooner! This would allow me to reach Opry  
Mills

in about 30 mins. by bike.  
I would use it often! Have you considered a  
crowdfunding campaign in case the grant doesn't  
come through or isn't sufficient (or to help  
speed up the timeline)?





August 13, 2024

The Honorable Butch Eley  
Commissioner of Transportation  
Tennessee Department of Transportation  
505 Deaderick Street, Suite 700  
Nashville, Tennessee 37243

**Subject: Transportation Alternatives Program Grant Application – Nashville, TN**

Dear Commissioner Eley,

I am pleased to offer this letter of support for the Metro Nashville Parks Department's Transportation Alternatives Program application. Metro Nashville's proposal to construct a multi-modal path from the Cumberland River Pedestrian Bridge to the Opry Mills Complex will not only provide connectivity to a major activity center but will improve safety and provide an alternative route for people walking and biking in the area.

GNRC is an association of local governments that convenes local and state leaders for the purposes of planning and programming state and federal investments into a range of social services and public infrastructure projects. GNRC serves as administrator of the Nashville Area Metropolitan Planning Organization (MPO) and staff to its Transportation Policy Board which represents the seven county Nashville metropolitan planning area which includes the Tennessee counties of Davidson, Maury, Robertson, Rutherford, Sumner, Williamson, and Wilson.

Metro Parks is committed to building out its greenway network and has had prior success delivering projects to improve non-motorized safety. This grant will allow Metro to further build out its active transportation network in a critical area with high demand. The grant will also help leverage existing funds already awarded to the project through the MPO's Active Transportation Program.

The project will be designed to accommodate all users and is a top priority for Metro Nashville. It will also advance GNRC's regional planning efforts by improving safety for the traveling public. GNRC supports Metro's commitment to improving its active transportation network.

Thank you for your consideration of the grant application from Metro Nashville Parks.

Sincerely,

A handwritten signature in blue ink, appearing to read 'MS', is positioned above the printed name of Michael Skipper.

Michael Skipper  
Executive Director

**Connecting Communities. Empowering People.**

44 Vantage Way, Suite 450 • Nashville, TN 37228 • [GNRC.org](http://GNRC.org)

Transportation Alternatives Program Application 2025

Organization: Nashville

TAP-2025-Nashville-00006

Project Budget

Project Costs & Requested Funding

To determine if you are in a distressed or at-risk county, please refer to the following webpage: <https://tnecd.com/research-and-data/publications/>.

Is your local governmental agency located in a distressed or at-risk county based on the current TNECD determination?

( ) Yes (✓) No

Type of Funds Requested	Total Funds Requested	Federal % (Maximum 80%)	Local % (Minimum 20%)	Federal Funds	Local Funds
FEDERAL Construction Funds	\$18,316,311.11	70%	30%	\$12,821,417.78	\$5,494,893.33

Project Budget

The applicant must complete and upload the standardized budget templates located on the [TAP website](#). Line items can be added or deleted as needed for any stage. If the application request is for a Historic Preservation and Rehabilitation of Historic Transportation Facilities project, please contact the TAP Office at [TDOT.Enhancements@tn.gov](mailto:TDOT.Enhancements@tn.gov) for a budget template for this specific activity.

Attach completed budget templates here:

[https://tdot.intelligrants.com/\\_Upload/17960\\_1175698\\_2-8.29.24-FINAL-TAPBudgetTmplteRev.xlsx](https://tdot.intelligrants.com/_Upload/17960_1175698_2-8.29.24-FINAL-TAPBudgetTmplteRev.xlsx)



## BUDGET TEMPLATE #1 Estimated Project Costs

Line items can be added or deleted as needed for any stage; however this is the format in which the budget must be submitted. TDOT line item numbers can be found at <https://www.tdot.tn.gov/APPLICATIONS/Roadway/Items>  
 Note that no add alternates or deductions will be allowed in the final construction cost estimate or bid documents.

**INSTRUCTIONS:** List all items necessary to develop and construct the project. The applicant is responsible for verifying all costs for accuracy. All cost overruns will be solely the responsibility of the Local Government.

### Stage I – NEPA/Design (PE)

All costs associated with application preparation and application public involvement are not reimbursable.

ITEM DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	TOTAL	0% FED. FUNDS	100% LOCAL EXPENSE (NON-REIMBURSABLE)
NEPA	LS	1	\$ -	\$ -	\$0.00	\$ -
Preliminary Design Plans (enough to establish the footprint for NEPA)	LS	1	\$ -	\$ -	\$0.00	\$ -
Local, State and Federal Permits	LS	1	\$ -	\$ -	\$0.00	\$ -
Plans, Specs & Estimates (PS&E) Completion	LS	1	\$ -	\$ -	\$0.00	\$ -
<b>TOTAL</b>				\$ -	\$0.00	\$ -

NEPA completed and consultant has been paid.



### Stage II – Right-of-Way/Utilities

All costs associated with right-of-way are not eligible for reimbursement

ITEM DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	TOTAL	0% FED. FUNDS	100% LOCAL EXPENSE (NON-REIMBURSABLE)
ROW Design Plans	LS	1		\$ -		\$ -
ROW Acquisition	LS	1		\$ -		\$ -
License Agreements, Easements, Recording Fees	LS	1		\$ -		\$ -
Utility Relocation and Certifications*	LS	1		\$ -		\$ -
<b>SUBTOTAL of non-reimbursable ROW Expenses</b>				\$ -		\$ -

Right of Way expenses have been paid

All costs associated with application, application public involvement, NEPA, design and right-of-way are not eligible for reimbursement nor applicable toward the 20% local construction match.



**Stage III – Construction**  
(Add and remove lines as needed)

Applicant will pay 30% with local funds. Column % have been adjusted accordingly

All projects must be competitively bid and awarded to the lowest responsive bidder

TDOT LINE ITEM #	ITEM DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	TOTAL	70% FED. FUNDS	30% LOCAL MATCH
<b>Clearing and Grubbing:</b>							
201-01	Removal and Disposal of Brush and Trees	LS	1	\$ 182,000.00	\$ 182,000.00	\$ 127,400.00	\$ 54,600.00
202-01	Removal of Structures and Obstructions	LS	1	\$ 57,985.72	\$ 57,985.72	\$ 40,590.00	\$ 17,395.72
202-08.10	Removal of Extruded Curb	LF	850	\$ 25.00	\$ 21,250.00	\$ 14,875.00	\$ 6,375.00
202-08.15	Removal of Curb and Gutter(6"curb w/12"gutter)	LF	25	\$ 25.00	\$ 625.00	\$ 437.50	\$ 187.50
202-03.01	Removal of Asphaltic Concrete Pavement	SY	785	\$ 22.00	\$ 17,270.00	\$ 12,089.00	\$ 5,181.00
407-20.05	Saw Cutting Asphalt Pavement	LF	1600	\$ 14.00	\$ 22,400.00	\$ 15,680.00	\$ 6,720.00
502.04-01	Saw cut pavement to full depth	LF	75	\$ 25.00	\$ 1,875.00	\$ 1,312.50	\$ 562.50
<b>Earthwork and Mineral:</b>							
105-01	Construction Stakes, Lines & Grades	LS	1	\$ 130,000.00	\$ 130,000.00	\$ 91,000.00	\$ 39,000.00
203-01.06	Road & Drainage Excavation (Unclassified)	LS	1	\$ 900,000.00	\$ 900,000.00	\$ 630,000.00	\$ 270,000.00
203-01.11	Pre-splitting of Rock Excavation	SY	300	\$ 70.00	\$ 21,000.00	\$ 14,700.00	\$ 6,300.00
203-05	Undercutting	CY	1200	\$ 125.00	\$ 150,000.00	\$ 105,000.00	\$ 45,000.00
204-06	Flowable Fill	CY	117	\$ 500.00	\$ 58,500.00	\$ 40,950.00	\$ 17,550.00
204-10	Foundation Preparation (Abutments)	EA	6	\$ 15,000.00	\$ 90,000.00	\$ 63,000.00	\$ 27,000.00
204-11	Bridge Excavation (unclassified)	CY	215	\$ 125.00	\$ 26,875.00	\$ 18,812.50	\$ 8,062.50
209	Erosion and Sediment Control	LS	1	\$ 67,200.00	\$ 67,200.00	\$ 47,040.00	\$ 20,160.00
209-03.21	Filter Sock 12"	LF	9500	\$ 11.00	\$ 104,500.00	\$ 73,150.00	\$ 31,350.00
740	Geotextile/Geogrid Reinforcement/Stabilization	LS	1	\$ 100,655.00	\$ 100,655.00	\$ 70,458.50	\$ 30,196.50
920-11	Tree Protection	LS	1	\$ 31,000.00	\$ 31,000.00	\$ 21,700.00	\$ 9,300.00
303	Mineral Aggregate, (base/surge)	TON	21045	\$ 80.00	\$ 1,262,700.00	\$ 883,890.00	\$ 378,810.00
709-05	Machined Rip-Rap	TON	1580	\$ 65.00	\$ 102,700.00	\$ 71,890.00	\$ 30,810.00
709-05	Machined Rip-Rap	TON	325	\$ 85.00	\$ 27,625.00	\$ 19,337.50	\$ 8,287.50
203-07	Furnish and Spread Topsoil	CY	1625	\$ 125.00	\$ 203,125.00	\$ 142,187.50	\$ 60,937.50
<b>Structures:</b>							
602/604	2 Pedestrian Bridge(s)	LS	1	\$ 970,000.00	\$ 970,000.00	\$ 679,000.00	\$ 291,000.00
606-02	Pedestrian Bridge Supports - pipes and piles	LS	1	\$ 80,000.00	\$ 80,000.00	\$ 56,000.00	\$ 24,000.00
604-03.08	Concrete Retaining Wall Cap	CY	135	\$ 2,100.00	\$ 283,500.00	\$ 198,450.00	\$ 85,050.00
604-07	Retaining Wall	SF	14240	\$ 400.00	\$ 5,696,000.00	\$ 3,987,200.00	\$ 1,708,800.00
920-13	Pedestrian Concrete Boardwalk	LF	205	\$ 2,100.00	\$ 430,500.00	\$ 301,350.00	\$ 129,150.00
<b>Fencing:</b>							
604-20.11	Painted Metal Handrailing	LF	105	\$ 350.00	\$ 36,750.00	\$ 25,725.00	\$ 11,025.00
620-02.01	Pedestrian Steel Safety Railing - Heavy Duty	LF	350	\$ 450.00	\$ 157,500.00	\$ 110,250.00	\$ 47,250.00
620-02.02	Pedestrian Steel Safety Railing - Standard	LF	705	\$ 300.00	\$ 211,500.00	\$ 148,050.00	\$ 63,450.00
707-06.01	Removal of Fence	LF	700	\$ 10.00	\$ 7,000.00	\$ 4,900.00	\$ 2,100.00
706-02.01	Removal and Reset of Guardrail	LF	62.5	\$ 50.00	\$ 3,125.00	\$ 2,187.50	\$ 937.50
920-11	Wood Rail Fence - Embedded Posts	LF	725	\$ 175.00	\$ 126,875.00	\$ 88,812.50	\$ 38,062.50
<b>Appurtenances:</b>							
702-01.01	Extruded Sloping Concrete Curb	LF	50	\$ 50.00	\$ 2,500.00	\$ 1,750.00	\$ 750.00
702-01.02	Concrete Curb	LF	1400	\$ 50.00	\$ 70,000.00	\$ 49,000.00	\$ 21,000.00
701-01.01	Concrete Sidewalk (4")	SF	15975	\$ 15.00	\$ 239,625.00	\$ 167,737.50	\$ 71,887.50
701-01.09	Concrete Sidewalk with Integral Color	SF	275	\$ 20.00	\$ 5,500.00	\$ 3,850.00	\$ 1,650.00
701-02.03	Concrete Pedestrian ADA Ramp	SF	540	\$ 25.00	\$ 13,500.00	\$ 9,450.00	\$ 4,050.00
701-02.06	Truncated Domes/Detectable Warning Mat	SF	175	\$ 75.00	\$ 13,125.00	\$ 9,187.50	\$ 3,937.50
702-10.02	Wheel Stops	EA	51	\$ 75.00	\$ 3,825.00	\$ 2,677.50	\$ 1,147.50
307-01.08	ACS mix (PG64-22) (BPMB-HM)	TON	100	\$ 185.00	\$ 18,500.00	\$ 12,950.00	\$ 5,550.00
402-01	Bituminous material for prime coat	TON	11	\$ 2,250.00	\$ 24,750.00	\$ 17,325.00	\$ 7,425.00
402-02	Cover Material Aggregate	TON	52	\$ 75.00	\$ 3,900.00	\$ 2,730.00	\$ 1,170.00
403-01	Bituminous material for Tack Coat	TON	1.5	\$ 1,600.00	\$ 2,400.00	\$ 1,680.00	\$ 720.00
411-01.11	ACS mix (PG64-22) (BPMB-HM)	TON	1050	\$ 200.00	\$ 210,000.00	\$ 147,000.00	\$ 63,000.00
415-01.02	Cold planing Bit'm	SY	2600	\$ 7.00	\$ 18,200.00	\$ 12,740.00	\$ 5,460.00
<b>Drainage:</b>							
607-01.02	12" Concrete Pipe Culvert (Class III)	LF	48	\$ 150.00	\$ 7,200.00	\$ 5,040.00	\$ 2,160.00
607-03.30	18" Concrete Pipe Culvert	LF	24	\$ 225.00	\$ 5,400.00	\$ 3,780.00	\$ 1,620.00
607-16.13	91" x 58" Horizontal Oval Concrete Pipe Culvert	LF	22	\$ 1,500.00	\$ 33,000.00	\$ 23,100.00	\$ 9,900.00
607-40.70	Corrugated Aluminum Box Culvert	LF	18	\$ 3,000.00	\$ 54,000.00	\$ 37,800.00	\$ 16,200.00
611-01	Manholes	EA	2	\$ 8,000.00	\$ 16,000.00	\$ 11,200.00	\$ 4,800.00
611-05.01	Trench Drains	LF	22	\$ 1,000.00	\$ 22,000.00	\$ 15,400.00	\$ 6,600.00

TDOT LINE ITEM #	ITEM DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	TOTAL	70% FED. FUNDS	30% LOCAL MATCH	
611-07	Class A Concrete Pipe Endwalls	CY	20	\$ 2,200.00	\$ 44,000.00	\$ 30,800.00	\$ 13,200.00	
611-07	Steel Bar Reinforcement Pipe Endwalls	LB	9010	\$ 5.00	\$ 45,050.00	\$ 31,535.00	\$ 13,515.00	
611-12	Catch Basins	EA	3	\$ 8,500.00	\$ 25,500.00	\$ 17,850.00	\$ 7,650.00	
621-03.07	48" Temp Drainage Pipe	LF	256	\$ 250.00	\$ 64,000.00	\$ 44,800.00	\$ 19,200.00	
710-09	8" Perforated vertical drain pipe and underdrain	LS	1	\$ 38,770.00	\$ 38,770.00	\$ 27,139.00	\$ 11,631.00	
797-10.03	Drainage Connections and Adjustments	LS	1	\$ 35,500.00	\$ 35,500.00	\$ 24,850.00	\$ 10,650.00	
<b>Pavement Markings:</b>								
716-05.03	Painted Pavement Marking (Cross-Walk)	LF	120	\$ 40.00	\$ 4,800.00	\$ 3,360.00	\$ 1,440.00	
716-05.04	Painted Pavement Marking (Channelization)	SY	1030	\$ 35.00	\$ 36,050.00	\$ 25,235.00	\$ 10,815.00	
920-14	Rumble Strips (thermoplastic lines)	LF	240	\$ 10.00	\$ 2,400.00	\$ 1,680.00	\$ 720.00	
716-05.08	Painted Pavement Parking Lines	LF	900	\$ 6.00	\$ 5,400.00	\$ 3,780.00	\$ 1,620.00	
<b>Lighting &amp; Signalization</b>								
920-12.04	Pedestrian Warning Sign - Solar Powered	EA	1	\$ 18,000.00	\$ 18,000.00	\$ 12,600.00	\$ 5,400.00	
714-03.01	Direct Burial Conduit (2" pvc schedule 40)	EA	45	\$ 25.00	\$ 1,125.00	\$ 787.50	\$ 337.50	
<b>Landscaping:</b>								
802-01	Trees (Footnote specific types)	EA	51	\$ 500.00	\$ 25,500.00	\$ 17,850.00	\$ 7,650.00	
802-03	Shrubs (Footnote specific types)	EA	19	\$ 75.00	\$ 1,425.00	\$ 997.50	\$ 427.50	
802-04	Groundcovers (Footnote specific types)	EA	278	\$ 65.00	\$ 18,070.00	\$ 12,649.00	\$ 5,421.00	
802-05	Watering	MG	10	\$ 200.00	\$ 2,000.00	\$ 1,400.00	\$ 600.00	
802-05.03	Irrigation Bag - Flexible	EA	32	\$ 100.00	\$ 3,200.00	\$ 2,240.00	\$ 960.00	
<b>Seeding and Sodding:</b>								
801-01	Seeding (with Mulch)	UNIT	25.5	\$ 500.00	\$ 12,750.00	\$ 8,925.00	\$ 3,825.00	
801-01.16	Bonded Fiber Matrix Hydromulch(permanent seed)	UNIT	110	\$ 500.00	\$ 55,000.00	\$ 38,500.00	\$ 16,500.00	
805-01.01	Turf Reinforcement Mat	SY	1800	\$ 10.00	\$ 18,000.00	\$ 12,600.00	\$ 5,400.00	
805-12.02	Erosion Control Blanket (type III)	SY	1825	\$ 5.50	\$ 10,037.50	\$ 7,026.25	\$ 3,011.25	
<b>Pedestrian Amenities:</b>								
920-10.04	Bike Racks	EA	6	\$ 2,500.00	\$ 15,000.00	\$ 10,500.00	\$ 4,500.00	
721-01.01	Benches	EA	2	\$ 3,000.00	\$ 6,000.00	\$ 4,200.00	\$ 1,800.00	
721-01.02	Trash Receptacles	EA	1	\$ 2,000.00	\$ 2,000.00	\$ 1,400.00	\$ 600.00	
<b>Maintenance of Traffic:</b>								
712-01	Traffic Control	LS	1	\$ 40,000.00	\$ 40,000.00	\$ 28,000.00	\$ 12,000.00	
920-13.04	Removable Bollard	EA	1	\$ 1,000.00	\$ 1,000.00	\$ 700.00	\$ 300.00	
712-07.03	Temporary Barricades (Type III)	LF	225	\$ 30.00	\$ 6,750.00	\$ 4,725.00	\$ 2,025.00	
712-05.01	Warning Lights (Type A)	EA	24	\$ 35.00	\$ 840.00	\$ 588.00	\$ 252.00	
707-08	Construction Fencing	LS	1	\$ 51,225.00	\$ 51,225.00	\$ 35,857.50	\$ 15,367.50	
712-04.01	Flexible Drums (Channelizing)	EA	55	\$ 35.00	\$ 1,925.00	\$ 1,347.50	\$ 577.50	
<b>Signs:</b>								
712-06	Signs (construction)	SF	533	\$ 15.00	\$ 7,995.00	\$ 5,596.50	\$ 2,398.50	
713-11.01	U-Channel Steel Posts	LB	1179	\$ 6.00	\$ 7,074.00	\$ 4,951.80	\$ 2,122.20	
713-13.02	Flat Sheet Aluminum Signs (0.080" thick)	SF	95	\$ 25.00	\$ 2,375.00	\$ 1,662.50	\$ 712.50	
713-15.02	Removal and Relocation of Sign and Support	EA	2	\$ 1,000.00	\$ 2,000.00	\$ 1,400.00	\$ 600.00	
713-16	Trailhead Sign and Kiosk	EA	2	\$ 50,000.00	\$ 100,000.00	\$ 70,000.00	\$ 30,000.00	
713-16.22	Mile Marker Signs	EA	10	\$ 1,400.00	\$ 14,000.00	\$ 9,800.00	\$ 4,200.00	
<b>SUBTOTAL of Itemized Quantities above</b>					\$ 1,628,492.22	\$ 13,070,222.22	\$ 9,149,155.55	\$ 3,921,066.67
<b>Mobilization and Engineering Services:</b>								
Contractor Mobilization		5%	of construction	\$ -	\$ 653,511.11	\$ 457,457.78	\$ 196,053.33	
TDOT Materials & Test (5%)		LS	1	\$ 653,511.11	\$ 653,511.11	\$ 457,457.78	\$ 196,053.33	
Construction Contingency		10%	of construction	\$ -	\$ 1,307,022.22	\$ 914,915.56	\$ 392,106.67	
TDOT Engineering Services		\$1,000 for every 30 days of construction		\$ -	\$ 18,000.00	\$ 12,600.00	\$ 5,400.00	
Construction Engineering Inspection (CEI) and Project Administration		20%	of construction	\$ -	\$ 2,614,044.44	\$ 1,829,831.11	\$ 784,213.33	
<b>TOTAL of reimbursable construction expenses</b>					\$ 2,280,003.33	\$ 18,316,311.11	\$ 12,821,417.78	\$ 5,494,893.33

\* If eligible in accordance with T.C.A. § 13-20-303 and 23 CFR 635.107

FREDDIE O'CONNELL, MAYOR



## METROPOLITAN BOARD OF PARKS AND RECREATION

Centennial Park Office  
Park Plaza at Oman Street  
Nashville, TN 37201

(615) 862-8400  
Fax (615) 862-8414  
[www.nashville.gov/parks](http://www.nashville.gov/parks)

Monique Horton Odom, Director

September 3, 2024

Ms. Cindy Harrison  
Assistant Director Greenways & Open Space  
Metro Board of Parks and Recreation  
P.O. Box 196340  
Nashville, Tennessee 37219-6340

Dear Ms. Harrison:

The Parks Board, at its meeting held Tuesday, September 3, 2024, granted staff approval to apply for and ultimately accept a Transportation Alternatives Program Grant for construction of the Opry Mills Greenway Connector, a vital link between the Stones River Greenway and the Cumberland River Greenway to the Opry Mills entertainment, retail, and hospitality campus.

The grant will fund up to 80% of the project cost which is estimated to be \$18,316,311.11. Greenways intends to apply for 70% of the estimated project cost in the amount of \$12,821,417.78 with Metro providing a 30% match of \$5,494,893.33.

Sincerely,

Monique Horton Odom, Director  
and Secretary to the Board

:lt

*"It is the mission of Metro Parks and Recreation to sustainably and equitably provide everyone in Nashville with an inviting network of parks and greenways that offer health, wellness and quality of life through recreation, conservation and community"*



FOR ADA ACCOMMODATIONS, PLEASE CONTACT 615-862-8400

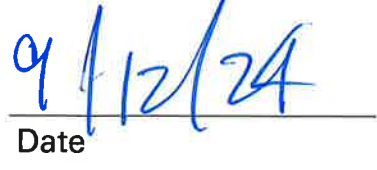
WE ARE AN EQUAL OPPORTUNITY EMPLOYER

**APPLICATION FOR**

**(Write name of grant here) Greenways TAP Opry Mills 2024**

**METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY**

  
Department

  
Date

### Certificate Of Completion

Envelope Id: F27A05BD36FE4690A12017E7BAFF5D55	Status: Completed
Subject: Complete with Docusign: Parks-Greenways TAP Opry Mills 24 App Ready_revised.pdf	
Source Envelope:	
Document Pages: 71	Signatures: 4
Certificate Pages: 16	Initials: 3
AutoNav: Enabled	Envelope Originator:
Envelope Stamping: Enabled	Juanita Paulson
Time Zone: (UTC-06:00) Central Time (US & Canada)	730 2nd Ave. South 1st Floor
	Nashville, TN 37219
	Juanita.Paulsen@nashville.gov
	IP Address: 170.190.198.190

### Record Tracking

Status: Original	Holder: Juanita Paulson	Location: DocuSign
9/19/2024 8:32:34 AM	Juanita.Paulsen@nashville.gov	
Security Appliance Status: Connected	Pool: StateLocal	
Storage Appliance Status: Connected	Pool: Metropolitan Government of Nashville and Davidson County	Location: DocuSign

### Signer Events

Signer Events	Signature	Timestamp
Rose Wood		Sent: 9/19/2024 8:48:59 AM
rose.wood@nashville.gov		Viewed: 9/19/2024 10:20:55 AM
Finance Admin		Signed: 9/19/2024 10:23:21 AM
Metro Finance Dept. OMB		
Security Level: Email, Account Authentication (None)	Signature Adoption: Pre-selected Style	
	Using IP Address: 170.190.198.190	

**Electronic Record and Signature Disclosure:**  
Not Offered via DocuSign

Aaron Pratt		Sent: 9/19/2024 10:23:23 AM
Aaron.Pratt@nashville.gov		Viewed: 9/20/2024 7:31:31 AM
Security Level: Email, Account Authentication (None)		Signed: 9/20/2024 7:31:36 AM
	Signature Adoption: Pre-selected Style	
	Using IP Address: 170.190.198.185	

**Electronic Record and Signature Disclosure:**  
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ID: fb45e391-c529-45a9-b3b6-acba8b6fd462

Kevin Crumbo/KB		Sent: 9/20/2024 7:31:38 AM
Kevin.Brown@nashville.gov		Resent: 9/20/2024 7:43:01 AM
Fin. Assistant Director		Resent: 9/20/2024 10:10:40 AM
Security Level: Email, Account Authentication (None)		Viewed: 9/20/2024 10:11:21 AM
	Signed: 9/20/2024 10:12:13 AM	
	Signature Adoption: Pre-selected Style	
	Using IP Address: 170.190.198.191	

**Electronic Record and Signature Disclosure:**  
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ID: 1d87d05a-3924-479a-80e7-d638d5dc6b80

Balogun Cobb		Sent: 9/20/2024 10:12:16 AM
balogun.cobb@nashville.gov		Viewed: 9/20/2024 10:18:26 AM
Insurance Division Manager		Signed: 9/20/2024 10:18:37 AM
Security Level: Email, Account Authentication (None)		
	Signature Adoption: Pre-selected Style	
	Using IP Address: 170.190.198.185	

**Electronic Record and Signature Disclosure:**



Signer Events	Signature	Timestamp
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Accepted: 9/20/2024 10:18:26 AM  
ID: a1301725-6a65-4ecd-946b-42825bfdccbf

Courtney Mohan  
Courtney.Mohan@nashville.gov  
Security Level: Email, Account Authentication (None)



Sent: 9/20/2024 10:18:39 AM  
Viewed: 9/20/2024 10:39:43 AM  
Signed: 9/20/2024 10:41:35 AM

Signature Adoption: Pre-selected Style  
Using IP Address: 174.219.130.100  
Signed using mobile

**Electronic Record and Signature Disclosure:**  
Accepted: 9/20/2024 10:39:43 AM  
ID: ec5b68d2-02b8-4a79-901c-ad3548cfa344

Kristin Wilson  
Kristin.Wilson@nashville.gov  
Security Level: Email, Account Authentication (None)



Sent: 9/20/2024 10:41:38 AM  
Viewed: 9/20/2024 10:51:00 AM  
Signed: 9/20/2024 10:51:18 AM

Signature Adoption: Pre-selected Style  
Using IP Address: 170.190.198.100

**Electronic Record and Signature Disclosure:**  
Accepted: 9/20/2024 10:51:00 AM  
ID: 0210b149-58da-4724-b8a9-e6a9d60b4c2c

Freddie O'Connell:mpw@nashville.gov  
mpw@nashville.gov  
Security Level: Email, Account Authentication (None)



Sent: 9/20/2024 10:51:20 AM  
Viewed: 9/20/2024 10:53:18 AM  
Signed: 9/20/2024 10:53:59 AM

Signature Adoption: Pre-selected Style  
Using IP Address: 12.208.109.5  
Signed using mobile

**Electronic Record and Signature Disclosure:**  
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ID: 4c53b757-1d7b-4530-bd14-6982f215b441

In Person Signer Events	Signature	Timestamp
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Editor Delivery Events	Status	Timestamp
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Agent Delivery Events	Status	Timestamp
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Intermediary Delivery Events	Status	Timestamp
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Certified Delivery Events	Status	Timestamp
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Carbon Copy Events	Status	Timestamp
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Alan Enzo  
Alan.Enzo@nashville.gov  
Security Level: Email, Account Authentication (None)

**COPIED**

Sent: 9/20/2024 10:54:02 AM

**Electronic Record and Signature Disclosure:**  
Not Offered via DocuSign

Chinita White  
Chinita.White@nashville.gov  
Security Level: Email, Account Authentication (None)

**COPIED**

Sent: 9/20/2024 10:54:03 AM

**Electronic Record and Signature Disclosure:**  
Accepted: 8/15/2024 9:35:12 AM  
ID: da7b3c31-6c72-42d1-9c24-8e73c7227898

Witness Events	Signature	Timestamp
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Notary Events	Signature	Timestamp
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Envelope Summary Events	Status	Timestamps
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Envelope Sent	Hashed/Encrypted	9/19/2024 8:48:59 AM
Envelope Updated	Security Checked	9/19/2024 9:14:25 AM
Envelope Updated	Security Checked	9/19/2024 9:14:25 AM
Envelope Updated	Security Checked	9/19/2024 9:22:25 AM
Envelope Updated	Security Checked	9/19/2024 9:22:25 AM
Envelope Updated	Security Checked	9/20/2024 7:43:00 AM
Envelope Updated	Security Checked	9/20/2024 7:43:00 AM
Envelope Updated	Security Checked	9/20/2024 10:10:37 AM
Envelope Updated	Security Checked	9/20/2024 10:10:37 AM
Certified Delivered	Security Checked	9/20/2024 10:53:18 AM
Signing Complete	Security Checked	9/20/2024 10:53:59 AM
Completed	Security Checked	9/20/2024 10:54:03 AM

Payment Events	Status	Timestamps
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Electronic Record and Signature Disclosure
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**Certificate Of Completion**

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Subject: Complete with DocuSign: Parks-Greenways TAP Opry Mills 24 App Ready been thru cma.pdf	
Source Envelope:	
Document Pages: 89	Signatures: 3
Certificate Pages: 15	Initials: 1
AutoNav: Enabled	Envelope Originator:
Envelope Stamping: Enabled	Juanita Paulson
Time Zone: (UTC-06:00) Central Time (US & Canada)	730 2nd Ave. South 1st Floor
	Nashville, TN 37219
	Juanita.Paulsen@nashville.gov
	IP Address: 170.190.198.185

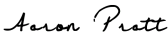
**Record Tracking**

Status: Original	Holder: Juanita Paulson	Location: DocuSign
9/25/2024 8:11:58 AM	Juanita.Paulsen@nashville.gov	
Security Appliance Status: Connected	Pool: StateLocal	
Storage Appliance Status: Connected	Pool: Metropolitan Government of Nashville and Davidson County	Location: DocuSign


**Signer Events**

Signer Events	Signature	Timestamp
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rose.wood@nashville.gov		Viewed: 9/25/2024 12:56:55 PM
Finance Admin		Signed: 9/25/2024 12:57:14 PM
Metro Finance Dept. OMB	Signature Adoption: Pre-selected Style	
Security Level: Email, Account Authentication (None)	Using IP Address: 170.190.198.185	

**Electronic Record and Signature Disclosure:**  
Not Offered via DocuSign

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Aaron.Pratt@nashville.gov		Viewed: 9/25/2024 1:53:16 PM
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ID: 4497e7f5-b618-47b5-a4b9-6aa4e7c4096f

Kevin Crumbo/mjw		Sent: 9/25/2024 1:53:23 PM
MaryJo.Wiggins@nashville.gov		Viewed: 9/26/2024 7:56:45 AM
Security Level: Email, Account Authentication (None)		Signed: 9/26/2024 7:57:48 AM
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	Using IP Address: 170.190.198.185	

**Electronic Record and Signature Disclosure:**  
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ID: 48376b87-2de7-4b6d-8a8a-56697294c9be

Courtney Mohan		Sent: 9/26/2024 7:57:51 AM
Courtney.Mohan@nashville.gov		Viewed: 9/27/2024 2:58:27 PM
Security Level: Email, Account Authentication (None)		Signed: 9/27/2024 3:08:55 PM
	Signature Adoption: Pre-selected Style	
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**Electronic Record and Signature Disclosure:**

Signer Events	Signature	Timestamp
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Editor Delivery Events	Status	Timestamp
Agent Delivery Events	Status	Timestamp
Intermediary Delivery Events	Status	Timestamp
Certified Delivery Events	Status	Timestamp
Carbon Copy Events	Status	Timestamp
Danielle Godin Danielle.Godin@nashville.gov Security Level: Email, Account Authentication (None) <b>Electronic Record and Signature Disclosure:</b> Not Offered via DocuSign	<b>COPIED</b>	Sent: 9/27/2024 3:08:57 PM Viewed: 9/27/2024 3:47:45 PM
Sally Palmer sally.palmer@nashville.gov Security Level: Email, Account Authentication (None) <b>Electronic Record and Signature Disclosure:</b> Accepted: 9/27/2024 3:35:05 PM ID: af34d824-34c9-4966-8582-ab0f6db6b6e0	<b>COPIED</b>	Sent: 9/27/2024 3:08:58 PM
Witness Events	Signature	Timestamp
Notary Events	Signature	Timestamp
Envelope Summary Events	Status	Timestamps
Envelope Sent	Hashed/Encrypted	9/25/2024 8:16:50 AM
Certified Delivered	Security Checked	9/27/2024 2:58:27 PM
Signing Complete	Security Checked	9/27/2024 3:08:55 PM
Completed	Security Checked	9/27/2024 3:08:58 PM
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Electronic Record and Signature Disclosure		