

**SUBSTITUTE ORDINANCE NO. BL2020-440**

**An ordinance to amend Title 17 of the Metropolitan Code of Laws, the Zoning Ordinance of The Metropolitan Government of Nashville and Davidson County, by amending a Specific Plan located at 1324 2nd Avenue North, at the southeast corner of 2nd Avenue North and Taylor Street, zoned SP (4.82 acres), to permit additional uses and update site plan, all of which is described herein (Proposal No. 2016SP-055-002).**

NOW, THEREFORE, BE IT ENACTED BY THE COUNCIL OF THE METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY:

Section 1. That Title 17 of the Code of Laws of The Metropolitan Government of Nashville and Davidson County, is hereby amended by changing the Official Zoning Map for Metropolitan Nashville and Davidson County, which is made a part of Title 17 by reference, as follows:

By amending a Specific Plan located at 1324 2nd Avenue North, at the southeast corner of 2nd Avenue North and Taylor Street, zoned SP (4.82 acres), to permit additional uses and update site plan, being Property Parcel No. 234 as designated on Map 082-09 of the Official Property Identification Maps of The Metropolitan Government of Nashville and Davidson County, all of which is described by lines, words and figures on the plan that was duly considered by the Metropolitan Planning Commission, and which is on file with the Metropolitan Planning Department and Metropolitan Clerk's Department and made a part of this ordinance as though copied herein.

Section 2. Be it further enacted, that the Metropolitan Clerk is hereby authorized and directed, upon the enactment and approval of this ordinance, to cause the change to be made on Map 082 of said Official Zoning Map for Metropolitan Nashville and Davidson County, as set out in Section 1 of this ordinance, and to make notation thereon of reference to the date of passage and approval of this amendatory ordinance.

Section 3. Be it further enacted, that the uses of this SP shall be limited to 232,500 square feet of non-residential uses and 280 multi-family units. The permitted uses for the non-residential uses shall be as specified on the plan. Short term rental property – owner occupied and short term rental property – not owner occupied shall be prohibited.

Section 4. Be it further enacted, that the following conditions shall be completed, bonded or satisfied as specifically required:

1. The applicant will continue to engage the Metro Greenway division of the Parks and Recreation Department to finalize the design of the pedestrian bridge as well as the ground furnishings and signage near the Metro Greenway adjacent to the site.
2. A mandatory referral for the aerial encroachment (pedestrian bridge) shall be approved prior to final site plan approval.
3. The adjacent Greenway shall remain free and clear for use of the public and shall not be encumbered by tables, chairs, or for events.
4. The proposed pedestrian bridge shall be privately owned and maintained for public access.

Section 4 5. Be it further enacted, a corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.

Section 5 6. Be it further enacted, minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual

site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.

Section 6 7. Be it further enacted, if a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the MUL-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.

Section 7 8. Be it further enacted, that this ordinance take effect immediately after its passage and such change be published in a newspaper of general circulation, the welfare of The Metropolitan Government of Nashville and Davidson County requiring it.

INTRODUCED BY:



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Councilmember Freddie O'Connell



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Kathleen Murphy  
Member of Council

2016SP-055-002  
1324 2ND AVENUE NORTH  
Map 082-09, Parcel(s) 234  
Subarea 08, North Nashville  
District 19 (O'Connell)  
Application fee paid by: Smith Gee Studio LLC

A request to amend a Specific Plan located at 1324 2nd Avenue North, at the southeast corner of 2nd Avenue North and Taylor Street, zoned SP (4.82 acres), to permit additional uses and update site plan, requested by Smith Gee Studio, applicant; Neuhoff Acquisition LLC, owner.





AMENDMENT TO SPECIFIC PLAN

**1324 2ND AVENUE NORTH  
NASHVILLE**

MPC# 2016SP-055-002 | JULY 1, 2020 3:34 PM

# 1324 2ND AVENUE NORTH



**New City Properties, LLC**  
699 Ponce de Leon Ave NE // Suite 403  
Atlanta, GA 30308  
[www.newcity-properties.com](http://www.newcity-properties.com)

Contact: Maitland Thompson  
[Maitland@newcity-properties.com](mailto:Maitland@newcity-properties.com)  
404.662.7378



**Morris Adjmi Architects**  
Master Planner and Designer  
1033 Jackson Avenue, Suite 201  
New Orleans, LA 70130  
[www.ma.com](http://www.ma.com)

Contact: Sarah Cancienne  
[sc@ma.com](mailto:sc@ma.com)  
504.708.1652



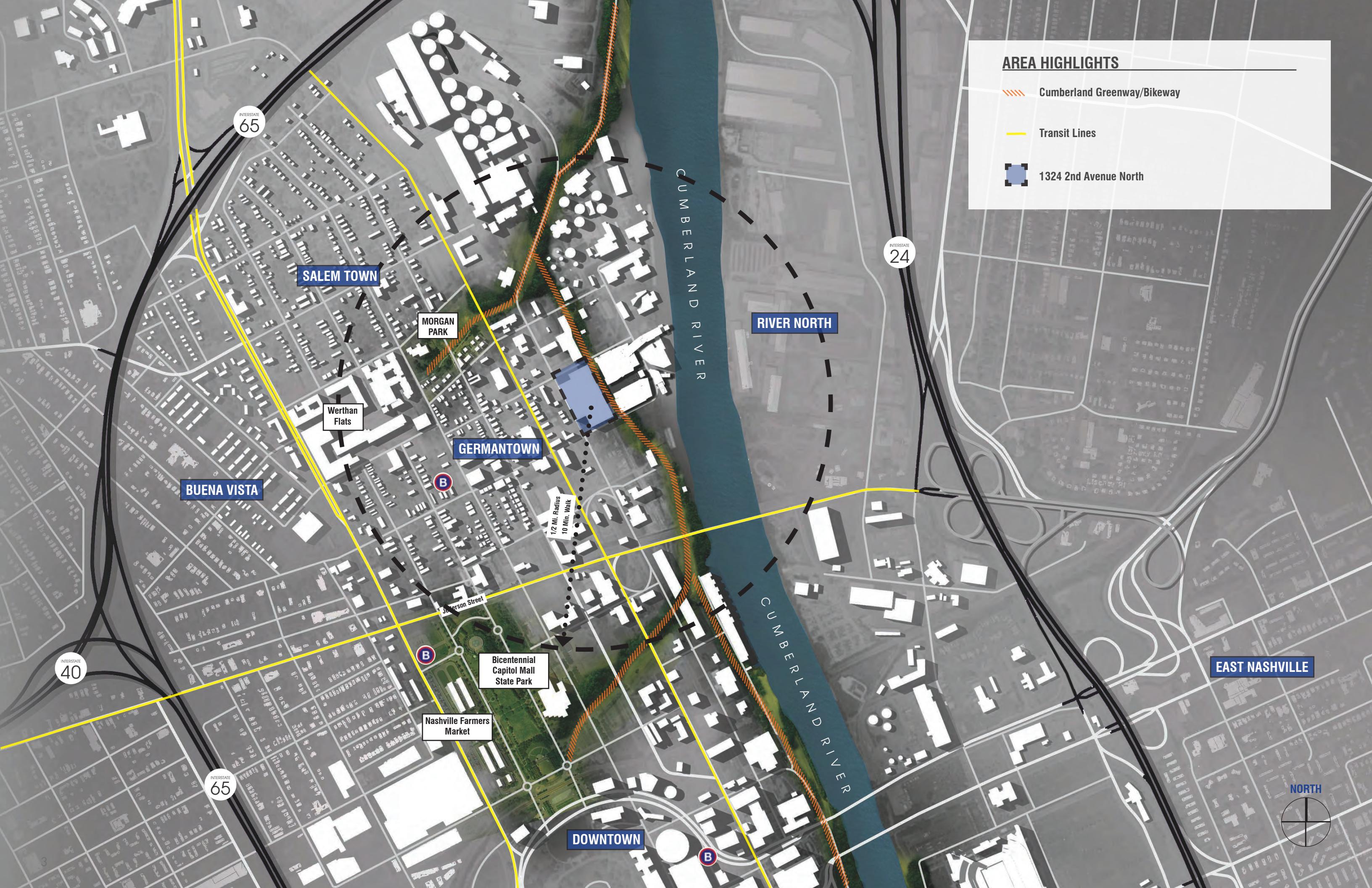
**SMITH GEE STUDIO, LLC**  
209 10th Ave. South // Suite 425  
Nashville, TN 37203  
[www.smithgeestudio.com](http://www.smithgeestudio.com)

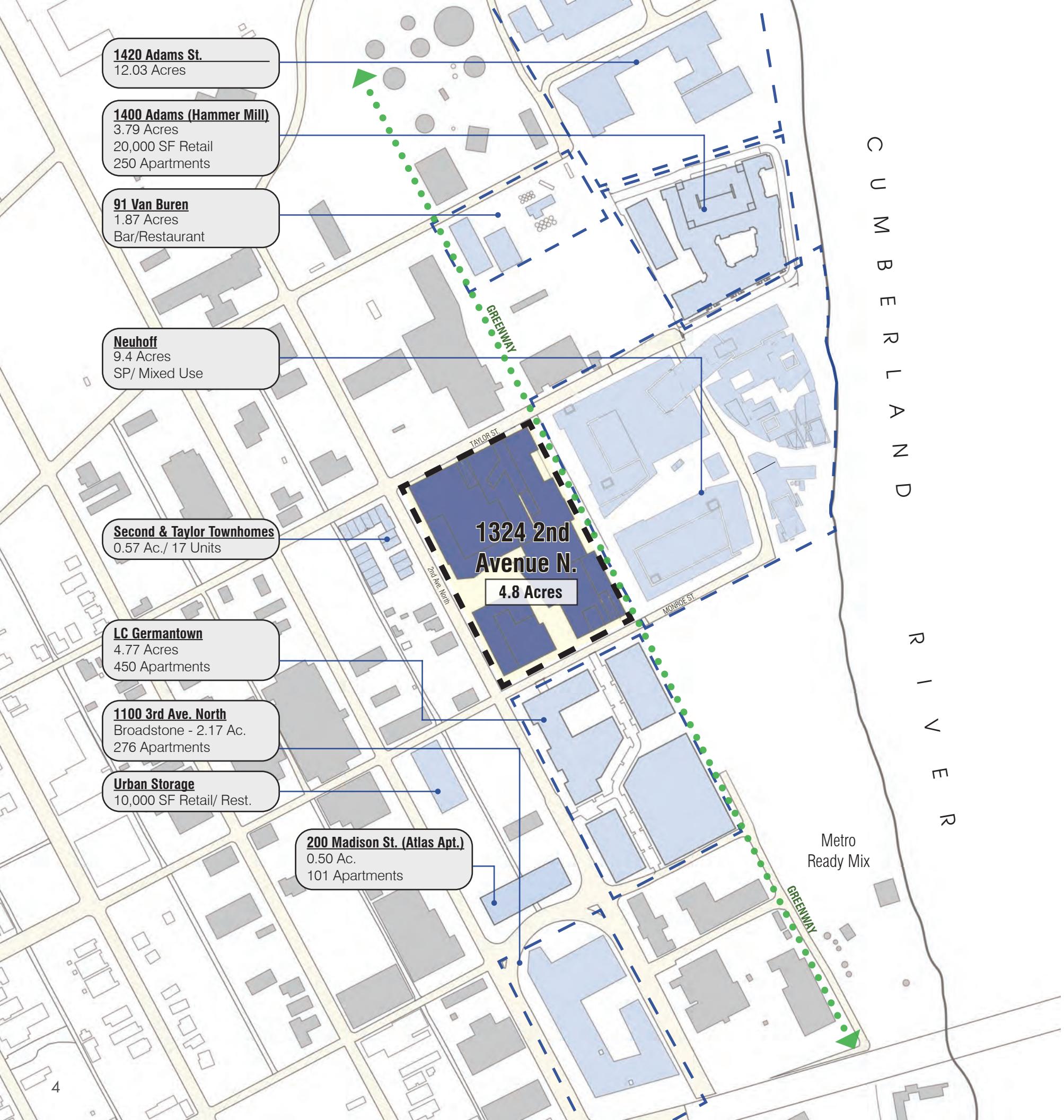
Contact: Scott Morton  
[smorton@smithgeestudio.com](mailto:smorton@smithgeestudio.com)  
615.645.5520

**Kimley-Horn**

**Kimley-Horn and Associates, Inc.**  
214 Oceanside Dr.  
Nashville, TN 37204  
[www.kimley-horn.com](http://www.kimley-horn.com)

Contact: Brendan Boles  
[brendan.boles@kimley-horn.com](mailto:brendan.boles@kimley-horn.com)  
615.564.2720





# SITE

C U M B E R L A N D

R I V E R

1324 2nd Avenue north is part of the emerging eastern area of the Germantown / Salemtown neighborhoods. Recent new construction has been single-family and two-family homes within the historic neighborhoods, and multi-family housing on former commercial and industrial properties. The site is located between the historic districts, existing industrial uses, and the neuhoff complex.

Existing Land Use Policy  
T4 Mixed Use Neighborhood

Existing Zoning  
Specific Plan (SP), UZ0

\*The site is located outside of the nearby Germantown Historic Preservation District, the Phillips Jackson Redevelopment District and the Germantown National Register District.

# 1324 2ND AVENUE NORTH

## CONCEPT PLAN





Floor Plan - Floor 01



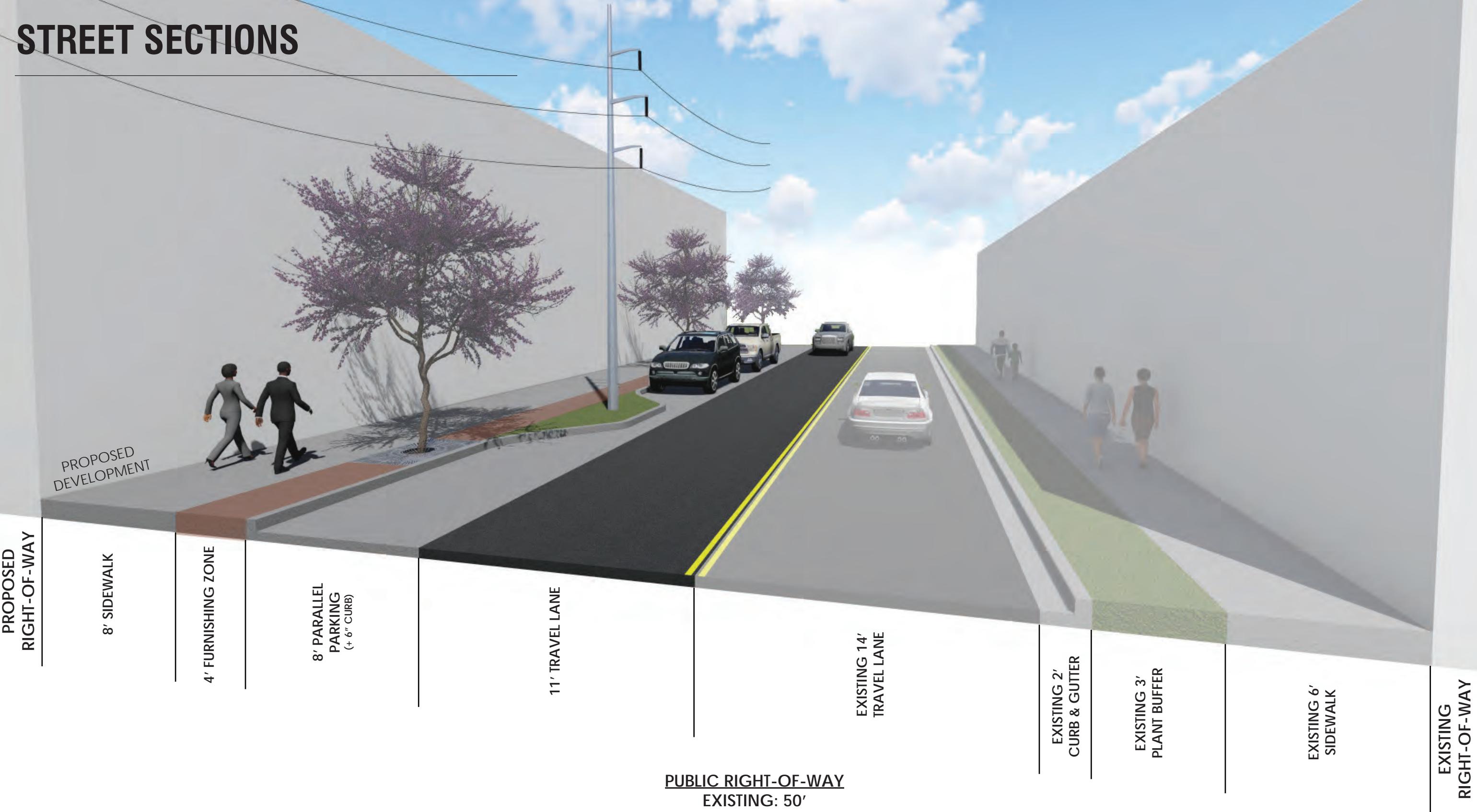
# REGULATIONS

Base Zoning		The standards of this SP shall follow MUL-A zoning district in the UZO except where standards in this document are more specific or change the standards of MUL-A.
FAR		2.5
ISR		1.0
Build-to-Zone		60% of the building facade shall be 0-15 feet from the back of the sidewalk on public streets.
Greenway Setback		5 feet, minimum average as measured over the entire length of the property line facing the greenway.
Minimum First Floor Height	A	As measured along public streets, finished floor to finished floor.
	<i>Grocery</i>	20 feet
	<i>All other uses</i>	16 feet
Max. Height	B	85 feet and 5 stories
Max. Height for Hotel	B	85 feet and 6 stories
Bicycle Parking Requirements		According to 17.20.135 of the zoning code.
Parking		According to MUL-A, UZO allowances, and all allowances in the zoning code.  Parking may be shared across the site and any future property lines, regardless of ownership.  Shared parking may be allowed according to the provisions of 17.20.100 of the zoning code.
Urban Design	C	Building facades fronting a public street shall provide a minimum of one principal pedestrian entrance.
	C	Building facades fronting a public street shall have a minimum of 40% glazing on the ground floor. Upper level facades shall have a minimum of 25% glazing.
	D	Upper level facades of parking structures facing public streets shall step back from the ground level facade by a minimum of 18 inches, and shall include material articulation.
	E	Upper level facades of parking structures facing public streets shall have openings for a minimum of 40% and a maximum of 60% of the total facade.
Prohibited Materials		EIFS, vinyl siding, fiber cement siding, and untreated wood.



Building Diagram

# STREET SECTIONS



## 2ND AVENUE

LOOKING SOUTH BETWEEN TAYLOR STREET AND MONROE STREET

Individual property owners are responsible for required street improvements along their frontage from the centerline of the right-of-way or easement.

Monroe Street and Taylor Street section designs are referenced in the appendix and were previously approved under 2019SP-029-001.

Kimley»Horn

# LAND USES

# CIVIL

**BASE FLOODPLAIN NOTE**

1. THE 100- YEAR BASE FLOODPLAIN FOR THE CUMBERLAND RIVER ELEVATION IS 415'
2. MINIMUM FFE OF ALL RESIDENTIAL BUILDINGS SHALL BE NO LOWER THAN 419'
3. MINIMUM FFE OF ALL OTHER BUILDINGS SHALL BE NO LOWER THAN 416'

**U.S. FEMA FIRM PANEL**

MAP NO. 4703037C0241H DATED APRIL 5, 2017

**SOIL SURVEY NOTE**

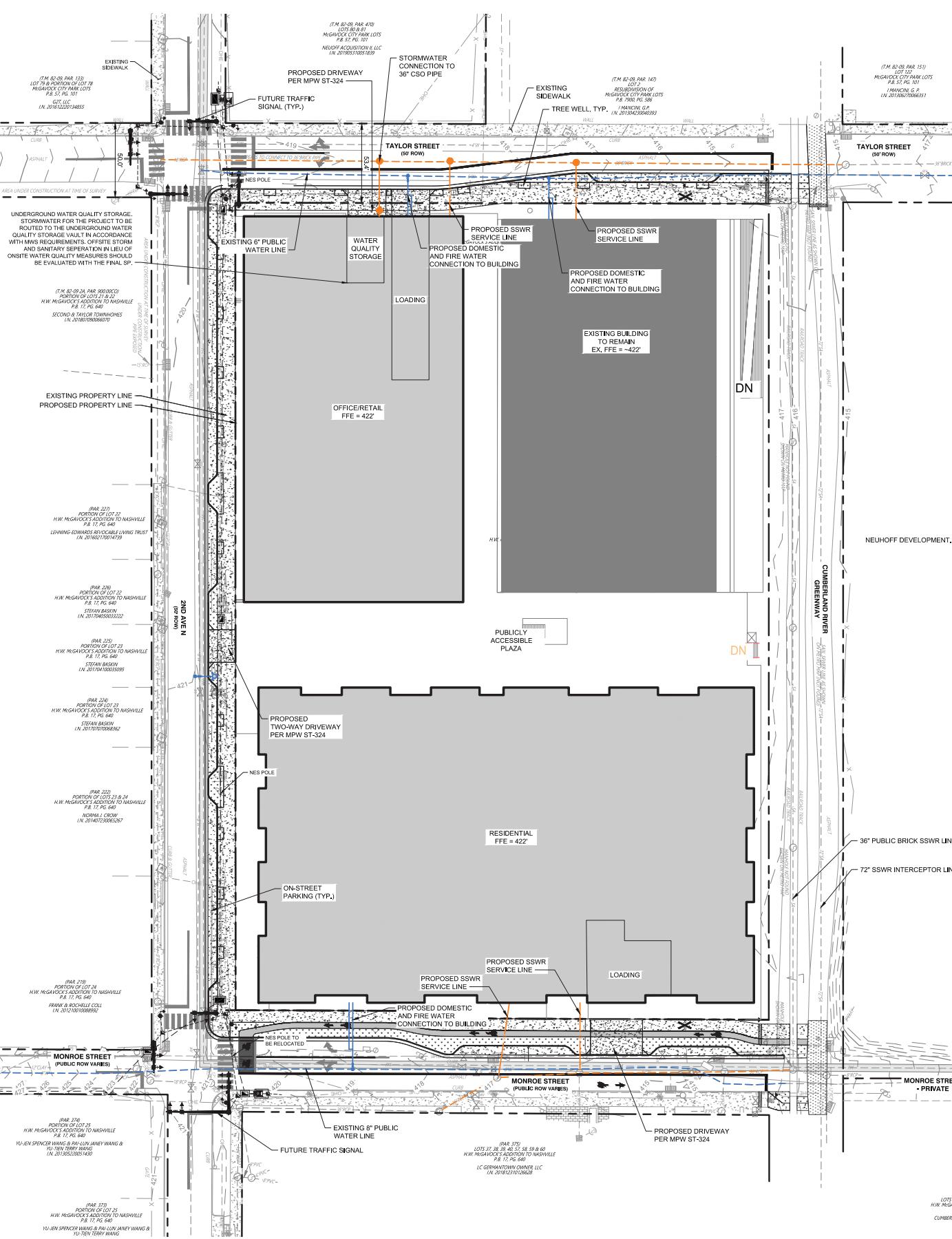
THIS SITE IS COMPRISED ENTIRELY OF LINDELL-URBAN LAND COMPLEX, 0 TO 3 PERCENT SLOPES (Ln)

**STORMWATER NOTES**

1. DRAWING IS FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT, AS IT PERTAINS TO STORMWATER APPROVAL/COMMENTS ONLY. THE LOT COUNT AND DETAILS OF THE PLAN SHALL BE GOVERNED BY THE APPROPRIATE STORMWATER REGULATIONS AT THE TIME OF FINAL APPLICATION
2. ANY EXCAVATION, FILL, OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORMWATER MANAGEMENT ORDINANCE NO. 78-840 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.
3. METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED INGRESS AND EGRESS AT ALL TIME IN ORDER TO MAINTAIN, REPAIR, REPLACE, AND INSPECT ANY STORMWATER FACILITIES WITHIN THE PROPERTY.
4. SIZE DRIVEWAY CULVERTS PER THE DESIGN CRITERIA SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL. (MINIMUM DRIVEWAY CULVERT IN METRO ROW IS 15" CMP.)
5. PROJECT INTENT FOR THIS SITE IS TO BE REDEVELOPED PER THE REQUIREMENTS OF THE CURRENT STORMWATER MANAGEMENT MANUAL. DETENTION WILL BE PROVIDED SUCH THAT POST DEVELOPED FLOWS WILL BE LESS THAN PREDEVELOPED.
6. THE BUFFER ALONG WATERWAYS WILL BE AN AREA WHERE THE SURFACE IS LEFT IN A NATURAL STATE, AND IS NOT DISTURBED BY CONSTRUCTION ACTIVITY. THIS IS IN ACCORDANCE WITH STORMWATER MANAGEMENT MANUAL VOLUME 1 - REGULATIONS.
7. THE SITE IS BEING ENTERED INTO THE VOLUNTARY BROWNFIELD AGREEMENT DUE TO SOIL CONTAMINATION THROUGH TDEC. AN LID WAIVER WILL BE REQUESTED DUE TO CONTAMINATED SOIL.

**UTILITY NOTES**

1. CONTRACTOR RESPONSIBLE FOR VERIFYING EXISTING ELEVATIONS COMPARED TO THOSE SHOWN ON PLAN PRIOR TO GRADING. NOTIFY OWNER'S REPRESENTATIVE IF DISCREPANCIES ARE FOUND.
2. AREAS FOR CONSTRUCTION THAT REQUIRE DE-WATERING FOR EXCAVATION WILL BE DONE AT NO ADDITIONAL COST TO THE OWNER.
3. ALL WATER AND SEWER MATERIALS AND CONSTRUCTION SHALL BE IN COMPLIANCE WITH METRO NASHVILLE WATER SERVICES STANDARD SPECIFICATIONS.
4. MAINTAIN MINIMUM 10 FEET HORIZONTAL SEPARATION BETWEEN WATER & SANITARY SEWER OR 18" VERTICAL SEPARATION AT CROSSING LOCATIONS.
5. REFER TO METRO NASHVILLE WATER SERVICES STANDARD SPECIFICATIONS FOR PIPE BEDDING REQUIREMENTS.
6. ALL WATER VALVES TO BE LOCATED OUTSIDE OF PAVED AREAS WHERE POSSIBLE.
7. THE CONTRACTOR SHALL PROVIDE A COMPLETE SET OF AS-BUILT DRAWINGS OF WATER & SANITARY SEWER CONSTRUCTION PRIOR TO THE ACCEPTANCE OF THE PUBLIC UTILITIES.
8. CONTRACTOR RESPONSIBLE FOR DISCONNECTING EXISTING WATER SERVICE LINES AT THE PUBLIC MAIN PER METRO WATER SERVICES STANDARDS. PAVEMENT REPAIR SHALL BE DONE IN ACCORDANCE TO METRO PUBLIC WORKS STANDARD.
9. SITE ELECTRICAL TO BE TAKEN UNDERGROUND WHERE FEASIBLE WITHIN THE PROJECT LIMITS.



**NOTE**

TRASH AND RECYCLING PICKUP TO BE HANDLED BY PRIVATE THIRD PARTY.

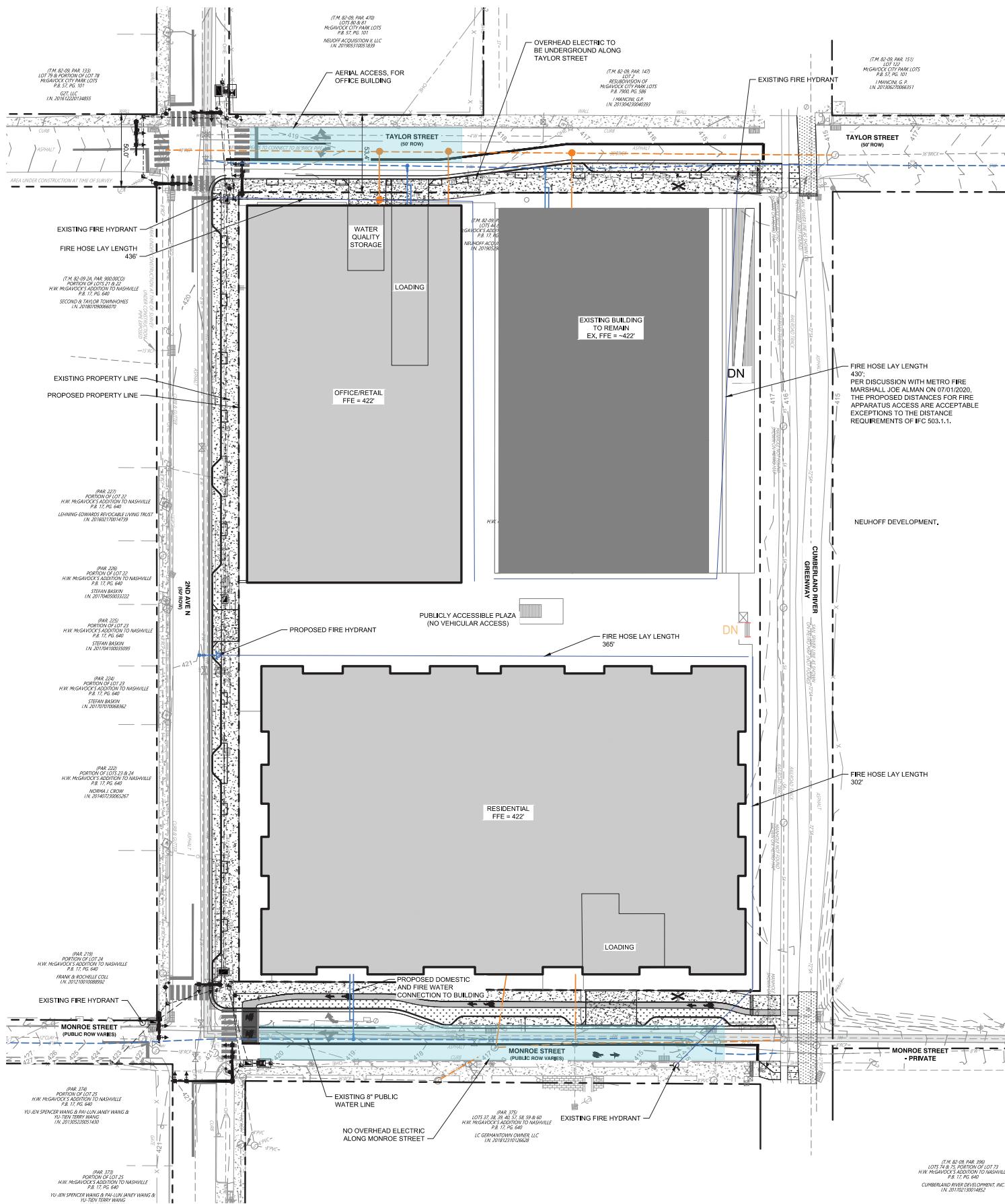
**EXISTING UTILITIES NOTE**

CONTRACTOR SHALL VERIFY LOCATION OF ALL EXISTING UTILITIES AND NOTIFY ENGINEER IMMEDIATELY OF ANY DISCREPANCIES AND/OR CONFLICTS WITH EXISTING OR PROPOSED UTILITIES PRIOR TO PROCEEDING.

**METRO NASHVILLE WATER SERVICES STANDARD NOTES**

1. ALL WATER AND SEWER CONSTRUCTION SHALL BE IN ACCORDANCE WITH SPECIFICATIONS AND STANDARD DETAILS OF THE METRO NASHVILLE WATER SERVICES.
2. THE CONTRACTOR IS RESPONSIBLE FOR REIMBURSING THE METRO NASHVILLE WATER SERVICES THE COST OF INSPECTION.
3. THE CONTRACTOR IS TO PROVIDE AND MAINTAIN THE CONSTRUCTION IDENTIFICATION SIGN FOR PRIVATE DEVELOPMENT APPROVED.
4. AFTER COMPLETION OF THE SANITARY SEWER, THE DEVELOPER IS RESPONSIBLE FOR THE TELEVISING OF THE LINES PRIOR TO FINAL ACCEPTANCE. THE VIDEOTAPING MUST BE COORDINATED WITH THE METRO NASHVILLE WATER SERVICES INSPECTION SECTION. ALL COSTS WILL BE BORNE BY THE DEVELOPER.
5. ALL CONNECTIONS TO EXISTING MANHOLES SHALL BE BY CORING AND RESILIENT CONNECTOR METHOD.
6. REDUCED PRESSURE BACKFLOW PREVENTION DEVICES (RPBP) OR DUAL CHECK VALVE WILL BE REQUIRED ON ALL TEST AND FILL LINES (JUMPER) NEEDED FOR WATER MAIN CONSTRUCTION AND MUST BE APPROVED BY THE METRO NASHVILLE WATER SERVICES.
7. ALL WATER METERS SHALL BE A MINIMUM OF 24" NOT TO EXCEED A MAXIMUM OF 28" BELOW FINISHED GRADE.
8. UPON COMPLETION OF CONSTRUCTION OF WATER AND/OR SEWER, THE ENGINEER SHALL PROVIDE THE DEPARTMENT WITH A COMPLETE SET OF AS-BUILT PLANS ON MOIST ERASABLE MYLARS IN REVERSE AND IN DIGITAL (\*.DWG) FORMAT. SEWER PLANS SHALL BE SEALED BY A LICENSED PROFESSIONAL ENGINEER OR A REGISTERED LAND SURVEYOR AND SHALL INCLUDE ACTUAL FIELD ANGLES BETWEEN LINES, ALL ACTUAL SERVICE LINES AND TEE LOCATIONS, THE DISTANCE OF THE END OF THE SERVICE LINE TO PROPERTY CORNERS AND LINES AND/OR STATION AND OFFSET FROM SEWER CENTERLINE TO END OF SERVICE LINE, THE DEPTH TO THE TOP OF THE END OF THE SERVICE LINE, AND SHALL REFLECT ALL ALIGNMENT AND GRADE CHANGES. WATER LINE PLANS SHALL BE SEALED BY A LICENSED PROFESSIONAL ENGINEER OR A REGISTERED LAND SURVEYOR AND SHALL INCLUDE OFFSET DISTANCE FROM THE ROADWAY CENTERLINE, OR PROPERTY LINE RIGHT OF WAY, LINE DEPTH, LOCATIONS OF HYDRANTS, VALVES, REDUCERS, TEES AND PRESSURE REDUCING DEVICES WHERE APPLICABLE. ALL DRAWINGS MUST BE COMPLETED AND SUBMITTED PRIOR TO ACCEPTANCE OF THE SEWERS OR WATER MAINS INTO THE PUBLIC SYSTEM AND ANY CONNECTIONS BEING MADE.
9. PRESSURE REGULATING DEVICES WILL BE REQUIRED ON THE CUSTOMER SIDE OF THE METER WHEN PRESSURES EXCEED 100 PSI.
10. PRESSURE REGULATING DEVICES WILL BE REQUIRED ON THE STREET SIDE OF THE METER WHEN PRESSURES EXCEED 150 PSI.
11. ALL WATER MAINS MUST BE LOCATED WITHIN THE PAVED AREA INCLUDING ALL BLOW-OFF ASSEMBLIES.
12. ALL WATER AND/OR SEWER SERVICES, ALONG WITH APPURTENANCES, SHALL BE INSTALLED IN ACCORDANCE WITH SPECIFICATIONS AND STANDARD DETAILS OF THE METRO NASHVILLE WATER SERVICES.
13. VERTICAL DOUBLE CHECK VALVE ASSEMBLIES, THAT ARE LOCATED IN INTERIOR ROOMS, CAN ONLY BE USED FOR FIRE SERVICES.
14. IRRIGATION LINE SHALL BE COPPER FROM THE METER TO THE BACKFLOW PREVENTER.
15. THE MINIMUM FEES OUTLINED IN THE CAPACITY LETTER MUST BE PAID BEFORE COMMERCIAL CONSTRUCTION PLANS CAN BE REVIEWED.
16. ALL SEWER SERVICES SHALL BE 6 INCHES IN DIAMETER, FROM CONNECTION AT THE MAIN UNTIL THE FIRST CLEAN OUT ASSEMBLY, UNLESS OTHERWISE NOTED.
17. BACKFLOW DEVICE TO REMAIN ACCESSIBLE AT ALL TIMES.
18. PLAN SIZE SHALL BE 24" X 36", AND SHALL SHOW CONTOURS AROUND METER BOXES.

# **FIRE APPARATUS PLAN**



# GROWING



## VISION

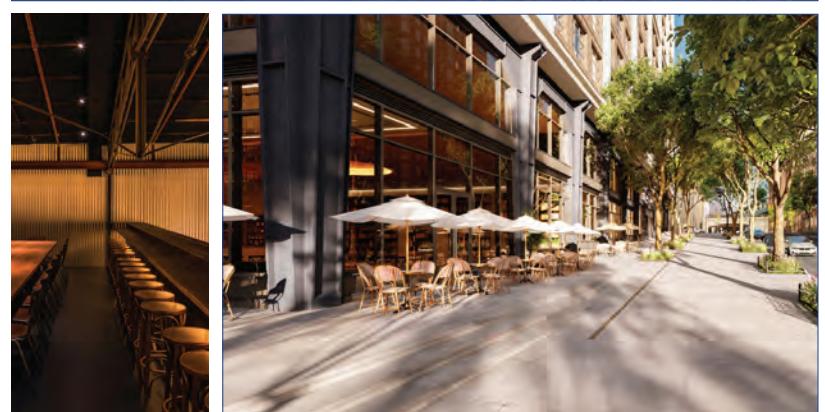
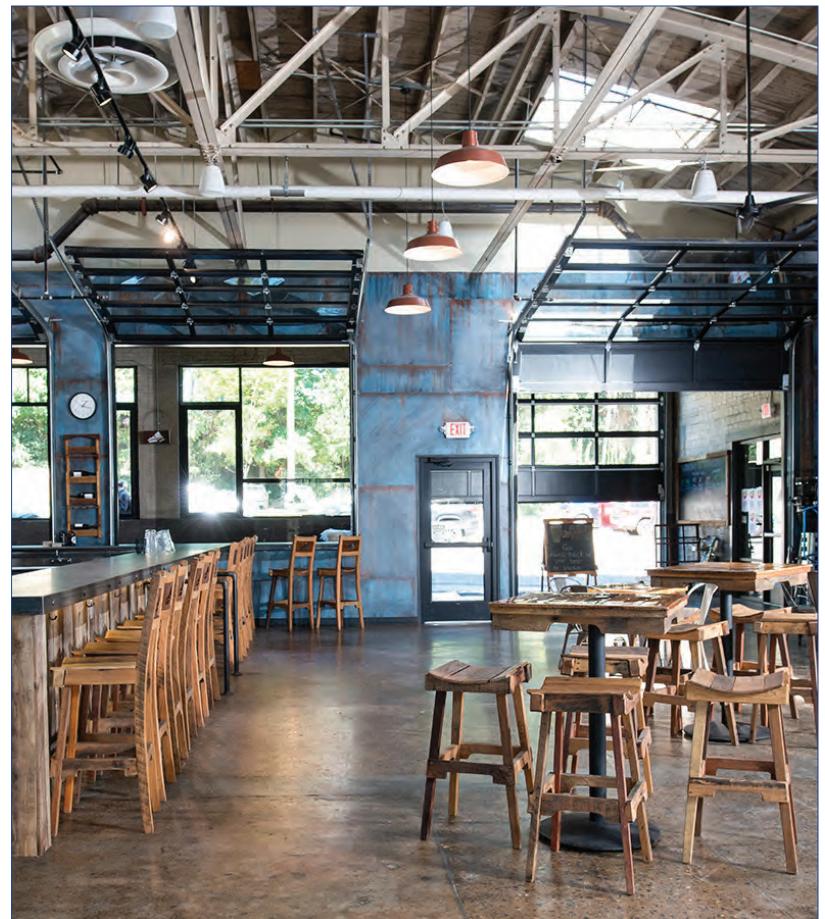
Nashville Block 5 is a mixed-use development that includes a five-story 190,000 sf office building, a five-story 280 unit residential building, 24,000 sf of retail, 5,000 sf of restaurant, and the renovation of an existing one-story bow truss building that will become the home of a 13,500 sf brewery.

This warehouse will be converted into a food and beverage service and retail center, with frontage along the Cumberland River Greenway, Taylor Street, and a public plaza in the center of Block 5. Its renovation will help reinforce and celebrate the industrial history of the Germantown neighborhood.

The Residential building features balconies that wrap the facade and engage the street fronts of 2nd Avenue and Monroe Street, as well as the greenway. Its material palette will also recall the industrial history of the neighborhood, with a masonry ground floor and a concrete clad facade on the floors above.

The office building will be clad in precast concrete and includes warehouse windows and a contemporary interpretation of classical detailing, and it will feature retail space on the central plaza level.

The frontage along the street takes advantage of the site's slight grade change, exposing the below grade parking garage, highlighting planted berms, vertical plantings, and other unique landscape features.



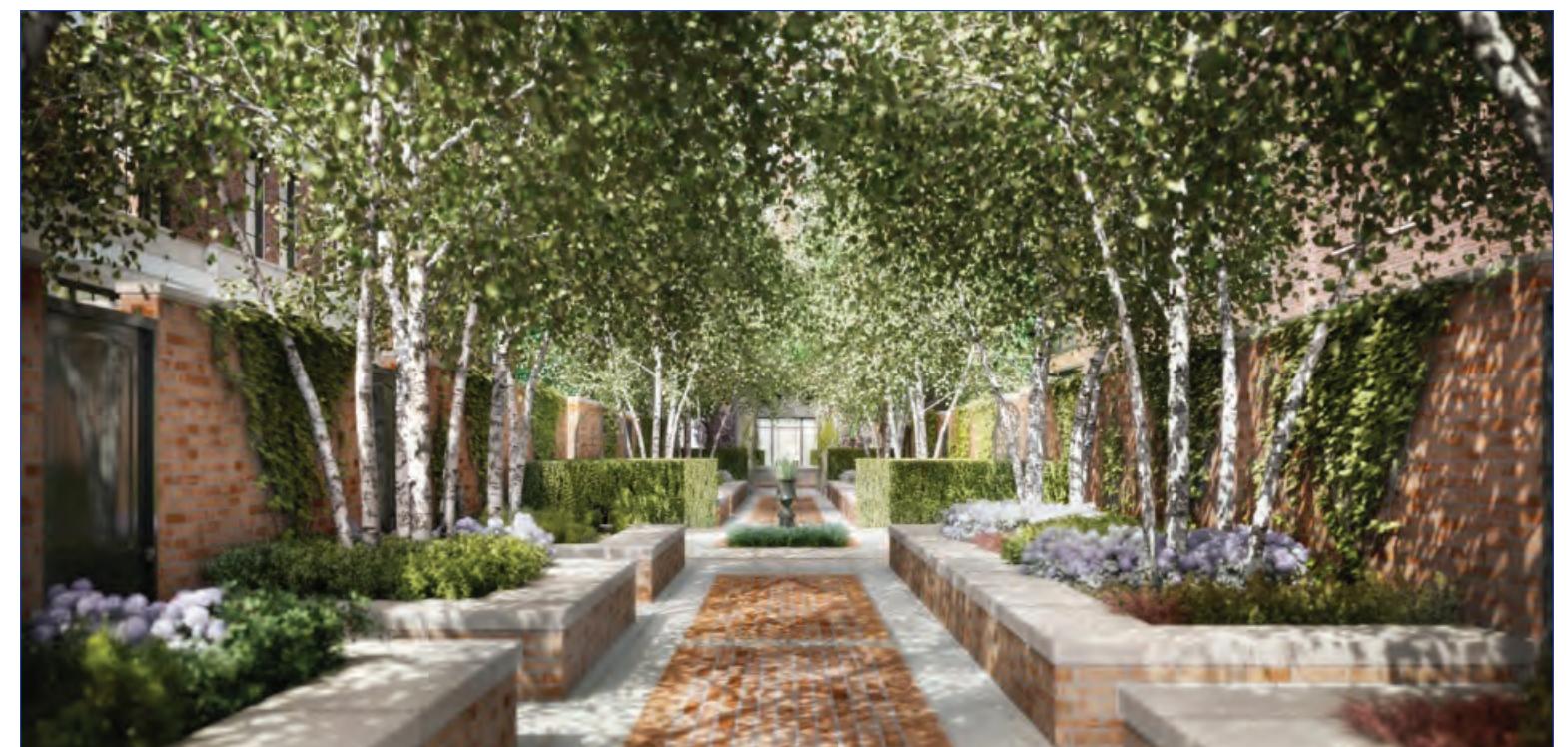
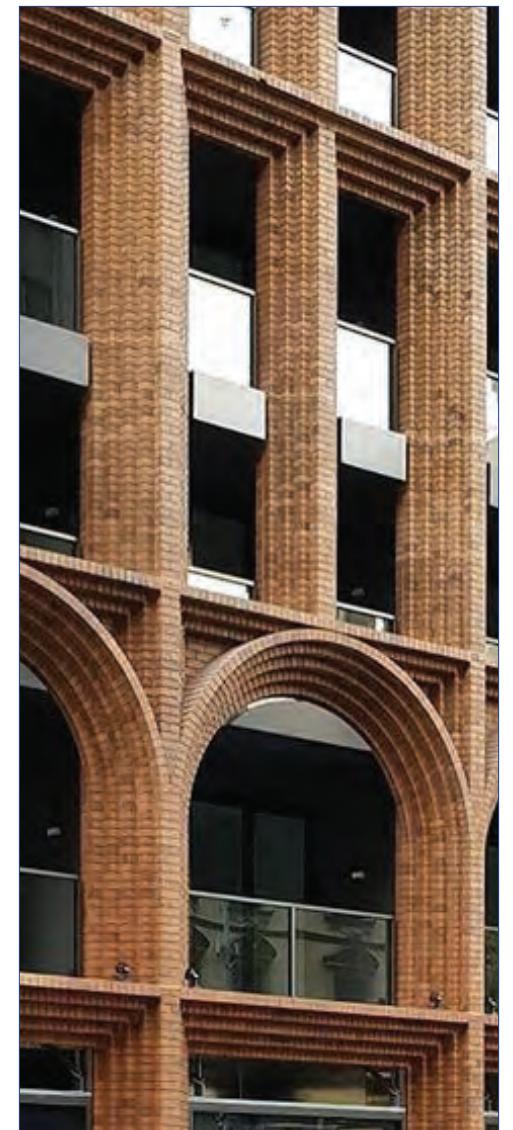
# CONCEPT PLAN



## 2ND AVENUE

Second Avenue will be lined with both street parking and street trees. The entry to the parking ramp will be marked for a two way ramp that provides an open, clearly visible access point to the parking garage below the central plaza.

The landscape along the building frontage is divided into two conditions: planted terraces that connect the sidewalk to the office building frontage, and a series of semi-private patios in front of the residential building's ground floor townhouses.

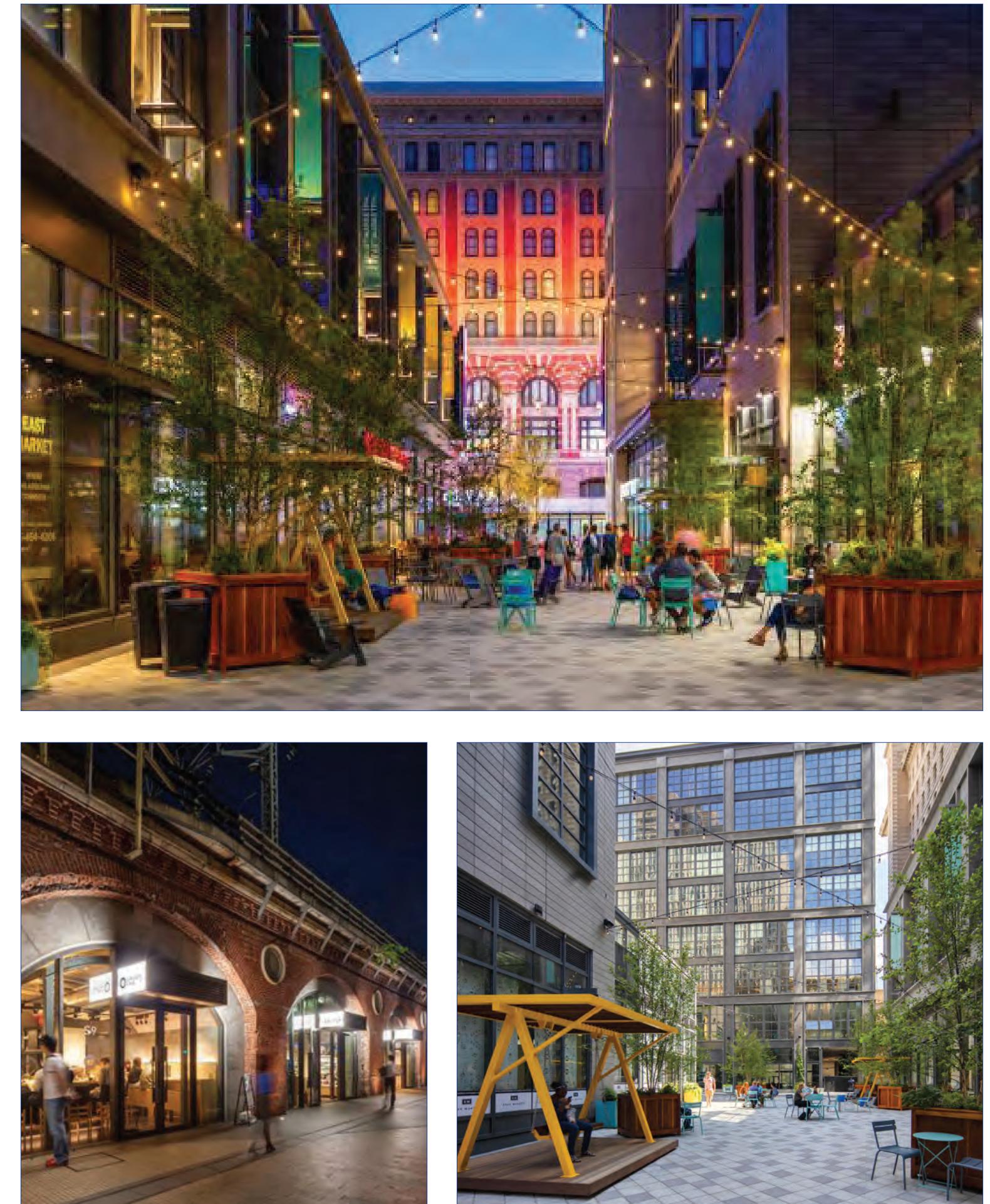




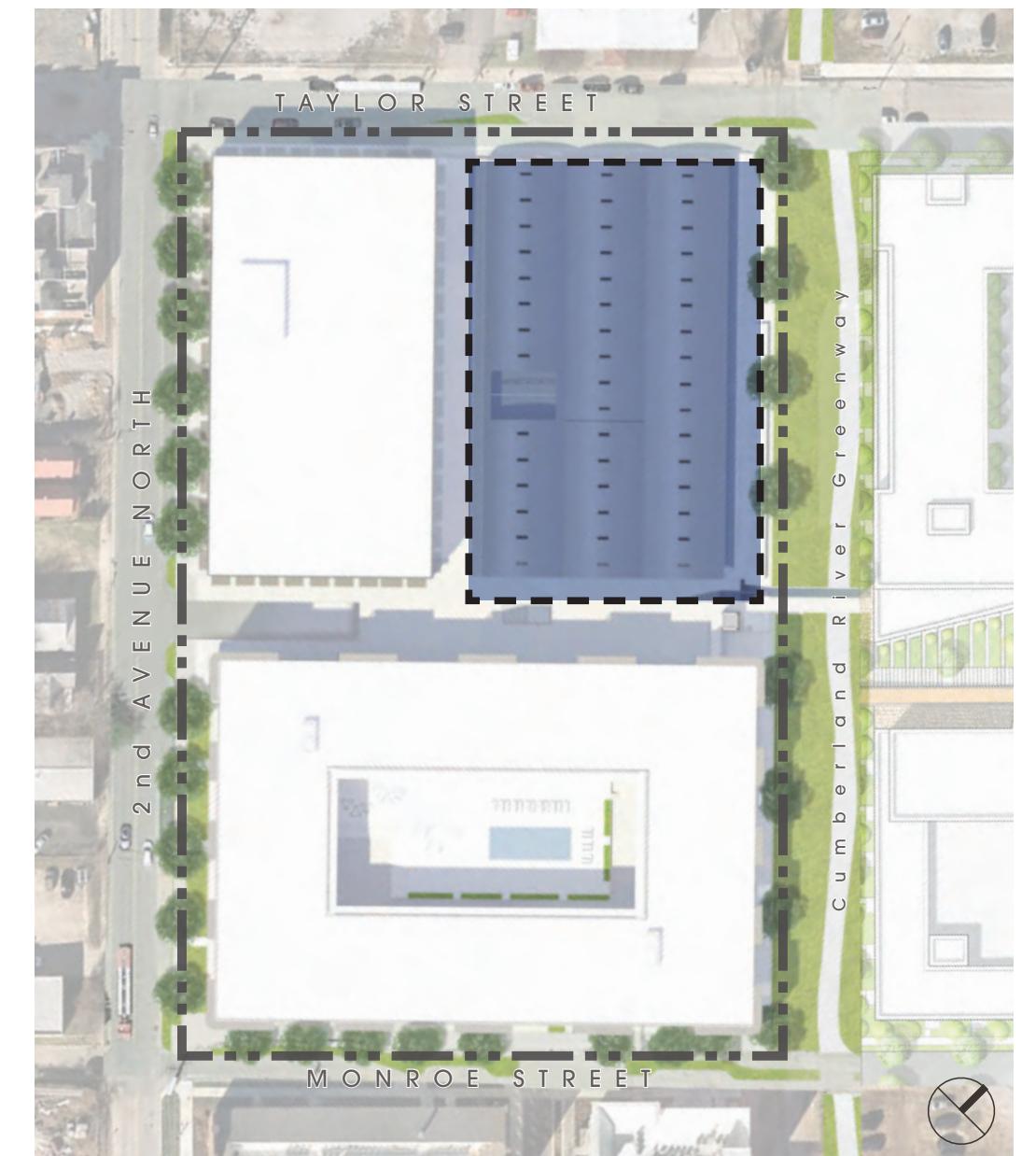
# 1324 2ND AVENUE NORTH



The plaza is located in the center of Block 5 and creates a pedestrian promenade lined with retail on both sides. It provides a visual connection to the adjacent Neuhoff development, with a bridge across the Cumberland River Greenway and active railway, accessible from the plaza level via stair or elevator.



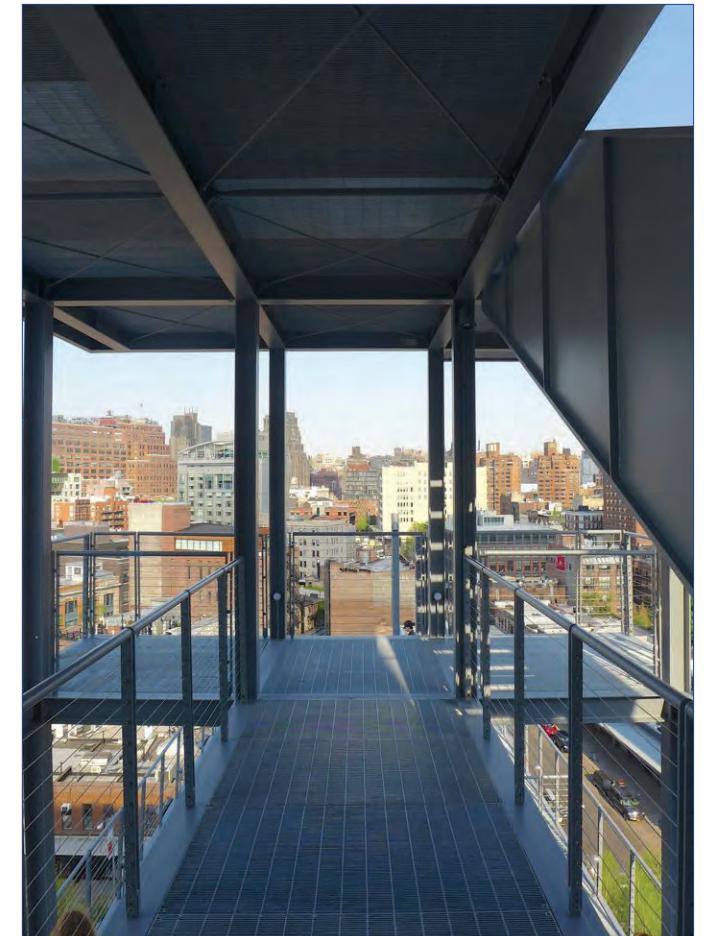
# BOW TRUSS



The renovation of the existing bow truss warehouse includes the preservation of its original structural trusses and columns, while its end walls will be replaced with contemporary glazing walls - creating the sense of an industrial ruin paired with high-tech, elegant materiality. The roof will be replaced with high performing systems that maintain the original clarity of the building's form, while including performative elements such as skylights.

## BRIDGE

The bridge crossing from Block 5 over the Cumberland River Greenway and active rail line is a major sculptural feature in the plaza. It connects to the plaza level and both levels of parking below by both a grand stair and an elevator, providing accessibility to any visitors who use the Block 5 parking amenities to visit the surrounding area. It will also serve as a connection to the Neuhoff project.





## THE GREENWAY

The Greenway is envisioned as a planted linear experience - it will be a front door for cyclists and pedestrian access to Block 5. The development's frontage along the Greenway is composed of a series of porches – the more private townhouse porches along the residential building, and a vital, public porch along the renovated bow truss building. There will be one primary on-grade pedestrian crossing connecting the Block 5 plaza across the Greenway to the Neuhoff development.







GREENWAY



2ND AVENUE

MATERIALS LEGEND	
1 PRECAST CONCRETE	8 PRECAST CONCRETE - DARK
2 ALUMINUM STOREFRONT	9 CURVED INSULATED ROOF SYSTEM
3 ALUMINUM WINDOWS	10 EXISTING COLUMN
4 METAL CANOPY	11 EXISTING TRUSS
5 RED BRICK	12 CORRUGATED SIDING
6 METAL GUARDRAILS	13 LOADING DOCK DOOR
7 LIGHTWEIGHT PRECAST	
CONCRETE PANEL	



TAYLOR STREET



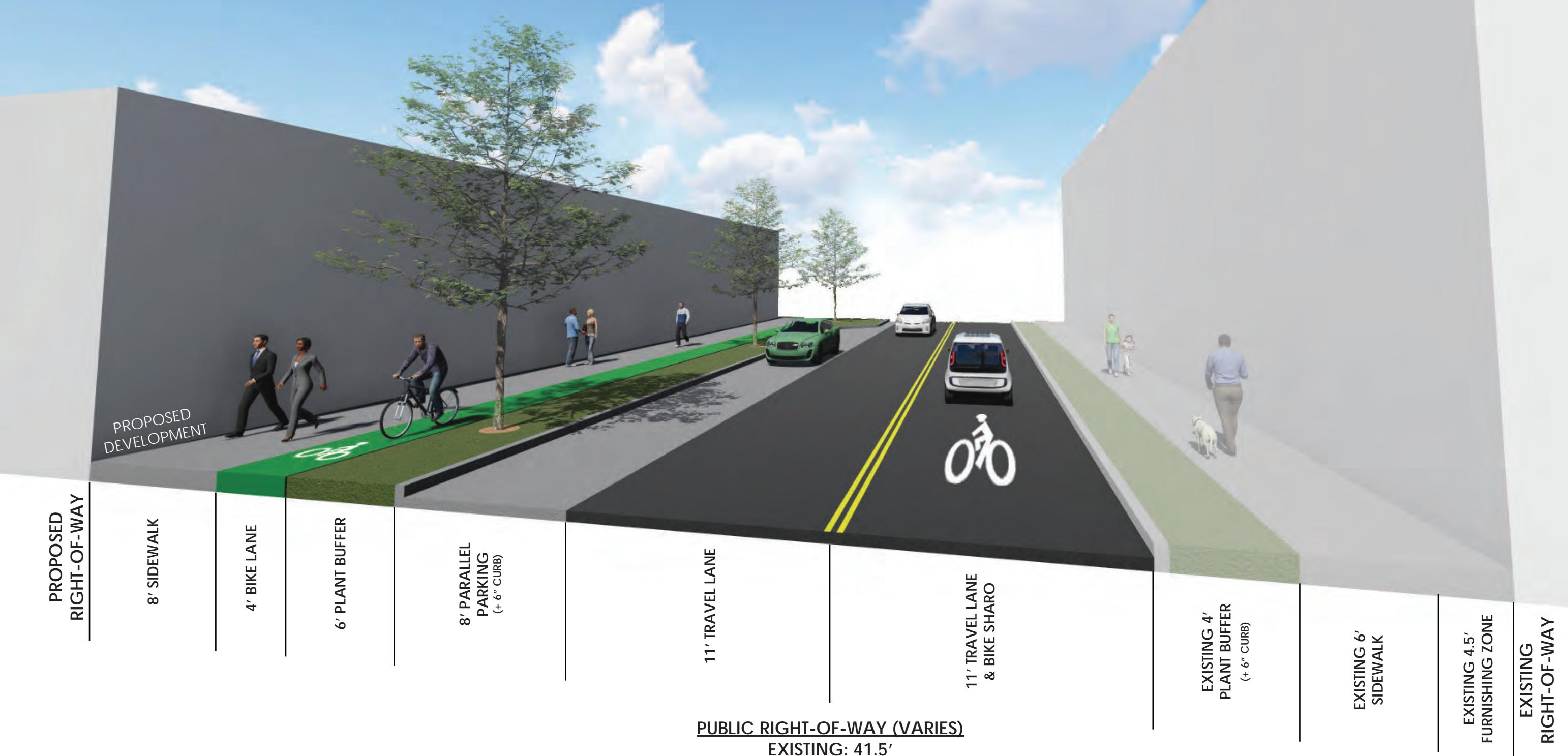
MONROE STREET

MATERIALS LEGEND	
1 PRECAST CONCRETE	8 PRECAST CONCRETE - DARK
2 ALUMINUM STOREFRONT	9 CURVED INSULATED ROOF SYSTEM
3 ALUMINUM WINDOWS	10 EXISTING COLUMN BEYOND
4 METAL CANOPY	11 EXISTING TRUSS BEYOND
5 RED BRICK	12 CORRUGATED SIDING
6 METAL GUARDRAILS	13 LOADING DOCK DOOR
7 LIGHTWEIGHT PRECAST CONCRETE PANEL	

# APPENDIX



# STREET SECTIONS



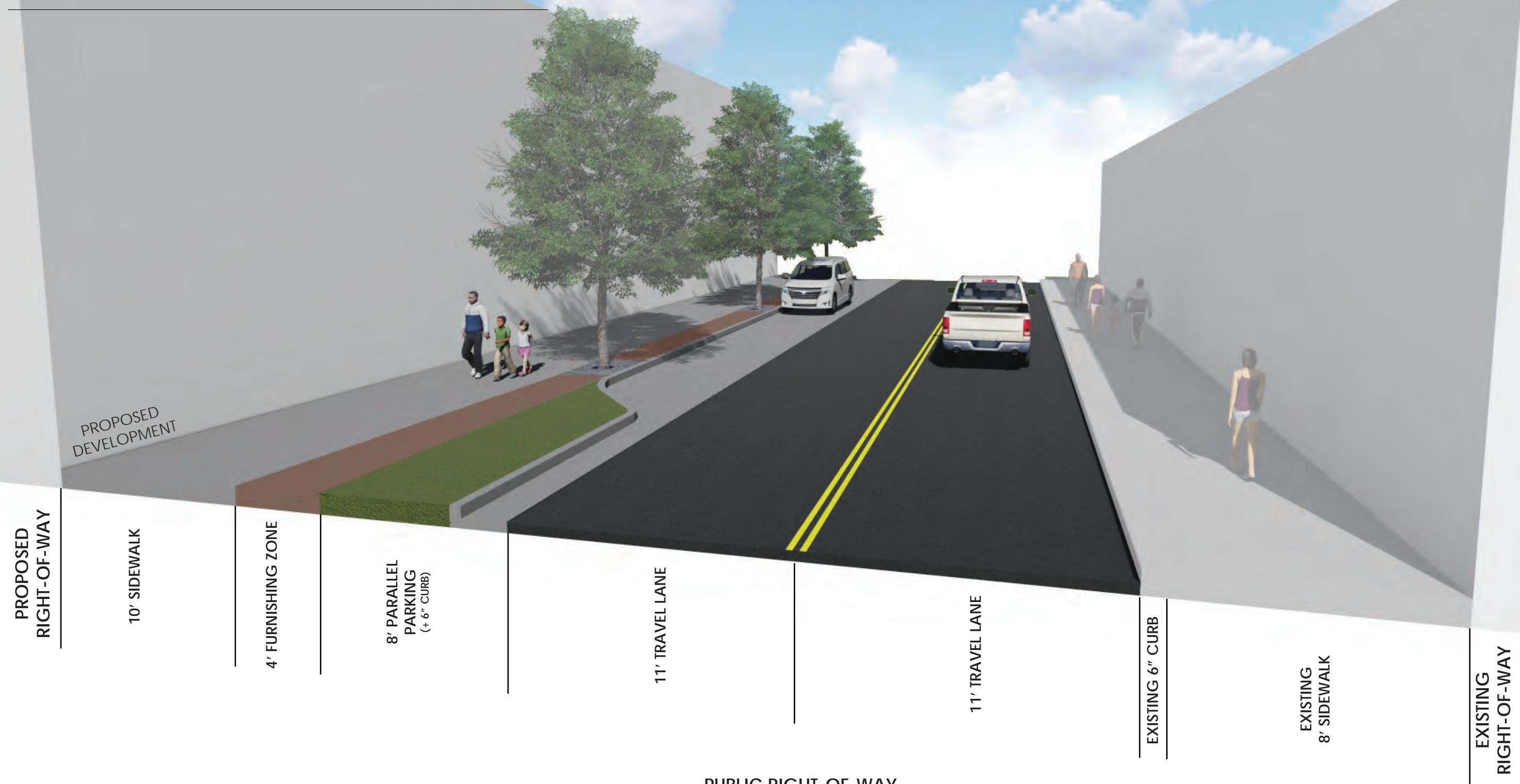
## MONROE STREET

LOOKING EAST BETWEEN 2ND AVENUE AND CUMBERLAND GREENWAY

Individual property owners are responsible for required street improvements along their frontage from the centerline of the right-of-way or easement.

Kimley»Horn

# STREET SECTIONS



## TAYLOR STREET

LOOKING WEST BETWEEN 2ND AVENUE AND CUMBERLAND GREENWAY

Individual property owners are responsible for required street improvements along their frontage from the centerline of the right-of-way or easement.

Kimley»Horn



1402

GYM

NEW  
CITY