

**GENERAL NOTES**

**DESIGN TEAM**

**DEVELOPER:**  
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**PROPERTY INFORMATION**  
 COUNCIL DISTRICT: (8) NANCY VANREECE

Address	Parcel ID	Acres	Owner
298 Broadmoor Dr	06100001300	15.4 ac	Paro South, LLC
292 Broadmoor Dr	06100001400	1.6 ac	Paro South, LLC
296 Broadmoor Dr	06100001500	2 ac	Paro South, LLC
329 Ben Allen Rd	06100008500	22.3 ac	BEN ALLEN LAND PARTNERS, LLC
341 Ben Allen Rd	06100001800	8.6 ac	BEN ALLEN LAND PARTNERS, LLC
349 Ben Allen Rd	06100007300	3 ac	BEN ALLEN LAND PARTNERS, LLC

**TOTAL SITE ACREAGE** **52.9 ac**

**PREVIOUS SP NO.:** 2017SP-034-001

**US FEMA FIRM NUMBER:** 47037C232H, 47037C025E1H Dated April 5, 2017

**R.O.W. DEDICATION AREA:** No public roads. Any required dedication to meet MCSP will be finalized in the Final SP upon detailed survey.

**COMMON OPEN SPACE:** 17.8 acres min including  
 • Community Open Space and Forest 14.5 acres  
 • Community Lawn 1.6 acres  
 • Community Garden and Orchard 0.6 acres  
 • Approx. 4000 l.f. of Primary Trail (Greenway) Easement 1.1 acres

**PURPOSE**  
 THE PURPOSE OF THIS PLAN IS TO AMEND THE CURRENT SPECIFIC PLAN 2017SP-034-001 TO PROVIDE FOR ADDITIONAL RESIDENTIAL UNITS WITH A RANGE OF HOUSING TYPES.

**GENERAL PLAN / COMMUNITY PLAN CONSISTENCY:**

The Broadmoor and Ben Allen SP is a TRAIL ORIENTED DEVELOPMENT consistent with the following policies within the Nashville Next Community Plan: T3-NE (Neighborhood Evolving)  
 T3-NC (Suburban Neighborhood Center)  
 The plan is consistent with the policies of the East Nashville Community Plan Amendment 2016CP-005-005

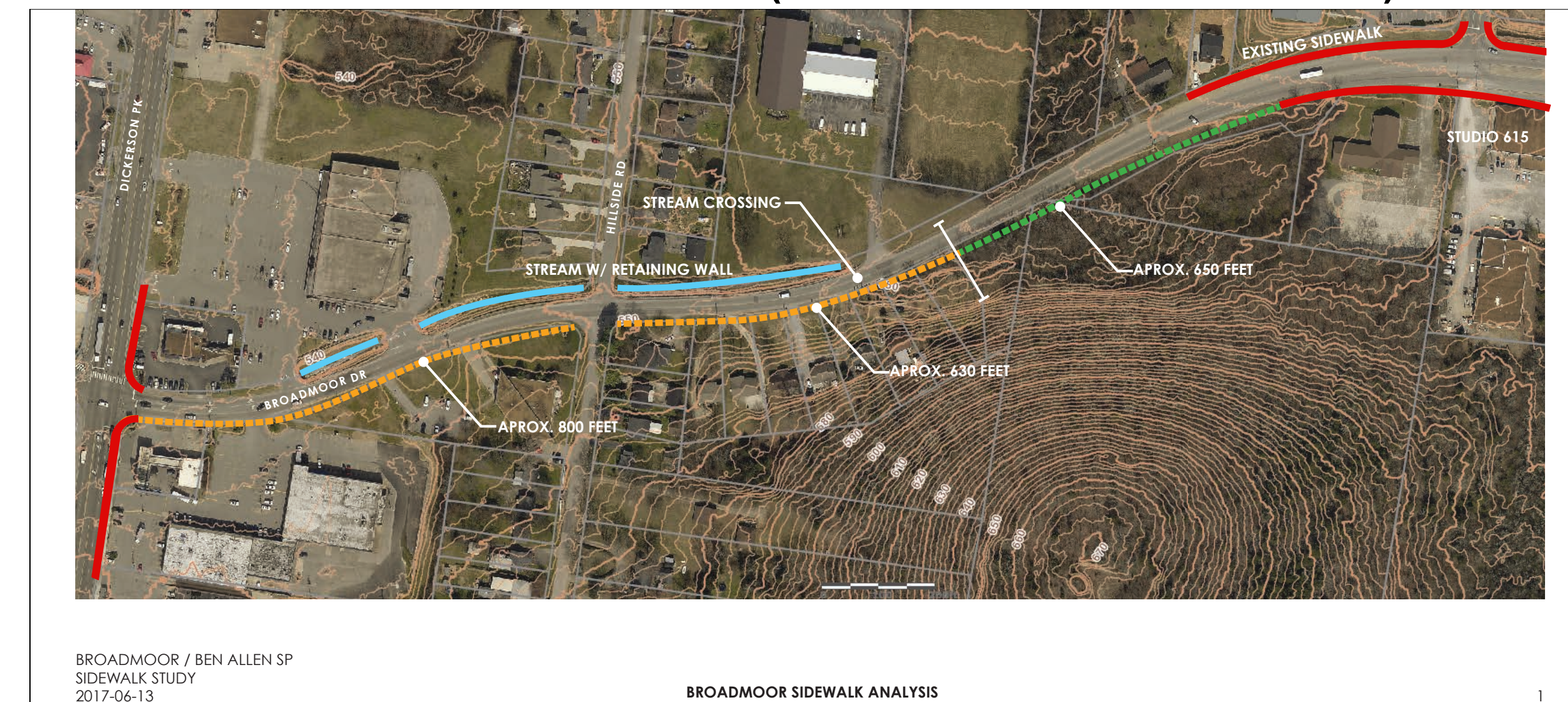
The development indicates neighborhood centers with mixed use development and with a mix of residential housing choices while oriented around pedestrian and bicycle connectivity, dedicated open space and providing a concentrated walkable development nodes along the arterial street to the north (Broadmoor) and the local street, Ben Allen, to the south to serve the neighborhood.

The residential portions of the site are policy T3-NE and indicate a variety of housing types with sensitivity to the identified stream, drainage ways and grades while oriented around pedestrian and bicycle connectivity via a primary trail and a private trail system and dedicated open space.

**LEGEND**

- EXISTING SIDEWALK
- PROPOSED SIDEWALK BY OTHERS
- OFFSITE SIDEWALK PROVIDED BY THIS PROJECT
- STREAM WITH RETAINING WALL

**SIDEWALKS TO BE PROVIDED OFFSITE (WITH COMPLETION OF PHASE 3):**



**OVERALL DEVELOPMENT AND PHASE DESIGNATIONS:**

**OVERALL DEVELOPMENT:**

**NON-RESIDENTIAL USES:**

NEIGHBORHOOD CENTERS (BROADMOOR AND BEN ALLEN): 28,800 S.F. EXISTING STRUCTURES (currently light manufacturing and multi-media production)  
 30,500 S.F. MAX. MIXED USE/COMMERCIAL USES  
 22,500 S.F. MAX. COMMUNITY EDUCATION  
 (Refer to LAND USE TABLE for range of application)

Fall back zoning for Neighborhood Centers is MUL-A

**OVERALL DEVELOPMENT PARKING**

Parking to be provided per Zoning Code. Total parking spaces shown in current plan: **685 SURFACE PARKING PLUS 430 GARAGE PARKING**

Determination of land use mix for NEIGHBORHOOD CENTERS shall be submitted with Final SP. Shared parking to be provided in NEIGHBORHOOD CENTERS based on mix of uses has not yet been calculated.

**OVERALL RESIDENTIAL UNITS\*: 482 TOTAL MAXIMUM**

- Multifamily: 174 maximum units
- Live/Work: 12 maximum units
- Cottages: 109 maximum units
- Townhomes: 162 maximum units
- Hillside Duplex: 21 maximum units
- \*\* estimated unit mix. Determination of unit mix shall be submitted with Final SP.

**OVERALL RESIDENTIAL PARKING**

Parking to be provided per Zoning Code.

**OVERALL MAXIMUM IMPERVIOUS SURFACE RATIO:** .60

**PHASE 1/BROADMOOR NEIGHBORHOOD CENTER:** approx. 12.6 acres (SP DEVELOPMENT FALL BACK STANDARD FOR PHASE 1 IS MUL-A)  
 BUILDING FRONTAGE BROADMOOR: 50% MINIMUM

**FLOOR AREA RATIO: .80 MAXIMUM FAR**  
 Uses Allowed: REFER TO LAND USE TABLE AND EXISTING USES TO REMAIN

**Building Mass:**  
 Build To Zone for new structures/ Broadmoor: 5-20\*\*  
 Side Setback: 0'  
 Rear Setback: 0'  
 Height Maximum: 4 stories  
 Height Minimum: 14' one story

Parking to be provided per Zoning Code with shared parking available with Neighborhood Center uses

dining areas are allowed to be zero setback provided that public sidewalk meeting MCSP is provided in adjacent public ROW. Build-to-lines are subject to proximity of overhead power lines.

**EXISTING STRUCTURE A and B:**  
 Existing Structures to Remain / 30k s.f. /  
 Uses Allowed: REFER TO LAND USE TABLE AND EXISTING USES TO REMAIN  
 Parking: Provided per zoning code with shared parking available with Phase 1 uses  
 EX. STRUCTURE A - Light Manufacturing / 12K S.F.  
 EX. STRUCTURE B - Multi-Media Production / 16.8K S.F.

**RETAIL BUILDING A**  
 Approx. 5000 S.F. / 2 stories maximum COMMERCIAL USES  
 Uses Allowed: REFER TO LAND USE TABLE AND EXISTING USES TO REMAIN  
 Parking: Provided per zoning code with shared parking available with Phase 1 uses

**MIXED USE BUILDING A:**  
 Approx. 8,000 s.f./ 3 stories maximum MIXED USE  
 Uses Allowed: REFER TO LAND USE TABLE  
 Parking: Provided per zoning code with shared parking available with Phase 1 uses

**MIXED USE BUILDING B:**  
 Approx. 21,100 s.f./ 3 stories with 7,000 s.f. ground floor commercial and upper floors of residential (20 units)  
 Uses Allowed: REFER TO LAND USE TABLE  
 Parking: Provided per zoning code with shared parking available with Phase 1 uses

**RESIDENTIAL (attached Townhomes):**  
 65 dwelling units maximum  
 Uses Allowed: REFER TO LAND USE TABLE  
**Building Mass:**  
 Private drive setback: 5' minimum edge of drive  
 Side Setback: 0'  
 Rear Setback: 0'  
 Height Maximum: 3 stories and 35' along private drive face including basement/carpport level to accommodate site grade change.  
 Parking: Provided per zoning code within attached garages or immediately adjacent surface parking.

**PHASE 2:** approximately 6.4 acres (SP DEVELOPMENT FALL BACK STANDARD FOR PHASE 2 IS RM15)

**RESIDENTIAL (detached Cottages):**  
 83 dwelling units (various sizes)  
 Uses allowed: REFER TO LAND USE TABLE  
**Building Mass:**  
 Private drive setback: 5' minimum from edge of drive  
 Rear Setback: 0'  
 Side Setback: 6' minimum between structures  
 Rear Setback: not applicable

**Height Maximum:** 2 stories and 35' along private drive face. A third exposed basement level shall be allowed as needed, to accommodate site grade change.  
**Parking:** Provided per zoning code within attached garages or immediately adjacent surface parking

**PHASE 3:** approximately 10.4 acres (SP DEVELOPMENT FALL BACK STANDARD FOR PHASE 3 IS RM15)

**RESIDENTIAL (Hillside Duplex):**  
 21 dwelling units maximum / maximum of 3 units attached  
 Uses allowed: REFER TO LAND USE TABLE  
**Building Mass:**  
 Private drive setback: 5' minimum from edge of drive  
 Rear Setback: 0'  
 Side Setback: 6' minimum between structures

**Height Maximum:** 3 stories and 35' along private drive face including basement/carpport level to accommodate site grade change.  
**Parking:** Provided per zoning code with surface parking or park under carport

**RESIDENTIAL (attached Townhomes):**  
 41 dwelling units maximum  
 Uses Allowed: REFER TO LAND USE TABLE  
**Building Mass:**  
 Private drive setback: 5' minimum edge of drive  
 Side Setback: 0'  
 Rear Setback: 0'

**Height Maximum:** 3 stories and 35' along private drive face including basement/carpport level to accommodate site grade change.  
**Parking:** Provided per zoning code within attached garages or immediately adjacent surface parking

**PHASE 4:** approximately 6.9 acres (SP DEVELOPMENT FALL BACK STANDARD FOR PHASE 4 IS RM15)

**RESIDENTIAL (detached Cottages):**  
 26 dwelling units (various sizes)  
 Uses allowed: REFER TO LAND USE TABLE  
**Building Mass:**  
 Private drive setback: 5' minimum from edge of drive  
 Rear Setback: 0'  
 Side Setback: 6' minimum between structures  
 Rear Setback: not applicable

**Height Maximum:** 2 stories and 35' along private drive face. A third exposed basement level shall be allowed, as needed, to accommodate site grade change.  
**Parking:** Provided per zoning code within attached garages or immediately adjacent surface parking

**RESIDENTIAL (attached Townhomes):**  
 60 dwelling units maximum  
 Uses Allowed: REFER TO LAND USE TABLE  
**Building Mass:**  
 Private drive setback: 5' minimum edge of drive  
 Side Setback: 0'  
 Rear Setback: 0'

**Height Maximum:** 3 stories and 35' along private drive face including basement/carpport level to accommodate site grade change.  
**Parking:** Provided per zoning code within attached garages or immediately adjacent surface parking

**EX. STRUCTURE A - Light Manufacturing / 12K S.F.**  
**EX. STRUCTURE B - Multi-Media Production / 16.8K S.F.**

**Private drive setback:** 10' minimum from edge of drive  
 Side Setback: 6' minimum between structures  
 Rear Setback: minimum of 20' to overall parcel property line  
**Height Maximum:** 2 stories and 35' along private drive face. A third exposed basement level shall be allowed, as needed, to accommodate site grade change.

**Parking:** Provided per zoning code within attached garages or immediately adjacent surface parking

**MIXED USE BUILDING A:**  
 Approx. 8,000 s.f./ 3 stories maximum MIXED USE  
 Uses Allowed: REFER TO LAND USE TABLE  
**Parking:** Provided per zoning code with shared parking available with Phase 1 uses

**MIXED USE BUILDING B:**  
 Approx. 21,100 s.f./ 3 stories with 7,000 s.f. ground floor commercial and upper floors of residential (20 units)  
 Uses Allowed: REFER TO LAND USE TABLE  
**Parking:** Provided per zoning code with shared parking available with Phase 1 uses

**RESIDENTIAL (attached Townhomes):**  
 65 dwelling units maximum  
 Uses Allowed: REFER TO LAND USE TABLE  
**Building Mass:**  
 Private drive setback: 5' minimum edge of drive  
 Side Setback: 0'  
 Rear Setback: 0'  
 Height Maximum: 3 stories and 35' along private drive face including basement/carpport level to accommodate site grade change.  
**Parking:** Provided per zoning code within attached garages or immediately adjacent surface parking.

**SIDEWALKS / TRAILS PROVIDED ON SITE:**



**SIDEWALK / TRAILS PROVIDED ON SITE SUMMARY:**

EX SIDEWALK IMPROVED IN ROW	637'
SIDEWALK ADDED IN ROW	579'
SIDEWALK ADDED W/IN DEVELOPMENT	6000'
PRIMARY TRAIL ADDED W/IN ROW	750'
PRIMARY TRAIL ADDED W/IN DEVELOPMENT	4000'

**BEN ALLEN PROP. SIDEWALK IN ROW**

219' NO SIDEWALK EXISTS. PROPOSE TO IMPROVE BY ADDING PLANTING STRIP FOR FULL PROPERTY BOUNDARY.

**DEVELOPMENT SCHEDULE**

Phasing is currently anticipated to be five phases as noted.

Private drive access from Ben Allen will be provided with Phase 3 development.

A finite timeline for phasing will be determined consistent with leasing and financing.

**LAND USE AND MODIFICATIONS**

Refer to LAND USE TABLE for Land Uses Appropriate for the entire site and for those uses appropriate for Neighborhood Centers

**ENCROACHMENTS**

EXPOSED, COVERED, OR ENCLOSED PORCHES AND DECKS MAY EXTEND INTO FRONT SETBACKS ALONG PUBLIC ROADS OR COMMON SPACE NOT EXCEEDING 8FT INTO COMMON SPACE OR 3FT ALONG PUBLIC ROADS.

REFER TO MUNICOD 17.12.040: OTHER SETBACKS FOR ALL OTHER ALLOWABLE ENCROACHMENTS NOT IDENTIFIED ABOVE.

**PARKING**

THIS DRAWING INDICATES THE BASIC DEVELOPMENT PLAN FOR THE SITE. THE PARKING LAYOUTS SHOWN ARE REPRESENTATIVE AND ARE SUBJECT TO CHANGE FOLLOWING DETAILED DESIGN. FINAL DETAILS OF THIS PLAN SHALL BE GOVERNED BY THE APPROPRIATE REGULATIONS AND REQUIREMENTS OF THE FINAL SP APPLICATION.

PARKING SHALL MEET METRO ZONING ORDINANCE FOR EACH USE. SHARED PARKING SHALL BE INCORPORATED FOR NEIGHBORHOOD CENTERS.

**LANDSCAPE**

THE DEVELOPMENT OF THIS PROJECT SHALL EXCEED THE REQUIREMENTS OF METRO ZONING CODE CHAPTER 17.24 ARTICLE II: TREE PROTECTION AND REPLACEMENT BY A MINIMUM 20%. AND SHALL MEET REQUIREMENTS OF CHAPTER 17.40 ARTICLE X: TREE PROTECTION AND REPLACEMENT PROCEDURES APPLICABLE AT THE TIME OF THIS SUBMITTAL. DETAILED LANDSCAPE ORDINANCE PLAN TO BE SUBMITTED WITH THE FINAL SP SUBMITTAL.

NO LANDSCAPE BUFFERS ARE PROVIDED.

THE BUFFER ALONG WATERWAYS WILL BE AN AREA WHERE THE SURFACE IS LEFT IN A NATURAL STATE, AND IS NOT DISTURBED BY CONSTRUCTION ACTIVITY. THIS IS IN ACCORDANCE WITH THE STORMWATER MANAGEMENT MANUAL VOLUME 1 - REGULATIONS.

SITE GRADING AND UTILITIES TO BE ROUTED TO MINIMIZE DISTURBANCE TO EXISTING FORESTED AREAS TO REMAIN.

**SOIL CLASSIFICATIONS**

MIMOSA SILT LOAM

**ROADWAY CLASSIFICATIONS**

BROADMOOR DRIVE: T4-R-CA2 (T4 Urban Residential Collector-Avenue)  
 BEN ALLEN ROAD: Local Street

REFER TO TRAFFIC IMPACT STUDY, PREPARED BY KCI TRANSPORTATION, INC. AND DATED DECEMBER 2022

**BUILDING ORIENTATION**

All mixed use buildings will provide a primary point of access to a Public Road and/or Private Drive.

All residential structures will have a primary entry onto Common Open Space or Private Road.

**NOTES:**

- Designs for all off-site improvements shall be included in the building permit plan set:
  - a. At Broadmoor & Walton Lane: Provide ADA compliant curb ramps, crosswalks, and detectable warning mats where missing. Add a stop bar on egress of south leg. And, construct a LTL w/ 125' min storage for WB approach within median.
  - b. At both site driveways on Ben Allen Road, provide ADA-compliant ramps, crosswalk, and detectable warning mats for the north leg. Ensure sight distance is not interfered with vegetation. And, a traffic control officer should be utilized during the morning arrival & afternoon dismissal periods of the proposed elementary school. Additional improvements may be warranted based on queue analysis.
  - c. At Hart Lane & Ben Allen Road/RS Gas Intersection: The SB approach should be restriped to include a LTL (w/ 100' storage) and 1 thru/RTL. And, complete a full signal warrant analysis prior to the completion of the final phase of development.
  - d. On Ben Allen Road in the School Zone: Provide a School Speed Limit assembly on the north and south side of Ben Allen Rd approximately 300' east and west of DW D facing WB traffic. This should include a fluorescent yellow-green 'school' plaque (S4-3P), a 15 mph sign (R2-1) and yellow-green 'when children are present' sign (S5-2). Provide an 'End School Zone' sign (S5-2) on the north side of Ben Allen Road approx. 300' west of DW D facing WB traffic. And, provide a fluorescent yellow-green School Advance Crossing sign (S1-1) with Ahead plaque (W16-9P) on South side of road approx. 300' west of DW D facing EB traffic.
  - e. Once the site plan for the elementary school has been finalized, a traffic engineer should provide a review of the internal circulation, pick-up/drop-off locations, and recommendations for a school zone for internal road network.
  - f. At Broadmoor Drive & the Ellington Parkway Ramps: A full signal warrant analysis should be conducted at both intersections with every phase submittal starting with Phase 3.
  - g. At Broadmoor Drive & Lemont: Install a RRFB on both sides of crosswalk and detectable warning mats.
  - h. Transit improvements coordinated with WeGo.

**GENERAL NOTES**

EXISTING COMMERCIAL STRUCTURES AND USES WITHIN BROADMOOR NEIGHBORHOOD CENTER TO REMAIN.

ALL DEVELOPMENT WITHIN THE BOUNDARIES OF THIS PLAN MEETS THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT AND FAIR HOUSING ACT.

THIS PROPERTY DOES NOT LIE WITHIN AN AREA DESIGNATED AS SPECIAL FLOOD HAZARD AREA ACCORDING TO FEDERAL EMERGENCY MANAGEMENT AGENCY FLOOD INSURANCE MAPS. (REFER TO CIVIL PLAN)

TRASH AND RECYCLING SERVICES TO BE CONTRACTED BETWEEN THE DEVELOPER/OWNER AND A PRIVATE HAULER FOR ALL UNITS ON PRIVATE DRIVES.

USPS MAIL DELIVERY WILL BE PROVIDED BY CENTRALIZED KIOSK/MAIL ROOMS WITHIN COMMERCIAL AND MULTI-FAMILY AND DETACHED DWELLINGS.

**SIDEWALKS AND TRAILS**

THE FINAL SITE PLAN/BUILDING PERMIT SITE PLAN SHALL DEPICT THE REQUIRED PUBLIC SIDEWALKS. ANY REQUIRED GRASS STRIP OR FRONTAGE ZONE AND THE LOCATION OF ALL EXISTING AND PROPOSED VERTICAL OBSTRUCTIONS WITHIN THE REQUIRED SIDEWALK AND GRASS STRIP OR FRONTAGE ZONE. PRIOR TO THE ISSUANCE OF USE AND OCCUPANCY PERMITS, EXISTING VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUTSIDE OF THE REQUIRED SIDEWALK. VERTICAL OBSTRUCTIONS ARE ONLY PERMITTED WITHIN THE REQUIRED GRASS STRIP OR FRONTAGE ZONE.

SIDEWALKS ALONG BROADMOOR DRIVE SHALL MEET MCSP FOR THE ENTIRE PROPERTY FRONTAGE. THE SIDEWALK ALONG BEN ALLEN ROAD SHALL BE 5.5' IN WIDTH AND INCLUDE A 5' PLANTING STRIP FOR THE ENTIRE PROPERTY FRONTAGE UNLESS INDICATED AS AN EXTENSION OF THE PRIMARY TRAIL SYSTEM.

PRIMARY TRAIL SYSTEM SHALL BE CONSTRUCTED TO MEET THE OUTDOOR AMERICAN WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES AND WILL BE PROVIDED AT A MINIMUM WIDTH OF 12' AND SHALL BE CONSTRUCTED TO MEET METRO PARKS AND GREENWAYS STANDARDS AS EACH PHASE OF DEVELOPMENT IS COMPLETED. PRIMARY TRAIL WILL BE LIT MEETING CRITERIA FOR DARK SKY LIGHTING OR OTHER LOW INTENSITY TECHNIQUES APPROVED BY STAFF.

TRAIL AMENITIES ALONG THE PRIMARY TRAIL WILL INCLUDE AT A MINIMUM: A TRAILHEAD NEAR EACH END OF THE PRIMARY TRAIL, PET WASTE DISPENSERS A MINIMUM OF ONE PER 1/4 MILE OF PRIMARY TRAIL, TWO WATER FOUNTAINS SERVING PEOPLE AND PETS ONE BIKE REPAIR STATION.

LIGHTING FOR STREETS AND THROUGHOUT THE DEVELOPMENT WILL UTILIZE LIGHTING MEETING THE CRITERIA FOR DARK SKY COMPLIANCE OR OTHER LOW INTENSITY TECHNIQUES APPROVED BY PLANNING STAFF. ACTUAL FIXTURES/METHODS WILL BE SUBMITTED IN THE FINAL SP.

ALL CONSTRUCTION WITHIN THE ROW IS TO COMPLY WITH ADA AND NDOT STANDARDS AND SPECIFICATIONS.

**STORMWATER/UTILITIES/INFRASTRUCTURE:**

STORMWATER PLANS FOR THIS DEVELOPMENT TO BE PROVIDED IN A MANNER CONSISTENT WITH THE STORMWATER VOLUME 5 LID MANUAL. THIS DRAWING IS FOR ILLUSTRATIVE PURPOSES TO INDICATE THE BASIC PREMISE OF THIS DEVELOPMENT AND PROPOSED STORMWATER LOCATIONS AND METHODS. FINAL DETAILS SHALL BE GOVERNED BY THE APPROPRIATE REGULATIONS AT THE TIME OF THE SUBMITTAL.

SIZE DRIVEWAY CULVERTS PER THE DESIGN CRITERIA SET FORTH BY METRO STORMWATER MANAGEMENT MANUAL.

METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED ACCESS IN ORDER TO MAINTAIN AND REPAIR UTILITIES ON THE SITE.

ALL WATER AND SEWER CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS AND STANDARDS SET FORTH BY METRO WATER SERVICES.

ALL UTILITY BOXES/DEVICES LOCATED ABOVE GROUND WILL BE SCREENED APPROPRIATELY.

ALL PROPOSED NEW PUBLIC UTILITIES AND SERVICES SHALL BE INSTALLED UNDERGROUND.

ALL ACCESS DRIVES AND DRIVEWAYS WITHIN THIS DEVELOPMENT SHALL BE PRIVATE.

ALL PRIVATE DRIVEWAYS/ALLEYS MAY BE PAVED WITH PERVIOUS PAVEMENT WITH MATERIAL AND CROSS-SECTION TO BE APPROVED BY METRO PUBLIC WORKS. NO FRONT-LOADED GARAGES ARE PERMITTED FRONTING ON A PUBLIC ROAD.

ALL SIDEWALKS LOCATED WITHIN THE PUBLIC R.O.W. ARE TO MEET METRO PUBLIC WORKS AND ADA STANDARDS.

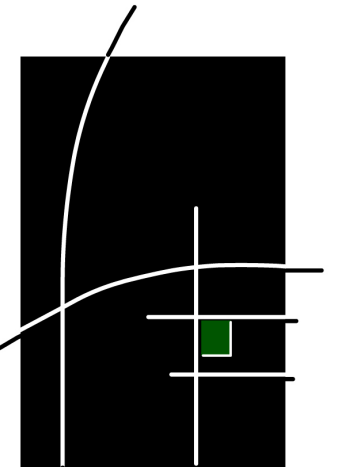
BUFFER FOR ON SITE STREAMS INDICATED IN ACCORDANCE WITH AQUATIC RESOURCE ASSESSMENT PERFORMED BY DAVEY RESOURCES GROUP DATED FEBRUARY 17TH, 2023. DEFINED TOP OF BANK WAS INDICATED FOR THE BEN ALLEN NEIGHBORHOOD CENTER STREAM SECTION. COPIES OF THIS STUDY ARE PROVIDED AS AN ATTACHMENT TO THIS PRELIM SP.

REFER TO CIVIL DRAWINGS FOR ALL OTHER UTILITIES/EASEMENTS.

SITE DATA TABLE	Indicates unchanged from 2018 Prel SP update			
	fallback MUL-A	non-resid sq footage	resl units	parking per code
<b>BROADMOOR CI PHASE 1</b>				** shared parking may be applied within all mixed use centers
Existing Structures			0	
Light Manufacturing		12,000		8
MultiMedia Production		16,800		22
Building A commercial		9,500	0	47
MIXED USE A commercial		8,000	0	50
MIXED USE B commercial		7,000		35
residential (1 BD)			20	20 surface park *combo of park under garage and surface park
Townhomes residential			65	130 surface park
		53,300	85	312
<b>BEN ALLEN CENT PHASE 5</b>	fallback MUL-A			
Multifamily A/B				*combo of park under garage and surface park
(17 2BD, 53 1 BD) residential			70	87 surface park
Live/Work B residential (MAX 2 BD/UNIT)			12	25 surface park
MIXED USE C/D commercial (6 2BD, 18 1 BD) residential		8500		71 surface park
MIXED USE E F community education (15 2BD, 45 1 BD) sr living (multifly)		22500	0	40 surface park
MIXED USE H commercial		2000	0	13 surface park
		33,000	166	326
<b>RESIDENTIAL USI Phases 2, 3, &amp; 4</b>	fallback RM-15			
Cottages residential (single family cottages parked per code at 2 per unit)			109	218 surface park
Townhomes residential (2BD per townhome)			101	202 surface park
Hillside Duplex residential (duplex's parked per code at 2 per unit)			21	42 surface park
			231	462
<b>TOTAL</b>		86,300 square feet	482 units	1,100 parking

**SP CASE NO.: 2017SP-**





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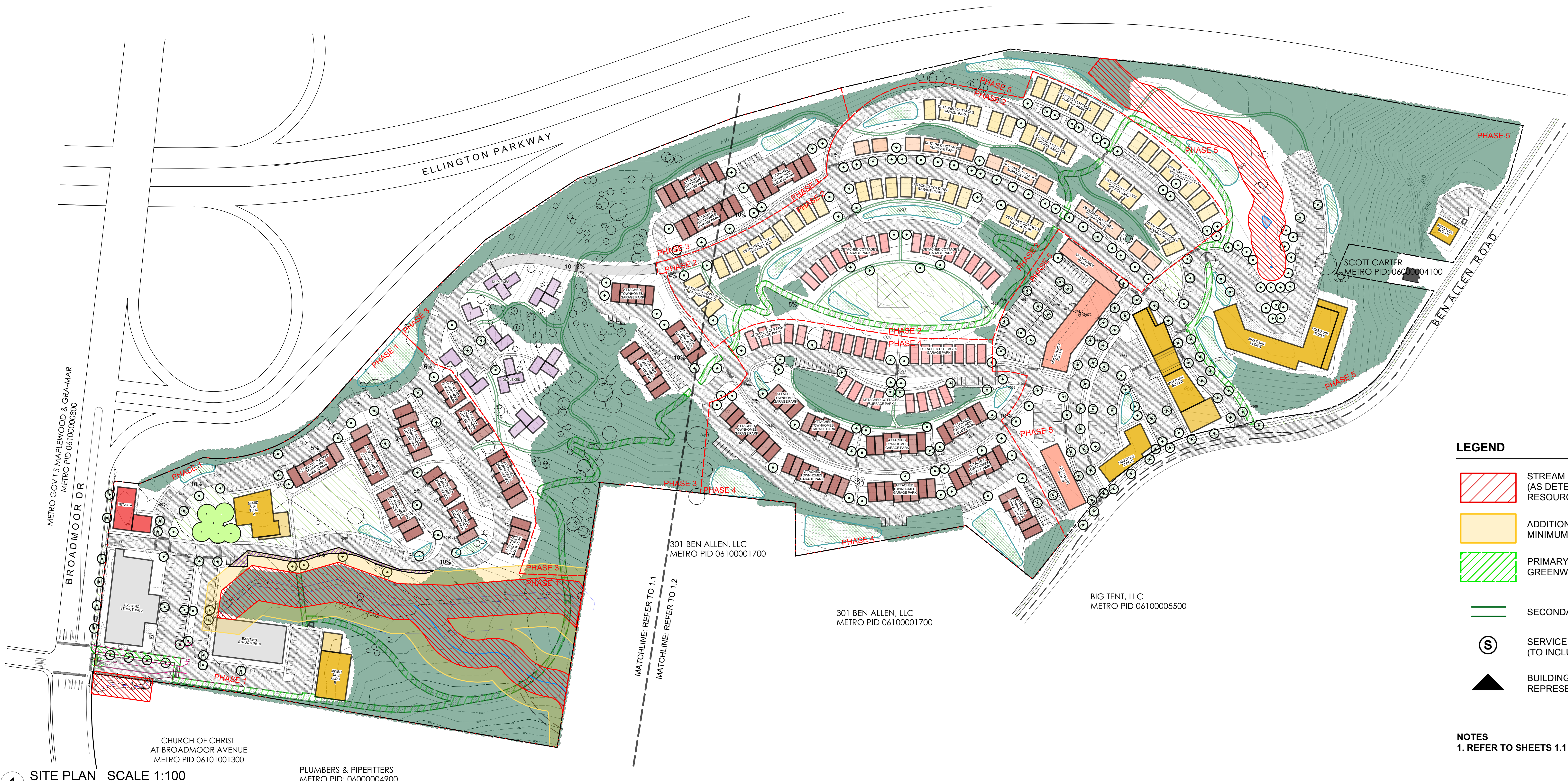
PRELIMINARY SP  
**PROJECT 615 MASTER PLAN**  
NASHVILLE, TENNESSEE

JOB NO.: 22-838  
DATE: 2023-02-21  
RESUBMITTAL

SHEET:

**L1.0**

OVERVIEW



**LEGEND**

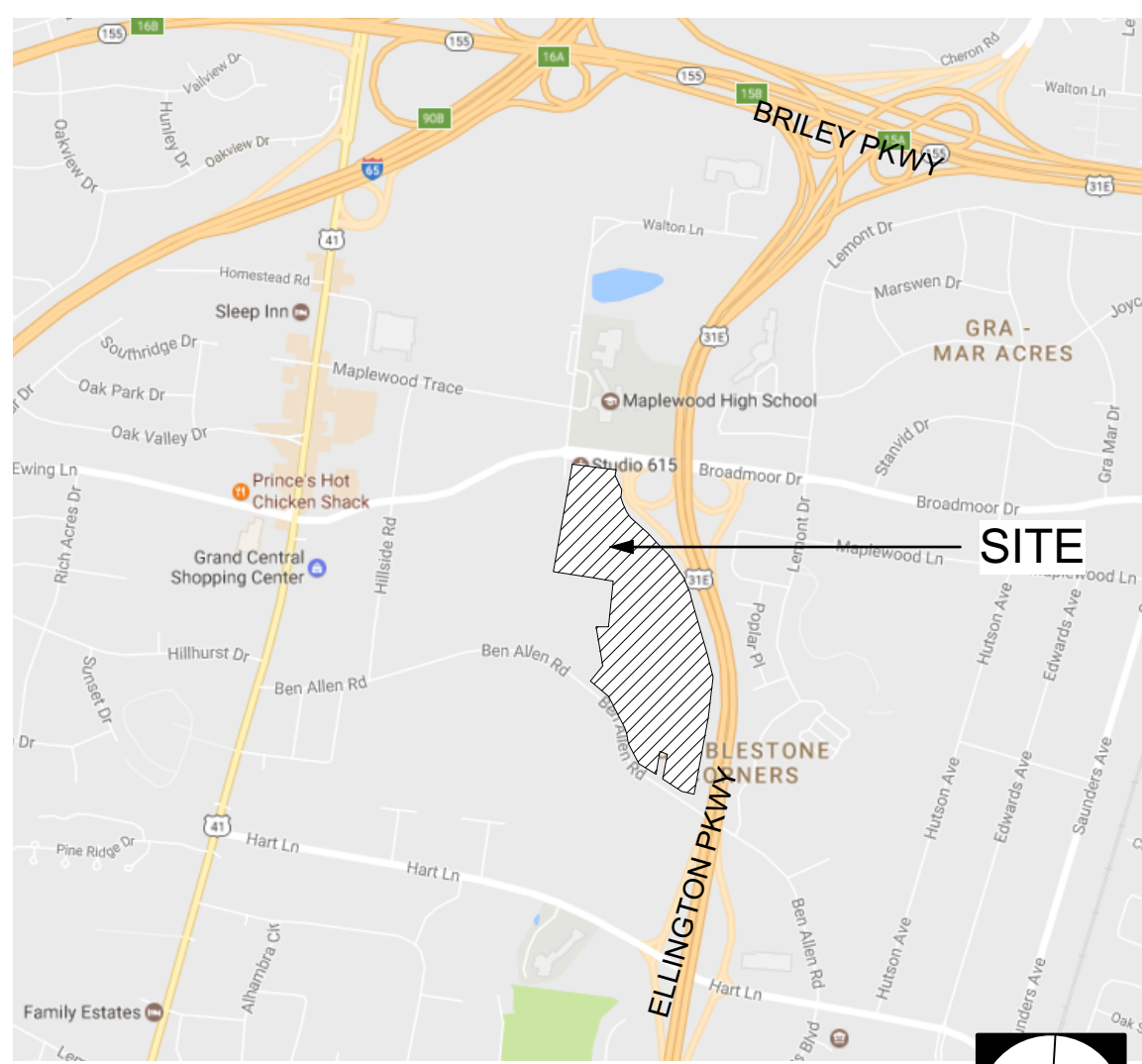
- STREAM BUFFER (AS DETERMINED BY DAVEY RESOURCES GROUP, DATED 2/17/2023)
- ADDITIONAL 30' STREAM BUFFER TO PROVIDE MINIMUM 60' AVERAGE BUFFER ZONE
- PRIMARY TRAIL AND GREENWAY EASEMENT
- SECONDARY TRAIL
- SERVICE AREA (TO INCLUDE WASTE AND RECYCLING FACILITIES)
- BUILDING ORIENTATION: REPRESENTS A PRIMARY ENTRANCE

**NOTES**

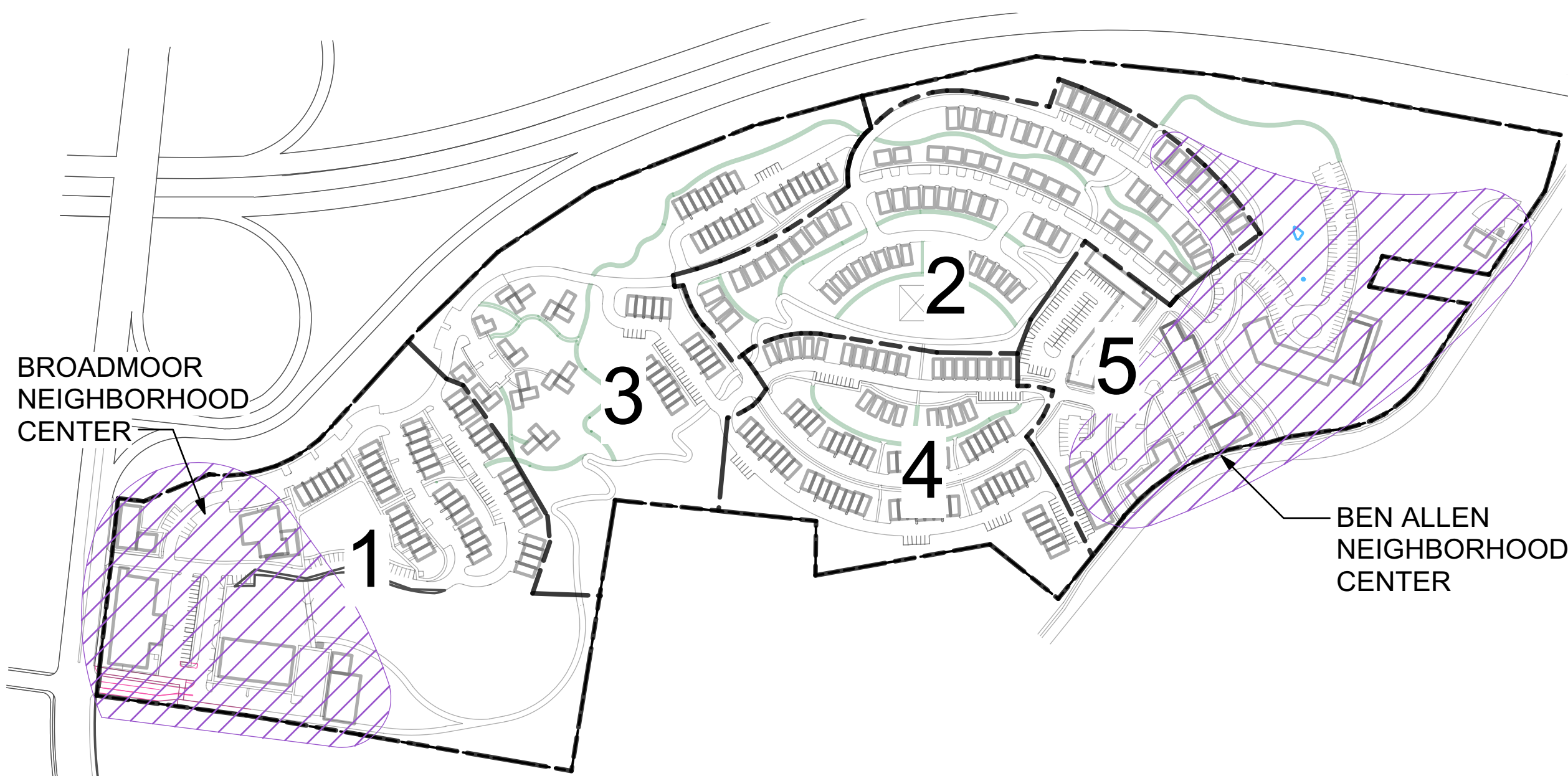
1. REFER TO SHEETS 1.1 AND 1.2 FOR ALL PLAN LABELS

1 SITE PLAN SCALE 1:100

PLUMBERS & PIPEFITTERS  
METRO PID: 06000004900



4 VICINITY MAP



2 PHASING DIAGRAM

**TABLE BASED ON M.U.L. M.U.A STANDARDS FOR LAND USE ZONING DISTRICT LAND USE TABLE ADJUSTMENTS**

**LEGEND**  
P - Permitted by right  
PC - Permitted subject to specific conditions  
SE - Permitted by special exception  
A - Permitted as accessory to a principal use  
O - Permitted only with an overlay district  
BLUE: Added to SP  
STRIKE: Excluded from SP

**NOTE:** THE LAND USE TABLE HAS BEEN ADJUSTED TO ELIMINATE COMMERCIAL AMUSEMENT (OUTSIDE), AND TO INCLUDE SMALL OUTDOOR MUSIC EVENT IN THE PHASE 1 BROADMORE NODE ONLY.

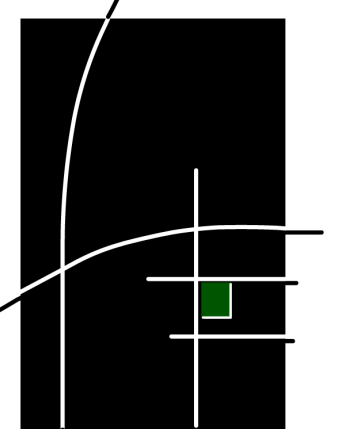
NONRESIDENTIAL USES ARE LIMITED TO PHASES 1 & 5 WITHIN THE AREAS DESIGNATED AS NEIGHBORHOOD CENTER ON THE PHASING DIAGRAM.

nonresidential uses are limited to Phases 1 and 5 within the areas designated as Neighborhood Center on the phasing diagram.

<b>Residential Uses:</b>	<b>Educational Uses:</b>	<b>Industrial Uses:</b>
Single-family P	Business school P	Artisan distillery P
Two-family P	College or university P	Distributive business/wholesale PC
Multifamily P	Community education P	Manufacturing, Artisan P
Elderly Housing P	Dormitory P	Manufacturing, medium P
Boarding house P	Personal instruction P	Manufacturing, light P
Consignment sale P	Vocational school PC	Microbrewery P
Garage sale A	<b>Commercial Uses:</b>	Tasting room P
Historic bed and breakfast homestay O	Animal boarding facility P	Warehouse PC
Historic home events P	ATM PC	<b>Transportation Uses:</b>
Home occupation A	Auction house P	Bus transfer station SE
Security residence P	Automobile convenience PC	Computer rail SE
STRP - Owner Occupied A^	Automobile parking P	<b>Utility Uses:</b>
<b>Institutional Uses:</b>	Bar or nightclub P	Power/gas substation PC
Cultural center P	Bed and breakfast inn P	Reservoir/water tank PC
Day care center (Up to 75) PC	Bad and breakfast inn P	Safety services P
<b>Day care center (Over-75)</b> PC	<b>Beer and cigarette market</b> P	Waste water treatment SE
Day care home- Large PC	Business service P	Water/sewer pump station P
Daycare Home - Small PC	<b>Car wash</b> PC	Water treatment plant SE
Day care - Parent's day out A	Community gardening (noncommercial) P	Wind energy facility (small) PC
School day care A	Custom assembly PC	<b>Waste Management Uses:</b>
Monastery or convent P	Donation center, drop-off PC	Construction/demolition waste processing (project specific) PC
<b>Orthopedic</b> P	Flea market P	Recycling collection center A
Religious institution P	Furniture store P	<b>Recreation and Entertainment Uses:</b>
<b>Office Uses:</b>	Grocery store P	Club P
<b>Alternative Financial Services</b> PC	Home improvement sales PC	Commercial (inside) P*
Financial institution P	Hotel/motel A	Commercial (outside) P
General office P	Inventory stock A	<b>Commercial-amusement (outside)</b> P
Leasing/sales office P	Mobile storage unit PC	<b>Driving range</b> PC
<b>Medical Uses:</b>	Nano brewery P	<b>Golf course</b> P
Assisted-care living P	Personal care services P	Greenway P
Hospice P	<b>Restaurant, fast-food</b> P	Park P
Medical appliance sales P	Restaurant, full-service P	Recreation center P
Medical office P	Restaurant, take-out P	Rehearsal hall P
Medical or scientific lab P	Retail P	<b>Small outdoor music event</b> P*
Nursing home P	Self-service storage P	<b>Temporary festival</b> SE*
Outpatient clinic P	<b>Short-Term Rental - Not Owner-Occupied</b> PC^	Theater P
<b>Rehabilitation services</b> P	Communication Uses:	<b>Other Uses:</b>
Residence for handicapped, more than eight individuals P	Amateur radio antenna P	On-site agricultural sales P
Veterinarian PC	Audio/video tape transfer P	Pond/lake P
	Communications hut PC	
	Multi-media production P	
	Printing and publishing P	
	Satellite dish P	
	Telecommunication PC	
	Facility P	

4 LAND USE TABLE





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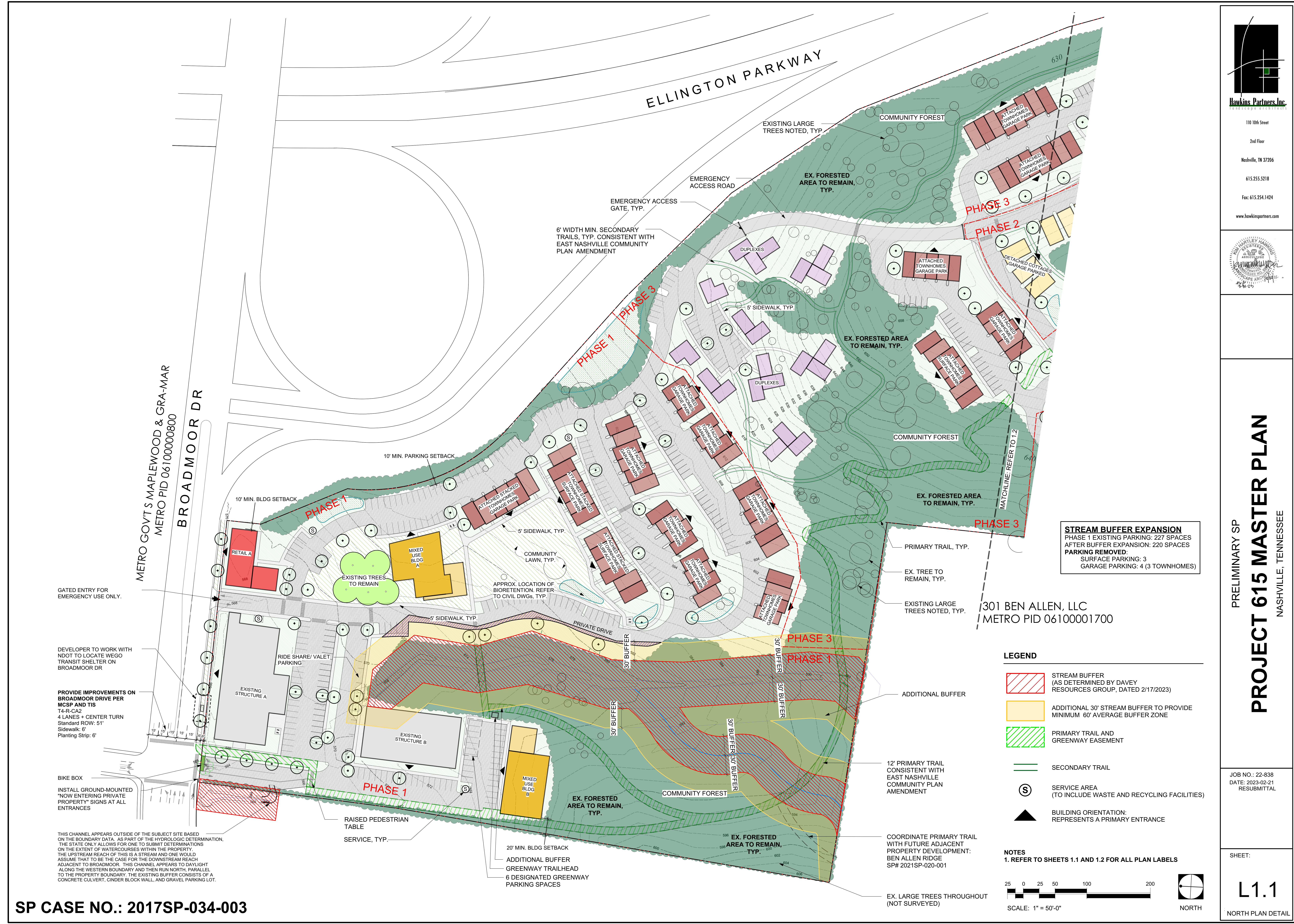


PRELIMINARY SP  
PROJECT 615 MASTER PLAN  
NASHVILLE, TENNESSEE

JOB NO.: 22-838  
DATE: 2023-02-21  
RESUBMITTAL

SHEET:

L1.1  
NORTH PLAN DETAIL



**STREAM BUFFER EXPANSION**  
PHASE 1 EXISTING PARKING: 227 SPACES  
AFTER BUFFER EXPANSION: 220 SPACES  
**PARKING REMOVED:**  
SURFACE PARKING: 3  
GARAGE PARKING: 4 (3 TOWNHOMES)

- LEGEND**
- STREAM BUFFER (AS DETERMINED BY DAVEY RESOURCES GROUP, DATED 2/17/2023)
  - ADDITIONAL 30' STREAM BUFFER TO PROVIDE MINIMUM 60' AVERAGE BUFFER ZONE
  - PRIMARY TRAIL AND GREENWAY EASEMENT
  - SECONDARY TRAIL
  - SERVICE AREA (TO INCLUDE WASTE AND RECYCLING FACILITIES)
  - BUILDING ORIENTATION: REPRESENTS A PRIMARY ENTRANCE

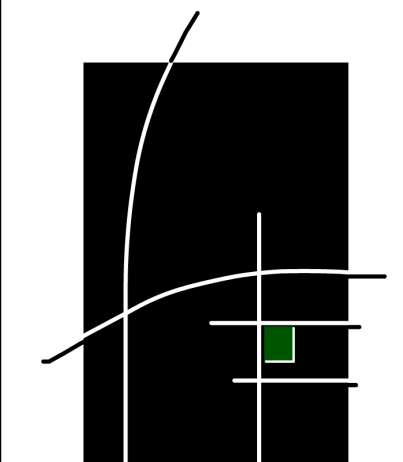
**NOTES**  
1. REFER TO SHEETS 1.1 AND 1.2 FOR ALL PLAN LABELS

SCALE: 1" = 50'-0"

NORTH

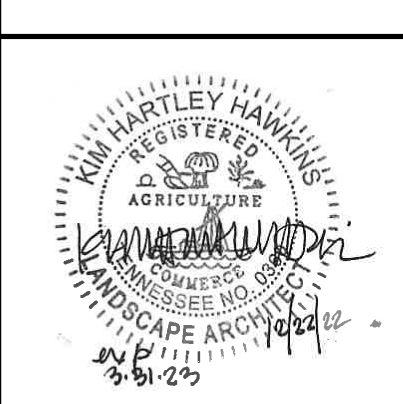
SP CASE NO.: 2017SP-034-003





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Fax: 615.254.1424  
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PRELIMINARY SP

# PROJECT 615 MASTER PLAN

NASHVILLE, TENNESSEE

JOB NO.: 22-838  
DATE: 2023-02-21  
RESUBMITTAL

SHEET:

## L1.2

SOUTH PLAN DETAIL

### ELLINGTON PARKWAY

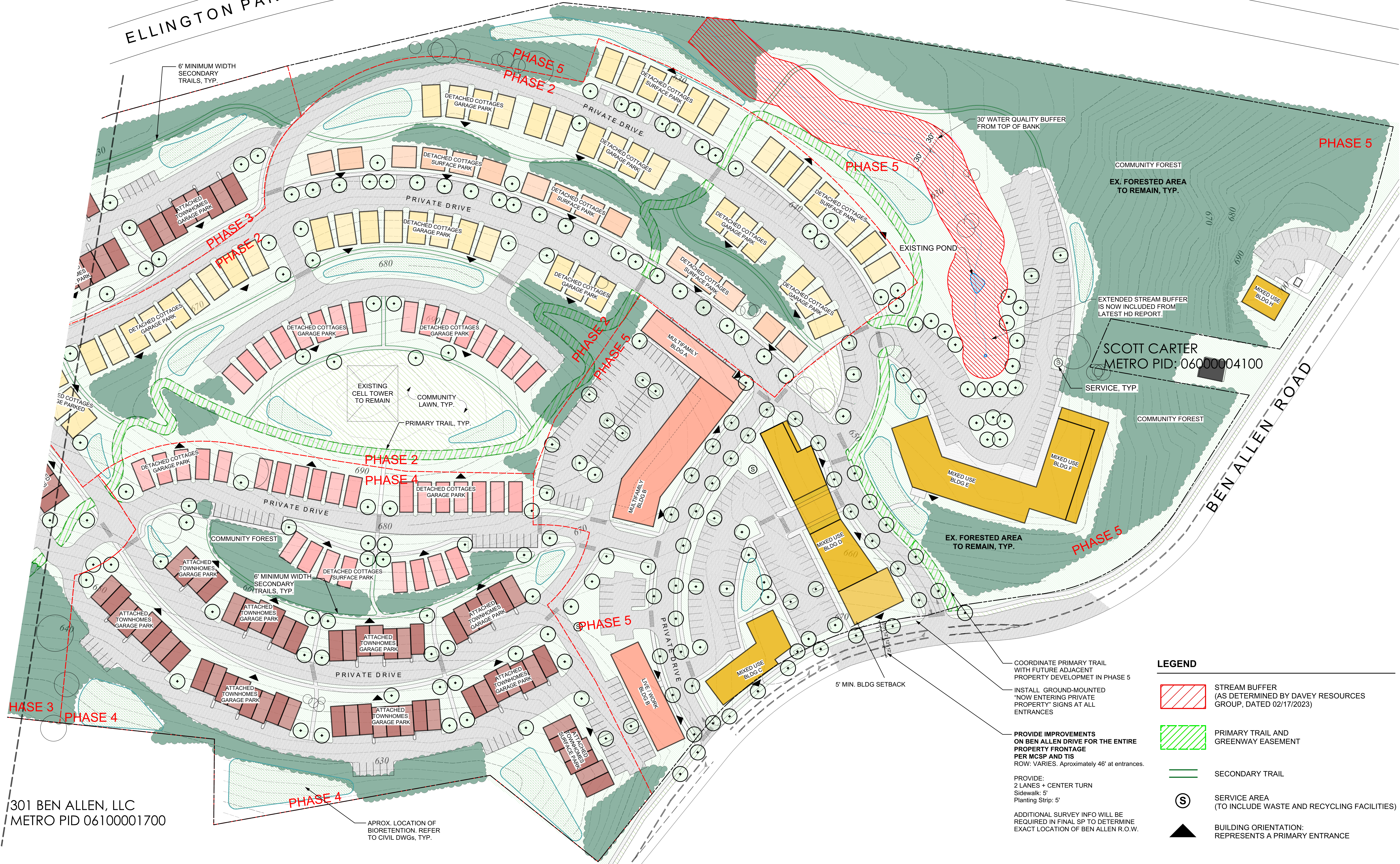
### BEN ALLEN ROAD

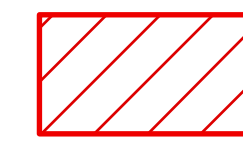
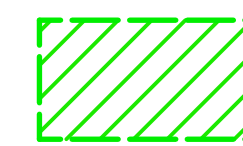



301 BEN ALLEN, LLC  
METRO PID 06100001700

301 BEN ALLEN, LLC  
METRO PID 06100001700

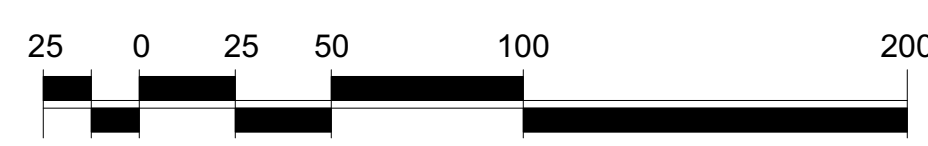
BIG TENT, LLC  
METRO PID 06100005500

SP CASE NO.: 2017SP-034-003

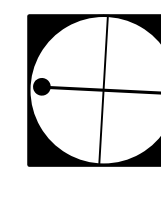


- LEGEND**
-  STREAM BUFFER (AS DETERMINED BY DAVEY RESOURCES GROUP, DATED 02/17/2023)
  -  PRIMARY TRAIL AND GREENWAY EASEMENT
  -  SECONDARY TRAIL
  -  SERVICE AREA (TO INCLUDE WASTE AND RECYCLING FACILITIES)
  -  BUILDING ORIENTATION: REPRESENTS A PRIMARY ENTRANCE

**NOTES**  
1. REFER TO SHEETS 1.1 AND 1.2 FOR ALL PLAN LABELS

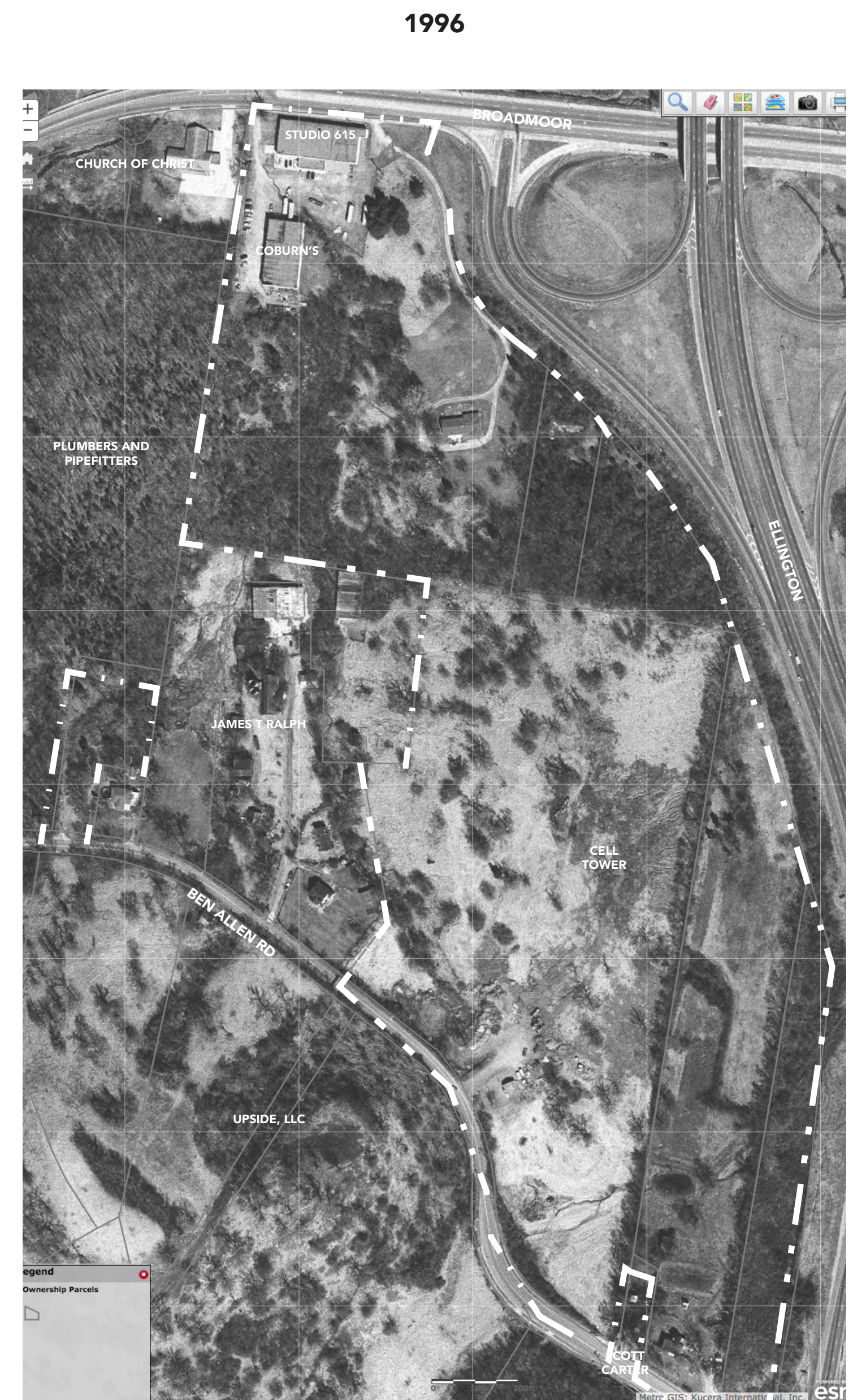
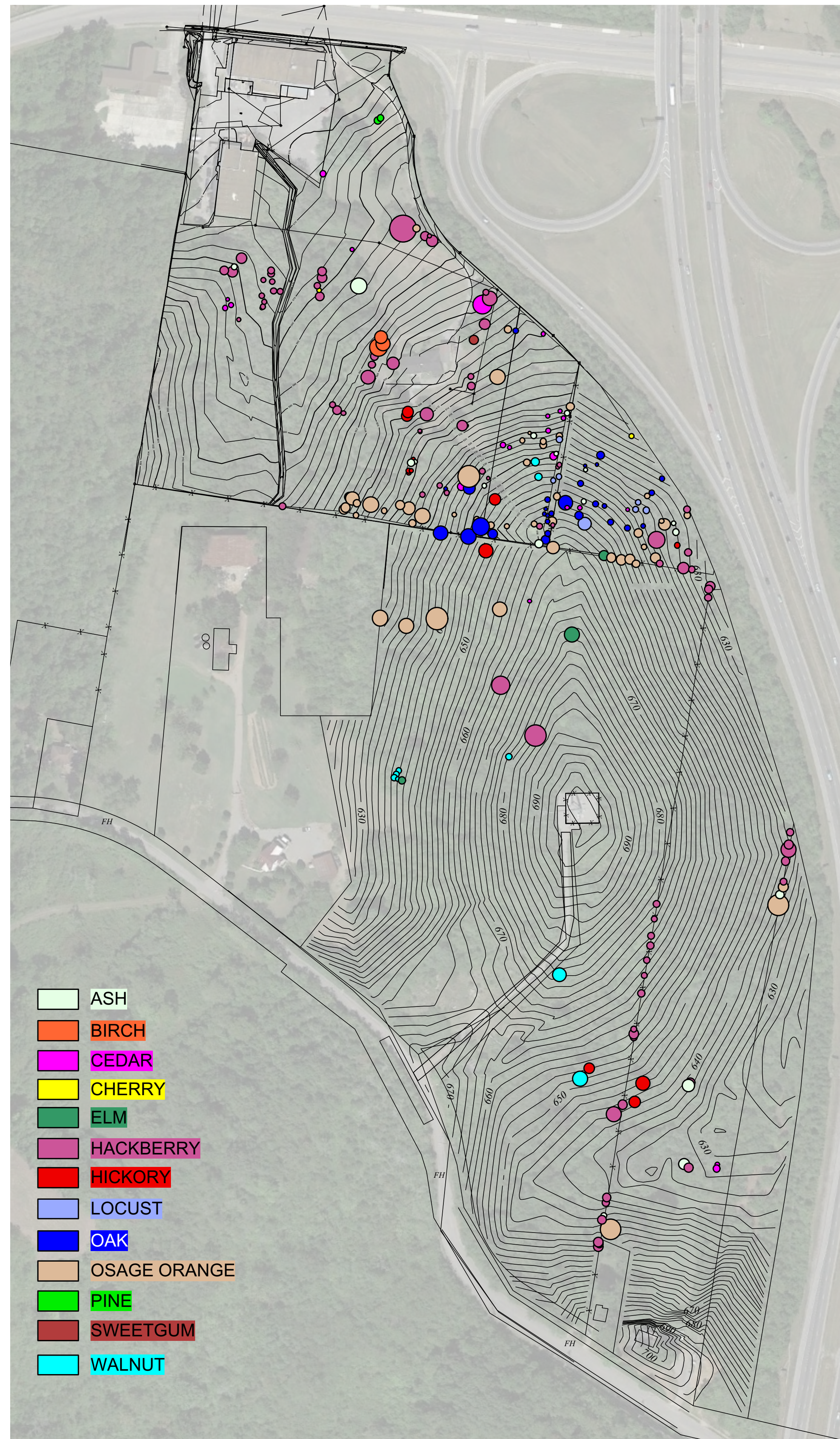


SCALE: 1" = 50'-0"

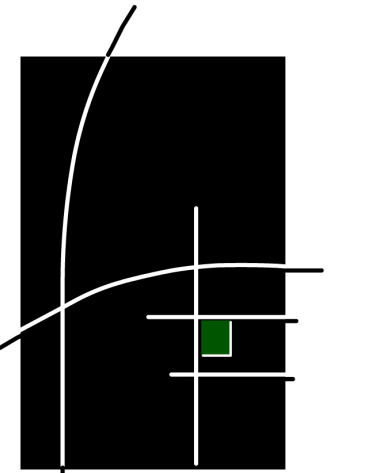


NORTH





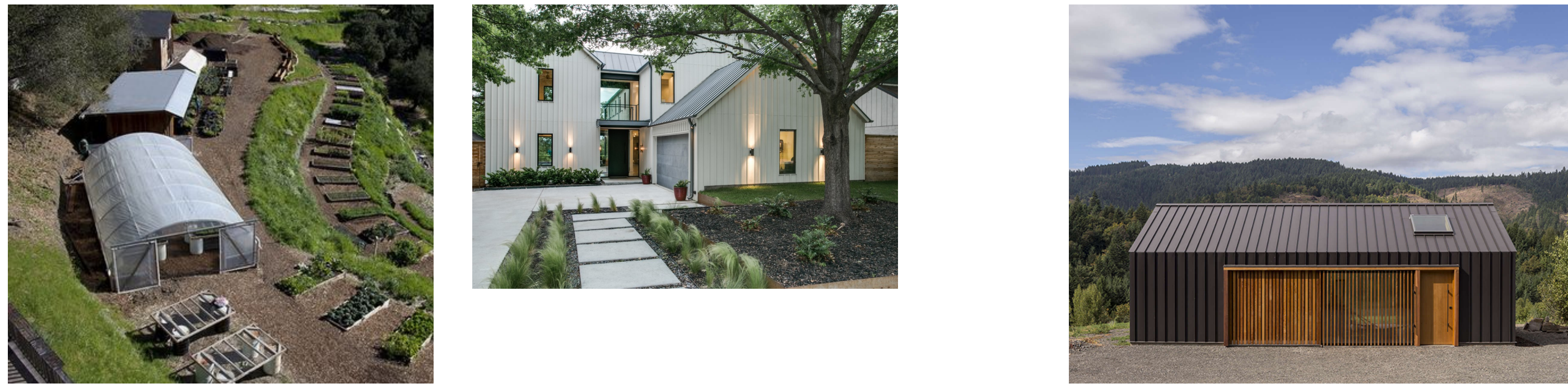
A Partial Tree Survey was conducted to identify the larger, more prominent trees on the site. Those trees are identified on the proposed plans in simple black circles and labeled. As seen in the above historical aerials, the site was primarily open farmland for decades up until the recent past when it was left to reforest. As a result, the forested areas of the site are young first-growth forests. The plan identifies keeping a number of these areas as indicated in dark green on the proposed plan. A more detailed tree survey will be done as needed and can be submitted with the Final SP.







1 ARCHITECTURAL REFERENCE IMAGES: NEIGHBORHOOD CENTER AT BROADMOOR



3 ARCHITECTURAL REFERENCE IMAGES: NEIGHBORHOOD CENTER AT BEN ALLEN



6 ARCHITECTURAL REFERENCE IMAGES: COTTAGES



2 ARCHITECTURAL REFERENCE IMAGES: HILLSIDE DUPLEXES



4 ARCHITECTURAL REFERENCE IMAGES: DETACHED DWELLINGS W/ ACCESSORY UNIT



7 ARCHITECTURAL REFERENCE IMAGES: TOWNHOMES

**Character Defining Guidelines**

The final building elevations shall be submitted with the final site plan and shall be consistent with the following architectural standards:

**General Neighborhood Center Notes**

- A. Storefronts, restaurants and offices shall be activated from and front onto Broadmoor and Ben Allen
- B. Mixed use buildings shall provide access via glazed doors from both the front and rear of the structure.

**Neighborhood Center at Broadmoor: "Maker Center"**

This is the first of two mixed-use areas (One each fronting on Broadmoor and Hart Lane). This area includes the existing Studio 615 Building and an existing Steel Fabrication Building. New structures in this area are intended to complement the existing buildings to surround parking for visitors to these areas and events on The Green.

- A. New construction and materials should be compatible with the existing industrial maker space/fabrication buildings.
- B. Use of industrial metal panels and roofing are encouraged, although materials aren't limited to these choices.
- C. Use of EIFS, vinyl siding and aluminum clapboard siding and fake wood grain siding are prohibited
- D. Ground floors shall include a minimum of 40% glazing; additional stories shall include a minimum of 25% glazing.

A central feature of the community, The Green is seen as a site for outdoor concerts/maker's markets/etc. It is lined with 2-over-1 townhomes that provide the ability to have maker spaces/small retail/studio spaces lining the green. The outdoor pavilion building may contain a stage/restaurant/gift shop/green rooms and restrooms.

**Neighborhood Center at Ben Allen: "The Farmstead"**

This second mixed-use area is more rural in character and provides areas for agricultural tourism related uses such as a small farm, farmers' market, hotel, farm to table restaurant(s), retail focused on locally made products(although not limited to this) and camping pavilions. There is also an opportunity for providing housing for seniors and younger residents in this area with the provision of micro-units and upper level residents with close access to services.

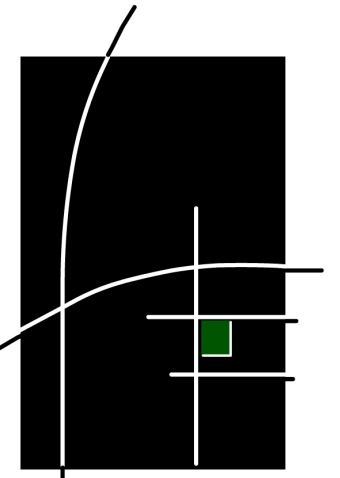
- A. Wood and agricultural metal siding and metal roofs are encouraged, although materials are not limited to these choices.
- B. Use of EIFS, vinyl siding and aluminum clapboard siding and fake wood grain siding are prohibited
- C. Ground floors shall include a minimum of 30% glazing or open area; additional stories shall include a minimum of 25% glazing.
- D. New structures are encouraged to reference agricultural buildings and farm groupings, although this is not a requirement.

**Residential: Townhomes, Duplexes, Cottages and Detached Dwellings with Accessory Units**

- Residential is included throughout. Townhomes are concentrated within phase one. While phases two, three and four contain a diverse mix of housing types suited for trail-oriented and hillside development such as duplexes, detached housing with accessory dwelling, and cottages.
- A. Front entry garages and parking in front of dwellings are permitted as a way to work with slope.
  - B. Dwellings may have main front door facing walking trails in lieu of vehicular drives.
  - C. A variety of materials and building forms are encouraged.
  - D. Use of EIFS, vinyl siding and aluminum clapboard siding and fake wood grain siding are prohibited
  - E. Duplexes:
    - a. May have primary entrance on primary or secondary trails.
    - b. May be units set on piers in order to work with grade.
  - F. For Townhomes, Cottages and Detached Dwellings with Accessory Units:
    - a. Building facades facing a street, provide one principal entrance and a minimum of 25% glazing.

**BUILDING ORIENTATION**  
All mixed use buildings will provide a primary point of access to a Public Road and/or Private Drive. All residential structures will have a primary entry onto Common Open Space or Private Road.

5 ARCHITECTURAL GUIDELINES



Hawkins Partners, Inc.  
ARCHITECTURE

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PRELIMINARY SP  
**PROJECT 615 MASTER PLAN**  
NASHVILLE, TENNESSEE

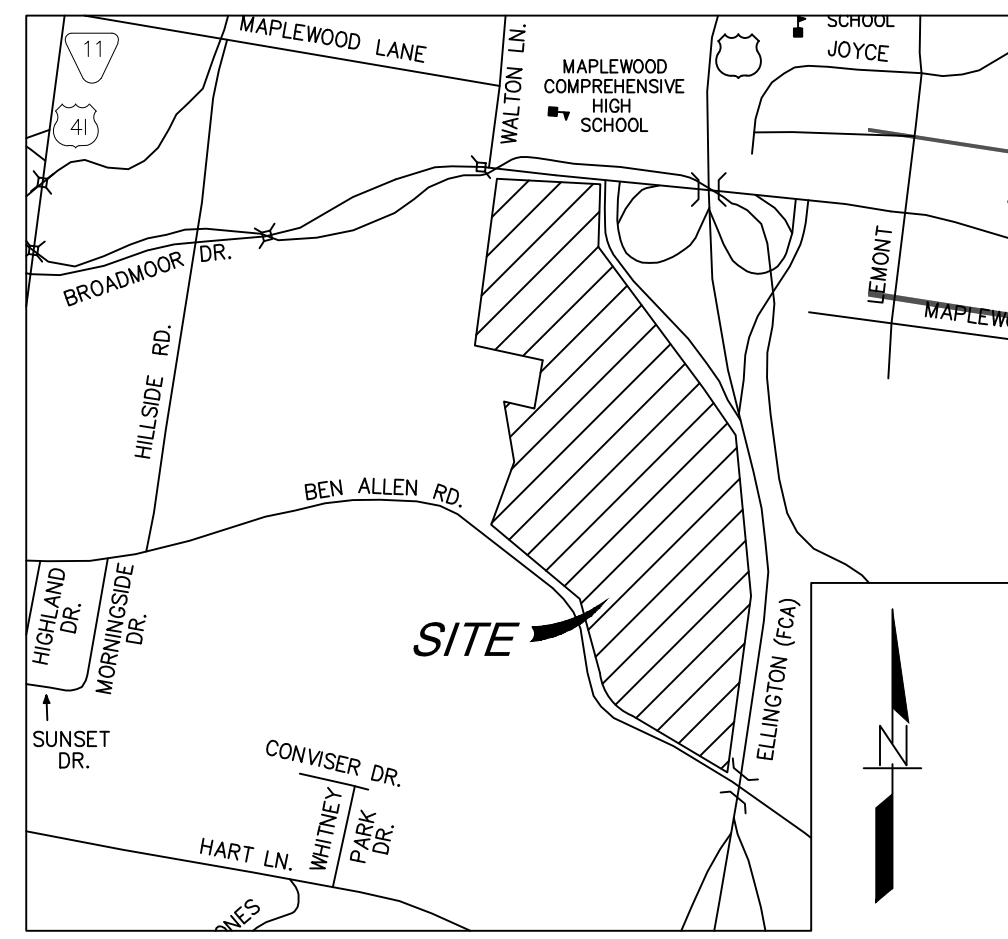
JOB NO.: 22-838  
DATE: 2023-02-21  
RESUBMITTAL

SHEET:

**A2.0**

ARCHITECTURE  
GUIDELINES





VICINITY MAP  
NTS

**METRO PUBLIC WORKS NOTES:**

1. THE FINAL CONSTRUCTION DRAWINGS SHALL COMPLY WITH THE DESIGN REGULATIONS ESTABLISHED BY THE DEPARTMENT OF PUBLIC WORKS, IN EFFECT AT THE TIME OF THE APPROVAL OF THE PRELIMINARY DEVELOPMENT PLAN OR FINAL DEVELOPMENT PLAN OR BUILDING PERMIT, AS APPLICABLE. FINAL DESIGN MAY VARY BASED ON FIELD CONDITIONS.
2. ALL CONSTRUCTION WITHIN THE RIGHT OF WAY SHALL COMPLY WITH ADA AND METRO PUBLIC WORKS STANDARDS AND SPECIFICATIONS.
3. THERE SHALL BE NO VERTICAL OBSTRUCTIONS (SIGNS, POWER POLES, FIRE HYDRANTS, ETC.) WITHIN THE PROPOSED SIDEWALKS. VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUT OF THE PROPOSED SIDEWALKS, WHERE APPLICABLE.

**METRO PLANNING NOTE:**

1. THE FINAL SITE PLAN/BUILDING PERMIT SHALL DEPICT THE REQUESTED PUBLIC SIDEWALKS. ANY REQUESTED GRASS STRIP OR FRONTAGE ZONE AND THE LOCATION OF ALL EXISTING AND PROPOSED VERTICAL OBSTRUCTIONS WITHIN THE REQUESTED SIDEWALK AND GRASS STRIP OR FRONTAGE ZONE. PRIOR TO THE ISSUANCE OF USE AND OCCUPANCY PERMITS, EXISTING VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUTSIDE OF THE REQUESTED SIDEWALK WHERE FEASIBLE. VERTICAL OBSTRUCTIONS ARE ONLY PERMITTED WITHIN THE REQUIRED GRASS STRIP OR FRONTAGE ZONE.

**FEMA NOTE:**

1. NO PORTION OF THIS PARCEL DESCRIBED HEREON LIES WITHIN FLOOD HAZARD AREA IN ACCORDANCE WITH INSURANCE RATE MAP PANEL NUMBERS 4707020203H AND 470702021H, DATED APRIL 5, 2017.

**METRO WATER SERVICES NOTES:**

1. ANY EXCAVATION, FILL OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO. 78-86 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.
2. METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED INGRESS AND EGRESS AT ALL TIMES IN ORDER TO MAINTAIN, REPAIR, REPLACE AND INSPECT ANY STORMWATER FACILITIES WITHIN THE PROPERTY.

**STORMWATER NOTES:**

1. ANY EXCAVATION, FILL OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO. 78-86 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.
2. THIS DRAWING IS FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT. THE FINAL LOT COUNT AND DETAILS OF THE PLAN SHALL BE GOVERNED BY THE APPROPRIATE REGULATIONS AT THE TIME OF FINAL APPLICATION.
3. METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED INGRESS AND EGRESS AT ALL TIMES IN ORDER TO MAINTAIN, REPAIR, REPLACE AND INSPECT ANY STORMWATER FACILITIES WITHIN THE PROPERTY.
4. SIZE DRIVEWAY CULVERTS PER THE DESIGN CRITERIA SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL (MINIMUM DRIVEWAY CULVERT IN METRO ROW IS 15" CMP).
5. PROJECT INTENT IS TO BE REDEVELOPED PER THE REQUIREMENTS OF VOLUME 5 (UD) OF THE STORMWATER MANUAL. DETENTION WILL BE PROVIDED OR POST DEVELOPED RUNOFF WILL BE LESS THAN PREDEVELOPED RUNOFF DUE TO LID IMPLEMENTATION.

**ON-SITE PARKING NOTE:**

1. ALL USES TO BE PARKED ON SITE PER METRO ZONING ORDINANCE. PARKING TOTALS SHALL BE VERIFIED WITH FINAL DESIGN AND USE BEFORE ANY PERMITS ARE ISSUED.

**FIRE MARSHALL NOTES:**

1. NEW COMMERCIAL DEVELOPMENTS SHALL BE PROTECTED BY A FIRE HYDRANT THAT COMPLES WITH THE 2006 EDITION OF NFPA 1 TABLE F. TO SEE TABLE F GO TO: [HTTP://WWW.NASHFIRE.ORG/PRE/TABLES1.HTM](http://www.nashfire.org/pre/tables1.htm)
2. PROJECT ENGINEER NEEDS TO MEET WITH THE FIRE MARSHAL'S OFFICE CONCERNING THIS PROJECT.
3. NO PART OF ANY BUILDING SHALL BE MORE THAN 500 FT. FROM A FIRE HYDRANT VIA A HARD SURFACE ROAD. METRO ORDINANCE 095-1541 SEC. 1568.0208
4. ALL FIRE DEPARTMENT ACCESS ROADS SHALL BE 20 FT MINIMUM WIDTH AND SHALL HAVE AN UNOBSTRUCTED VERTICAL CLEARANCE OF 13.6 FT.
5. ALL DEAD END ROADS OVER 100 FT. IN LENGTH REQUIRE A 100 FT. DIAMETER TURNAROUND. THIS INCLUDES TEMPORARY TURNAROUNDS.
6. TEMPORARY T-TYPE TURNAROUNDS THAT LAST NO MORE THAN ONE YEAR SHALL BE APPROVED BY THE FIRE MARSHAL'S OFFICE.
7. IF MORE THAN THREE STORIES ABOVE GRADE, CLASS I STANDPIPE SYSTEM SHALL BE INSTALLED.
8. IF MORE THAN ONE STORY BELOW GRADE, CLASS I STANDPIPE SYSTEM SHALL BE INSTALLED.
9. WHEN A BRIDGE IS REQUIRED TO BE USED AS PART OF A FIRE DEPARTMENT ACCESS ROAD, IT SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH NATIONALLY RECOGNIZED STANDARDS.
10. A FIRE HYDRANT SHALL BE PROVIDED WITHIN 100 FT. OF THE FIRE DEPARTMENT CONNECTIONS.
11. FIRE HYDRANTS SHALL BE IN SERVICE BEFORE ANY COMBUSTIBLE MATERIAL IS BROUGHT ON SITE.

METRO GOV'TS OF WILKESBORO & GRA-MAR  
METRO PID 0610000800

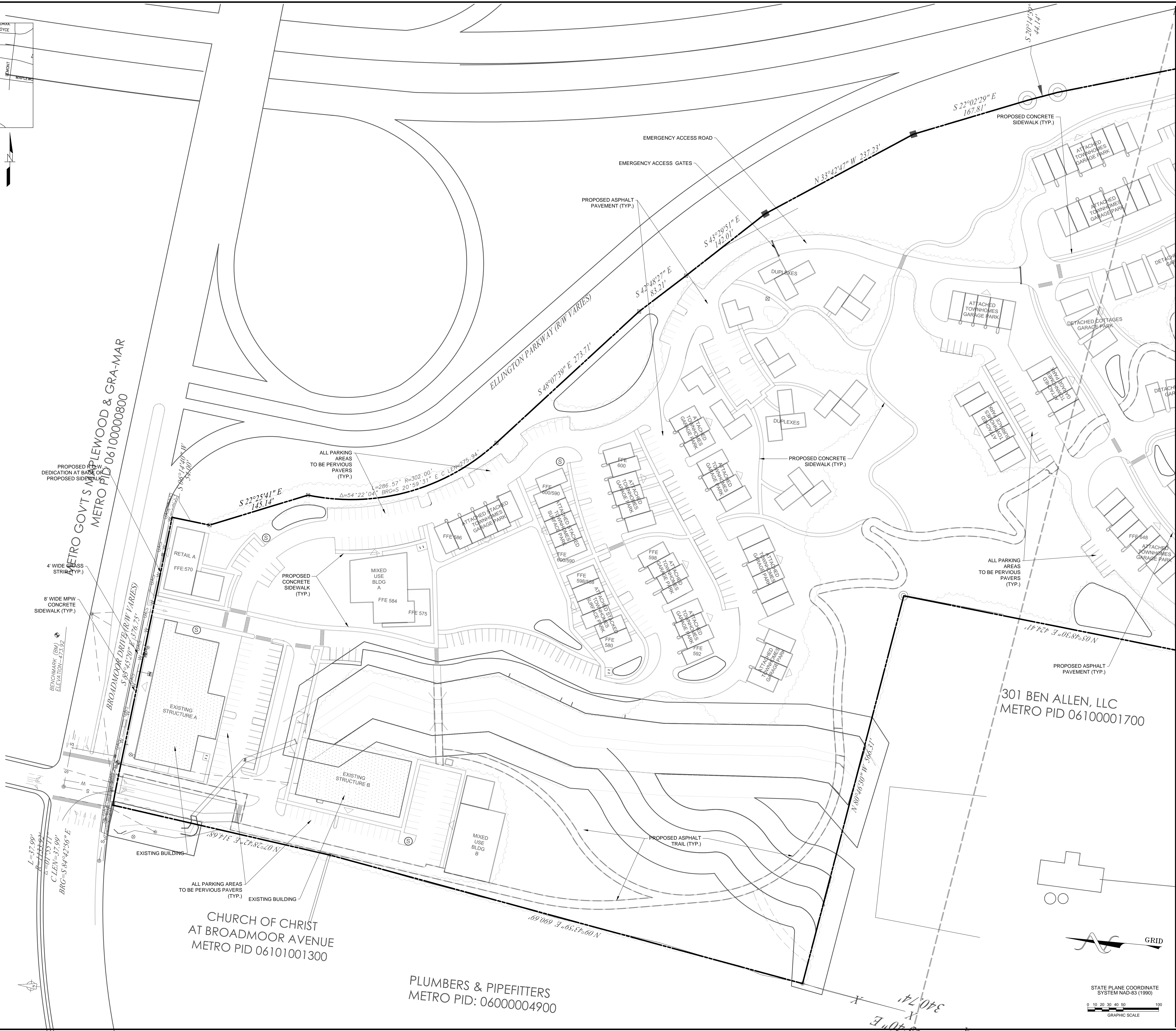
BROADMOOR DRIVE (R/W VARIES)  
S 88°45'20" E 376.25'

4' WIDE GRASS STRIP (TYP.)

8' WIDE MPW CONCRETE SIDEWALK (TYP.)

BENCHMARK (BM) ELEVATION = 43.92'

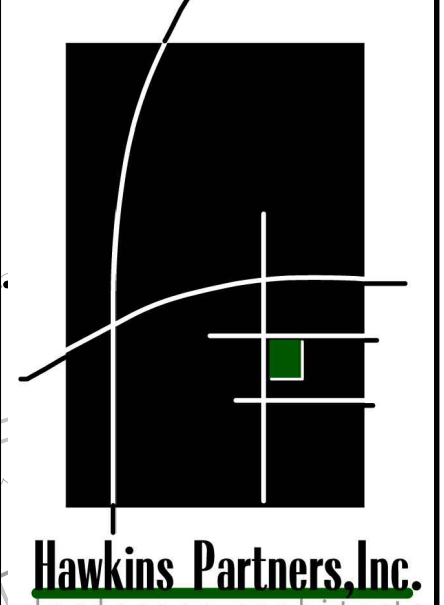
L=37.00'  
C=117.3317'  
C LEN=37.99'  
BAG=S 84°42'56" E



CHURCH OF CHRIST  
AT BROADMOOR AVENUE  
METRO PID 06101001300

PLUMBERS & PIPEFITTERS  
METRO PID: 06000004900

301 BEN ALLEN, LLC  
METRO PID 06100001700



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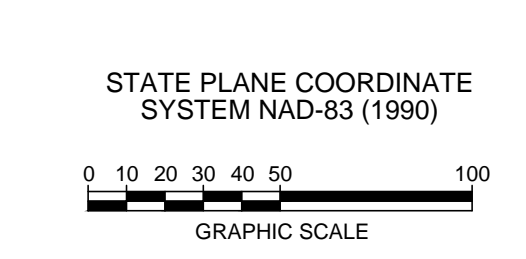
PRELIMINARY  
SP  
02-20-23

PRELIMINARY SP  
**BROADMOOR AND BEN ALLEN MASTER PLAN**  
NASHVILLE, TENNESSEE

JOB NO.: 18-222  
DATE: 12-22-22

SCALE: 1" = 50'-0"  
SHEET:

**C1.0**  
SITE LAYOUT



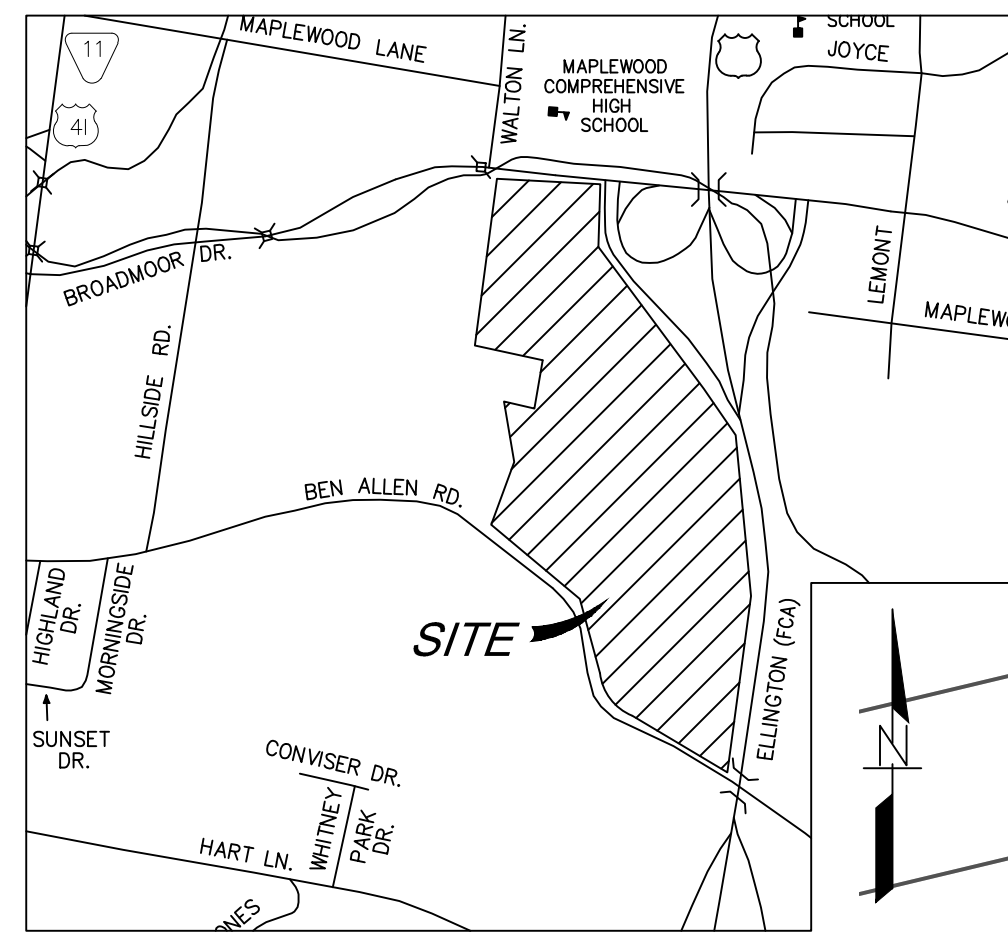
**BCA**  
**BARGE CIVIL ASSOCIATES**

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NASHVILLE, TN 37209  
615.356.9911 BCAcivil.com  
BCA FILE NO. 2872-02

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VICINITY MAP NTS

**METRO PUBLIC WORKS NOTES:**

1. THE FINAL CONSTRUCTION DRAWINGS SHALL COMPLY WITH THE DESIGN REGULATIONS ESTABLISHED BY THE DEPARTMENT OF PUBLIC WORKS, IN EFFECT AT THE TIME OF THE APPROVAL OF THE PRELIMINARY DEVELOPMENT PLAN OR FINAL DEVELOPMENT PLAN OR BUILDING PERMIT, AS APPLICABLE. FINAL DESIGN MAY VARY BASED ON FIELD CONDITIONS.
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**FEMA NOTE:**

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**METRO WATER SERVICES NOTES:**

1. ANY EXCAVATION, FILL OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO. 78.580 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.
2. METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED INGRESS AND EGRESS AT ALL TIMES IN ORDER TO MAINTAIN, REPAIR, REPLACE AND INSPECT ANY STORMWATER FACILITIES WITHIN THE PROPERTY.

**STORMWATER NOTES:**

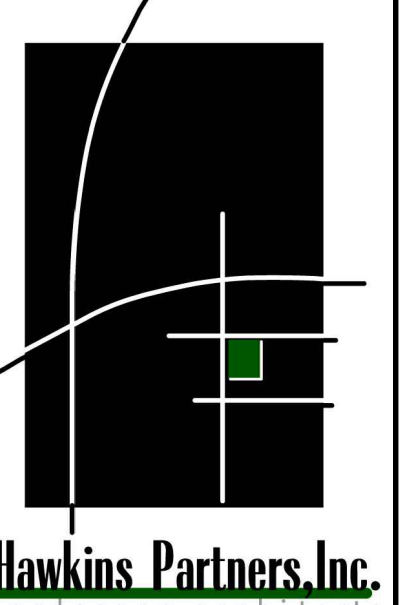
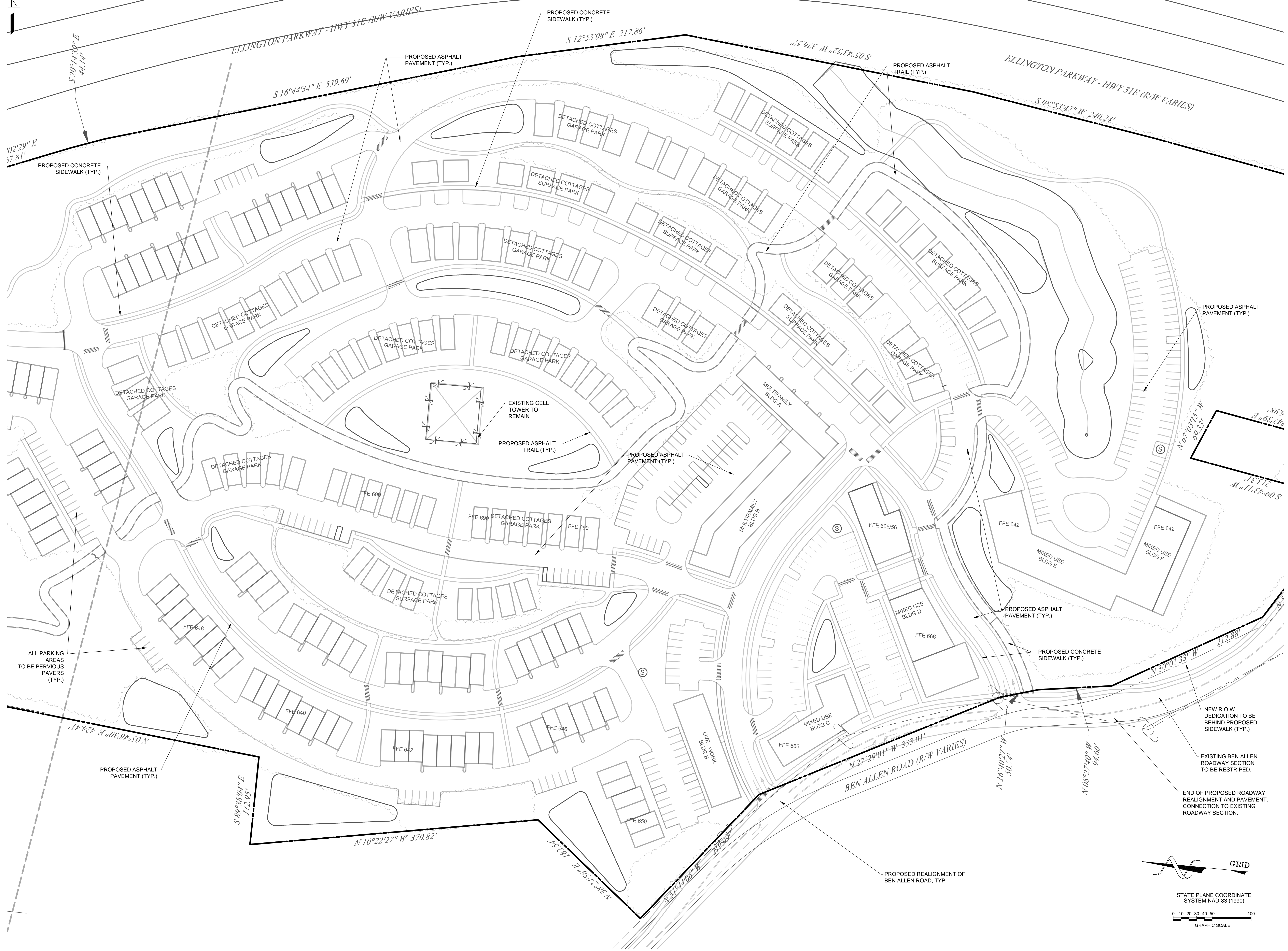
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7. IF MORE THAN THREE STORIES ABOVE GRADE, CLASS I STANDPIPE SYSTEM SHALL BE INSTALLED.
8. IF MORE THAN ONE STORY BELOW GRADE, CLASS I STANDPIPE SYSTEM SHALL BE INSTALLED.
9. WHEN A BRIDGE IS REQUIRED TO BE USED AS PART OF A FIRE DEPARTMENT ACCESS ROAD, IT SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH NATIONALLY RECOGNIZED STANDARDS.
10. A FIRE HYDRANT SHALL BE PROVIDED WITHIN 100 FT. OF THE FIRE DEPARTMENT CONNECTIONS.
11. FIRE HYDRANTS SHALL BE IN SERVICE BEFORE ANY COMBUSTIBLE MATERIAL IS BROUGHT ON SITE.



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**BROADMOOR AND BEN ALLEN MASTER PLAN**  
NASHVILLE, TENNESSEE

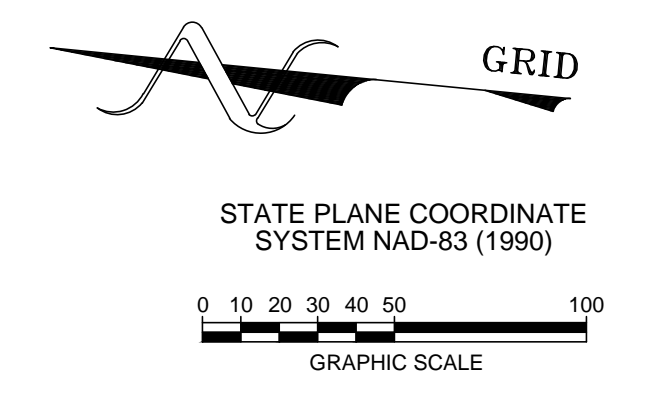
JOB NO.: 18-222  
DATE: 12-22-22

SCALE: 1" = 50'-0"  
SHEET:

**C1.1**  
SITE LAYOUT

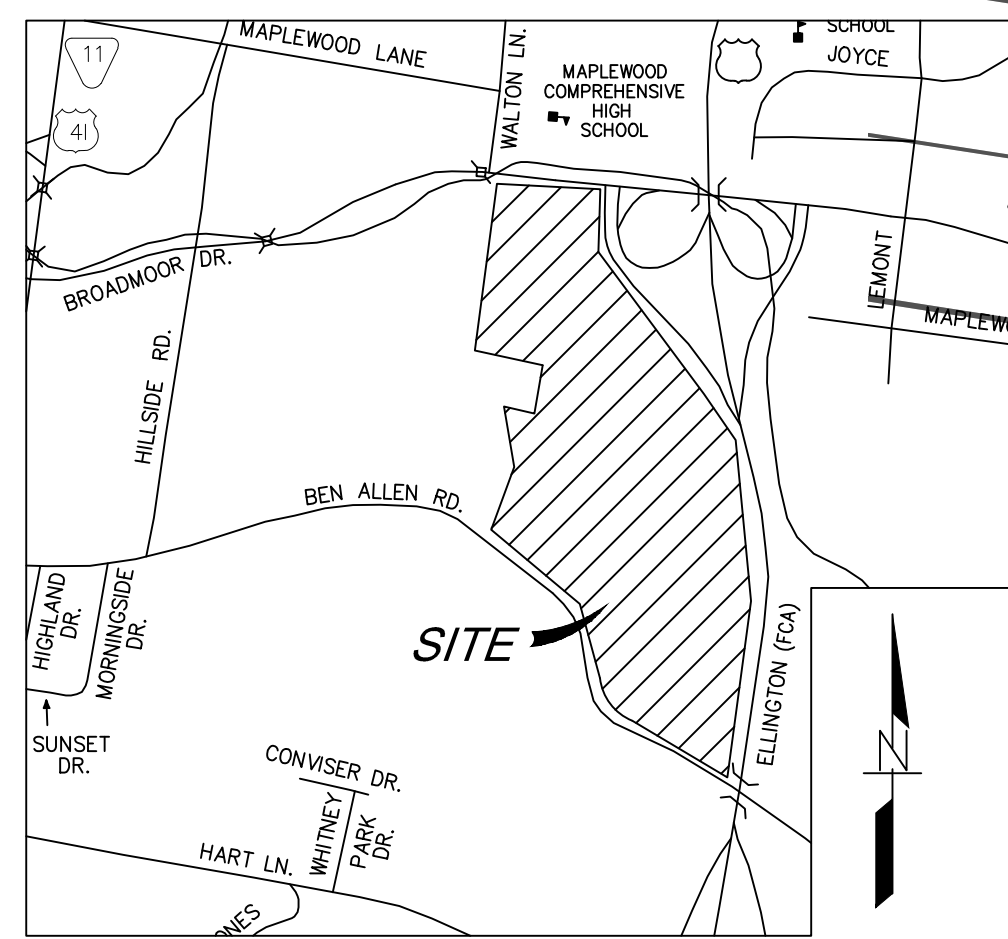
**BCA**  
**BARGE CIVIL ASSOCIATES**  
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VICINITY MAP  
NTS

**FEMA NOTE:**

1. NO PORTION OF THIS PARCEL DESCRIBED HEREON LIES WITHIN FLOOD HAZARD AREA IN ACCORDANCE WITH "INSURANCE RATE MAP PANEL NUMBERS 470370023H AND 470370025H", DATED APRIL 5, 2017.

**METRO WATER SERVICES NOTES:**

1. ANY EXCAVATION, FILL OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO. 78-840 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.  
2. METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED INGRESS AND EGRESS AT ALL TIMES IN ORDER TO MAINTAIN, REPAIR, REPLACE AND INSPECT ANY STORMWATER FACILITIES WITHIN THE PROPERTY.

**STORMWATER NOTES:**

1. ANY EXCAVATION, FILL OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO. 78-840 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.  
2. THIS DRAWING IS FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT. THE FINAL LOT COUNT AND DETAILS OF THE PLAN SHALL BE GOVERNED BY THE APPROPRIATE REGULATIONS AT THE TIME OF FINAL APPLICATION.  
3. METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED INGRESS AND EGRESS AT ALL TIMES IN ORDER TO MAINTAIN, REPAIR, REPLACE AND INSPECT ANY STORMWATER FACILITIES WITHIN THE PROPERTY.  
4. SIZE DRIVEWAY CULVERTS PER THE DESIGN CRITERIA SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL. MINIMUM DRIVEWAY CULVERT IN METRO ROW IS 15" CMP. PROJECT INTENT IS TO BE REDEVELOPED PER THE REQUIREMENTS OF VOLUME 5 (LID) OF THE STORMWATER MANUAL. DETENTION WILL BE PROVIDED ON POST DEVELOPED RUNOFF WILL BE LESS THAN PREDEVELOPED RUNOFF DUE TO LID IMPLEMENTATION.

**STREAM BUFFER NOTE:**

1. THE BUFFER ALONG WATERWAYS WILL BE AN AREA WHERE THE SURFACE IS LEFT IN A NATURAL STATE AND IS NOT DISTURBED BY CONSTRUCTION ACTIVITY. THIS IS IN ACCORDANCE WITH THE STORMWATER MANAGEMENT MANUAL VOLUME 1 REGULATIONS.

METRO GOVTS MAPLEWOOD & GRA-MAR  
METRO PID 0610000800

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METRO PID 0610000800

METRO GOVTS MAPLEWOOD & GRA-MAR  
METRO PID 0610000800

PROPOSED GRADING NOTE:  
GRADING INTENT WITH DRIVE AND BUILDING  
PADS IS TO CLOSELY MATCH EXISTING GRADE  
TO MINIMIZE EXCESS CUTS AND FILLS AND  
PRESERVE EXISTING VEGETATION.

ALL PROPOSED  
PARKING SPACES TO  
BE PERVIOUS PAVERS  
(TYP.)

PROPOSED  
CONCRETE HEADWALL  
(TYP.)

PROPOSED  
BIORETENTION AREA  
(TYP.)

PROPOSED  
AREA DRAIN  
(TYP.)

PROPOSED  
CONCRETE HEADWALL  
(TYP.)

PROPOSED  
BIORETENTION AREA  
(TYP.)

PROPOSED  
AREA DRAIN  
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BIORETENTION AREA  
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PROPOSED  
AREA DRAIN  
(TYP.)

PROPOSED  
CONCRETE HEADWALL  
(TYP.)

CHURCH OF CHRIST  
AT BROADMOOR AVENUE  
METRO PID 06101001300

PLUMBERS & PIPEFITTERS  
METRO PID: 06000004900

301 BEN ALLEN, LLC  
METRO PID 06100001700

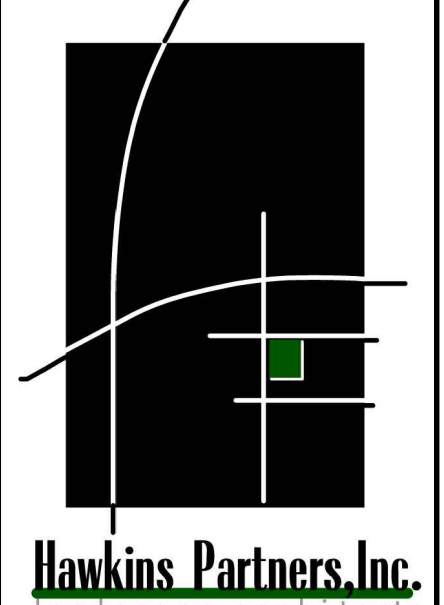
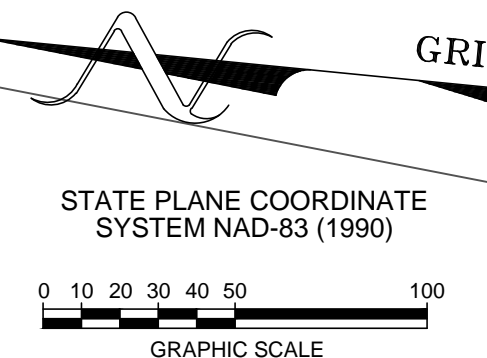


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PROPOSED GRADING NOTE:  
GRADING INTENT WITH DRIVE AND BUILDING  
PADS IS TO CLOSELY MATCH EXISTING GRADE  
TO MINIMIZE EXCESS CUTS AND FILLS AND  
PRESERVE EXISTING VEGETATION.

PROPOSED 30" "DO NOT DISTURB"  
STREAM BUFFER (TYP.)



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BROADMOOR AND BEN ALLEN MASTER PLAN  
NASHVILLE, TENNESSEE

JOB NO.: 18-222  
DATE: 12-22-22

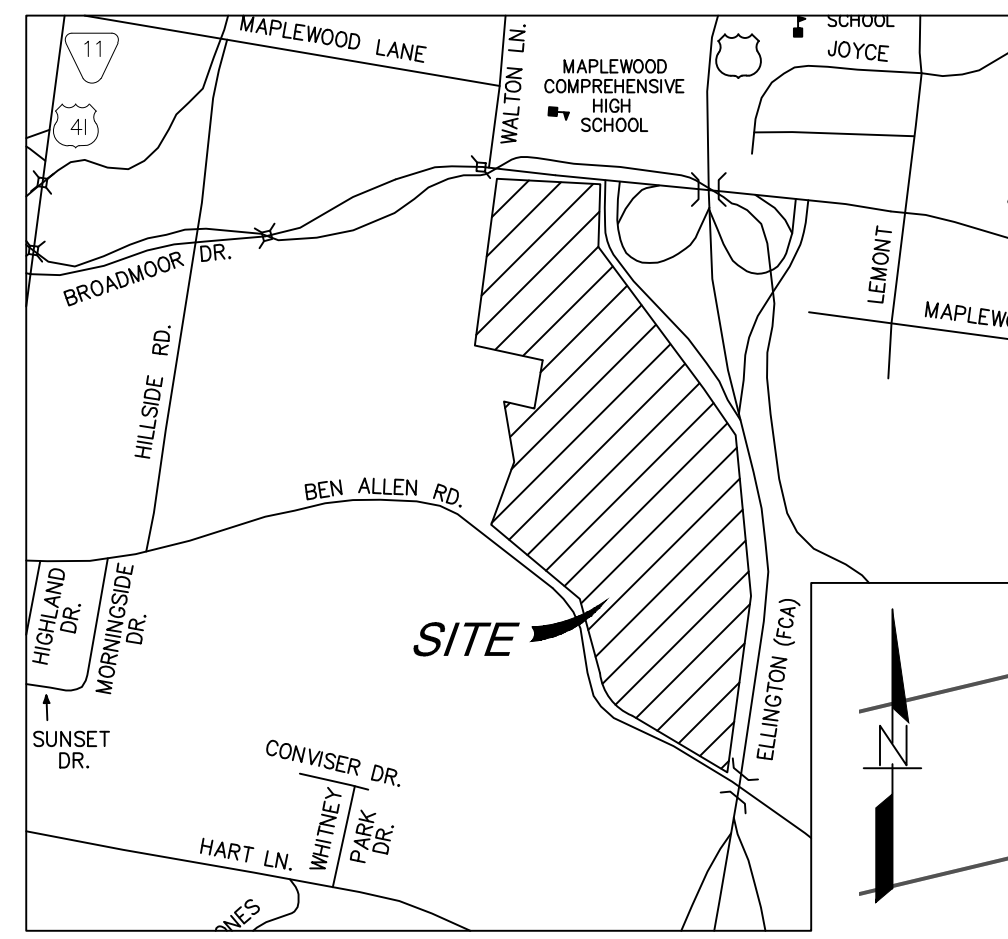
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SHEET:  
C2.0

SITE GRADING  
AND DRAINAGE

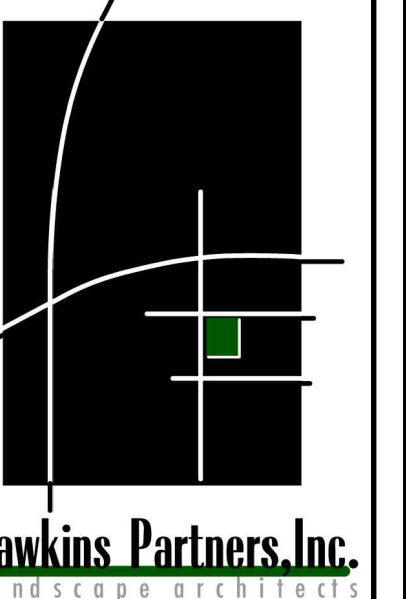
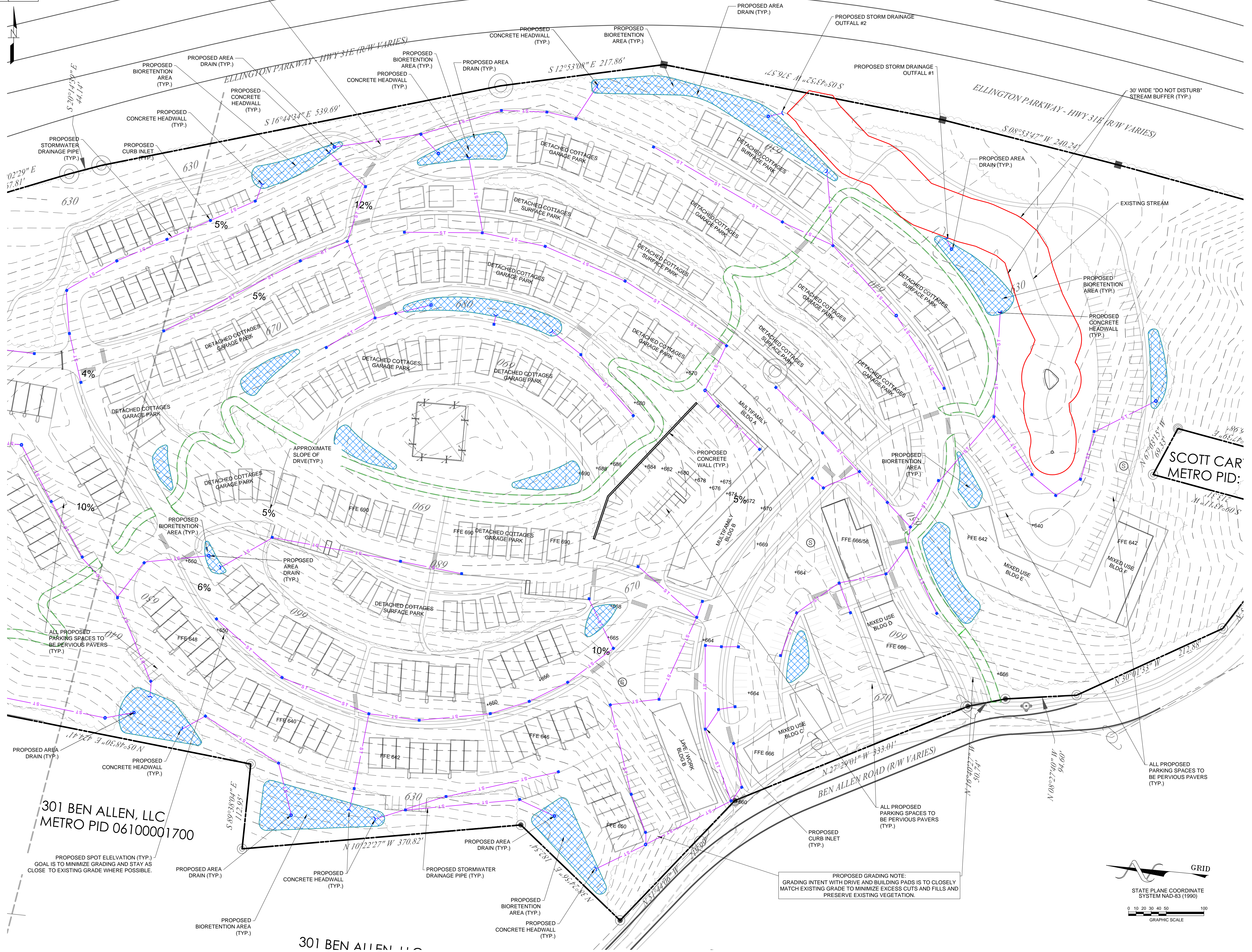
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VICINITY MAP  
NTS

PROPOSED GRADING NOTE:  
GRADING INTENT WITH DRIVE AND BUILDING PADS IS TO CLOSELY  
MATCH EXISTING GRADE TO MINIMIZE EXCESS CUTS AND FILLS AND  
PRESERVE EXISTING VEGETATION.



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NASHVILLE, TENNESSEE

JOB NO.: 18-222  
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SCALE: 1" = 50'-0"

SHEET:

**C2.1**

SITE GRADING  
AND DRAINAGE

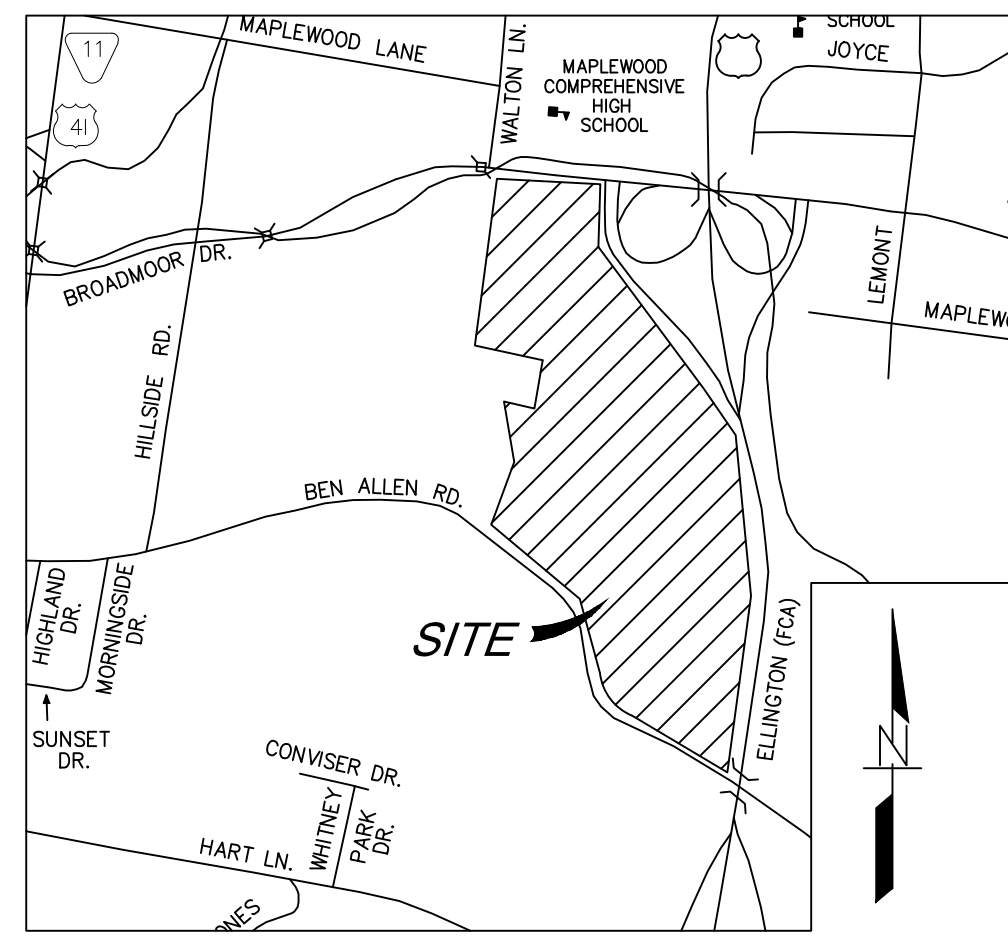


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VICINITY MAP  
NTS

**METRO PUBLIC WORKS NOTES:**

1. THE FINAL CONSTRUCTION DRAWINGS SHALL COMPLY WITH THE DESIGN REGULATIONS ESTABLISHED BY THE DEPARTMENT OF PUBLIC WORKS IN EFFECT AT THE TIME OF THE APPROVAL OF THE PRELIMINARY DEVELOPMENT PLAN OR FINAL DEVELOPMENT PLAN OR BUILDING PERMIT, AS APPLICABLE. FINAL DESIGN MAY VARY BASED ON FIELD CONDITIONS.
2. ALL CONSTRUCTION WITHIN THE RIGHT OF WAY SHALL COMPLY WITH ADA AND METRO PUBLIC WORKS STANDARDS AND SPECIFICATIONS.
3. THERE SHALL BE NO VERTICAL OBSTRUCTIONS (SIGNS, POWER POLES, FIRE HYDRANTS, ETC.) WITHIN THE PROPOSED SIDEWALKS. VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUT OF THE PROPOSED SIDEWALKS, WHERE APPLICABLE.

**METRO PLANNING NOTE:**

1. THE FINAL SITE PLAN/BUILDING PERMIT SHALL DEPICT THE REQUESTED PUBLIC SIDEWALKS, ANY REQUESTED GRASS STRIP OR FRONTAGE ZONE AND THE LOCATION OF ALL EXISTING AND PROPOSED VERTICAL OBSTRUCTIONS WITHIN THE REQUESTED SIDEWALK AND GRASS STRIP OR FRONTAGE ZONE. PRIOR TO THE ISSUANCE OF USE AND OCCUPANCY PERMITS, EXISTING VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUTSIDE OF THE REQUESTED SIDEWALK, WHERE FEASIBLE. VERTICAL OBSTRUCTIONS ARE ONLY PERMITTED WITHIN THE REQUESTED GRASS STRIP OR FRONTAGE ZONE.

**FEMA NOTE:**

1. NO PORTION OF THIS PARCEL DESCRIBED HEREON LIES WITHIN FLOOD HAZARD AREA IN ACCORDANCE WITH INSURANCE RATE MAP PANEL NUMBERS 4703PC0225H AND 4703PC0225H, DATED APRIL 5, 2017.

**METRO WATER SERVICES NOTES:**

1. ANY EXCAVATION, FILL OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO. 78-860 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.
2. METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED INGRESS AND EGRESS AT ALL TIMES IN ORDER TO MAINTAIN, REPAIR, REPLACE AND INSPECT ANY STORMWATER FACILITIES WITHIN THE PROPERTY.

**STORMWATER NOTES:**

1. ANY EXCAVATION, FILL OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO. 78-860 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.
2. THIS DRAWING IS FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT. THE FINAL LOT COUNT AND DETAILS OF THE DEVELOPMENT SHALL BE GOVERNED BY THE APPROPRIATE REGULATIONS AT THE TIME OF FINAL APPLICATION.
3. METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED INGRESS AND EGRESS AT ALL TIMES IN ORDER TO MAINTAIN, REPAIR, REPLACE AND INSPECT ANY STORMWATER FACILITIES WITHIN THE PROPERTY.
4. SIZE DRIVEWAY CULVERTS PER THE DESIGN CRITERIA SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL (MINIMUM DRIVEWAY CULVERT IN METRO ROW IS 15" CMP).
5. PROJECT INTENT IS TO BE REDEVELOPED PER THE REQUIREMENTS OF VOLUME 5 (LID) OF THE STORMWATER MANUAL. DETENTION WILL BE PROVIDED ON SITE. DEVELOPED RUNOFF WILL BE LESS THAN PREDEVELOPED RUNOFF DUE TO LID IMPLEMENTATION.

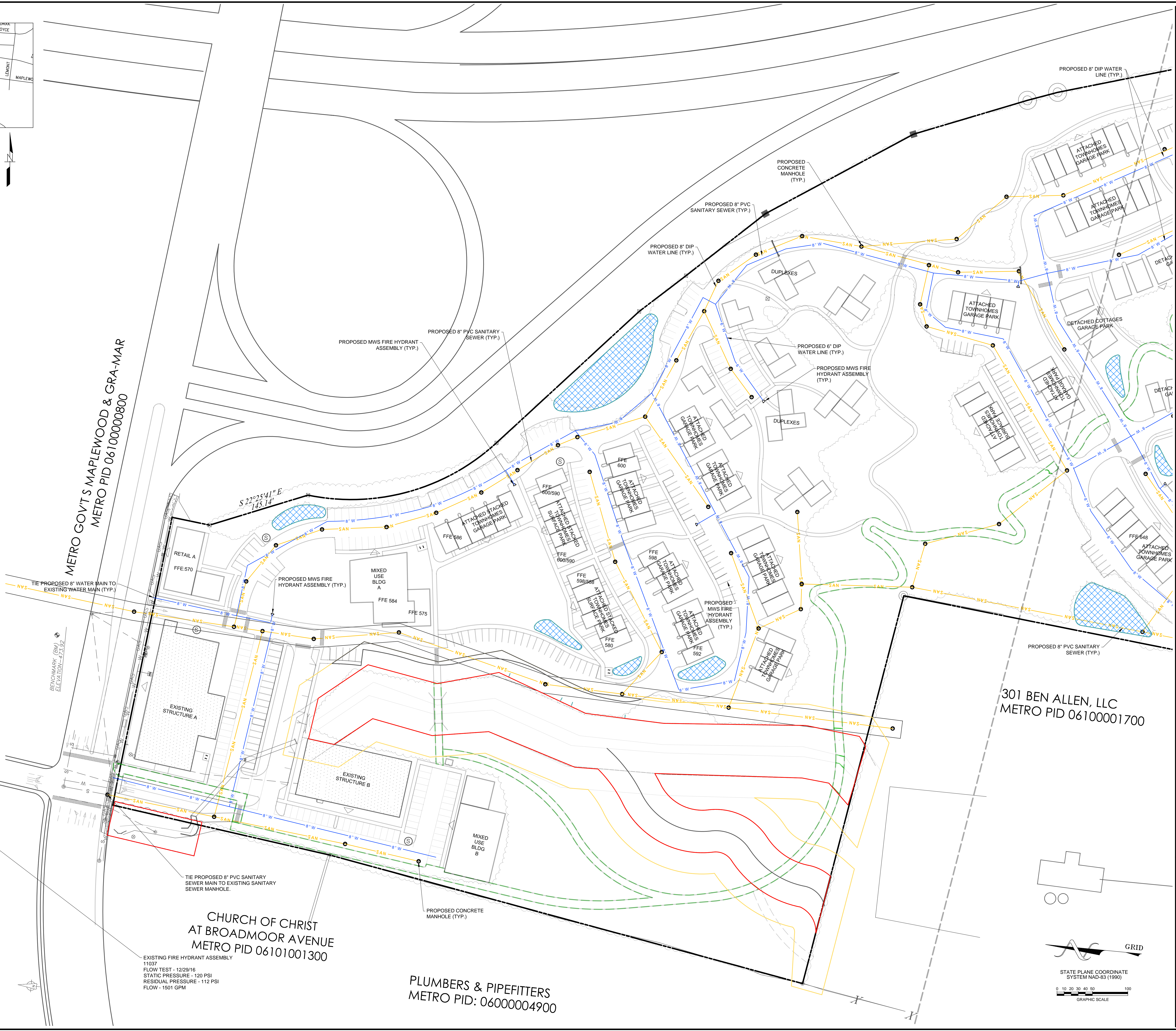
**ON-SITE PARKING NOTE:**

1. ALL USES TO BE PARKED ON SITE PER METRO ZONING ORDINANCE. PARKING TOTALS SHALL BE VERIFIED WITH FINAL DESIGN AND USE BEFORE ANY PERMITS ARE ISSUED.

**FIRE MARSHAL NOTES:**

1. NEW COMMERCIAL DEVELOPMENTS SHALL BE PROTECTED BY A FIRE HYDRANT THAT COMPLIES WITH THE 2006 EDITION OF NFPA 1 TABLE N TO SEE TABLE I GO TO (HTTP://WWW.NASHFIRE.ORG/PREV/TABLEH1-1.HTM)
2. PROJECT ENGINEER NEEDS TO MEET WITH THE FIRE MARSHAL'S OFFICE CONCERNING THIS PROJECT
3. NO PART OF ANY BUILDING SHALL BE MORE THAN 500 FT. FROM A FIRE HYDRANT VIA A HARD SURFACE ROAD. METRO ORDINANCE 095-1541 SEC. 1666.0209
4. ALL FIRE DEPARTMENT ACCESS ROADS SHALL BE 20 FT. MINIMUM WIDTH AND SHALL HAVE AN UNOBSTRUCTED VERTICAL CLEARANCE OF 13.6 FT.
5. ALL DEAD END ROADS OVER 150 FT. IN LENGTH REQUIRE A 100 FT. DIAMETER TURNAROUND. THIS INCLUDES TEMPORARY TURNAROUNDS.
6. TEMPORARY T-TYPE TURNAROUNDS THAT LAST NO MORE THAN ONE YEAR SHALL BE APPROVED BY THE FIRE MARSHAL'S OFFICE.
7. IF MORE THAN THREE STORIES ABOVE GRADE, CLASS I STANDPIPE SYSTEM SHALL BE INSTALLED.
8. IF MORE THAN ONE STORY BELOW GRADE, CLASS I STANDPIPE SYSTEM SHALL BE INSTALLED.
9. WHEN A BRIDGE IS REQUIRED TO BE USED AS PART OF A FIRE DEPARTMENT ACCESS ROAD, IT SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH NATIONALLY RECOGNIZED STANDARDS.
10. A FIRE HYDRANT SHALL BE PROVIDED WITHIN 100 FT. OF THE FIRE DEPARTMENT CONNECTIONS.
11. FIRE HYDRANTS SHALL BE IN-SERVICE BEFORE ANY COMBUSTIBLE MATERIAL IS BROUGHT ON SITE.

METRO GOV'TS MAPLEWOOD & GRA-MAR  
METRO PID 0610000800



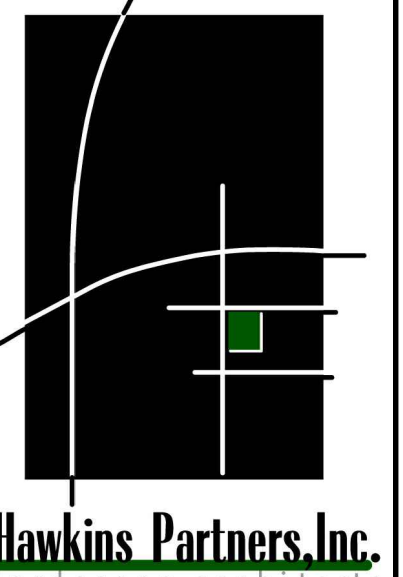
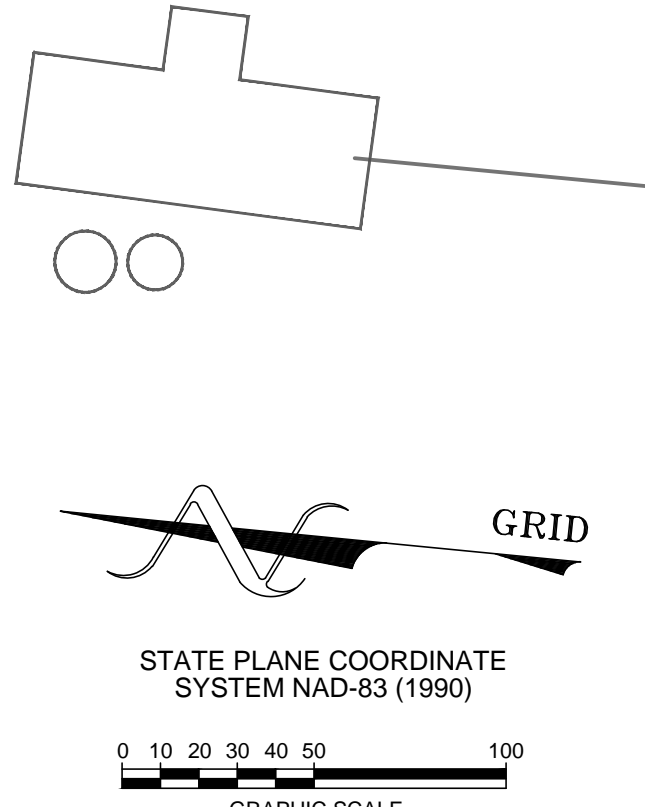
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CHURCH OF CHRIST  
AT BROADMOOR AVENUE  
METRO PID 06101001300

PLUMBERS & PIPEFITTERS  
METRO PID: 0600004900

EXISTING FIRE HYDRANT ASSEMBLY  
11037  
FLOW TEST - 12/29/16  
STATIC PRESSURE - 120 PSI  
RESIDUAL PRESSURE - 112 PSI  
FLOW - 1501 GPM



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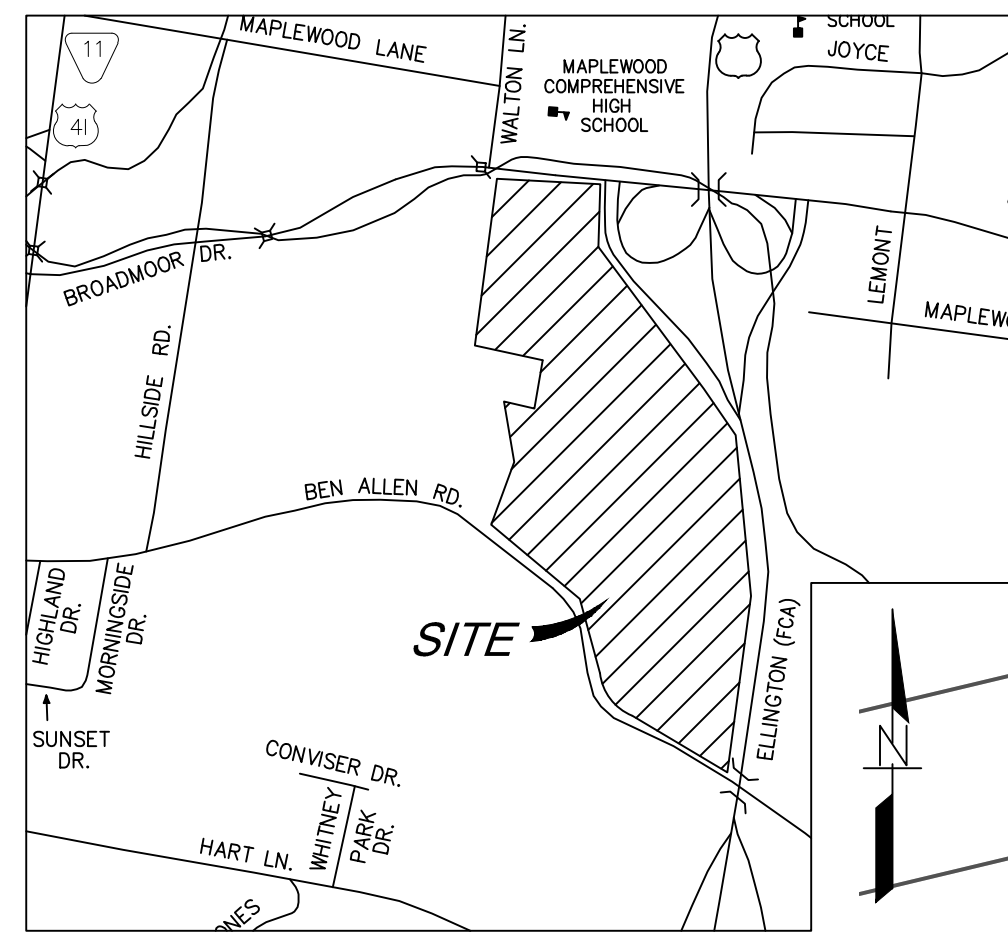
SCALE: 1" = 50'-0"

SHEET:

C3.0

SITE UTILITY





VICINITY MAP  
NTS

**METRO PUBLIC WORKS NOTES:**

1. THE FINAL CONSTRUCTION DRAWINGS SHALL COMPLY WITH THE DESIGN REGULATIONS ESTABLISHED BY THE DEPARTMENT OF PUBLIC WORKS, IN EFFECT AT THE TIME OF THE APPROVAL OF THE PRELIMINARY DEVELOPMENT PLAN OR FINAL DEVELOPMENT PLAN OR BUILDING PERMIT, AS APPLICABLE. FINAL DESIGN MAY VARY BASED ON FIELD CONDITIONS.
2. ALL CONSTRUCTION WITHIN THE RIGHT OF WAY SHALL COMPLY WITH ADA AND METRO PUBLIC WORKS STANDARDS AND SPECIFICATIONS.
3. THERE SHALL BE NO VERTICAL OBSTRUCTIONS (SIGNS, POWER POLES, FIRE HYDRANTS, ETC.) WITHIN THE PROPOSED SIDEWALKS. VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUT OF THE PROPOSED SIDEWALKS, WHERE APPLICABLE.

**METRO PLANNING NOTE:**

1. THE FINAL SITE PLANNING PERMIT SHALL DELINEATE THE REQUESTED PUBLIC SIDEWALKS. ANY REQUESTED GRASS STRIP OR FRONTAGE ZONE AND THE LOCATION OF ALL EXISTING AND PROPOSED VERTICAL OBSTRUCTIONS WITHIN THE REQUESTED SIDEWALK AND GRASS STRIP OR FRONTAGE ZONE, PRIOR TO THE ISSUANCE OF USE AND OCCUPANCY PERMITS. EXISTING VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUTSIDE OF THE REQUESTED SIDEWALK, WHERE FEASIBLE. VERTICAL OBSTRUCTIONS ARE ONLY PERMITTED WITHIN THE REQUESTED GRASS STRIP OR FRONTAGE ZONE.

**FEMA NOTE:**

1. NO PORTION OF THIS PARCEL DESCRIBED HEREON LIES WITHIN FLOOD HAZARD AREA IN ACCORDANCE WITH THE INSURANCE RATE MAP PANEL NUMBERS 47020223H AND 470372025H, DATED APRIL 5, 2017.

**METRO WATER SERVICES NOTES:**

1. ANY EXCAVATION, FILL OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO. 78-840 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.
2. METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENUMBERED INGRESS AND EGRESS AT ALL TIMES IN ORDER TO MAINTAIN, REPAIR, REPLACE AND INSPECT ANY STORMWATER FACILITIES WITHIN THE PROPERTY.

**STORMWATER NOTES:**

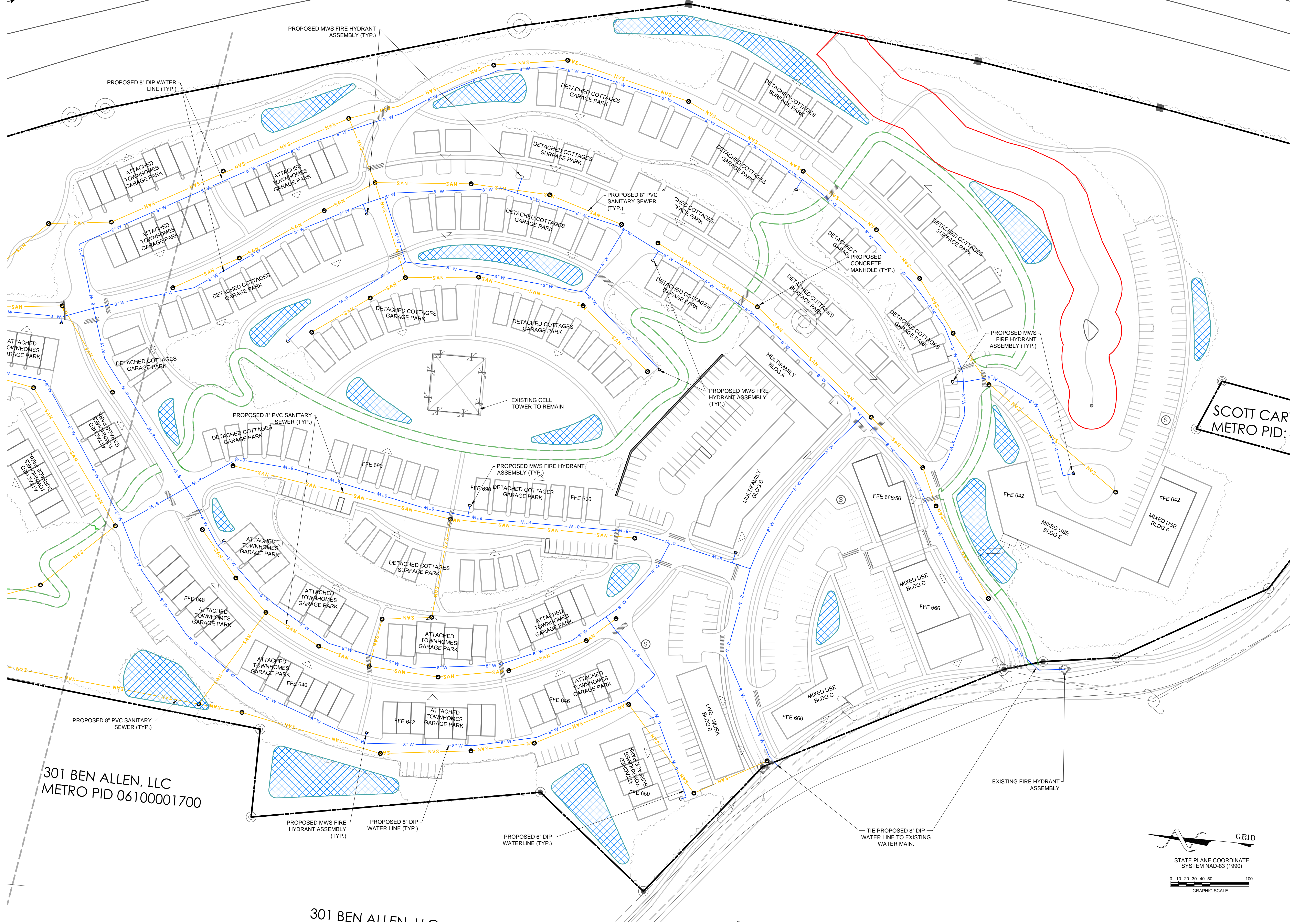
1. ANY EXCAVATION, FILL OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO. 78-840 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.
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4. SIZE BRICKWAY CULVERTS PER THE DESIGN CRITERIA SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL (MINIMUM DRIVEWAY CULVERT IN METRO ROW IS 15" CMP).
5. PROJECT INTENT IS TO BE REDEVELOPED PER THE REQUIREMENTS OF VOLUME 5 (LD) OF THE STORMWATER MANUAL. DETENTION WILL BE PROVIDED OR POST DEVELOPED RUNOFF WILL BE LESS THAN PREDEVELOPED RUNOFF DUE TO LID IMPLEMENTATION.

**ON-SITE PARKING NOTE:**

1. ALL USES TO BE PARKED ON SITE PER METRO ZONING ORDINANCE. PARKING TOTALS SHALL BE VERIFIED WITH FINAL DESIGN AND USE BEFORE ANY PERMITS ARE ISSUED.

**FIRE MARSHAL NOTES:**

1. NEW COMMERCIAL DEVELOPMENTS SHALL BE PROTECTED BY A FIRE HYDRANT THAT COMPLIES WITH THE 2006 EDITION OF NFPA 1 TABLE H. TO SEE TABLE H GO TO: (HTTP://WWW.NASHVIRE.ORG/PRE/TABLEH1.HTM)
2. PROJECT ENGINEER NEEDS TO MEET WITH THE FIRE MARSHAL'S OFFICE CONCERNING THIS PROJECT.
3. NO PART OF ANY BUILDING SHALL BE MORE THAN 500 FT. FROM A FIRE HYDRANT VIA A HARD SURFACE ROAD. METRO ORDINANCE 095-1541 SEC. 1568.0208
4. ALL FIRE DEPARTMENT ACCESS ROADS SHALL BE 30 FT. MINIMUM WIDTH AND SHALL HAVE AN UNOBSTRUCTED VERTICAL CLEARANCE OF 13.8 FT.
5. ALL DEAD END ROADS OVER 150 FT. IN LENGTH REQUIRE A 100 FT. DIAMETER TURNAROUND. THIS INCLUDES TEMPORARY TURNAROUNDS.
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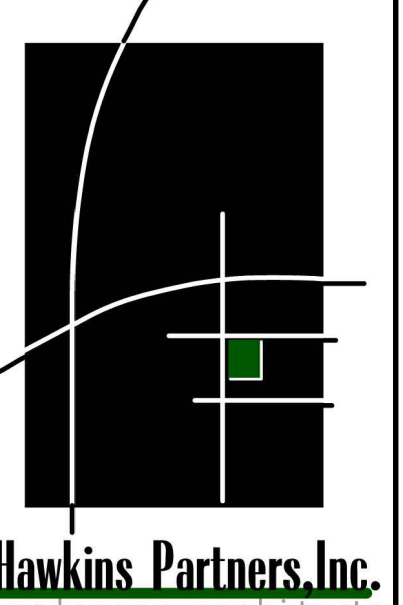
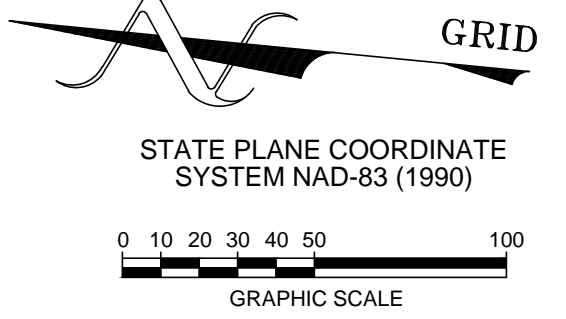


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METRO PID 0610001700

SCOTT CAR  
METRO PID:



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**C3.1**

SITE UTILITY

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